

Engineering

Land/Site
Development
Municipal
Infrastructure
Environmental/
Water Resources
Traffic/
Transportation
Recreational

Planning

Land/Site
Development
Planning Application
Management
Municipal Planning
Urban Design
Expert Witness
(LPAT)
Wireless Industry

Landscape Architecture

Streetscapes &
Public Amenities
Open Space, Parks &
Recreation
Community &
Residential
Commercial &
Institutional
Environmental
Restoration

Zoning By-law Amendment Application 910 March Road Planning Rationale



Prepared for: March Road Land Holdings

**910 MARCH ROAD
OTTAWA, ONTARIO**

**PLANNING RATIONALE
IN SUPPORT OF APPLICATION FOR
ZONING BY-LAW AMENDMENT**

Prepared for:
March Road Land Holdings c/o Lépine Corporation



Prepared By:



NOVATECH
Suite 200, 240 Michael Cowpland Drive
Ottawa, Ontario
K2M 1P6

January 27, 2023

Novatech File: 121186
Ref: R-2022-173

January 27, 2023

City of Ottawa
Planning, Real Estate, and Economic Development
110 Laurier Avenue West, 4th Floor
Ottawa, Ontario
K1P 1J1

Attention: Alison Hamlin – Manager, Development Review West

**Reference: Planning Rationale in Support of an Application for Zoning By-law
Amendment Recirculation - City File No. D02-02-20-0050
910 March Road
Our File No.: 121186**

Novatech has been retained by March Road Land Holdings Inc. c/o Lépine Corporation (“*the owner*”) to prepare this Planning Rationale in support of a revised application for Zoning By-law Amendment for its property municipally known as 910 March Road in Ward 5 – West Carleton March, Ottawa, Ontario (herein referred to as ‘*the Subject Site*’).

There are active applications for Zoning By-law Amendment (City File No.: D02-02-20-0050) and Site Plan Control (City File No.: D07-12-20-0089) filed by Wexcom Developments (March Rd.) Ltd for the Subject Site. Lépine Corporation has recently acquired this site from the former owner and is pursuing a revised concept for a mid-rise mixed-use development which is to replace the original commercial development proposal. The proposed development was discussed with City staff at consultations on September 18, 2021 and June 8, 2022.

Lépine Corporation specializes in creating quality apartment communities at desirable locations across the City which offer long-term rental accommodations with high-end amenities. It is proposing the conceptual development of a nine-storey mixed-use building arranged in two (2) terraced blocks consisting of approximately three hundred ninety (390) apartment dwelling units and five hundred one (501) m² of commercial space on the ground floor. The proposed development will feature resident and visitor parking primarily underground with commercial and additional short-term parking and lay-by spaces at grade within a roundabout drive aisle. A total of five hundred sixty-eight (568) parking spaces and one hundred ninety-eight (198) bicycle parking spaces will be provided for the proposed development. A landscaped courtyard will provide associated pathway connections and outdoor amenity space for residents. The Subject Site is currently vacant.

This Planning Rationale demonstrates how the revised development proposal is consistent with the *Provincial Policy Statement (PPS)*, conforms to the *City of Ottawa Official Plan (2022)*, maintains the general intent and purpose of *Zoning By-law 2008-250*, respects the applicable design guidelines and represents good land use planning.

Sincerely,

NOVATECH



Miranda Virginillo, M.PL.
Planner, Planning & Development

M:\2021\121186\DATA\Applications\Zoning & Site Plan\20230127_PlanningRationale.docx

TABLE OF CONTENTS

1.0	INTRODUCTION AND PROPOSAL DETAILS	4
1.1	SITE DESCRIPTION AND SURROUNDING USES	4
1.2	PROPOSED DEVELOPMENT	9
1.3	PURPOSE OF ZONING BY-LAW AMENDMENT	10
2.0	ADDITIONAL REPORTS	11
3.0	PLANNING POLICY AND REGULATORY FRAMEWORK	11
3.1	PROVINCIAL POLICY STATEMENT (PPS) (2020).....	11
3.2	CITY OF OTTAWA OFFICIAL PLAN.....	14
3.2.1	<i>OFFICIAL PLAN (2022)</i>	14
3.2.2	<i>TRANSIT-ORIENTED DEVELOPMENT GUIDELINES (TOD GUIDELINES)</i>	25
3.2.3	<i>KANATA NORTH COMMUNITY DESIGN PLAN (CDP) (2016)</i>	26
3.3	ZONING BY-LAW 2008-250.....	29
3.3.1	<i>EXISTING ZONING</i>	29
3.3.2	<i>SUGGESTED ZONING</i>	29
4.0	PUBLIC CONSULTATION STRATEGY	35
5.0	CONCLUSION	36

APPENDICES

APPENDIX A: SITE PLAN PREPARED BY NEUF ARCHITECTES DATED JANUARY 26, 2023.

FIGURES

FIGURE 1:	EXCERPT FROM KANATA NORTH COMMUNITY DESIGN PLAN WITH MARKUP BY NOVATECH SHOWING THE SUBJECT SITE.	6
FIGURE 2:	EXCERPT OF KANATA NORTH CDP FIGURE 6 - NAMING CONVENTIONS FOR WATER CHANNELS WITH MARKUP BY NOVATECH SHOWING SUBJECT SITE.....	7
FIGURE 3:	SUBJECT SITE AND SURROUNDING USES.....	8
FIGURE 4:	EXCERPT FROM SCHEDULE B5 - SUBURBAN (WEST) TRANSECT.	15
FIGURE 5:	EXCERPT OF SCHEDULE C2 - TRANSIT NETWORK (ULTIMATE) WITH MARKUP BY NOVATECH SHOWING SUBJECT SITE.	20
FIGURE 6:	EXCERPT OF SCHEDULE C4 - URBAN ROAD NETWORK WITH MARKUP BY NOVATECH SHOWING SUBJECT SITE.	20
FIGURE 7:	EXCERPT FROM KANATA NORTH COMMUNITY DESIGN PLAN - CROSS SECTION FOR MARCH ROAD - INTERIM.	21
FIGURE 8:	EXCERPT FROM KANATA NORTH COMMUNITY DESIGN PLAN - CROSS SECTION FOR MARCH ROAD - ULTIMATE.....	21
FIGURE 9:	EXCERPT FROM KANATA NORTH COMMUNITY DESIGN PLAN - CROSS SECTION OF CREEK CORRIDOR	28
FIGURE 10:	EXCERPT OF THE SUBJECT SITE'S EXISTING ZONING FROM GEOOTTAWA.	30
FIGURE 11:	EXCERPT OF OFFICIAL PLAN FIGURE 17 - SETBACKS FROM SURFACE WATER FEATURES.	33
FIGURE 12:	EXCERPT OF SUGGESTED ZONING FIGURE PREPARED BY NOVATECH DATED DECEMBER 2022.	35

1.0 INTRODUCTION AND PROPOSAL DETAILS

Novatech has been retained by March Road Land Holdings Inc. c/o Lépine Corporation (“*the owner*”) to prepare this Planning Rationale in support of a revised application for Zoning By-law Amendment for its property municipally known as 910 March Road in Ward 5 – West Carleton March, Ottawa, Ontario (herein referred to as “*the Subject Site*”).

There are active applications for Zoning By-law Amendment (City File No.: D02-02-20-0050) and Site Plan Control (City File No.: D07-12-20-0089) filed by Wexcom Developments (March Rd.) Ltd for the Subject Site. Lépine Corporation has recently acquired this site from the former owner and is pursuing a revised concept for a mid-rise mixed-use development which is to replace the original commercial development proposal. The proposed development was discussed with City staff at consultations on September 18, 2021 and June 8, 2022.

Lépine Corporation is proposing the conceptual development of a nine-storey mixed-use building arranged in two (2) terraced blocks consisting of approximately three hundred ninety (390) apartment dwelling units and five hundred one (501) m² of commercial space on the ground floor. The proposed development will feature resident and visitor parking primarily underground with commercial and additional short-term parking and lay-by spaces at grade within a roundabout drive aisle. A total of five hundred sixty-eight (568) parking spaces and one hundred ninety-eight (198) bicycle parking spaces will be provided for the proposed development. A landscaped courtyard will provide associated pathway connections and outdoor amenity space for residents. The Subject Site is currently vacant.

This report provides an analysis of relevant provincial and municipal planning documents with a view of demonstrating how the revised development proposal:

- is consistent with the *Provincial Policy Statement (PPS)*,
- conforms to the *City of Ottawa Official Plan (2022)*,
- maintains the general intent and purpose of *Zoning By-law 2008-250*,
- respects the *Transit-Oriented Development Guidelines (2007)*,
- provides transition to the vision for Kanata North in the *Kanata North Community Design Plan (2016)*, and
- represents good land use planning.

1.1 Site Description and Surrounding Uses

The Subject Site is situated on the east side of March Road to the north-east of the intersection of March Road and Maxwell Bridge Road in the City of Ottawa, immediately south of the growing Kanata North Community. The Subject Site has approximately 168 m frontage along March Road and a total area of 2.7 hectares.

The Subject Site is located near the edge of the established built-up area of Kanata North which is expanding north as a new mixed-use community. A review of historical aerial photography from GeoOttawa has shown that the Subject Site has been used for residential and accessory agricultural vehicle storage related purposes from 1976. The residential dwelling and accessory structures have since been removed (in 2019) and the Subject Site is presently vacant.

The Subject Site is within the Shirley's Brook sub-watershed and is transected to the east, north, and south by Tributaries 2, 3 and 4 of Shirley's Brook.

The Subject Site is legally described as follows:

PART OF LOTS 11 & 12 CONCESSION 4 MARCH PART 1 PLAN 4R24361 CITY OF OTTAWA

The surrounding area is generally characterized by a mix of low-profile residential and commercial development. This area is intended to continue to grow and achieve a more urban built form offering a greater variety of uses and building forms anticipated to range from low-rise to mid-rise focused along March Road.

The Subject Site is situated immediately south of the study area for the *Kanata North Community Design Plan (Kanata North CDP)* as shown in Figure 1. The *Kanata North CDP* provides a community-wide land-use framework for the Kanata North Urban Expansion Area (KNUEA) that reflects the principles, objectives and policies for community development as directed by the *Official Plan*. Although the Subject Site is located outside the boundaries of the KNUEA, the proposed development should be considered a part of the neighbourhood gateway for the Kanata North Urban Expansion Area. Additionally, the Subject Site is bounded by Tributaries 2, 3 and 4 of Shirley's Brook as described in *Section 5.3 - Environmental Management Plan* of the *Kanata North CDP (2016)*. Throughout this report, the tributaries will be referred to using the naming conventions of the *Environmental Management Plan* as shown in Figure 2. As a result, the KNUEA is considered in the review of the proposed development in this Rationale.

