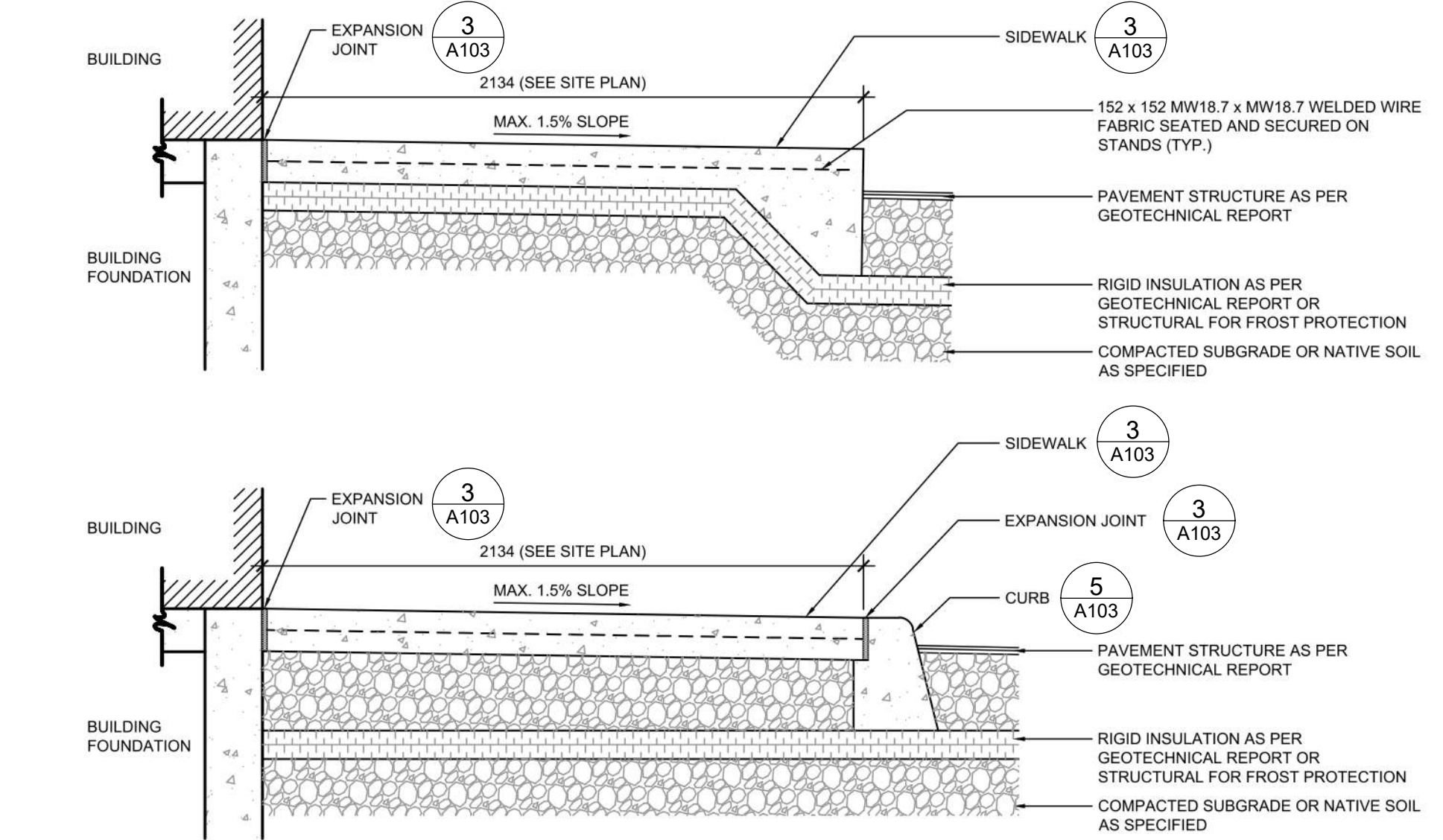
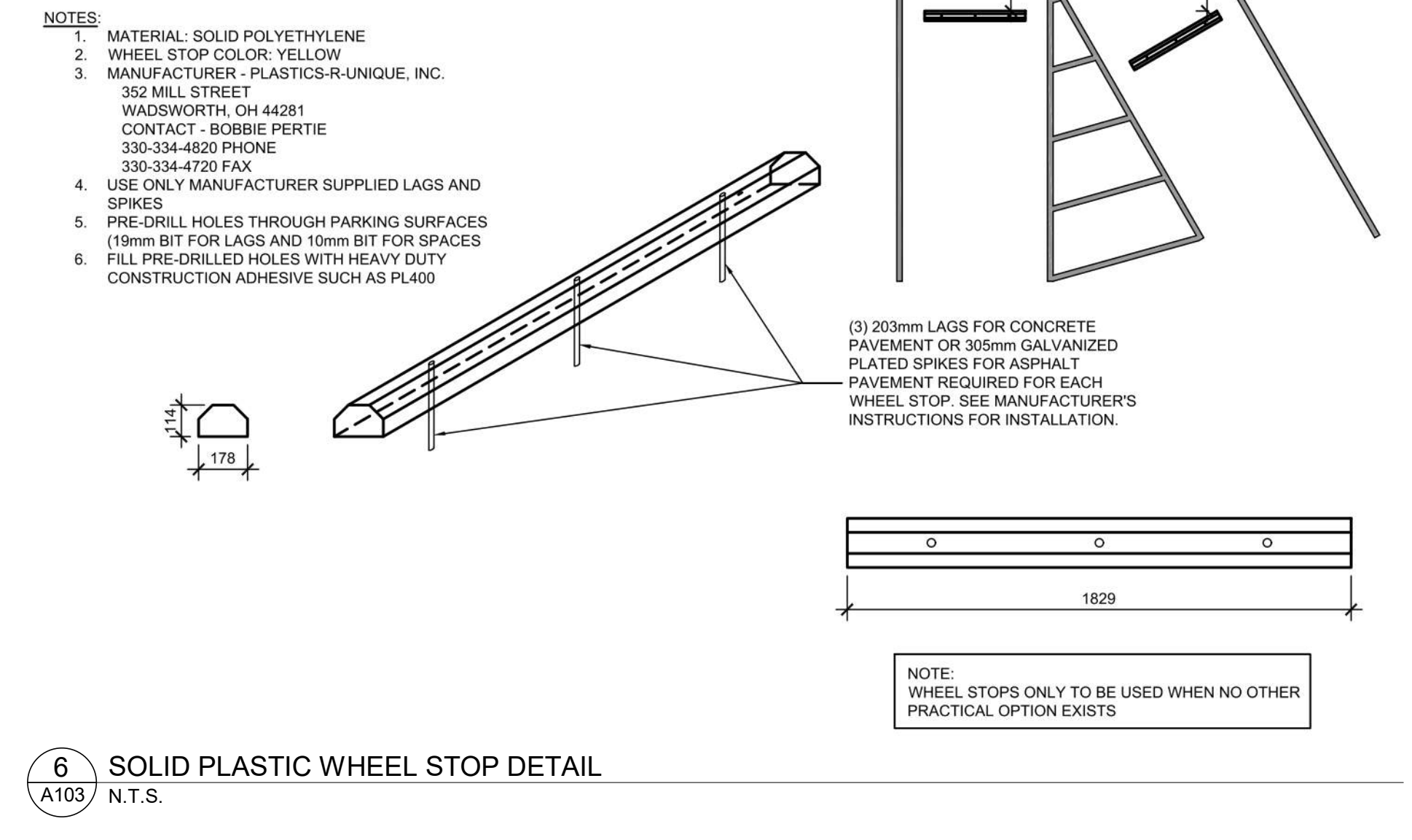
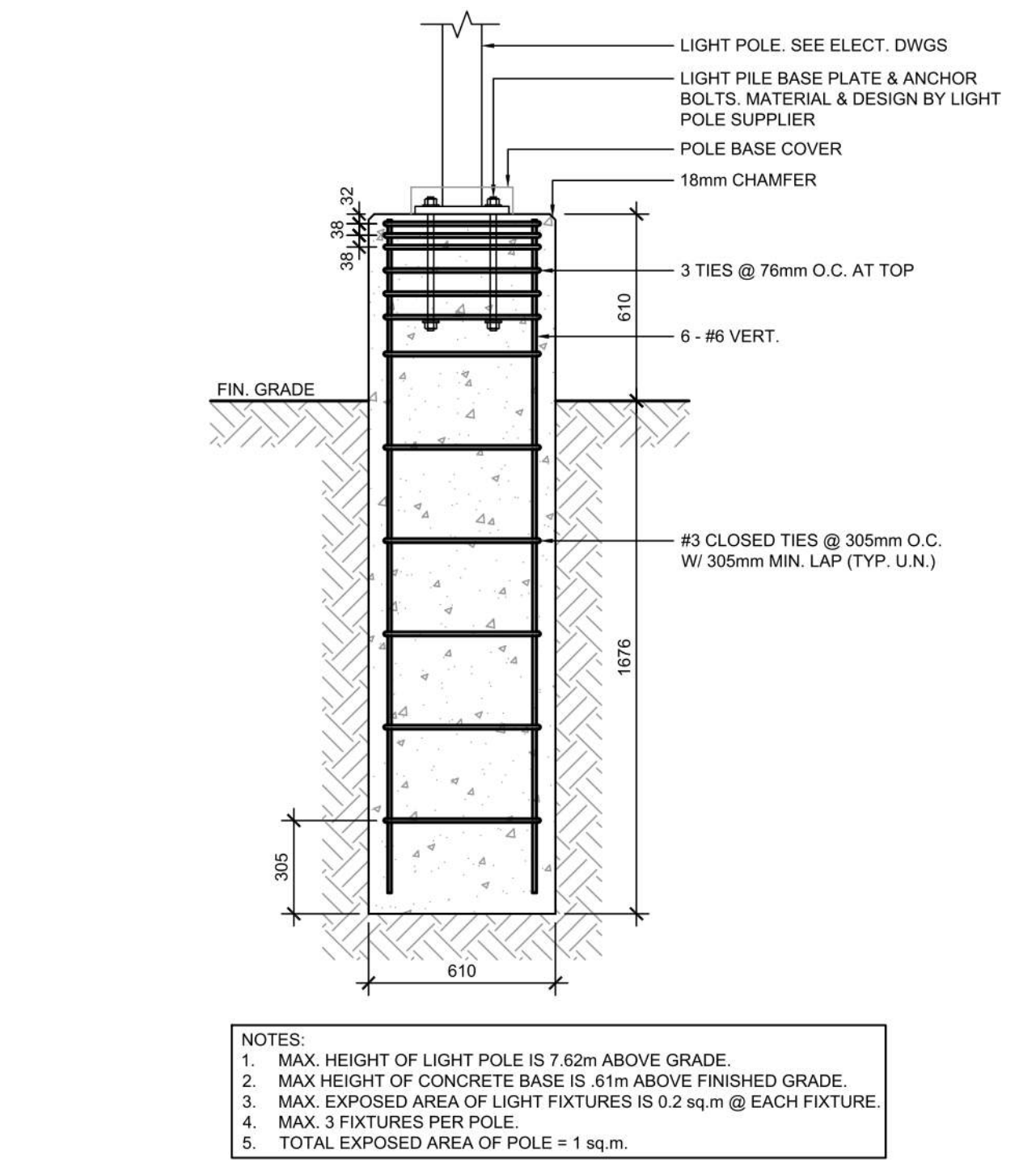
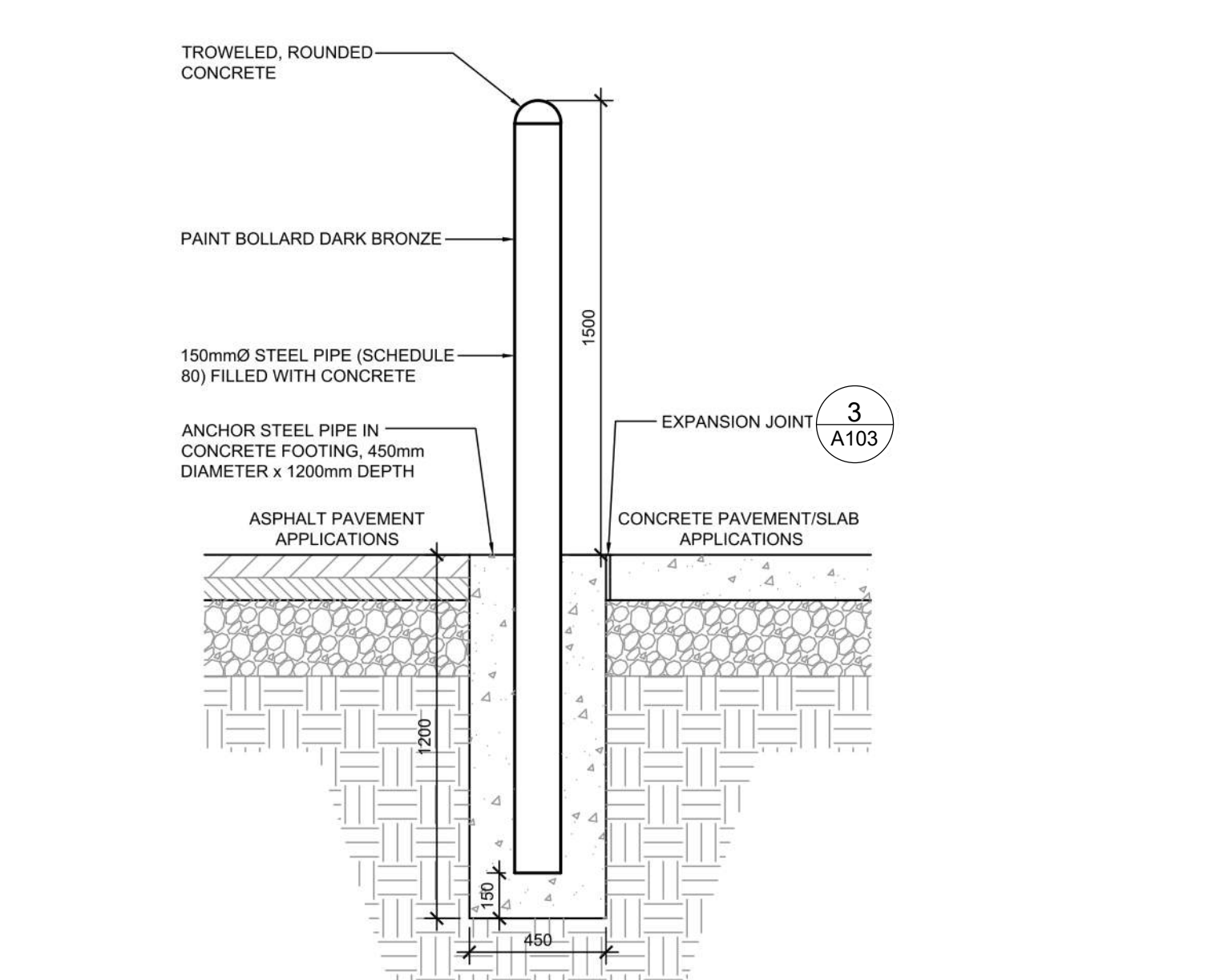