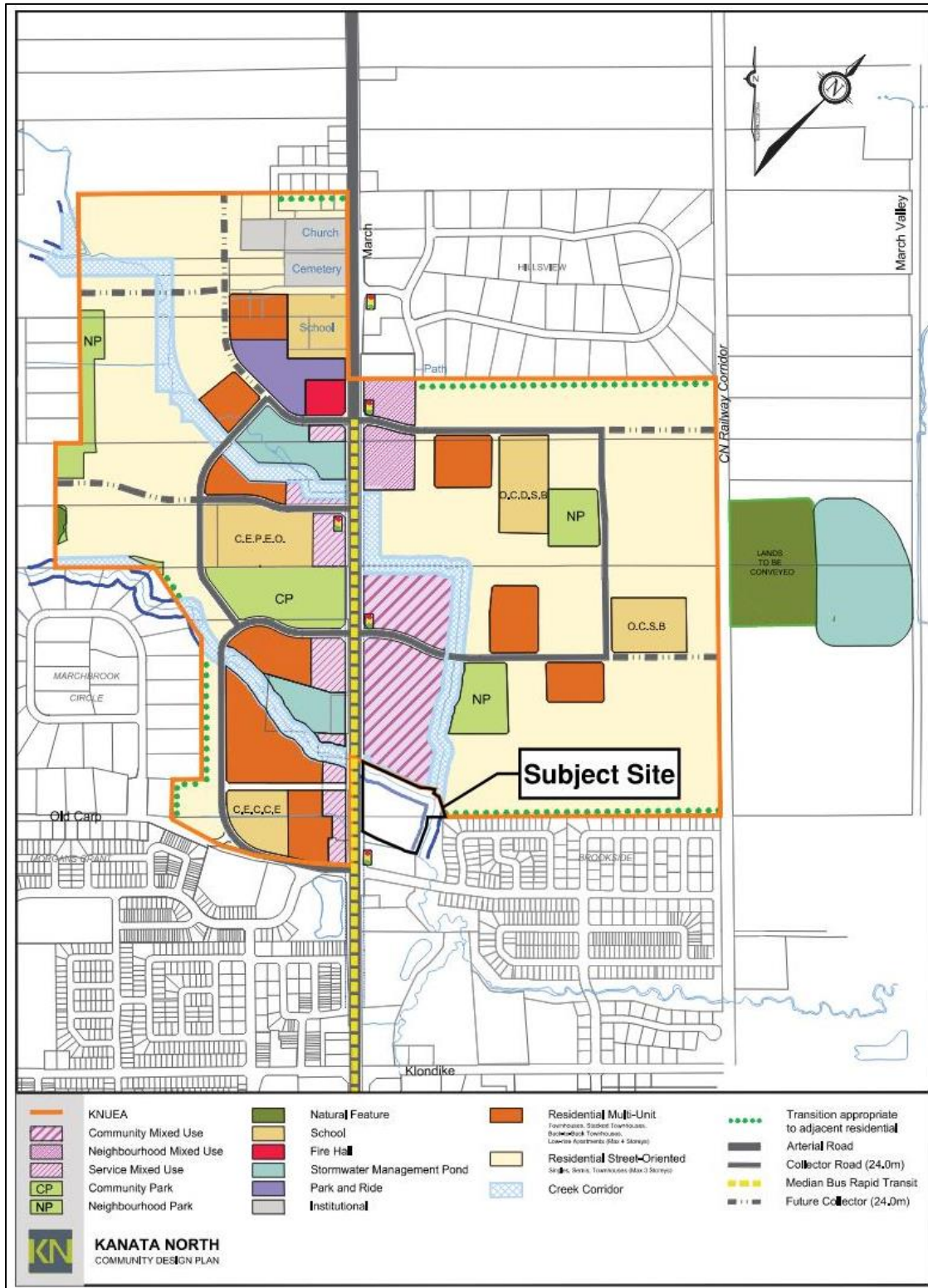


Figure 1: Excerpt from Kanata North Community Design Plan with markup by Novatech showing the Subject Site.



Figure 2: Excerpt of Kanata North CDP Figure 6 - Naming Conventions for Water Channels with markup by Novatech showing Subject Site.



Figure 3: Subject Site and surrounding uses.

The following describes the land uses adjacent to the Subject Site as shown on Figure 3. For the purposes of describing the adjacent land uses below, March Road is oriented in a north-south direction.

North: The Subject Site is separated from the adjacent lot to the north by the Tributary 3 corridor of Shirley's Brook. Immediately north of the Subject Site, mixed-use commercial development is proposed along March Road (PINs 045271013 and 045271014). North of the Subject Site, Minto Communities has received draft plan approval to subdivide the lands at 930 March Road to consist of eight hundred fifty-four (854) detached, semi-detached and townhouse dwellings as well as a school, neighbourhood park and community mixed-use blocks fronting on March Road (*City File No.: D02-02-18-0019 and D07-16-18-0032*). Further north of the Subject Site, Cavanagh Developments is proposing a future mixed-use subdivision at 1020 – 1070 March Road consisting of seven hundred ninety (790) detached, semi-detached and townhouse dwellings as well as a school, neighbourhood park, and neighbourhood mixed-use blocks fronting on March Road (*City File No.: D07-16-19-0020 and D02-02-19-0090*). To the northwest at 1053, 1075 and 1145 March Road, CU Developments Inc. has received draft approval for a plan of subdivision including 825 residential units, institutional blocks, a neighbourhood park, and a stormwater management pond (*City File No.: D07-16-18-0023 and D02-02-18-0076*). Open space blocks for the realignment of Tributary 2 of Shirley's Brook form part of the applications north of the Subject Site.

East: The Subject Site is separated from the existing residential lots to the east by the Tributary 2 corridor of Shirley's Brook. An open space block owned by the City of Ottawa at 349 Maxwell Bridge Road provides an additional buffer between the Subject Site and existing ground-oriented single-detached residential development located to the east along Windance Crescent.

South: A McDonald's drive-thru restaurant is located on the lot zoned commercial immediately south of the Subject Site. The adjacent lot to the south provides a naturalized area along Tributary 4 and joins with Tributaries 2 in the southeast corner of the Subject Site. More commercial development is located across Maxwell Bridge Road in the RioCentre Kanata shopping plaza; commercial uses include a liquor store, a bank, personal service businesses, restaurants, a grocery store and a drug store.

West: Across March Road from the Subject Site, Brigil is developing the lands at 927 March Road for a future mixed-use subdivision consisting of one thousand eight hundred sixty-one (1861) detached, townhouse, and apartment dwellings (*City File No.: D01-01-20-0027, D02-02-20-0138, and D07-16-20-0034*). The future development will also include a school, neighbourhood park, open space for Shirley's Brook (Tributary 3), and community mixed-use blocks for the lands fronting March Road. A new local road connection serving this development is proposed immediately opposite of the Subject Site.

1.2 Proposed Development

Lépine Corporation is an established Kanata-based builder that specializes in creating quality apartment communities in Ottawa and surrounding municipalities. It offers long-term rental accommodations with a focus on liveable and innovative units, high-end and durable finishes, resort-like amenities and beautifully landscaped grounds to promote appealing lifestyles for all

demographics. The Subject Site offers a desirable location for Lépine Corporation's proposed new apartment community.

Lépine Corporation is proposing the conceptual development of a nine-storey mixed-use building consisting of approximately three hundred ninety (390) apartment dwelling units and approximately five hundred one (501) m² of commercial space on the ground floor. The proposed development will feature resident and visitor parking primarily underground with commercial and additional short-term parking and lay-by spaces at grade within a roundabout drive aisle. The concept plan provides for a total of five hundred sixty-eight (568) parking spaces and one hundred ninety-eight (198) bicycle parking spaces. A landscaped courtyard will provide associated pathway connections and outdoor amenity space for residents.

The proposed development features one full movement vehicular access from March Road, approximately 215 m north of the intersection of Maxwell Bridge Road. The Transportation Impact Assessment prepared by CGH Transportation dated December 22, 2022, recommends an unsignalized intersection with stop-control as an interim condition with potential signalization of the site access intersection in the future. Detailed recommendations and findings are available in this report.

The objectives for this proposal are:

- to achieve intensification in a manner that complements the evolving character of the community, and
- to provide additional housing options and small-scale commercial opportunity to meet the needs of individuals and families in the Kanata North community.

The concept proposes two (2) terraced residential and mixed-use blocks connected by a three-storey link. Block A will be in the range of nine storeys and will be oriented to the street frontage of March Road. Block B will be in the range of seven storeys and will be oriented towards the centre of the site with private rear amenity space towards the north and east tributaries. The blocks will be accessed by a series of internal walkways which will connect to future sidewalks along March Road and the surface parking area. Commercial units will be oriented towards March Road and designed to accommodate small-scale retail and other service-commercial uses.

The development is to be connected to municipal water and sanitary services available in March Road. The preliminary stormwater management approach proposes quantity and quality control measures for stormwater which is to be collected in storage tanks prior to outlet to Tributary 2.

Further design details will be provided at a later date upon recirculation of a revised Site Plan Control application.

1.3 Purpose of Zoning By-law Amendment

The purpose of the Zoning By-law Amendment is to change the current zoning of the Subject Site in order to enable development as envisioned by the Official Plan. The amendment will address permitted use and form to achieve intensification which is compatible with the planned community context in this area of Kanata North.

The Subject Site is currently zoned *Development Reserve - DR* and *Rural Countryside – RU* which no longer reflect appropriate use of the property within this growing urban community. The *Flood Plain Overlay* also applies to portions of the site located within the current 1:100 flood elevation associated with the tributaries of Shirley’s Brook.

It is suggested to rezone the developable area of the Subject Site to a *General Mixed-Use – GM zone* to permit the proposed mid-rise mixed-use development. It is anticipated that a site-specific exception will modify performance standards relating to the maximum building height as well as refine the permitted uses on the Subject Site. It is suggested that a *Parks and Open Space – O1* or *Environmental Protection – EP* zone apply to the tributary corridors to protect these features and allow passive recreational use. No change to the *Flood Plain Overlay* limit is proposed.

2.0 ADDITIONAL REPORTS

This report should be read in conjunction with the following reports and plans:

- Serviceability Report prepared by Novatech, dated January 26, 2023;
- Concept Plan prepared by Neuf Architectes, dated January 26, 2023;
- Design Brief prepared by Neuf Architectes, dated January 27, 2023;
- Environmental Impact Statement prepared by Gemtec, dated December 21, 2022;
- Geotechnical Investigation prepared by Paterson Group, dated November 30, 2021;
- Phase I Environmental Site Assessment prepared by Paterson Group dated August 16, 2021;
- Topographic Survey prepared by Annis O’Sullivan Vollebakk Ltd., dated January 17, 2022; and
- Transportation Impact Assessment prepared by CGH Transportation dated December 22, 2022.

3.0 PLANNING POLICY AND REGULATORY FRAMEWORK

3.1 Provincial Policy Statement (PPS) (2020)

The *Provincial Policy Statement (PPS) (2020)* provides policy direction on land use planning and development matters of provincial interest by setting the policy foundation for regulating the development and use of land as set out in *Section 2* of the *Planning Act*. The decisions that affect all matters “shall be consistent with” relevant policy statements under the authority of *Section 3* of the *Planning Act*. The following is an overall review of the applicable *PPS* policies:

Building Strong Healthy Communities

Section 1.1 of the *PPS* speaks to managing and directing land use to achieve efficient and resilient development and land use patterns.

Policy 1.1.1 states that “*healthy, liveable, and safe communities are sustained by*”:

- a) *promoting efficient development and land use patterns which sustain the financial well-being of the Province and municipalities over the long term;*

- b) *accommodating an appropriate affordable and market-based range and mix of residential types (including single-detached, additional residential units, multi-unit housing, affordable housing and housing for older persons), employment (including industrial and commercial), institutional (including places of worship, cemeteries and long-term care homes), recreation, park and open space, and other uses to meet long-term needs;*
 - c) *avoiding development and land use patterns which may cause environmental or public health and safety concerns;*
 - d) *avoiding development and land use patterns that would prevent the efficient expansion of settlement areas in those areas which are adjacent or close to settlement areas;*
 - e) *promoting the integration of land use planning, growth management, transit-supportive development, intensification and infrastructure planning to achieve cost-effective development patterns, optimization of transit investments, and standards to minimize land consumption and servicing costs;*
 - f) *improving accessibility for persons with disabilities and older persons by addressing land use barriers which restrict their full participation in society;*
 - g) *ensuring that necessary infrastructure and public service facilities are or will be available to meet current and projected needs;*
 - h) *promoting development and land use patterns that conserve biodiversity; and*
 - i) *preparing for the regional and local impacts of a changing climate.*
- **The proposed development will achieve an efficient land use pattern by maximizing the Subject Site's existing land resources and amenities for development.**
 - **The proposed development will consist of purpose-built rental residential dwelling units that will contribute to a greater range of housing typologies within the overall Kanata North community.**
 - **As part of the proposed development, potential impacts to the biodiversity of Tributaries 2, 3, and 4 of Shirley's Brook will be mitigated with particular regard for Blanding's turtles and fish habitat. An Environmental Impact Statement (EIS) was prepared by Gemtec dated December 21, 2022 for the proposed development. Detailed information and findings can be found in that report.**
 - **The proposed development will be served by municipal infrastructure including water and sanitary services. Stormwater will be controlled for quality and quantity. Serviceability Report was prepared by Novatech dated January 26, 2023 for the proposed development and contains detailed information and findings regarding the stormwater design for the Subject Site.**

Section 1.1.3 speaks to settlement areas which are urban areas and rural settlement areas, and include cities, towns, villages, and hamlets. The vitality and regeneration of settlement areas is critical to the long-term economic prosperity of our communities.

Policy 1.1.3.1 requires that settlement areas shall be the focus of growth and development.