**1 EARTH BIN DETAIL**  
1:50



**4 SIDEWALK AT BUILDING DETAILS**  
N.T.S.



*Allison Hamlin*

**ALLISON HAMLIN**  
MANAGER (A), DEVELOPMENT REVIEW WEST  
PLANNING, REAL ESTATE & ECONOMIC DEVELOPMENT  
DEPARTMENT, CITY OF OTTAWA

**APPROVED**  
By Allison Hamlin at 1:54 pm, Sep 19, 2023

#	DATE	DESCRIPTION	BY
6	2023-07-18	ISSUE FOR SPA SUBMISSION 3	MHB
5	2023-07-13	ISSUE FOR SPA SUBMISSION 2	MHB
4	2023-06-30	ISSUE FOR SPA SUBMISSION 2	MHB
2	2023-03-14	ISSUE FOR SPA SUBMISSION 2	MHB
1	2023-03-07	ISSUE FOR REVIEW	MHB



**PROJECT**  
CHECK-FIL TANGER OUTLETS OTTAWA  
3000 PALLADIUM DRIVE, STITTSVILLE, ON K2S 1B9

**DRAWING**  
**SITE DETAILS**

PROJECT NO. 12.258P01	PROJECT DATE 2023-02-14
DRAWN BY Author	CHECKED BY Checker
SCALE As indicated	

DRAWING NO. <b>A103</b>	REV. <b>6</b>
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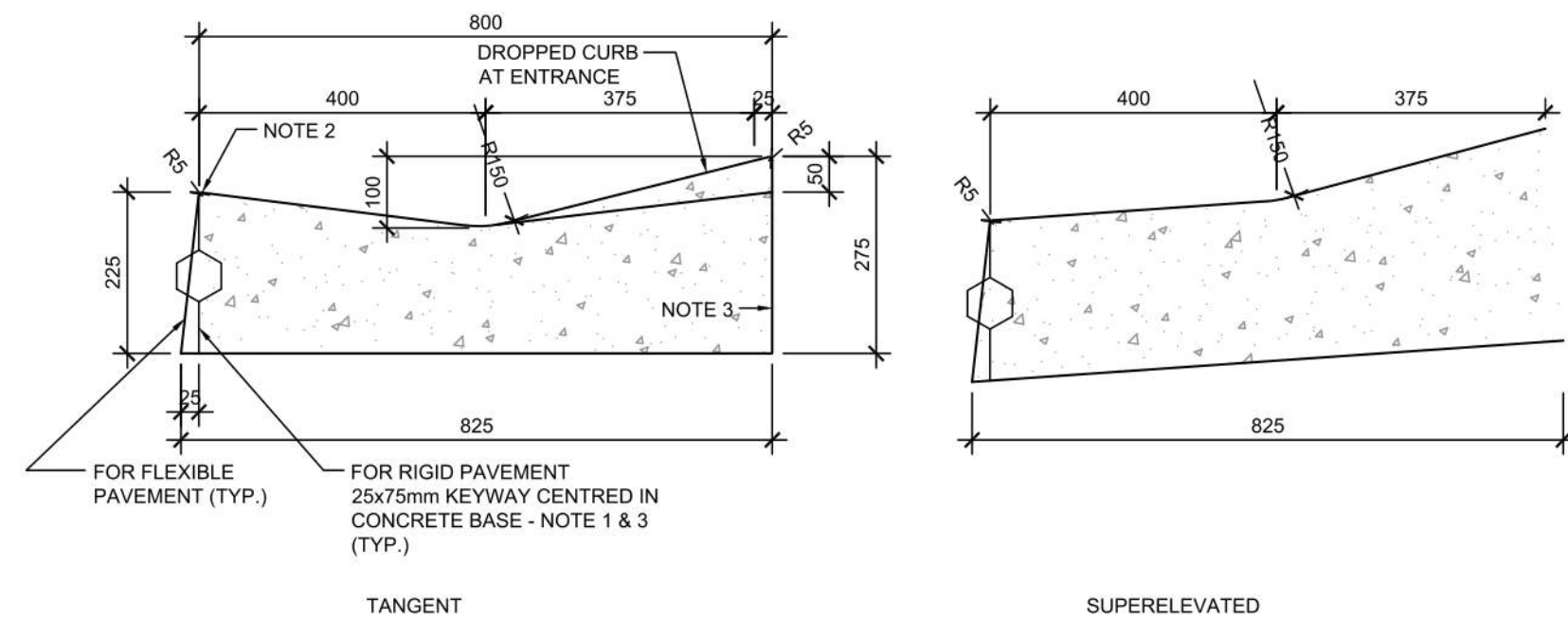


*Allison Hamlin*

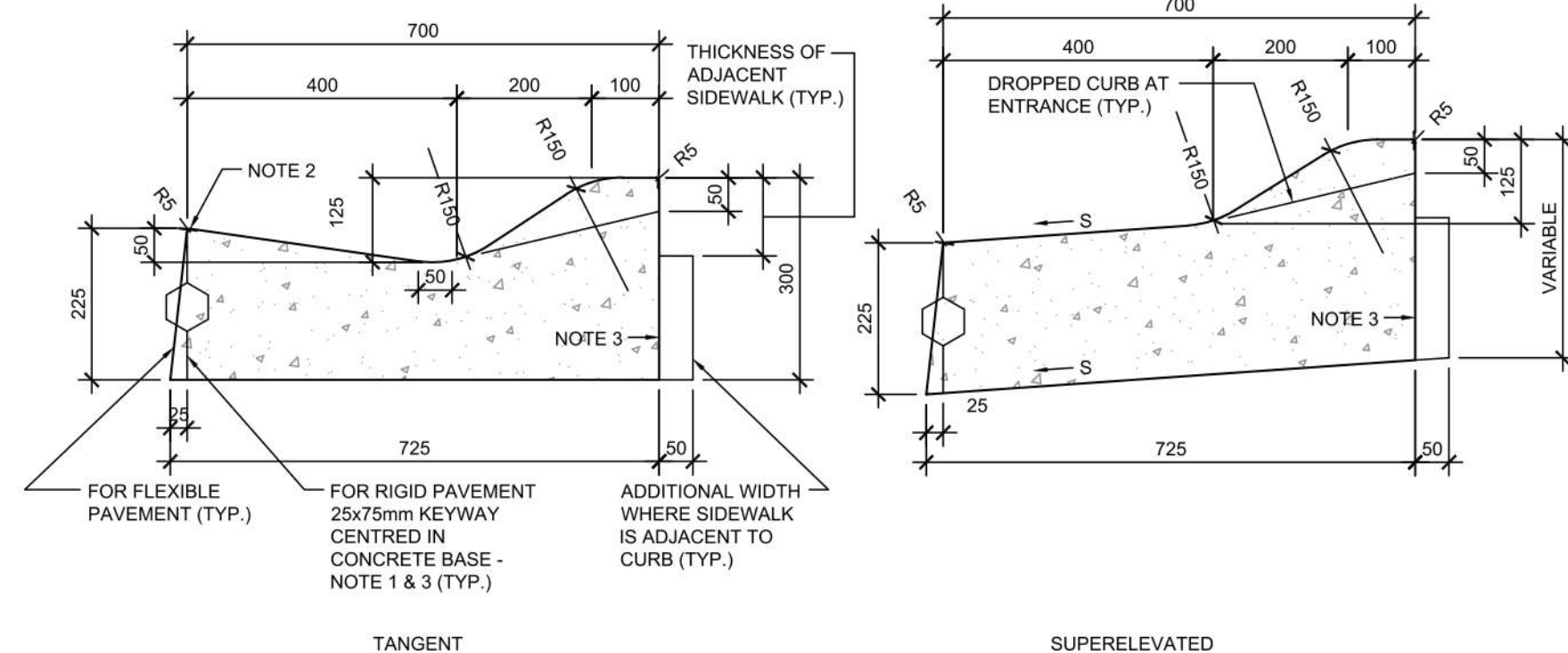
**ALLISON HAMLIN**  
MANAGER (A), DEVELOPMENT REVIEW WEST  
PLANNING, REAL ESTATE & ECONOMIC DEVELOPMENT  
DEPARTMENT, CITY OF OTTAWA

**APPROVED**

By Allison Hamlin at 1:54 pm, Sep 19, 2023



CONCRETE MOUNTABLE CURB WITH WIDE GUTTER (AS PER OPSD 600.030)

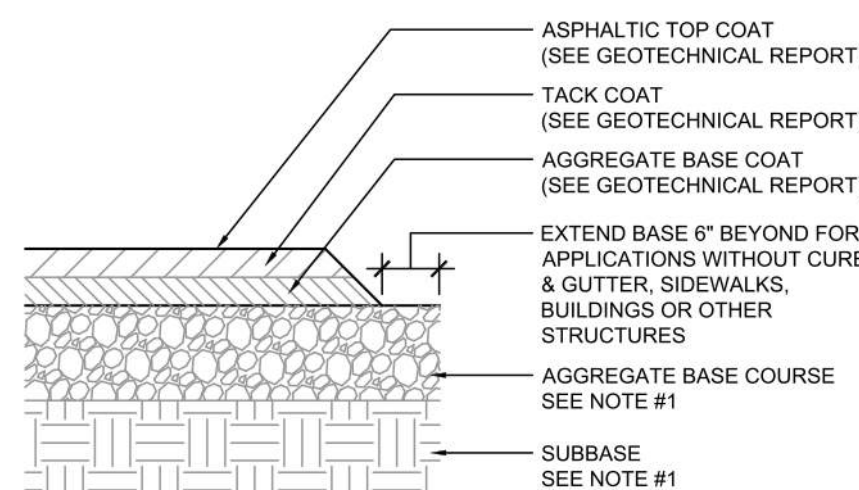


CONCRETE SEMI-MOUNTABLE CURB WITH WIDE GUTTER (AS PER OPSD 600.020)

- NOTES:
- WHEN CURB AND GUTTER IS ADJACENT TO CONCRETE PAVEMENT OR BASE, THIS DRAWING IS TO BE USED IN CONJUNCTION WITH OPSD-552.010 AND 552.020.
  - FLEXIBLE AND COMPOSITE PAVEMENT SHALL BE PLACED 5mm ABOVE THE ADJACENT EDGE OF GUTTER.
  - FOR SLIPFORMING PROCEDURE, A 5% BATTER IS ACCEPTABLE.
  - TREATMENT AT ENTRANCES SHALL CONFORM WITH OPSD-351.010.
  - OUTLET TREATMENT SHALL CONFORM WITH OPSD-610 SERIES.
  - THE TRANSITION FROM ONE CURB TYPE TO ANOTHER SHALL BE A MINIMUM LENGTH OF 3.0m, EXCEPT IN CONJUNCTION WITH GUIDE RAIL WHERE IT SHALL BE ACCORDING TO THE OPSD-900 SERIES.
  - ALL DIMENSIONS ARE IN MILLIMETRES UNLESS OTHERWISE SHOWN.