Policy 1.1.3.2 states that land use patterns within settlement areas shall be based on densities and a mix of land uses which:

- a) Efficiently use land and resources;*
- b) Are appropriate for, and efficiently use, the infrastructure and public service facilities which are planned or available, and avoid the need for their unjustified and/or uneconomical expansion;*
- d) Prepare for the impacts of a changing climate;*
- e) Support active transportation;*
- f) Are transit-supportive, where transit is planned, exists or may be developed.*

Policy 1.1.3.6 states that new development taking place in designated growth areas should occur adjacent to the existing built-up area and should have a compact form, mix of uses and densities that allow for the efficient use of land, infrastructure and public service facilities.

- **The proposed development will achieve a residential density which represents efficient use of land and resources in a developing community. The development will be served by municipal infrastructure.**
- **The Subject Site is situated within close proximity to the future public at-grade bus rapid transit network along March Road with a stop planned at Klondike Road, approximately 600 metres from the Subject Site.**

Section 1.6.6 addresses sewage, water, and stormwater services.

Policy 1.6.6.2 states that municipal sewage services and municipal water services are the preferred form of servicing for settlement areas to support protection for the environment and minimize potential risks to human health and safety. Within settlement areas with existing municipal sewage services and municipal water services, intensification and redevelopment shall be promoted wherever feasible to optimize the use of the services.

- **The Subject Site will be served by municipal infrastructure including water and sanitary services.**

Wise Use and Management of Resources

Section 2.0 of the PPS speaks to conserving biodiversity and protecting the health of Great Lakes, natural heritage, water, agriculture mineral aggregate, petroleum, cultural heritage and archaeological resources for the long-term prosperity, environmental health, and social well-being of Ontario.

Section 2.1.1 requires that natural features and areas shall be protected for the long term.

Policy 2.1.2 states that the diversity and connectivity of natural features in an area, and the long-term ecological function and biodiversity of natural heritage systems should be maintained, restored or, where possible, improved, recognizing linkages between and among natural heritage features and areas, surface water features and ground water features.

Policy 2.1.7 states that site alteration shall not be permitted in the habitat of endangered and threatened species, except in accordance with provincial and federal requirements.

- **The proposed development has been designed to respect the Subject Site's surrounding natural and environmental features and to ensure their long-term protection and prosperity.**
- **An Environmental Impact Statement (EIS) was prepared by Gemtec dated December 21, 2022. Detailed information and findings can be found in that report.**

Protecting Public Health and Safety

Section 3.0 describes the province's *long-term prosperity, environmental health and social wellbeing* which are dependent on reducing the potential for public cost or risk to Ontario's residents from natural or human-made hazards. Development shall be directed away from areas of natural or human-made hazards where there is an unacceptable risk to public health or safety or of property damage, and not create new or aggravate existing hazards.

- **A Phase I Environmental Site Assessment (ESA) was prepared by Paterson Group dated August 16, 2021. As no environmental concerns were identified during the Phase I ESA, a Phase II ESA is not required for the Subject Site.**
- **Detailed information and findings can be found in the Phase I ESA report which will accompany this submission.**

Based on the foregoing, the proposed zoning amendment is consistent with the provincial interests set out in the PPS by promoting efficient use of land, protecting biodiversity of natural features, and mitigating public health and safety risks.

3.2 City of Ottawa Official Plan

At the time of initial application, the *Official Plan* (2003) was in force and effect. Given this is a recirculation, the approach taken was to review the revised proposal for conformity with the *Official Plan* (2022).

The City of Ottawa's *Official Plan* received approval from the Province of Ontario's Ministry of Municipal Affairs and Housing on November 4, 2022. The *Official Plan* provides future policy direction to the year 2046.

3.2.1 Official Plan (2022)

The *Official Plan* sets out a policy framework intended to prioritize residential growth within the built-up urban area to support an evolution towards 15-minute neighbourhoods. Under the *Official Plan*, the Subject Site is to be designated as *Corridor – Mainstreet* as shown on *Schedule B5 – Suburban (West) Transect* as shown on **Figure 4**. To support intensification objectives, *Section 3.2* of the *Official Plan* identifies a minimum residential density requirement for intensification along Mainstreets of 120 dwellings per net hectare. A minimum of 5% of dwellings should be large-household dwellings. The proposed intensification of the Subject Site

will contribute to the minimum density and dwelling size proportion targets to support the evolution of this corridor towards a 15-minute neighbourhood.

The applicable policies under the relevant sections of the *Official Plan* are listed below. A description of how the proposal responds to the policies then follows.

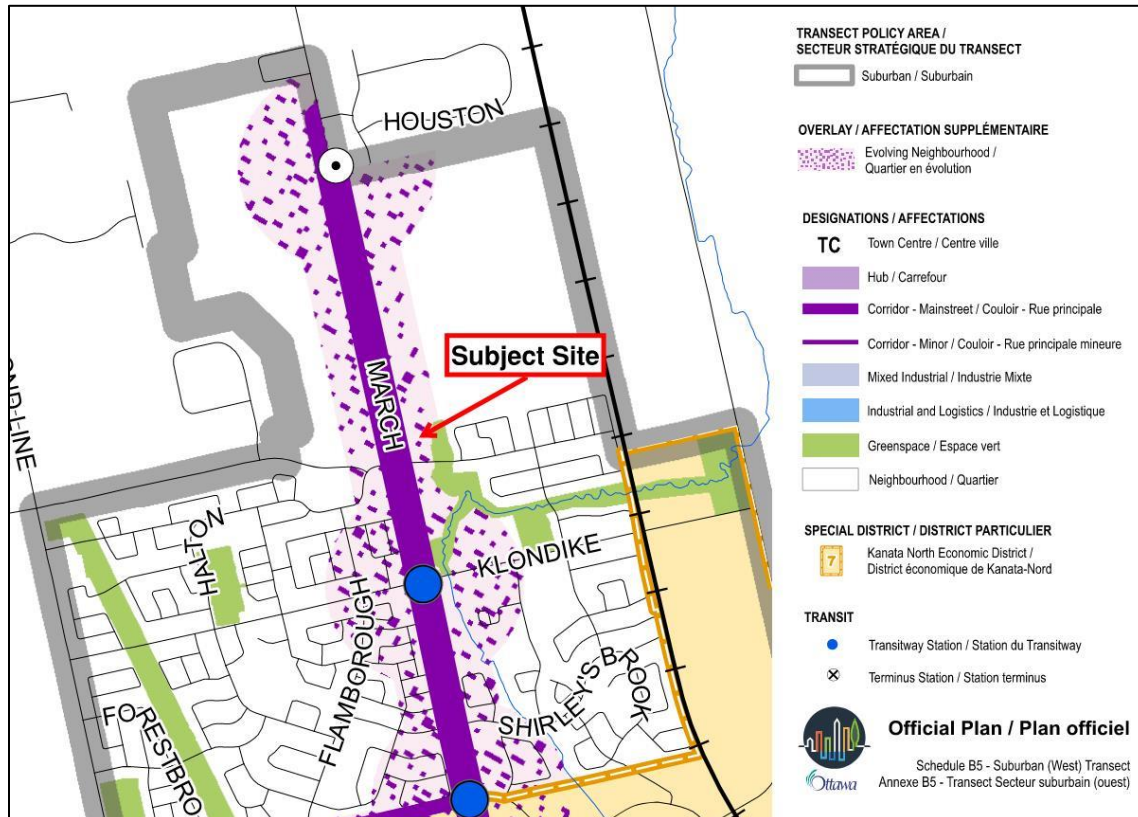


Figure 4: Excerpt from Schedule B5 - Suburban (West) Transect.

The *Suburban Transect* comprises neighbourhoods within the urban boundary located outside the Greenbelt. The objectives of the *Suburban Transect* as per Section 5.4 of the *Official Plan* include an evolution towards 15-minute neighbourhoods, enhancing mobility options and street connectivity, provide direction for Corridors, and provide direction for new development.

Section 5.4.1(2)(c) states that “the *Suburban Transect* is generally characterized by Low- to Mid-density development”:

Development shall be:

- c) *Mid-rise along Mainstreet Corridors, however the following policy direction applies;*
 - i. *Where the lot fabric can provide a suitable transition to abutting Low-rise areas, High-rise development may be permitted;*

- ii. *The setback requirements for buildings shall be proportionate to the width of the abutting right of way, and consistent with the objectives in the urban design section on Mid-rise and High-rise built form in Subsection 4.6.6, Policies 7), 8) and 9); and*
 - iii. *The Zoning By-law may restrict buildings to a Low-rise category on lots which are too small to accommodate an appropriate height transition;*
- **The proposed development is supported by the above policies and will consist of mid-rise mixed-use blocks up to nine (9) storeys in height with appropriate transition which is in keeping with the planned context along the March Road Corridor. The proposed building height is generally proportionate to the width of the March Road right of way.**

Section 5.4.3(3) states “along Mainstreet Corridors, permitted building heights, except where a secondary plan or area specific policy specifies different heights and subject to appropriate height transitions, setbacks and angular planes, maximum building heights as follows”:

- a) *Generally, not less than 2 storeys and up to 9 storeys except where a secondary plan or area-specific policy specifies greater heights; however*
 - b) *The wall heights directly adjacent to a street of such buildings, or the podiums of high-rise buildings shall be of a height proportionate to the width of the abutting right of way, and consistent with the objectives in the urban design section on mid-rise and high-rise built form in Subsection 4.6.6, Policies 7), 8) and 9); and*
 - c) *Such buildings may be limited to 4 storeys on lots too small to accommodate an appropriate height transition. Along Minor Corridors, subject to appropriate height transitions and setbacks, permitted building heights are up to 4 storeys, however,*
 - i. *.Mid-rise buildings, between 5 to 7 storeys, may be considered through a rezoning without amendment to this Plan;*
 - ii. *Mid-rise buildings above 7 storeys may be permitted through an Area-Specific Policy or Secondary Plan, and*
 - iii. *Mid-rise or high-rise buildings may be permitted through a Secondary Plan.*
- **This Zoning By-law Amendment application seeks a mid-rise form in keeping with the policies for *Mainstreet Corridors* above. The mid-rise building height of 32 m is proportionate to the width of March Road and provides appropriate transition to existing and planned residential uses east of March Road.**

Section 5.4.4(1) states that “Greenfield development in the Suburban Transect will contribute to the evolution towards 15-minute neighbourhoods to the extent possible by incorporating”:

- a) *A planned arrangement of streets, blocks, buildings, parks, public art, greenspaces, active transportation corridors and linear parks that create a sense of place and orientation, by creating view corridors, focal points and generally framing a high-quality public realm;*
- d) *Active transportation linkages that safely and efficiently connect residential areas to schools, places of employment, retail and entertainment, parks, recreational facilities, cultural assets and transit, natural amenities and connections to the existing or planned surrounding urban fabric, including to existing pedestrian and cycling routes;*

- e) *Hubs and corridors that act as the focal point of the neighbourhood, consisting of higher-density residential, office employment, commercial services catering to neighbourhood, as well as neighbourhood or regional needs, and community infrastructure such as recreational facilities or institutional uses;*
 - f) *Hubs and corridors that closely integrate and safely connect pedestrians and cyclists to surrounding neighbourhoods, are oriented to reinforce the neighbourhood-focus function of streets, and that can be conveniently accessed by public transit, including rapid transit where relevant;*
 - g) *Treed corridors, including arterial roads and collector streets that are lined with building typologies containing small-scale, street-oriented convenience and neighbourhood commercial services and other neighbourhood-oriented uses, including medium-density residential uses;*
 - h) *Avoid rear lotting on higher traffic streets by providing rear lane access for properties along arterials and major collector roads, or parallel local streets (window streets) and rear lanes for properties along arterials;*
 - i) *Screened parking lots, where surface parking is proposed, with visual impacts on the public realm mitigated by setbacks, landscaping, location on site or a combination of these measures; and*
 - j) *Planned design which optimizes the available supply, means of supplying, efficient use and conservation of energy.*
- **The Subject Site fronts onto March Road which is an existing 2-lane arterial roadway identified on Schedule C4 as shown in Figure 4.**
 - **March Road is currently designed as a rural cross section but is planned to be widened in the interim to a 4-lane urban cross section as shown in Figure 5. The ultimate widening will include the median Bus Rapid Transit (BRT) system shown in Figure 6.**
 - **The proposed development is located within 800 metres of the proposed bus rapid transit network along March Road with a planned stop at Klondike Road identified on Schedule C2 as shown in Figure 7.**
 - **A road widening allowance previously taken from the Subject Site ensures that a 44.5 metre Right-of-Way along March Road is protected for the ultimate widening which will include sidewalks and raised cycle tracks.**
 - **The proposed development features one full movement vehicular access from March Road, approximately 215 m north of the intersection of Maxwell Bridge Road. The Transportation Impact Assessment prepared by CGH Transportation dated December 22, 2022, recommends an unsignalized intersection with stop-control as an interim condition with potential signalization of the site access intersection in the future. Detailed recommendations and findings are available in this report.**
 - **The proposed mixed-use development will be in close proximity to planned pedestrian, cycling and transit facilities and to retail and commercial services including a grocery store, pharmacy, banks, and restaurants.**

- **The proposed development includes purpose-built rental residential apartment dwelling units that will contribute to a greater range of housing typologies within the overall Kanata North community and supports a movement towards greater residential density. Contributing to a greater mix of unit typologies within a suburban context provides opportunity for a community to evolve and support a diversity of residents.**
- **The proposed development orients ground floor commercial space towards March Road to create an active frontage. Visitor and commercial parking spaces are located away from the street to create an aesthetically pleasing streetscape.**

The *Corridor* designation applies to “bands of land along specified streets whose planned function combines a higher density of development, a greater degree of mixed uses and a higher level of street transit service than abutting Neighbourhoods, but lower density than nearby Hubs. The *Corridor* designation includes two sub-designations, *Mainstreet Corridors* (also referred to as *Mainstreets*) and *Minor Corridors*” per Section 6.2.