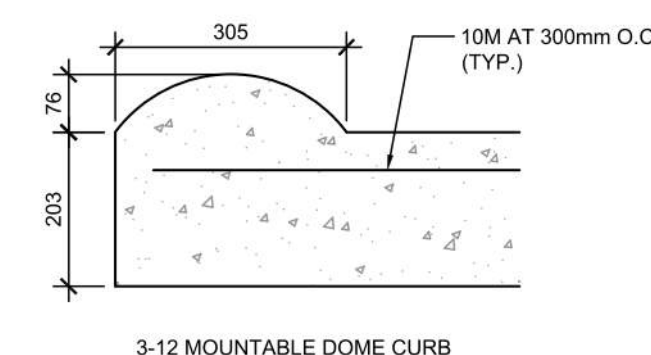
LEGEND:  
S - RATE OF PAVEMENT SUPERELEVATION IN PERCENT, %.

6 CONCRETE MOUNTABLE CURB DETAILS 1  
A104 N.T.S.



IF PRESENT ON-SITE, UNSUITABLE SUBGRADE MATERIAL SHALL BE REPLACED WITH SUITABLE MATERIAL AS SPECIFIED IN THE GEOTECHNICAL REPORT.

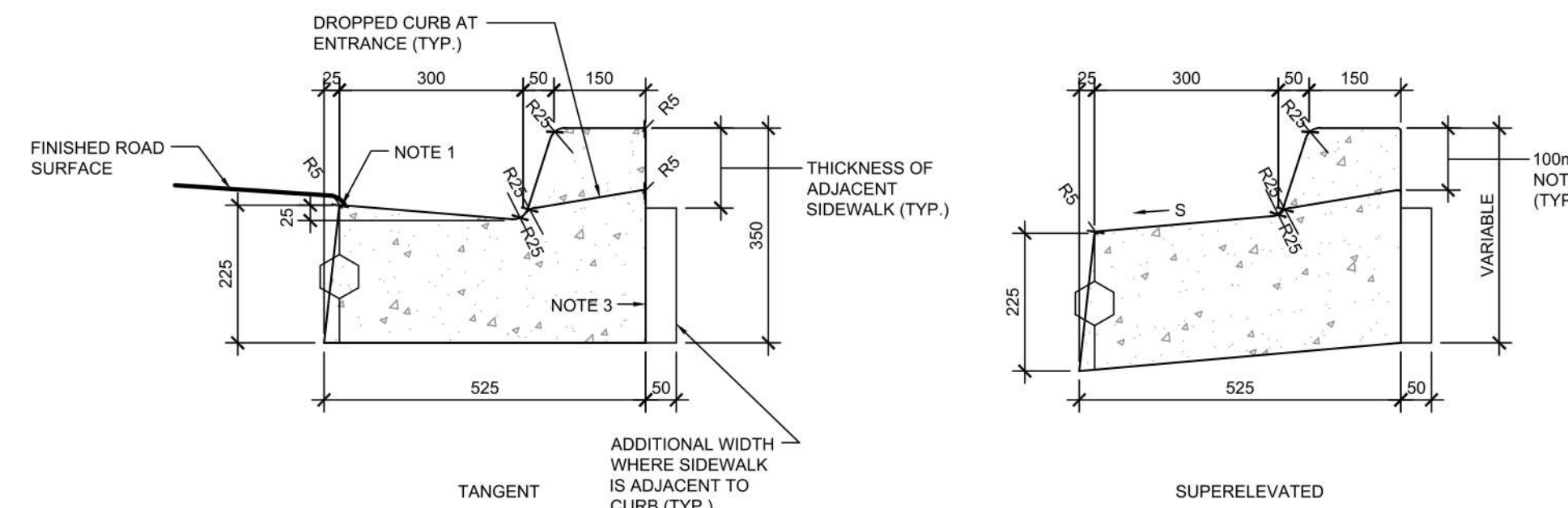
8 TYPICAL ASPHALT SECTION  
A104 N.T.S.



- NOTES:
- PRE-CUT EXPANSION JOINT MATERIAL SHALL BE USED IN ALL EXPANSION JOINTS.
  - DUMMY JOINTS SHALL BE INSTALLED AT 10 ft. INTERVALS, MAXIMUM.
  - INSTALL EXPANSION JOINTS EVERY 40 ft., MAXIMUM, AT ENDS OF RADIUS, AND A MINIMUM OF 5 ft. FROM INLET STRUCTURES.
  - BREAK AS NEEDED TO ENSURE POSITIVE DRAINAGE
  - CONCRETE TO BE 3,000 P.S.I.
  - MOUNTABLE CURB TO BE PAINTED YELLOW WITH 6" BLACK STRIPE PAINTED EVERY 24" O.C..

- NOTES:
- GENERAL CONTRACTOR SHALL REFERENCE GEOTECHNICAL REPORT FOR PAVEMENT SECTION REQUIREMENTS
  - EXPANSION JOINT FILLER SHALL BE FLEXIBLE, LIGHTWEIGHT, NON-STAINING, POLYETHYLENE, CLOSED-CELL EXPANSION JOINT

5 CONCRETE MOUNTABLE CURB DETAILS  
A104 N.T.S.

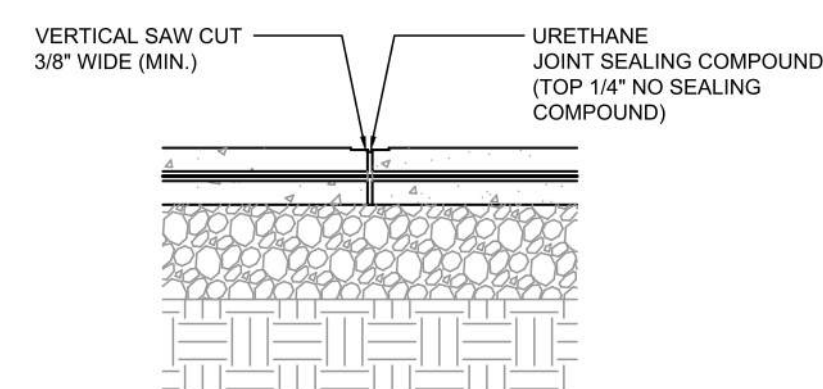


CONCRETE BARRIER CURB WITH STANDARD GUTTER (AS PER OPSD 600.040)

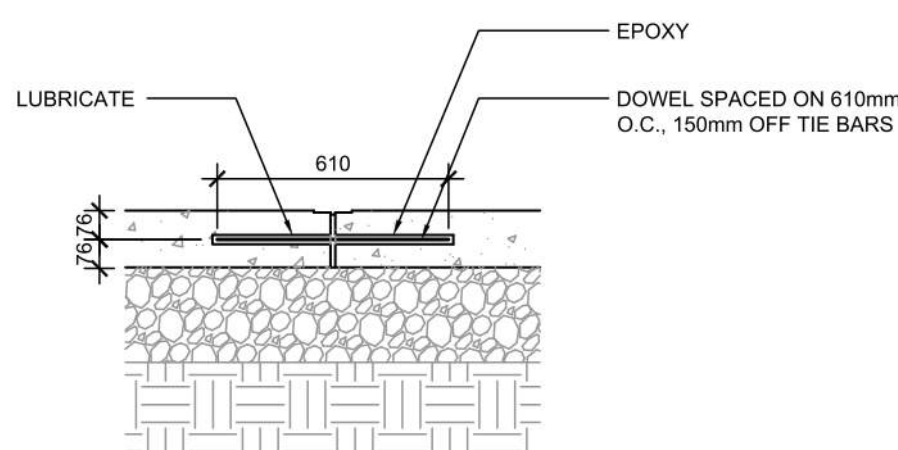
- NOTES:
- FLEXIBLE AND COMPOSITE PAVEMENT SHALL BE PLACED 5mm ABOVE THE ADJACENT EDGE OF GUTTER.
  - WHERE SIDEWALK IS CONTINUOUSLY ADJACENT, REDUCE THE DROPPED CURB AT ENTRANCES TO 75mm.
  - FOR SLIPFORMING PROCEDURE, A 5% BATTER IS ACCEPTABLE.
  - FOR COMPOSITE PAVEMENT THE DEPTH OF CONCRETE CURB SHALL BE ADJUSTED TO DEPTH OF CONCRETE PAVEMENT.
  - WHEN TIE BARS ARE SPECIFIED, REFER TO OPSD 552.010 AND 552.020 FOR DETAILS.
  - TREATMENT AT ENTRANCES SHALL CONFORM WITH OPSD-351.010.
  - OUTLET TREATMENT SHALL CONFORM WITH OPSD-610 SERIES.
  - THE TRANSITION FROM ONE CURB TYPE TO ANOTHER SHALL BE A MINIMUM LENGTH OF 3.0m, EXCEPT IN CONJUNCTION WITH GUIDE RAIL WHERE IT SHALL BE ACCORDING TO THE OPSD-900 SERIES.
  - ALL DIMENSIONS ARE IN MILLIMETRES UNLESS OTHERWISE SHOWN.

LEGEND:  
S - RATE OF PAVEMENT SUPERELEVATION IN PERCENT, %.

1 CONCRETE CURB WITH GUTTER DETAILS  
A104 N.T.S.

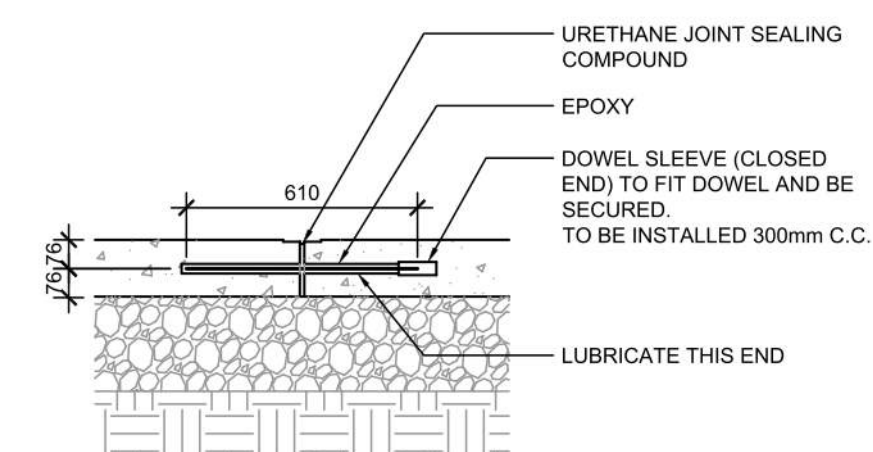


9 CONCRETE PAVING DRIVE-THRU LANE 1  
A104 N.T.S.



- NOTES:
- NO. 5 SMOOTH DOWEL BAR MAY BE USED IN 125mm AND 150mm PAVEMENT THICKNESS.
  - LONGITUDINAL BUTT CONSTRUCTION MAY BE UTILIZED IN PLACE OF LONGITUDINAL HINGED (KEYWAY) JOINT AT CONTRACTORS OPTION.
  - DOWEL BARS SHALL BE DRILLED & EPOXIED INTO PAVEMENT HORIZONTALLY BY USE OF MECHANICAL EQUIP.
  - PUSHING DOWEL BARS INTO WET CONCRETE NOT ACCEPTABLE.

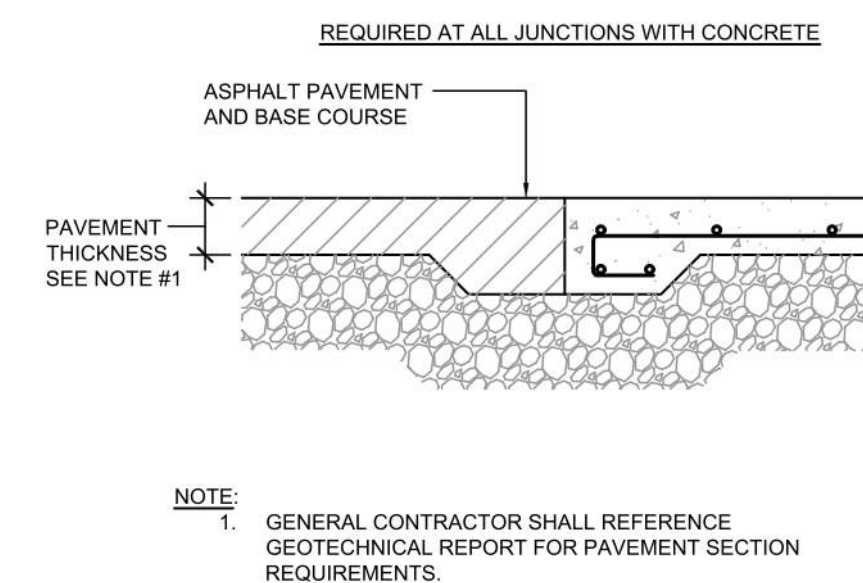
12 LONGITUDINAL BUTT JOINT  
A104 N.T.S.



- NOTES:
- NO. 5 SMOOTH DOWEL BAR MAY BE USED IN 125mm AND 150mm PAVEMENT THICKNESS.
  - LONGITUDINAL BUTT CONSTRUCTION MAY BE UTILIZED IN PLACE OF LONGITUDINAL HINGED (KEYWAY) JOINT AT CONTRACTORS OPTION.
  - DOWEL BARS SHALL BE DRILLED INTO PAVEMENT HORIZONTALLY BY USE OF A MECHANICAL EQUIPMENT
  - DRILLING BY HAND IS NOT ACCEPTABLE, PUSHING DOWEL BARS INTO WET CONCRETE NOT ACCEPTABLE.
  - JOINT SPACING TO BE 24"x24" (EVERY OTHER JOINT)

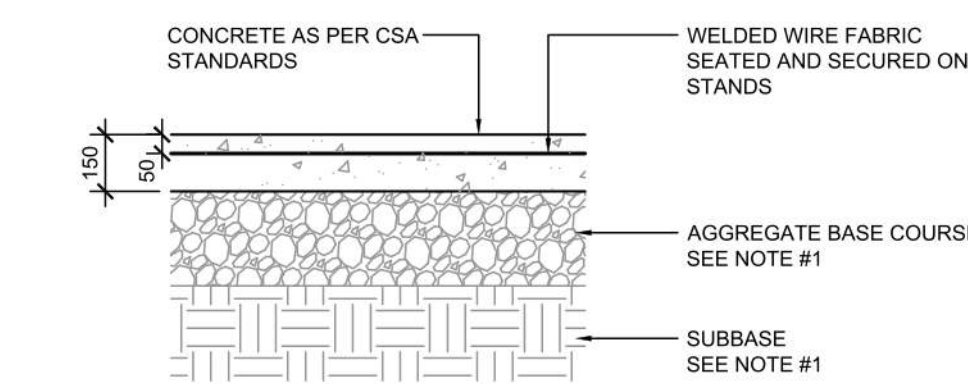
2 EXPANSION JOINT  
A104 N.T.S.

10 PAVEMENT EDGE DETAIL  
A104 N.T.S.



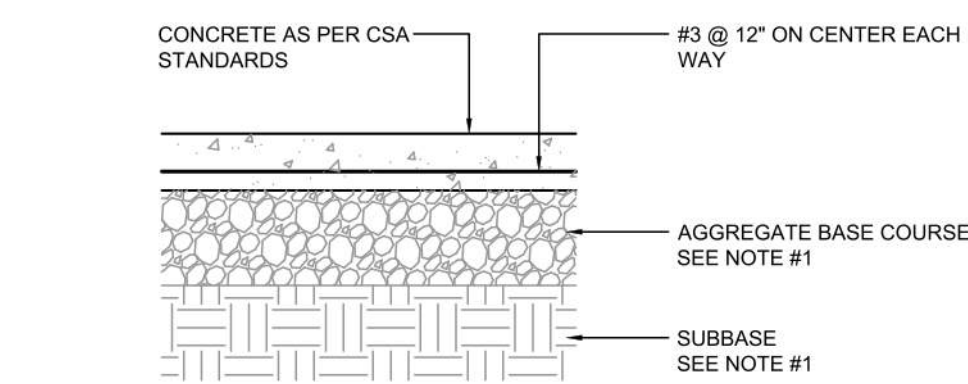
- NOTE:
- GENERAL CONTRACTOR SHALL REFERENCE GEOTECHNICAL REPORT FOR PAVEMENT SECTION REQUIREMENTS.