Section 6.2.1(1) states that *Corridors* “are shown as linear features in the B-series of schedules. The *Corridor* designation applies to any lot abutting the *Corridor*, subject to”:

- a) Generally, a maximum depth of:
 - i. In the case of *Mainstreet Corridors*, a maximum depth of 220 metres from the centreline of the street identified as a *Mainstreet Corridor*;
 - ii. In the case of *Minor Corridors*, a maximum depth of 120 metres from the centreline of the street identified as a *Minor Corridor*;
 - iii. Where part of a lot lies beyond the maximum depths specified in Policies i) and ii), that part of the lot is excluded from the *Corridor* designation; and
 - iv. Despite Policy iii) above, where that part of the lot excluded from the *Corridor* designation is less than 20 metres in depth, the *Corridor* designation may extend to the entire lot;
- **The Subject Site is designated as a *Mainstreet Corridor* measured 220 metres from the centreline of March Road. The *Mainstreet Corridor* designation extends to the entire lot as the lot depth is 215 metres at the furthest point.**

Section 6.2.1(3) notes that *Corridors* “will generally permit residential uses and such non-residential uses that integrate with a dense, mixed-use urban environment”.

- **The Subject Site will be rezoned to permit a mix of residential and commercial uses. The suggested uses will contribute to a greater diversity of land uses that are compatible and complementary to the surrounding land uses and emphasize active uses at grade.**
- **The proposed land uses will provide convenient access to a range of services within walking and cycling distance that will serve the future Kanata North community and contribute to principles of 15-minute neighbourhoods.**

Section 4 of the *Official Plan* provides City-Wide Policies for development.

Section 4.1 lists general policies related to mobility and transportation planning.

- **The Subject Site fronts onto March Road which is an existing 2-lane arterial roadway identified on Schedule C4 as shown in Figure 4.**
- **March Road is currently designed as a rural cross section but is planned to be widened in the interim to a 4-lane urban cross section as shown in Figure 5. The ultimate widening will include the median Bus Rapid Transit (BRT) system as shown in Figure 6.**
- **The proposed development is located within 800 metres of the proposed bus rapid transit network along March Road with a planned stop at Klondike Road identified on Schedule C2 as shown in Figure 7.**
- **A road widening allowance previously taken from the Subject Site ensures that a 44.5 metre Right-of-Way along March Road is protected for the ultimate widening which will include sidewalks and raised cycle tracks.**
- **The proposed development features one full movement vehicular access from March Road, approximately 215 m north of the intersection of Maxwell Bridge Road. The Transportation Impact Assessment prepared by CGH Transportation dated December 22, 2022, recommends an unsignalized intersection with stop-control as an interim condition with potential signalization of the site access intersection in the future. Detailed recommendations and findings are available in this report.**
- **The proposed development will promote healthy 15-minute neighbourhoods by providing housing within 15-minutes of planned safe and convenient pedestrian, cycling and bus rapid transit routes.**
- **Transportation Demand Management measures may be implemented, as required, to promote multi-modal forms of transportation through the site plan control process.**



Figure 5: Excerpt of Schedule C2 - Transit Network (Ultimate) with markup by Novatech showing Subject Site.

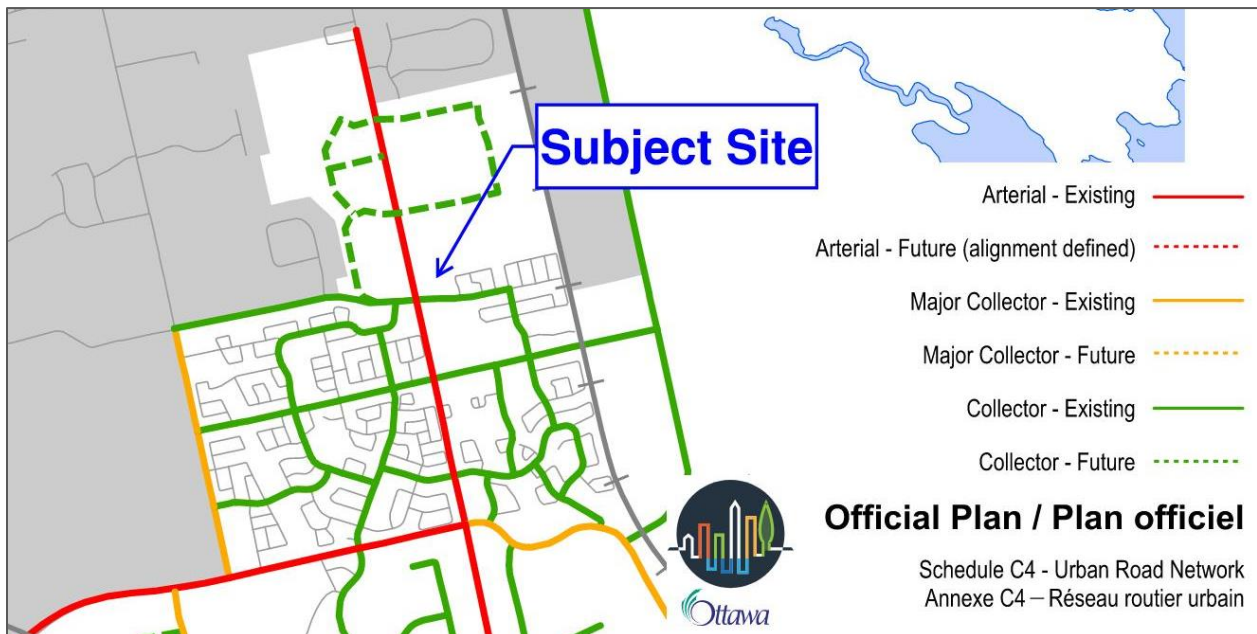


Figure 6: Excerpt of Schedule C4 - Urban Road Network with markup by Novatech showing Subject Site.

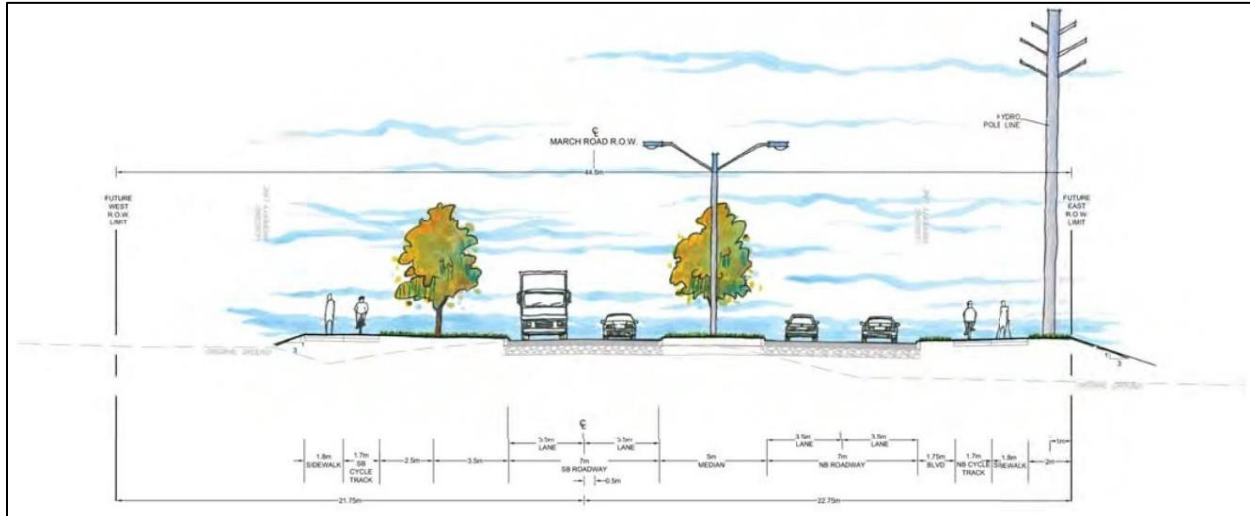


Figure 7: Excerpt from Kanata North Community Design Plan - Cross Section for March Road - Interim.

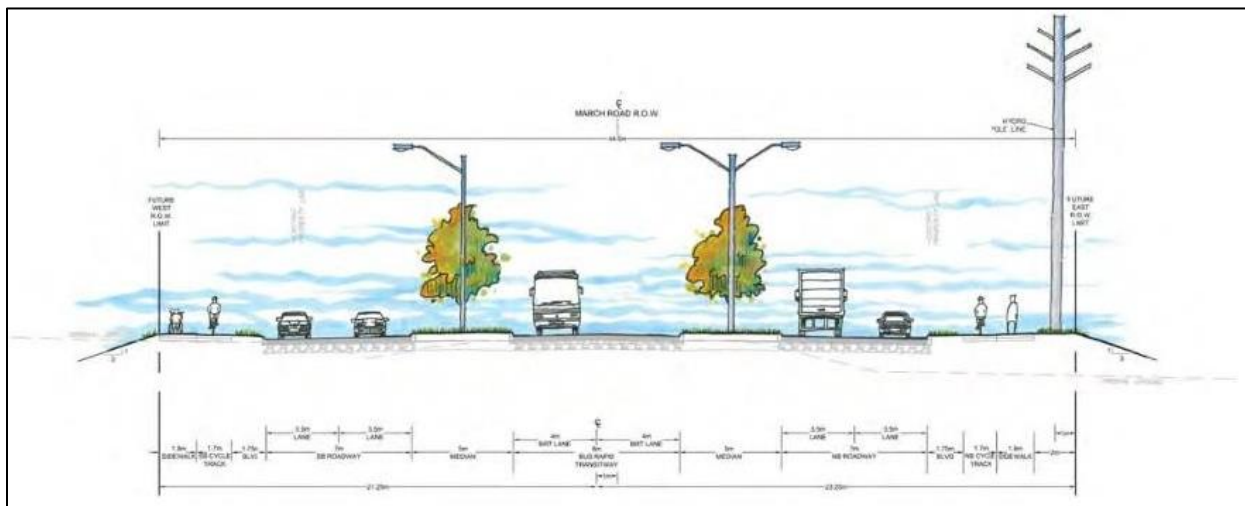


Figure 8: Excerpt from Kanata North Community Design Plan - Cross Section for March Road - Ultimate.

Section 4.2 - Housing provides general policies for residential development regarding diversity of housing types, tenures and affordability.

- **Approximately three hundred ninety (390) purpose-built rental apartments in a range of sizes and price ranges are proposed for the Subject Site. The residential development will be in close proximity to planned pedestrian, cycling and transit facilities and to retail and commercial services including a grocery store, pharmacy, banks, and restaurants.**

Section 4.3 speaks to the development of *large-scale institutions and facilities*.

- **No large-scale institutions or facilities are proposed for the Subject Site.**

Section 4.4 lists policies related to *parks and recreation facilities*.

- **Parkland dedication will be fulfilled through a subsequent revised Site Plan Control application.**
- **Future residents will have access to existing public greenspaces at 349 Maxwell Bridge Road and 1080 Halton Terrace as well as three (3) future parks currently being developed north of the Subject Site.**

Section 4.5 relates to Cultural Heritage and Archaeology.

- **No cultural heritage resources are identified on or adjacent to the Subject Site.**
- **Should any potential archaeological resources be encountered, provincial protocol will be followed.**

Section 4.6 provides general direction to development regarding urban design.

- **The Subject Site is not located in a *Design Priority Area* nor in a *Protected Viewshed*.**

Policy 4.6.2(4) states:

4) *Development abutting Scenic Routes, as identified on Schedule C13, shall contribute to conserving or creating a desirable context by such means as:*

- Protecting the opportunity to view natural and cultural heritage features;*
 - Preserving and restoring landscaping, including but not limited to distinctive trees and vegetation along the right of way;*
 - Orienting buildings towards the Scenic Route and providing direct pedestrian access, where appropriate; and*
 - Providing screening by way of opaque fencing or landscape buffers to hide surface parking lots or outside storage; and*
 - Managing the intensity and spill-over of lighting on adjacent parcels.*
- **The Subject Site is located along March Road which is identified as a *Scenic Route* on *Schedule C13 – Scenic Routes*.**
 - **The proposed development will protect and enhance views of the natural corridors of the tributary, introduce quality landscaping and direct pedestrian access in the right-of-way, orient the façade and ground-floor commercial towards the street, locate parking areas away from the public realm and ensure compliant lighting.**
 - **Detailed information can be found in the Design Brief prepared by Neuf Architectes dated January 27, 2023 which will accompany this submission.**

Subsection 4.6.4 encourages “innovative design practices and technologies in site planning and building design”.

- **The proponent follows energy efficient construction practices, specifies high quality products and utilizes durable, long-lasting materials to promote sustainability and longevity of all its buildings. Of particular significance is the proposed concrete**

superstructure which allows for repurposing or alterations over time to promote adaptability and resiliency.

Subsection 4.6.6 provides general policies that aim to “enable the sensitive integration of new development of Low-rise, Mid-rise and high-rise buildings to ensure Ottawa meets its intensification targets while considering liveability for all”. Applicable policies are included and addressed below.

- 2) *Transitions between Mid-rise and High-rise buildings, and adjacent properties designated as Neighbourhood on the B-series of schedules, will be achieved by providing a gradual change in height and massing, through the stepping down of buildings, and setbacks from the Low-rise properties, generally guided by the application of an angular plane as may be set in the Zoning by-law or by other means in accordance with Council-approved Plans and design guidelines.*
- **The concept plan proposes two terraced blocks of up to nine (9) storeys, with the tallest portions directed closer to the *Mainstreet Corridor* and separated from existing and planned residential development by the naturalized tributary corridors.**
- 4) *Amenity areas shall be provided in residential development in accordance with the Zoning By-law and applicable design guidelines. These areas should serve the needs of all age groups, and consider all four seasons, taking into account future climate conditions. The following amenity area requirements apply for mid-rise and high-rise residential*
 - a. *Provide protection from heat, wind, extreme weather, noise and air pollution; and*
 - b. *With respect to indoor amenity areas, be multi-functional spaces, including some with access to natural light and also designed to support residents during extreme heat events, power outages or other emergencies.*
- **Multiple access points will lead to a communal landscaped courtyard.**
- **Amenity areas will be provided as required by the Zoning By-law. The design of amenity areas will be discussed during the Site Plan Control recirculation.**
- 7) *Mid-rise buildings shall be designed to respond to context, and transect area policies, and should:*
 - a) *Frame the street block and provide mid-block connections to break up large blocks;*
 - b) *Include a base with active frontages and a middle portion that relates to the scale and character of the surrounding buildings, or, planned context;*
 - c) *Be generally proportionate in height to the width of the right of way as illustrated in the Figure below, with additional height permitted in the Downtown Core Transect; and*
 - d) *Provide sufficient setbacks and setbacks to:*
 - i. *Provide landscaping and adequate space for tree planting;*
 - ii. *Avoid a street canyon effect; and*
 - iii. *Minimize microclimate impacts on the public realm and private amenity areas.*

- The proposed development responds to its context, by proposing ground-floor commercial with active frontages on March Road, and a height and scale proportional to what is contemplated for the Kanata North Urban Expansion Area and broader March Road corridor, immediately north and south of the Subject Site.
- The proposed development provides setbacks and stepbacks to the naturalized areas along the revitalized tributary corridors and provides adequate space for landscaping and tree planting in the central courtyard and perimeter areas.
- The subsequent revised Site Plan Control application will provide additional detail and address relevant considerations.

Section 4.7 lists general policies related to *Drinking Water, Wastewater and Stormwater Infrastructure*.

- As previously discussed, the proposed development will be served by municipal infrastructure including water and sanitary services. Clean stormwater will filter through the vegetated perimeter and outlet to the adjacent tributaries. Unclean stormwater collected from the drive aisles and parking lots will filter through an Oil-Grit Separator unit to underground storage tanks to control for water quality and quantity before being outlet to Tributary 2.
- A Serviceability Report was prepared by Novatech dated January 26, 2023 and outlines the details of how the proposed development will be serviced.

Section 4.8 provides general direction to development regarding the conservation of *Natural Heritage, Greenspace and the Urban Forest*.

- As previously noted, the Subject Site is bounded by tributaries of Shirley's Brook to the north, east and south identified in the Kanata North CDP Environmental Management Plan as Blanding's Turtle habitat and supported by the Environmental Impact Statement prepared by Gemtec on December 21, 2022.
- The appropriate recommended setback from the Shirley's Brook tributaries based on site-specific assessment and mitigation measures was determined as 20 m from the centreline of the watercourse for Tributaries 2 and 3, and 10 m from top of slope for Tributary 4. The recommended setback of 20 m from the centreline of the watercourse is consistent with the recommendations of the Kanata North CDP and Environmental Management Plan as approved in consultation with the Ministry of Environment, Conservation and Parks (MECP) and Mississippi Valley Conservation Authority (MVCA). Detailed information and findings can be found in the EIS accompanying this submission.
- The mitigation measures outlined in the EIS will require a permit from the MVCA for development within the regulatory limit. Additionally, the proponent has applied through the Ontario MECP for an Overall Benefit Permit under *Section 17.2.2(b)* of the *Endangered Species Act* for compensation to the Blanding's Turtle habitat on the Subject Site.

Section 4.9 lists general policies related to the protection of *water resources*.

- **The Subject Site is bounded by tributaries of Shirley’s Brook to the north, east and south.**
- **A Serviceability Report was prepared by Novatech dated January 26, 2023 and outlines the details of how the proposed development will manage storm water and implement water quality and quantity controls.**
- **A Geotechnical Investigation was prepared by Paterson Group dated November 30, 2021. The report provides recommendations for the design and construction of the conceptual development based on subsurface conditions. Detailed information and findings can be found in the report which will accompany this submission.**
- **As previously discussed, A Phase I Environmental Site Assessment (ESA) was prepared by Paterson Group dated August 16, 2021. Based on the results of the assessment, a Phase II Environmental Site Assessment (ESA) was not required for the Subject Site. Detailed information and findings can be found in the Phase I ESA report accompanying this submission.**
- **As noted above, the EIS prepared by Gemtec, dated December 21 2022 recommends a range of mitigation measures for the proposed development. Detailed findings can be found in the EIS accompanying this application.**

Section 4.10 lists general policies related to the development of *School Facilities*.

- **There are three (3) existing school facilities within 1 km of the Subject Site. Four (4) school sites are planned north of the Subject Site in the KNUEA.**

Based on the foregoing, the proposed zoning amendment is in keeping with the policy direction of the *Official Plan* by protecting natural heritage features and offering diversity of housing types and tenures at a higher-density, mid-rise form along a new Corridor targeted for intensification.

3.2.2 Transit-Oriented Development Guidelines (TOD Guidelines)

The City of Ottawa’s *Transit-Oriented Development Guidelines (TOD Guidelines)* were adopted by Council in September 2007 and provide guidance to assess, promote and achieve appropriate Transit-Oriented Development within the municipality. Transit-Oriented Development is considered “a mix of moderate to high-density transit-supportive land uses located within an easy walk of a rapid transit stop or station that is oriented and designed to facilitate transit use”. The *TOD Guidelines* apply to development within a 600 m walking distance of a planned rapid transit stop or station; as previously discussed, the Subject Site is located within a 600 m walking distance of a planned bus rapid transit stop at the intersection of Klondike Road and March Road.

The *TOD Guidelines* are organized into six general sections: *land use, layout, built form, pedestrians & cyclists, vehicles & parking, and streetscape & environment*. An assessment of how the proposed development responds to the applicable guidelines is included below. Guidelines that are applicable to detailed site design will be discussed during the recirculation of the revised Site Plan Control application.

Guideline (G) Topics	Proposed Development
Land Use (G: 1 - 3)	The proposed development introduces transit supportive uses such as apartment dwellings within 600 m walking distance of a planned rapid transit station (G: 1). Non-transit-supportive land uses, such as car dealerships, drive through facilities and warehouse storage, are not proposed for the Subject Site and may be prohibited through the site-specific zoning exception in consultation with City staff (G: 2). A multi-purpose destination will be created for both transit users and local residents with the variety of residential housing types, local services and amenities suggested for the Subject Site and available in the Kanata North community (G: 3).
Layout (G: 4 – 10)	The proposed development locates the building along the front of the street to encourage ease of walking to public transit (G: 7). The tallest heights proposed by the conceptual development are located along the street and step downs are provided towards the rear of the site to transition to the surrounding Kanata North community (G: 8 and 9).
Built Form (G: 11 - 15)	Step-backs are provided in the conceptual development in order to maintain a more human scale along the sidewalk (G: 11). Windows, a variety of building materials, and projections will provide architectural variety on the lower storey of the proposed building to provide visual interest to pedestrians (G: 14).
Pedestrians & Cyclists (G: 16 - 31)	The ground-floor commercial façade will be designed to provide weather protection and to appeal to pedestrians through architectural projections (G: 27 – 28).
Vehicles & Parking (G: 32 - 47)	Parking is primarily located underground with limited surface parking located in the central courtyard to mitigate impacts on the public realm (G: 35, 39). A network of direct and safe pedestrian linkages will be provided throughout the Subject Site (G: 41).
Streetscape & Environment (G: 48 - 56)	Not applicable.

Based on the foregoing, the proposed zoning amendment respects the *Transit-Oriented Development Guidelines* by providing a mix of land uses in a pedestrian supportive environment oriented and designed to facilitate bus rapid transit use in the Kanata North community.

3.2.3 Kanata North Community Design Plan (CDP) (2016)

The Subject Site is situated immediately south of the Kanata North Urban Expansion Area (KNUEA) as shown on the *Kanata North Community Design Plan – Land Use Plan* in *Figure 1*. The *Kanata North CDP* was adopted in June 2016 as part of *Official Plan Amendment (OPA) 173* and establishes a community-wide land use framework for the KNUEA that reflects the principles, objectives and policies for community development as directed by the *Official Plan*.

While the *Kanata North CDP* does not provide guidance with respect to the Subject Site, it sets out a vision for the future development of the lands to the immediate north and west. The

proposed development is in keeping with the *Community Mixed Use* and *Creek Corridors* designations applied to the lands abutting March Road directly north of the Subject Site.