13 CONCRETE PAVING DRIVE-THRU LANE 2  
A104 N.T.S.

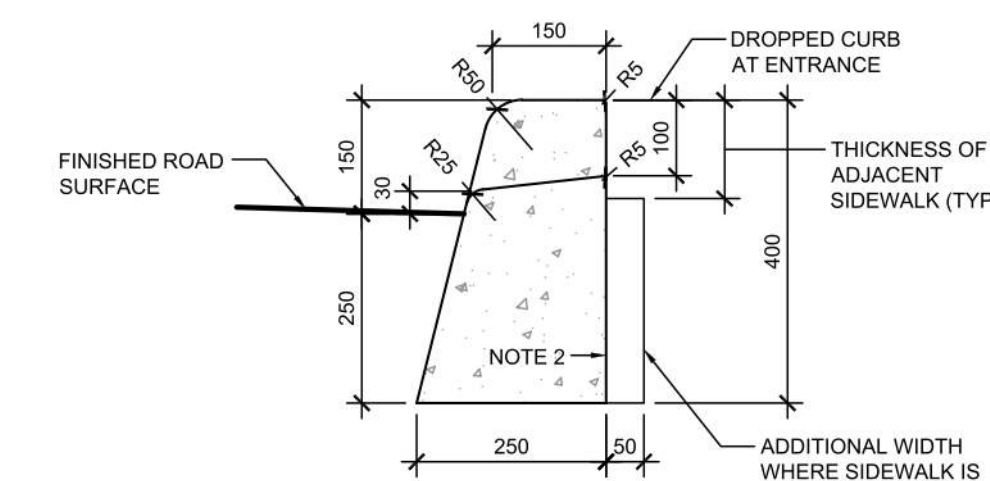


- NOTE:
- GENERAL CONTRACTOR SHALL REFERENCE GEOTECHNICAL REPORT FOR PAVEMENT SECTION REQUIREMENTS.
  - MINIMUM PAVEMENT THICKNESS SHALL BE 6" CONCRETE, 4" AGGREGATE BASE

3 CONCRETE PAVING PARKING LOT  
A104 N.T.S.



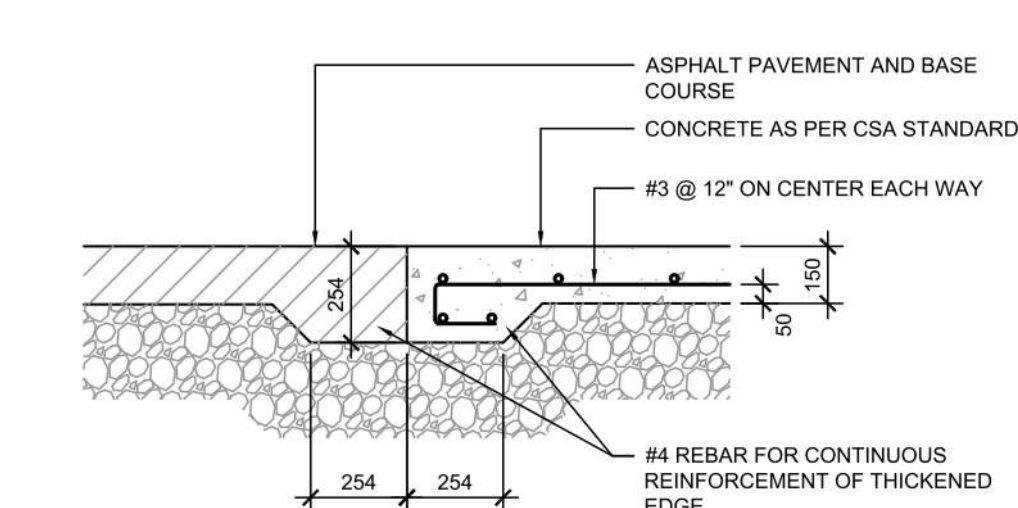
- NOTE:
- GENERAL CONTRACTOR SHALL REFERENCE GEOTECHNICAL REPORT FOR PAVEMENT SECTION REQUIREMENTS.
  - MINIMUM PAVEMENT THICKNESS SHALL BE 6" CONCRETE, 4" AGGREGATE BASE



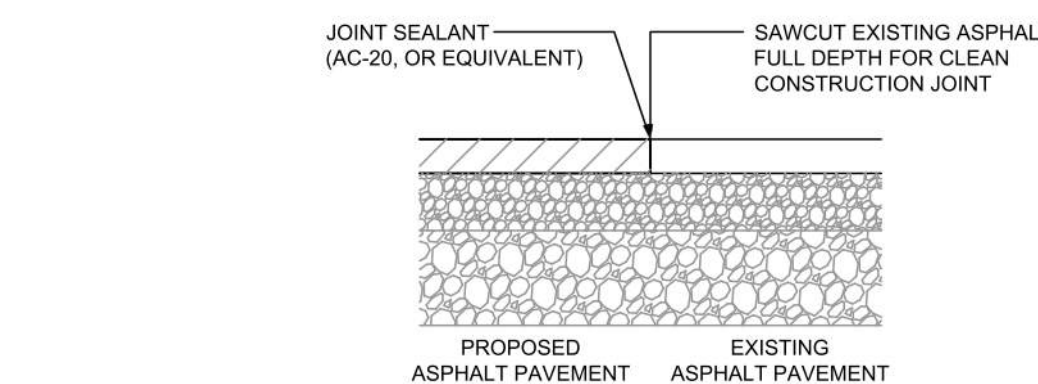
CONCRETE BARRIER CURB (AS PER OPSD 600.110)

- NOTES:
- WHERE SIDEWALK IS CONTINUOUSLY ADJACENT, REDUCE THE DROPPED CURB AT ENTRANCES TO 75mm.
  - FOR SLIPFORMING PROCEDURE, A 5% BATTER IS ACCEPTABLE.
  - TREATMENT AT ENTRANCES SHALL CONFORM WITH OPSD-351.010.
  - OUTLET TREATMENT SHALL CONFORM WITH OPSD-610 SERIES.
  - THE LENGTH OF TRANSITION FROM ONE CURB TYPE TO ANOTHER SHALL BE 3.0m, EXCEPT IN CONJUNCTION WITH GUIDE RAIL, IT SHALL CONFORM TO OPSD-900 SERIES.
  - ALL DIMENSIONS ARE IN MILLIMETRES UNLESS OTHERWISE SHOWN.

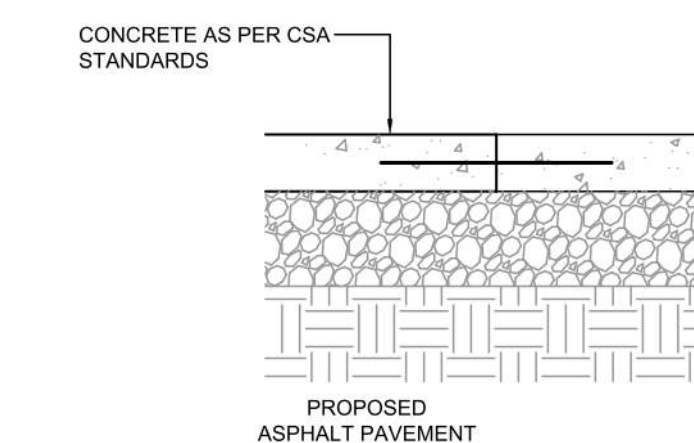
7 CONCRETE CURB DETAILS  
A104 1: N.T.S.