As per *Section 4.3.2 – Mixed Use*, the *Community Mixed Use* designation is “*intended primarily for community wide commercial uses such as a shopping centre*”. The commercial uses permitted within this designation include but are not limited to:

- *banks or other financial services;*
- *business, medical and professional offices;*
- *community uses such as a library or community centre;*
- *drive through;*
- *gas bar;*
- *institutional uses such as hospitals, retirement homes, residential care facilities or medical facilities;*
- *office type uses;*
- *personal service uses;*
- *recreational and athletic facilities;*
- *restaurants;*
- *retail and convenience stores;*
- *service and repair uses; and*
- *shopping centre.*

In combination with the permitted uses listed above, the following residential uses will be permitted within the *Community Mixed Use* designation, in standalone buildings or in mixed use buildings above ground floor commercial:

- *townhouses;*
- *stacked townhouses;*
- *back-to-back townhouses;*
- *and low-rise apartments*
- **The future mixed-use block on the Subject Site will be developed to include a variety of local commercial and residential uses to serve residents and the travelling public.**
- **Following the build out of the Subject Site and the *KNUEA*, residents of the surrounding Kanata North community will have access to a variety of commercial uses within close proximity. This will contribute to principles of 15-minute neighbourhoods where the daily and weekly needs of future residents can be achieved within a short walk.**
- **The proposed development is compatible with the uses contemplated in the *Community Mixed Use* designation. March Road is to accommodate development of similar scale and height to that proposed where in proximity to rapid transit stations.**

As per *Section 4.3.7 – Creek Corridors*, the *Creek Corridor* designation applies to Tributaries 2 and 3 of Shirley’s Brook which are “*protected by a 40m wide corridor providing a 20m setback from the centreline of the watercourse. In some locations, an additional 6m outside of the creek corridors will accommodate a recreational pathway*”.

- **The proposed development respects an appropriate setback to the tributary creek corridors determined through the Environmental Impact Statement (EIS), as follows:**
 - **For Tributaries 2 and 3, a minimum 20 m setback from the centreline of the watercourse channel; and**
 - **For Tributary 4, a minimum 10 m setback from the top of slope.**

- The refined setbacks for Tributaries 2 and 3 represent a consistent approach to maintaining an overall 40 m corridor as established within the KNUEA through the Kanata North CDP and Environmental Management Plan (2016), developed in consultation with the Ontario Ministry of Environment, Conservation and Parks (MECP) and the Mississippi Valley Conservation Authority (MVCA). This is based on providing environmental enhancements and compensation measures at a site-level, to be determined in consultation with the MECP.

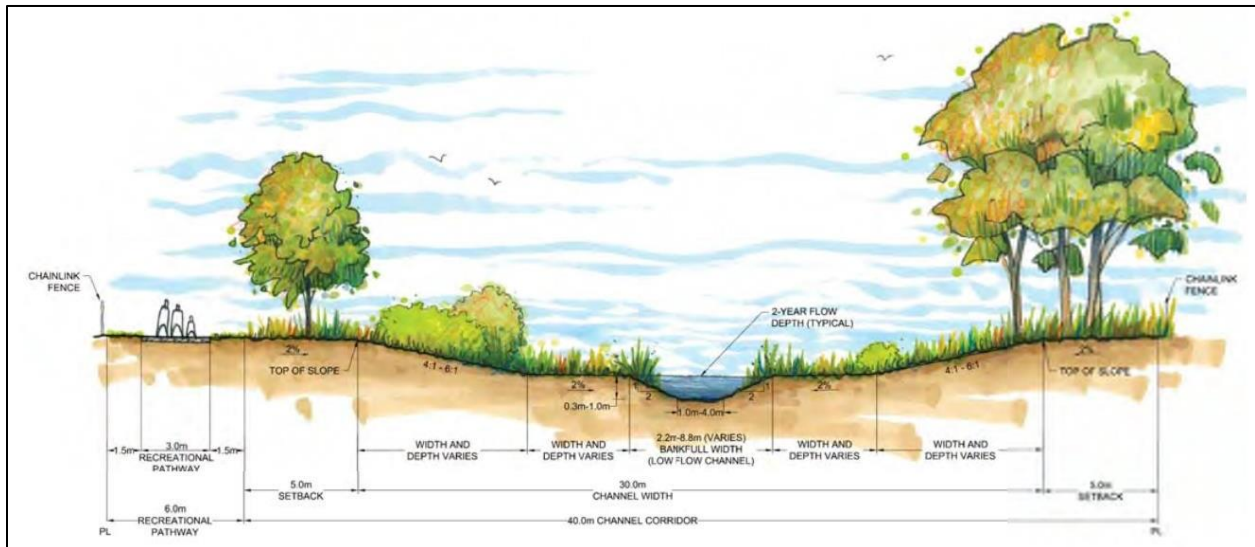


Figure 9: Excerpt from Kanata North Community Design Plan - Cross Section of Creek Corridor

Section 6.2 provides design guidelines for the Kanata North Urban Expansion Area.

Section 6.2.1 – *Mixed-Use Commercial Sites* provides site-specific design guidelines for the *Community Mixed Use* designation regarding the built form, pedestrian realm and connectivity, landscape and parking, loading/screening/garbage areas and proximity to the creek.

- The proposed development maintains the design principles of the *Kanata North CDP* by orienting ground-floor commercial close to the street to create an active public realm and providing opportunities for pedestrian connections on and off-site.

Section 6.2.6 – *Creek Corridor Guidelines* provides design guidelines for development adjacent to Tributaries 2 and 3 of Shirley’s Brook, as illustrated in **Figure 9**. The Creek Corridors “are a key amenity feature of Kanata North and a major component of the active transportation system. Open frontage along creek corridors should be encouraged wherever possible”.

- Views to the creek corridors will be maintained along the Subject Site’s pedestrian and amenity spaces.

Based on the foregoing, the proposed zoning amendment facilitates a compatible and complementary use to the vision for the Kanata North community outlined in the *Kanata North CDP*.

3.3 Zoning By-law 2008-250

3.3.1 Existing Zoning

The Subject Site is currently zoned *Development Reserve – DR* and *Rural Countryside - RU* under the *City of Ottawa’s Zoning By-law 2008-250* as shown on **Figure 10**. The *DR* zone generally recognizes lands intended for future urban development. The *RU* zone recognizes the former rural-based uses as existed prior to the expansion of the urban boundary.

The *Flood Plain Overlay* also applies to portions of the site located within the current 1:100 flood elevation associated with the tributaries of Shirley’s Brook. The *Flood Plain Overlay* intends to restrict development in a floodplain area so as to minimize the threat of injury or loss of life where flooding may compromise the ability to deliver essential services or where flood may cause unacceptable risk of property damage. No changes to the limits of the *Flood Plain Overlay* are proposed.

To facilitate the future proposed development as shown on the Draft Site Plan, a *Zoning By-law Amendment* will be required to change the zoning so as to permit a variety of urban uses at the Subject Site. The intent of the amendment is to accommodate a mid-rise mixed-use building consisting of ground floor commercial and residential apartment units.

3.3.2 Suggested Zoning

It is suggested to rezone the Subject Site from *Development Reserve Zone - DR* and *Rural Countryside Zone – RU* to *General Mixed-Use Zone – GM* for the future mixed-use building as shown on Figure 12. The suggested *General Mixed-Use Zone – GM* will permit a range of residential, commercial and institutional uses that will enable and support principles of 15-minute neighbourhoods while ensuring transition to and consistency with the surrounding community context.



Figure 10: Excerpt of the Subject Site's existing zoning from GeoOttawa.

The purpose of the *General Mixed-Use Zone – GM* is to:

- 1) *allow residential, commercial and institutional uses, or mixed-use development in the General Urban Area and in the Upper Town, Lowertown and Sandy Hill West Character Areas of the Central Area designations of the Official Plan;*
- 2) *limit commercial uses to individual occupancies or in groupings in well defined areas such that they do not affect the development of the designated Traditional and Arterial Mainstreets as viable mixed-use areas;*
- 3) *permit uses that are often large and serve or draw from broader areas than the surrounding community and which may generate traffic, noise or other impacts provided the anticipated impacts are adequately mitigated or otherwise addressed; and*
- 4) *impose development standards that will ensure that the uses are compatible and complement surrounding land uses.*

The intent of the Zoning By-law amendment is to change the current zoning of the developable area of the Subject Site in order to enable development at higher intensity in proximity to transit

as envisioned by the Official Plan. Following the pre-application consultation meeting on June 28, 2022 with City of Ottawa Staff, the *General Mixed-Use Zone – GM* was suggested for rezoning.

As summarized in **Table 1**, a site-specific exception is anticipated to modify certain standards of the *GM* zone, such as:

Permitted Uses: The *GM* zone permits a wide range of residential and non-residential uses. The *GM* zone is recommended for the Subject Site to accommodate the proposed mixed-use building which will include residential dwelling units as well as retail or commercial services in smaller units intended for small businesses. The *GM* zone contemplates a number of other uses which may generate traffic, noise or other impacts not suitable for this location and, as such, the site-specific exception may limit permitted uses to those appropriate for this location. Further discussion with City of Ottawa staff will refine the list of permitted uses in the *GM* zone to ensure compatibility with the surrounding community.

Maximum Building Height: The *GM* zone permits a maximum building height up to 18 m or approximately 6 storeys. The proposed height of up to 32 m accommodates a standard commercial ground floor height of 4.2 m with up to 8 residential storeys above. The proposed development concentrates the tallest heights along March Road and allows for terraced step backs and step downs towards the rear of the site. The proposed maximum building height of 32 m is intended to achieve an appropriate level of intensification in a mid-rise form that respects the surrounding natural heritage features on the Subject Site.

Table 1: Proposed GM exception provisions

		Exception Provisions		
I Exception Number	II Applicable Zone	III Additional Land Uses Permitted	IV Land Uses Prohibited	V Provisions
XXXX (By-law 2023-XX)	GM [XXXX] H(32)	<p>The uses listed under subsections 187(1) and 187(2) are not permitted and only the following uses are permitted:</p> <p>Apartment dwelling, low rise; Apartment dwelling, mid rise; Artist studio; Catering establishment; Click and collect facility; Community health and resource centre; Convenience store; Dwelling unit; Home-based business; Instructional facility; Medical facility; Office; Personal service Business; Residential care facility; Restaurant; Retail food store; Retail store; Retirement home; and Retirement home, converted.</p>		Maximum building height: 32 m

The proposed mixed-use building will be designed to meet all other standards of the Zoning By-law to ensure orderly and functional development.

For the watercourses, it is suggested to rezone the portion of the Subject Site within the minimum setbacks from *Rural Countryside Zone – RU* and *Development Reserve Zone – DR* to *Parks and Open Space Zone – O1* or *Environmental Protection Zone – EP*.

The purpose of the *Parks and Open Space Zone – O1* is to:

- 1) *permit parks, open space and related and compatible uses to locate in areas designated as General Urban Area, General Rural Area, Major Open Space, Mixed Use Centre, Village, Greenbelt Rural and Central Area as well as in Major Recreational Pathway areas and along River Corridors as identified in the Official Plan, and*
- 2) *ensure that the range of permitted uses and applicable regulations is in keeping with the low scale, low intensity open space nature of these lands.*

The purpose of the *Environmental Protection Zone – EP* is to:

- 1) *recognize lands which are designated in the Official Plan as Significant wetlands, Natural Environment Areas and Urban Natural Features that contain important environmental resources which must be protected for ecological, educational and recreational reasons;*
- 2) *permit only those uses which are compatible with and assist in the protection of the environmental attributes of these lands, or are in keeping with applicable Official Plan policies; and*
- 3) *regulate development to minimize the impact of any buildings or structures within these environmental areas.*

Section 69 of the Zoning By-law states:

- 1) *Subject to subsection (3), despite the provisions of the underlying zone, the minimum setbacks set forth in subsection (2) must be provided to provide a margin of safety from hazards associated with flooding and unstable slopes and to help protect the environmental quality of watercourses and waterbodies.*
- 2) *Except for flood or erosion control works, or a public bridge or a marine facility, no building or structure, including any part of a sewage system, which does not require plan of subdivision, or site plan control approval, shall be located closer than:*
 - a) *30 m to the normal high-water mark of any watercourse or waterbody, or*
 - b) *15 m to the top of the bank of any watercourse or waterbody, whichever is the greater.*
- 3) *Development requiring a plan of subdivision or that is subject to site plan control must provide the watercourse or waterbody setbacks set forth in subsection (2) unless, as established through conditions of approval, a different setback is determined to be appropriate in accordance with the criteria set forth in the Official Plan. (By-law 2009-347)*

- The normal high-water mark and top of bank referenced above coincide with “bank full flow” and “top of slope” respectively as shown in Figure 11.
- As per *Subsection 69(3)* of the Zoning By-law, a revised Site Plan application will be filed with these recommended alternative setbacks anticipated as a condition of approval.

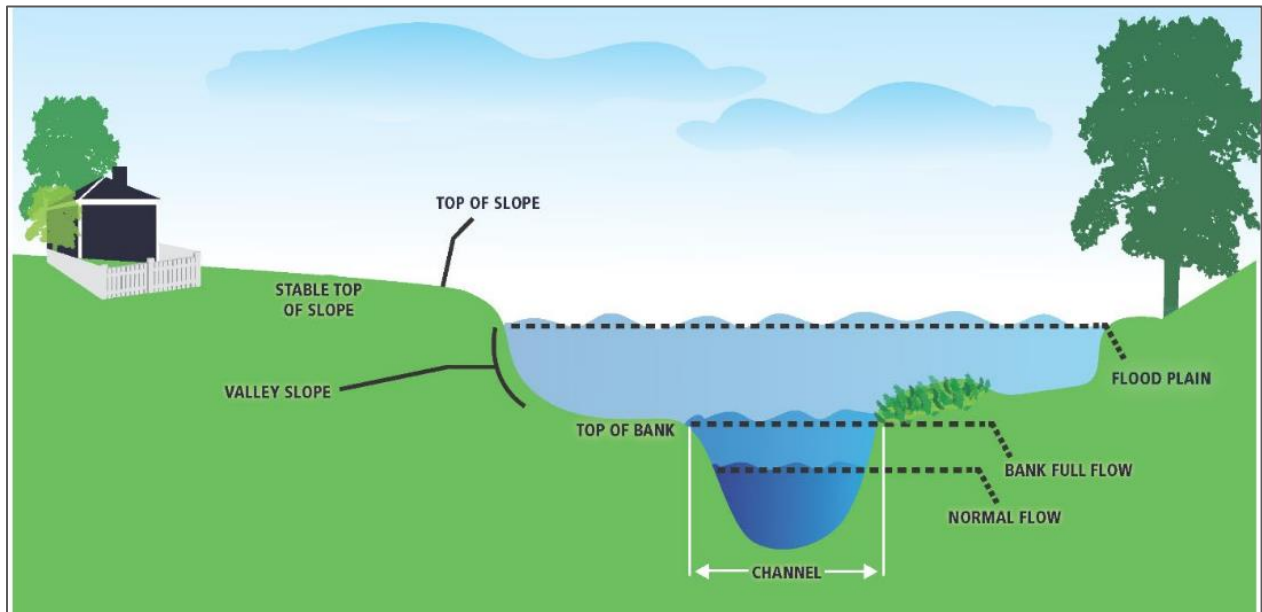


Figure 11: Excerpt of Official Plan Figure 17 - Setbacks from Surface Water Features.

Policy 4.9.3(7) of the Official Plan (2022) states:

- 7) *Exceptions to the setbacks in Policy 2) shall be considered by the City in consultation with the conservation authority in situations where development is proposed on existing lots where, due to the historical development in the area, it is impossible to achieve the minimum setback because of the size or location of the lot, approved or existing use on the lot or other physical constraint, providing the following conditions are met to the City’s satisfaction:*
 - a. *The ecological function of the site is restored and enhanced, to the greatest extent possible, through naturalization with native, non-invasive vegetation and bioengineering techniques to mitigate erosion and stabilize soils; and*
 - b. *Buildings and structures are located, or relocated, to an area within the existing lot that improves the existing setback, to the greatest extent possible, and does not encroach closer to the surface water feature.*

Appropriate setbacks have been established from all tributaries through the recommendations of the Environmental Impact Statement (EIS) prepared by Gemtec dated December 21, 2022, the Geotechnical Investigation prepared by Paterson Group dated August 16, 2021 and the Serviceability Report prepared by Novatech dated January 26, 2023. The EIS addresses the criteria in *Policy 4.9.3(5)* of the Official Plan, including:

- a) *Evaluation and description of the project site, sensitivity of the headwater drainage features and sampling methods;*
 - b) *Assessment and classification of hydrological function, riparian conditions, fish and fish habitat and terrestrial habitat; and*
 - c) *Management recommendations regarding the need to protect, conserve, mitigate, maintain recharge or maintain/replicate terrestrial linkages of the headwater drainage features and a corresponding recommendation for an appropriate minimum setback.*
- **The proposed minimum setbacks vary based on the field-studied watercourses, as discussed in the EIS. The proposed minimum setbacks are 20 m from the centreline of the watercourse channel for Tributaries 2 and 3, and 10 m from the top of slope for Tributary 4, consistent with the terminology and definitions provided in the *Official Plan*.**
 - **The proposed setbacks for Tributaries 2 and 3 maintain the intent of the 40 m tributary corridor supported by on-site compensation for Blanding's Turtle habitat proposed in the *Kanata North CDP*. On-site compensation for Blanding's Turtle habitat on the Subject Site is being determined in consultation with the Ministry of Environment, Conservation and Parks (MECP). The 20 m setback (40 m corridor) is consistent with the approvals given to the development applications north of the Subject Site within the KNUEA and developed in consultation with the MECP and Mississippi Valley Conservation Authority (MVCA).**
 - **The proposed 10 m setback for Tributary 4 reflects its primary function as a stormwater flow channel. Further, Tributary 4 is not considered in the EIS to provide suitable fish or Blanding's turtle habitat.**
 - **Additional information regarding the recommended setbacks can be found in the EIS prepared by Gemtec dated December 21, 2022.**
 - **Section 7 - Storm Servicing & Stormwater Management of the Serviceability Report prepared by Novatech dated January 26, 2023 provides additional details about the treatment of stormwater on the Subject Site to mitigate potential impacts on the environmental quality of the watercourses. No development is proposed within the floodplain and lands within the minimum setbacks will only be developed so as to implement the recommended mitigation measures outlined in the EIS and will maintain a generally naturally vegetated condition to protect the ecological function of the tributaries.**

The suggested zoning is illustrated in Figure 12.

Based on the foregoing, the suggested zoning is appropriate for the use of the Subject Site and introduces standards which are in keeping with the intent of the Zoning By-law.

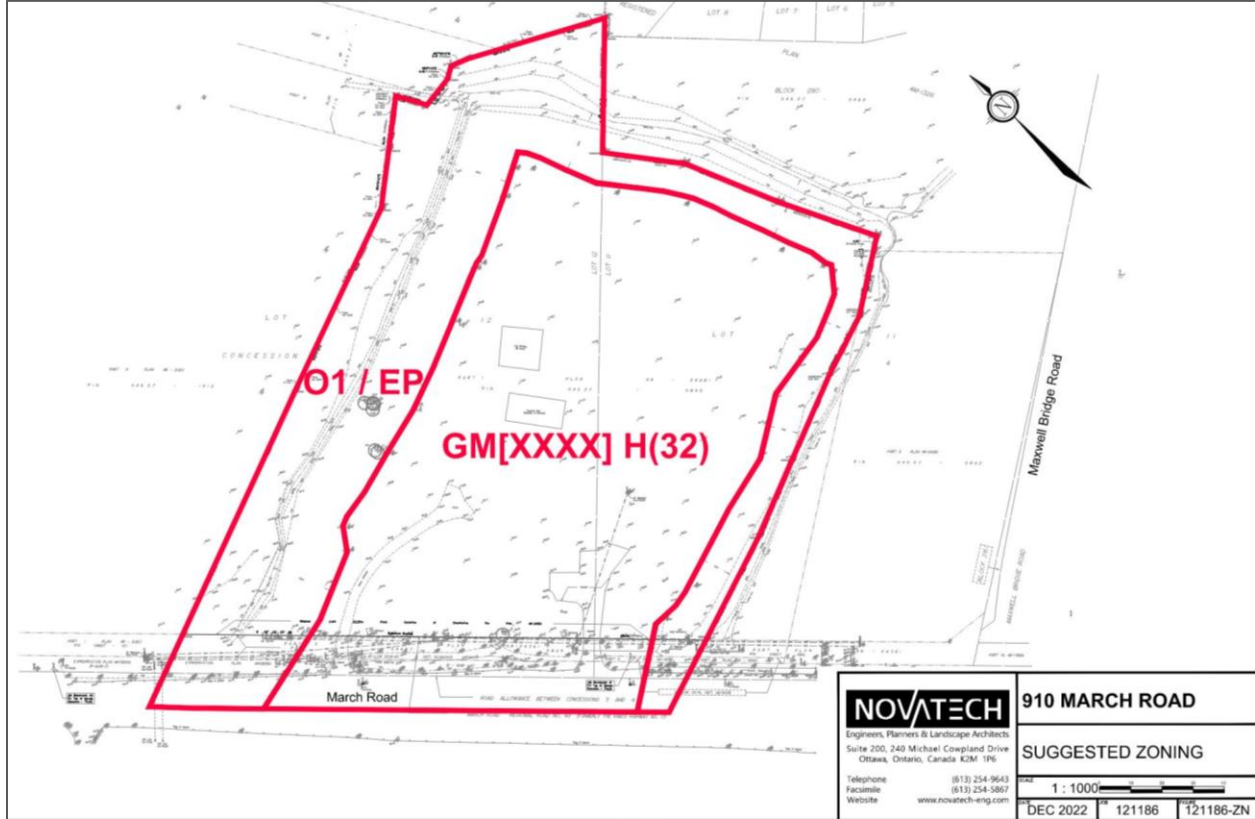


Figure 12: Excerpt of Suggested Zoning figure prepared by Novatech dated December 2022.

4.0 PUBLIC CONSULTATION STRATEGY

The public consultation strategy will involve a variety of methods as follows:

- Signage posting on the Subject Site providing members of the public with details of the proposed development and means of contacting the file lead to provide comments and/or questions.
- Digital copies of all required supporting studies and plans will be made available for public viewing through the City of Ottawa's Development Applications webpage (<https://devapps.ottawa.ca/en/>).
- Community organization(s) will be notified of the details of the proposed development through a 'heads up' by City staff.
- Consultation with the local ward councillor will take place through the application process.
- Mailed notice will be sent by City staff to residents within 120 m of the subject site and any registered community groups to inform the community of the statutory meeting to be held at the scheduled Planning Committee meeting prior to consideration by City Council.

5.0 CONCLUSION

This planning rationale has been prepared in relation to a revised application for Major Zoning By-law Amendment for the development of a proposed mid-rise, mixed-use community at 910 March Road.

It is our assessment that the proposed development is consistent with the *Provincial Policy Statement (2020)* by promoting efficient use of land, protecting biodiversity of natural features, and mitigating public health and safety risks.

The proposed development conforms with the *City of Ottawa Official Plan (2022)* by protecting natural heritage features and offering diversity of housing types and tenures at a higher-density, mid-rise form along a Mainstreet Corridor.

The proposed development respects the *Transit-Oriented Development Guidelines* by providing a mix of land uses in a pedestrian supportive environment oriented and designed to facilitate bus rapid transit use in the Kanata North community. Further, the proposed zoning amendment facilitates a compatible and complementary use to the vision for the Kanata North community immediately north of the Subject Site, as outlined in the *Kanata North Community Design Plan*.

The proposed amendment to *Zoning By-law 2008-250* to accommodate site-specific provisions for permitted uses and form is in keeping with the intent of the by-law by protecting the Subject Site's natural heritage features while also achieving a desirable built form in a compact and pedestrian oriented manner that transitions between the existing and future Kanata North community.

This Planning Rationale along with the associated technical studies supports the proposed zoning amendment by illustrating appropriate site development with respect to compatibility with the surrounding existing and evolving character of the Kanata North community.

In conclusion, the proposed development is an appropriate and desirable addition to the Kanata North community and represents good planning.