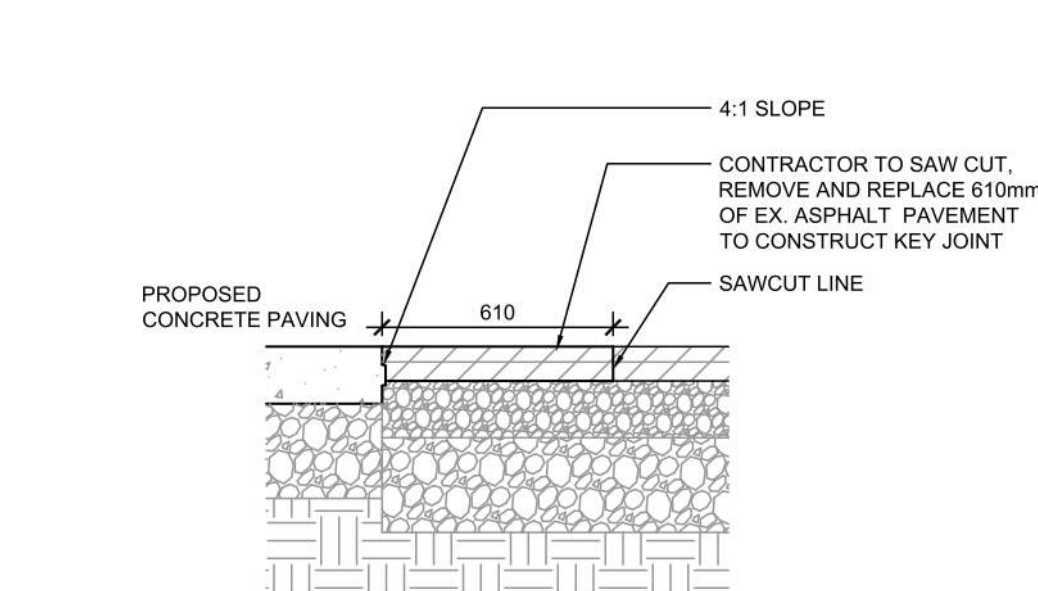
11 CONCRETE APRON AT GARBAGE ENCLOSURE  
A104 N.T.S.



14 BUTT JOINT  
A104 N.T.S.



15 TRANSVERSE & LONGITUDINAL DOWELLED CONSTRUCTION JOINT  
A104 N.T.S.



4 KEYED CONSTRUCTION JOINT  
A104 N.T.S.

#	DATE	ISSUE FOR REVIEW	DESCRIPTION	BY
7	2023-08-03	ISSUE FOR SPA SUBMISSION 3		MHB
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2	2023-08-14	ISSUE FOR SPA SUBMISSION 2		MHB
1	2023-03-07	ISSUE FOR REVIEW		MHB

**RIOCAN**

**PROJECT CHECK-FIL TANGER OUTLETS OTTAWA**

3000 PALLADIUM DRIVE, STITTSVILLE, ON K2S 1B9

DRAWING

**SITE DETAILS**

PROJECT NO. 12.258P01	PROJECT DATE 2023-02-14
DRAWN BY Author	CHECKED BY Checker
SCALE N.T.S.	

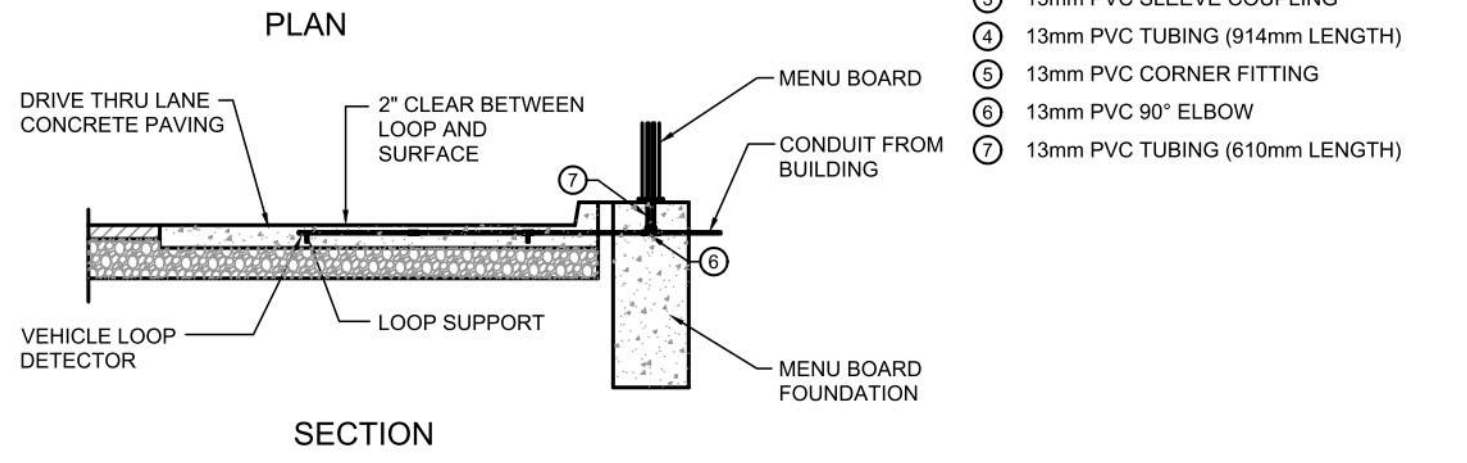
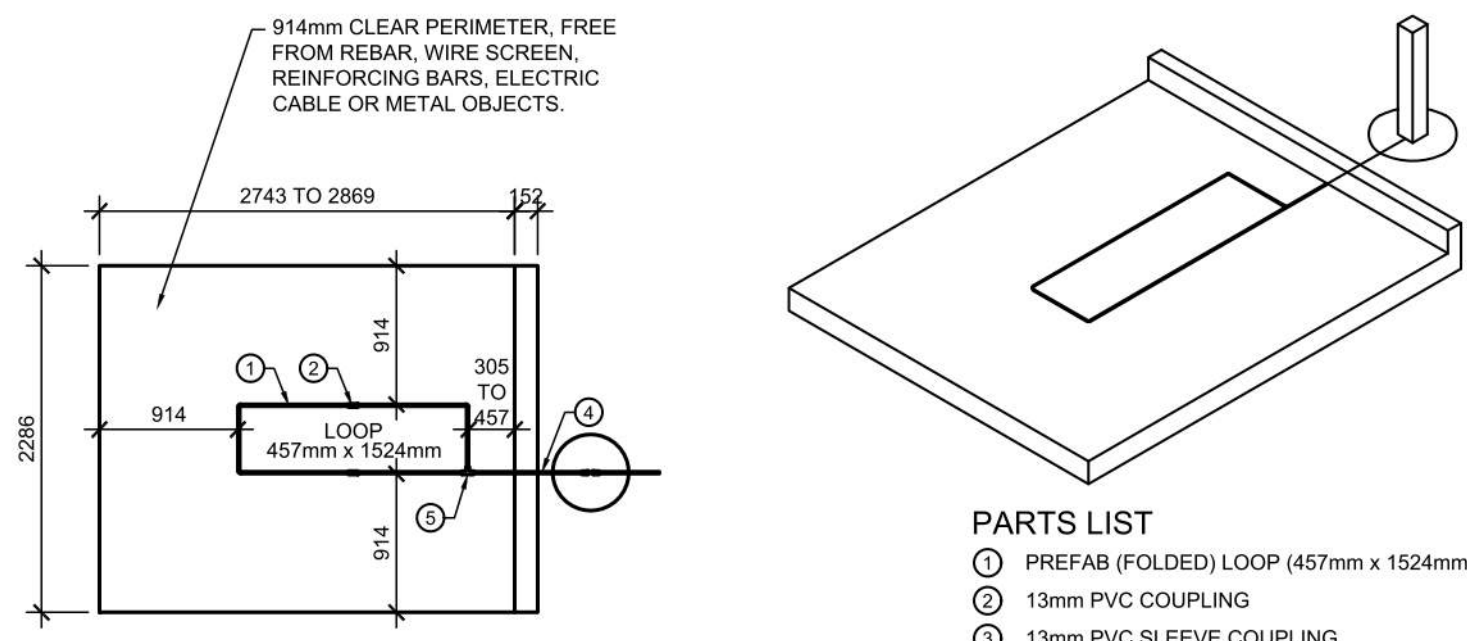
DRAWING NO. **A104** REV **7**



*Allison Hamlin*

**ALLISON HAMLIN**  
MANAGER (A), DEVELOPMENT REVIEW WEST  
PLANNING, REAL ESTATE & ECONOMIC DEVELOPMENT  
DEPARTMENT, CITY OF OTTAWA