Sincerely,

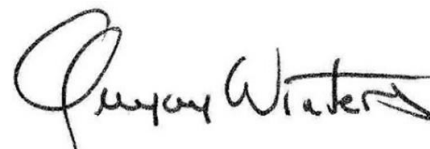
NOVATECH

Prepared by:



Miranda Virginillo, M.PL.
Planner, Planning & Development

Reviewed by:



Greg Winters, MCIP, RPP
Director, Planning & Development

APPENDIX A

SITE PLAN

PREPARED BY NEUF ARCHITECTES

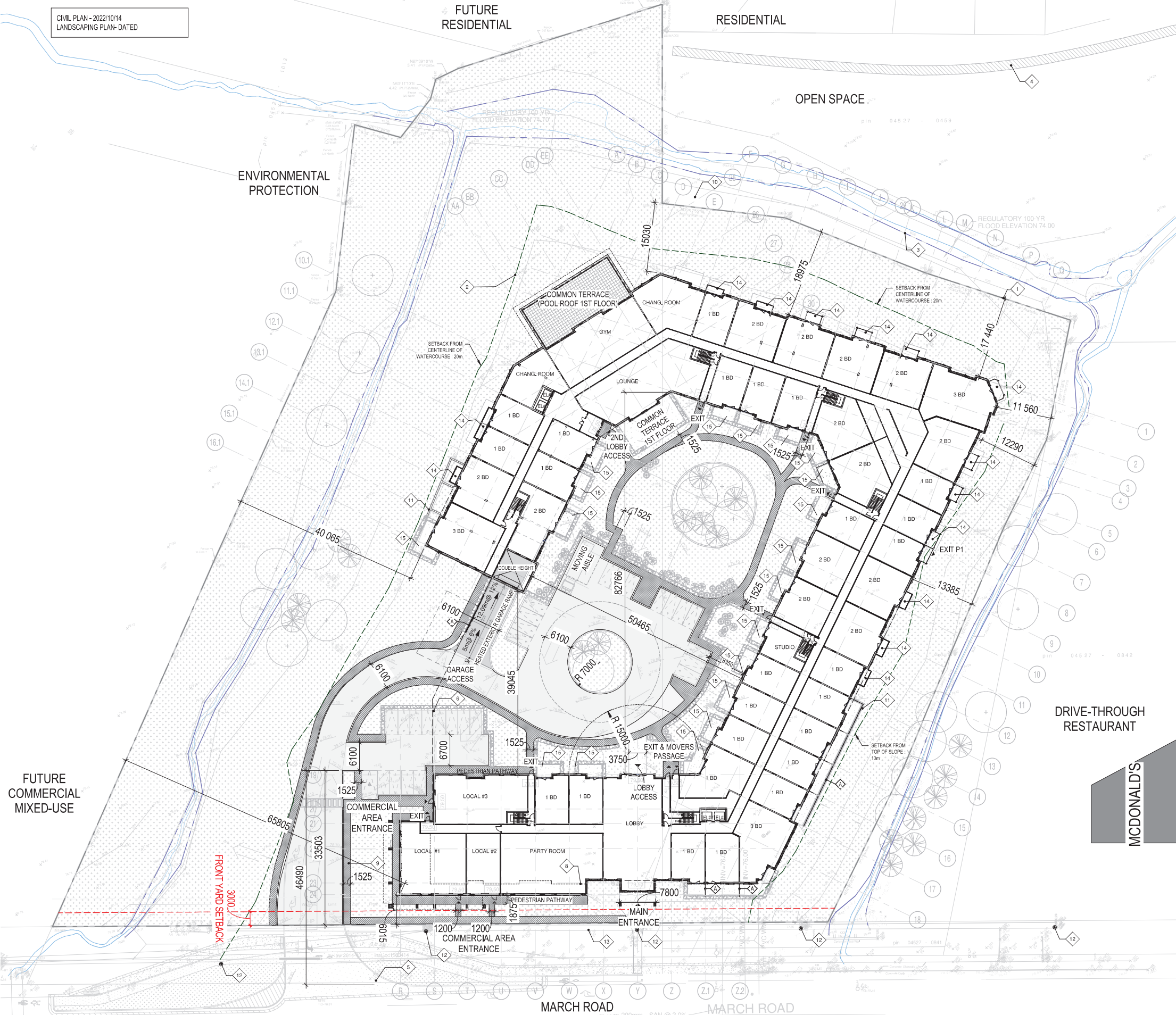
DATED JANUARY 26, 2023

CIVIL PLAN - 2022/10/14
LANDSCAPING PLAN-DATED

FUTURE RESIDENTIAL
RESIDENTIAL

OPEN SPACE

ENVIRONMENTAL PROTECTION



DRIVE-THROUGH RESTAURANT

MCDONALD'S

FUTURE COMMERCIAL MIXED-USE

MARCH ROAD

SITE PLAN

1:400

GENERAL NOTES	
# note	Description
1	PROPERTY LINE
2	SETBACKS AS PER PROPOSED ZONING
3	TOP OF BANK /ORDINARY HIGH WATER MARK
4	EXISTING MUNICIPAL MUP/TRAIL
5	EXISTING SIDEWALK
6	UNDERGROUND PARKING P1 OUTLINE
7	GROUND FLOOR OUTLINE
8	2ND FLOOR - OUTLINE
9	NEW SIDEWALK
10	EXISTING TRIBUTARIES
11	RETAINING WALL
12	UTILITY POLE
13	OVERHEAD WIRES
14	PRIVATE BALCONY
15	PRIVATE TERRACE

SURFACE LEGEND	
	ASPHALT
	CONCRETE ON-GRADE
	STONE BALLAST ROOF
	LANDSCAPING / EXISTING VEGETATION
	PEDESTRIAN TRAFFIC
	EXISTING BUILDING

910 MARCH ROAD - SITE STATISTICS	
LOT AREA	27 156 m ²
FOOTPRINT	7 303 m ²
CONSTRUCTION AREA ABOVE GROUND	49684 m ²
CONSTRUCTION AREA UNDERGROUND (BUILDING & PARKING)	25533 m ²
GROSS FLOOR AREA (ABOVE + BELOW GRADE)	75 369 m ²

SITE STATISTICS AS PER ZONING BY-LAW	
GROSS FLOOR AREA AS PER ZONING BY-LAW (ABOVE + BELOW GRADE)	40 723 m ²
RESIDENTIAL GROSS FLOOR AREA AS PER ZONING BY-LAW	40 222 m ²
COMMERCIAL GROSS FLOOR AREA AS PER ZONING BY-LAW	501 m ²

910 MARCH ROAD - NUMBER OF UNITS	
P1 PARKING LEVEL	13
1ST FLOOR LEVEL	42
2ND FLOOR LEVEL	62
3RD FLOOR LEVEL	62
4TH FLOOR LEVEL	51
5TH FLOOR LEVEL	48
6TH FLOOR LEVEL	41
7TH FLOOR LEVEL	35
8TH FLOOR LEVEL	19
9TH FLOOR LEVEL	17
Total général	390

NUMBER OF PARKING SPACES	
1 Residents (parking space / dwelling unit)	390 (MIN REQ.)
Dwelling units in a mixed-use building, Area C or D	470 (PROVIDED)
1.2 Residents (parking space / dwelling unit)	79 (MIN REQ.)
Dwelling units in a mixed-use building, Area C or D	79 (PROVIDED)
0.2 Visitors (parking space / dwelling unit)	18 (MIN REQ.)
Commercial parking (3.4 X 100SQM REQ.)	19 (PROVIDED)
Commercial parking (6.4 X 100SQM REQ.)	19 (PROVIDED)
TOTAL PARKING REQUIRED	487 (MIN REQ.)
TOTAL PARKING PROVIDED	568 (PROVIDED)
INCLUDED BARRIER FREE PARKING	12 SPACES REQUIRED (6 TYPE A & 6 TYPE B)
MINIMUM BARRIER FREE PARKING REQUIRED	12 (MIN. REQ.)

910 MARCH ROAD		
CITY OTTAWA ZONING BY-LAW 2008-250		
GMP(XXXX) H(32)		
PROPOSED MID-RISE MIXED-USE DEVELOPMENT		
ZONING PROVISION	REQUIRED	PROPOSED
GENERAL MIXED-USE ZONE PROVISIONS		
PERMITTED USES	RESIDENTIAL: COMMERCIAL	RESIDENTIAL: COMMERCIAL
FRONT YARD SETBACK (MIN./MAX.)	3m	6m
REAR YARD SETBACK (MIN.)	7.5 m WHERE ABUTTING A RESIDENTIAL ZONE	> 7.5 m
INTERIOR SIDE YARD SETBACK (MIN.)	5 m FOR MIXED USE BUILDING ABUTTING A RESIDENTIAL ZONE NO MINIMUM ALL OTHER CASES	> 5 m 11.5 m / 40 m
FLOOR SPACE INDEX (MAX.)	2	1.1
BUILDING HEIGHT (MAX.)	32 m	32 m
LANDSCAPED AREA (MIN. WIDTH ABUTTING A STREET)	3m	6m
PARKING PROVISIONS (AREA C & AREA D)		
BICYCLE PARKING SPACES - RESIDENTIAL (MIN.)	0.5 PER DWELLING UNIT = 195	0.5 PER DWELLING UNIT = 195
BICYCLE PARKING SPACES - NON-RESIDENTIAL (MIN.)	1 PER 250 m ² GFA RETAIL = 3	1 PER 250 m ² GFA RETAIL = 3
AMENITY AREA PROVISIONS		
TOTAL PRIVATE AMENITY AREA (MIN.)	6 m ² PER DWELLING UNIT = 2 346 m ²	6 m ² PER DWELLING UNIT = 2 346 m ²
COMMUNAL AMENITY AREA (MIN.)	50% OF REQUIRED TOTAL AMENITY ARE = 1173 m ²	50% OF PROPOSED TOTAL AMENITY ARE = 1173 m ²
WATERCOURSE SETBACK (MIN.)		
I) FOR TRIBUTARIES 1 AND 2 (NORTH AND EAST): 30 m TO CENTERLINE OF WATERCOURSE.	II) FOR TRIBUTARIES 3 AND 3 (NORTH AND EAST): 20 m TO CENTERLINE OF WATERCOURSE.	II) FOR TRIBUTARY 4 (SOUTH): 10 m TO TOP OF SLOPE

NOTES GÉNÉRALES General Notes

1. Ces documents d'architecture sont la propriété exclusive de NEUF architect(e)s et ne peuvent être utilisés, reproduits ou copiés sans autorisation écrite préalable. / These architectural documents are the exclusive property of NEUF architect(e)s and cannot be used, copied or reproduced without written pre-authorization.

2. Les dimensions apparaissant sur ces documents doivent être vérifiées par l'entrepreneur avant le début des travaux. / All dimensions which appear on the documents must be verified by the contractor before to start the work.

3. Veuillez aviser l'architecte de toute dimension erronée et/ou divergences entre ces documents et ceux des autres professionnels. / The architect must be notified of all errors, omissions and discrepancies between these documents and those of the others professionals.

4. Les dimensions sur ces documents doivent être lues et non mesurées. / The dimensions on these documents must be read and not measured.

ARCHITECTE PAYSAGER Landscape Architect
Gino Aiello
50 Camelot Drive, Nepean, ON K2G 5X8
T 613 852 1343

CIVIL CIVIL
NOVATECH
240 Michael Cowpland Drive, Suite 200, Ottawa, ON K2M 1P6
T 613 254 9643 novatech-eng.com

INGÉNIEUR MÉCANIQUE Mechanical Engineer
NATIONAL MULTI-RESIDENTIAL INNOVATIVE DESIGN
555 Legget Drive, Tower A, Suite 212, Ottawa, ON K2K 2X3
T 613 224 2761 Fax 613 951 0586

INGÉNIEUR EN STRUCTURE Structural Engineer

ARCHITECTES Architect
NEUF architect(e)s
630, boul. René-Lévesque E, 32e étage, Montréal QC H3B 1S6
T 514 847 1117 NEUFarchitectes.com

SCEAU / Seal

ONTARIO ASSOCIATION OF ARCHITECTS
KIM PHAM
LICENCE 9305

NEUF
ARCHITECT(E)S

NEUF ARCHITECTES SENIOR

CLIENT Client
LÉPINE
206-555 Legget Dr., Tower A, Ottawa, ON K2K 2X3
T 613 591 9090 F 613 591 9095

COMPANY NAME
910 MARCH & MAXWELL
EMPLACEMENT Location
910 MARCH ROAD NO. PROJET No.
12712.00

NO. RELEASE DATE (aa-mm-jj)
0 ISSUED FOR ZONING 2023.01.26

DESIGNÉ PAR Drawn by
ZS CI VÉRIFIÉ PAR Checked
K.P.
DATE (aa-mm-jj) 23.01.26 ÉCHELLE Scale
As indicated
TITRE DU DESSIN Drawing Title
1ST FLOOR SITE PLAN CONCEPTUAL
PREVIOUS REVISION 03
CURRENT REVISION 010
REVISION Revision NO. DESSIN Dwg Number
0.10 A060

Autodesk Docs://12712_910 MARCH ROAD/M&M_12712_ARC_BBD_R22.V1