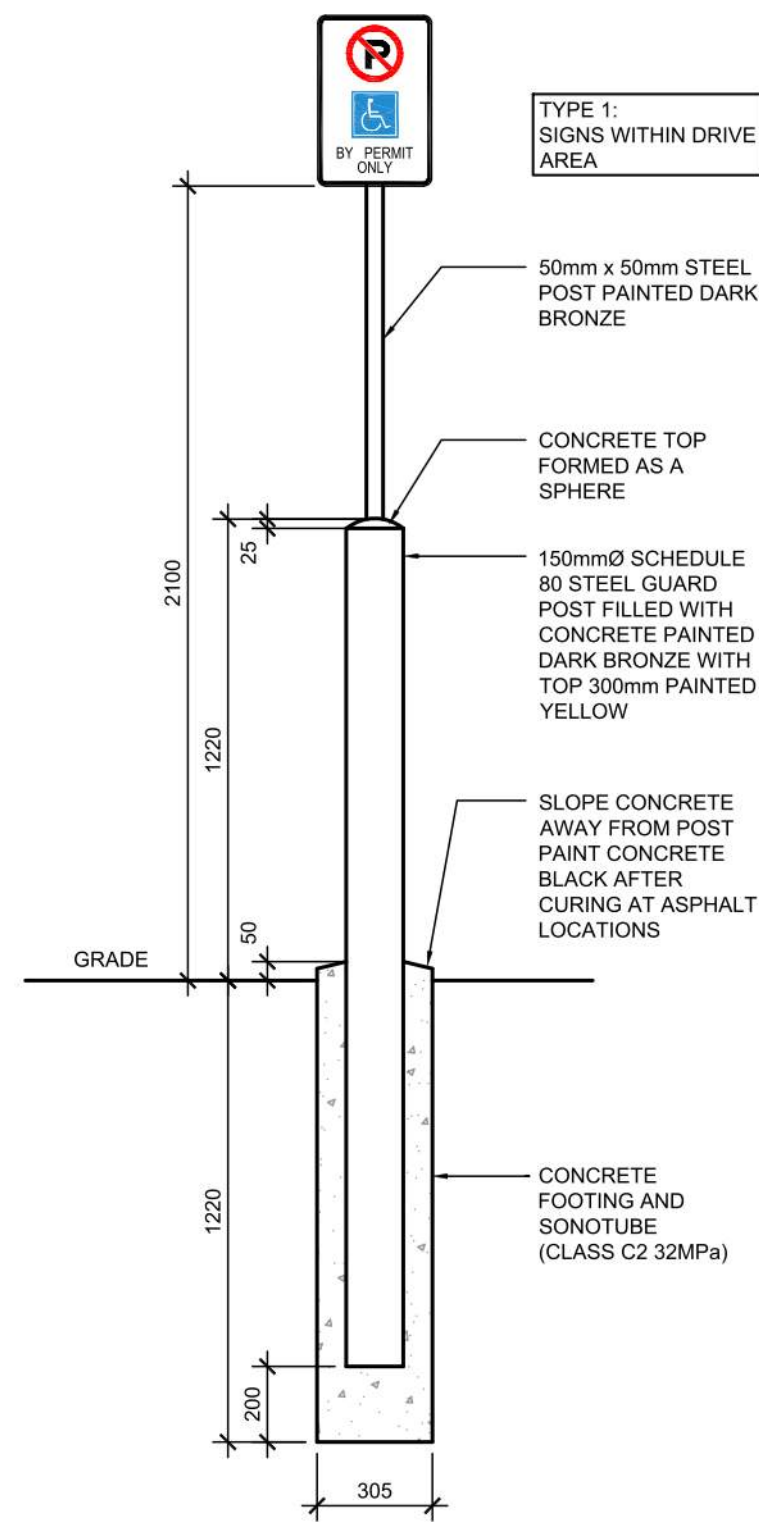
**APPROVED**  
By Allison Hamlin at 1:54 pm, Sep 19, 2023



- PARTS LIST**
- ① PREFAB (FOLDED) LOOP (457mm x 1524mm)
  - ② 13mm PVC COUPLING
  - ③ 13mm PVC SLEEVE COUPLING
  - ④ 13mm PVC TUBING (914mm LENGTH)
  - ⑤ 13mm PVC CORNER FITTING
  - ⑥ 13mm PVC 90° ELBOW
  - ⑦ 13mm PVC TUBING (610mm LENGTH)

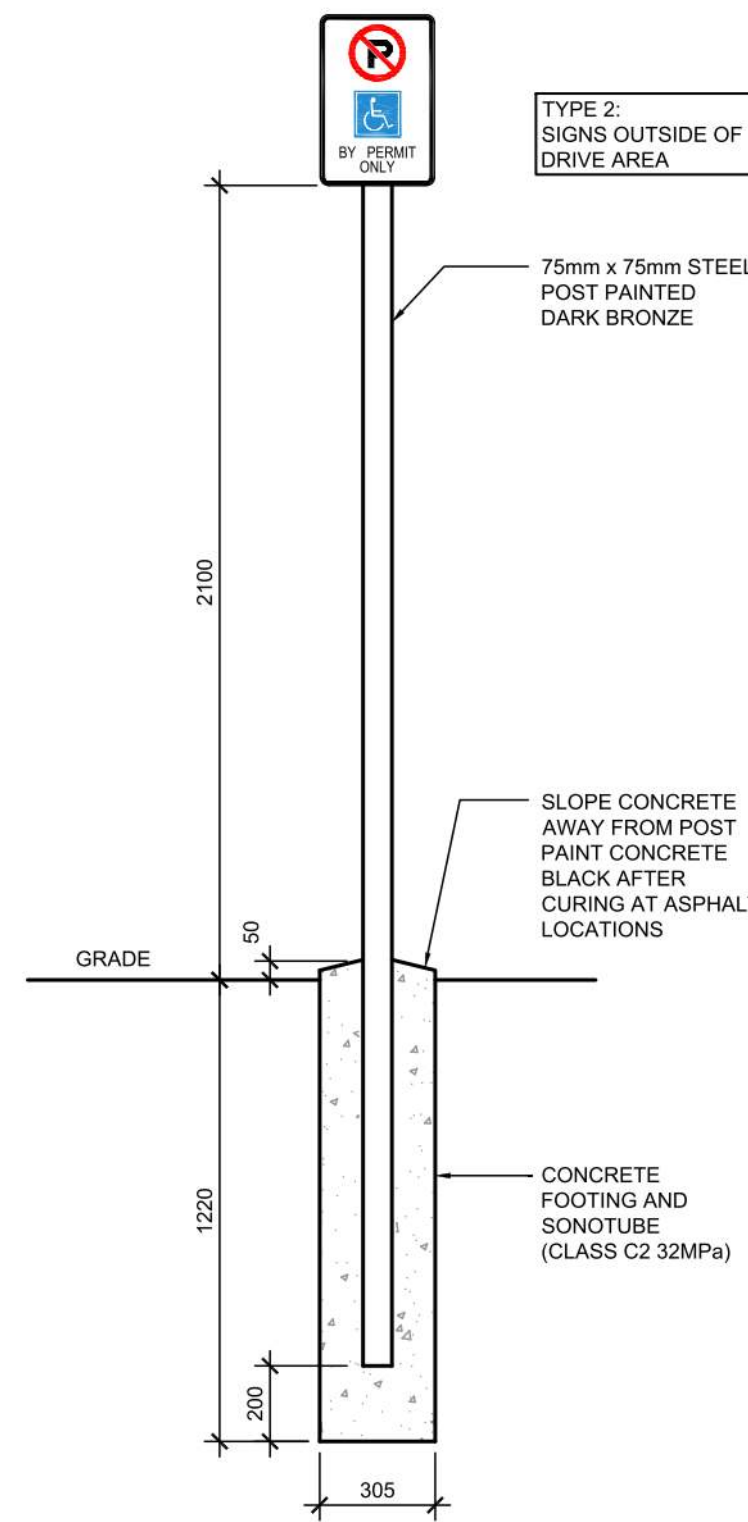
- NOTES:**
1. LOOP DETECTOR IS MODEL NO. VDL100 VEHICLE DETECTION LOOP MANUFACTURED BY MH ELECTRONICS, INC.
  2. FOLLOWING MANUFACTURER'S RECOMMENDATIONS FOR INSTALLATION.

**1 MENU BOARD LOOP SYSTEM**  
A105 N.T.S.



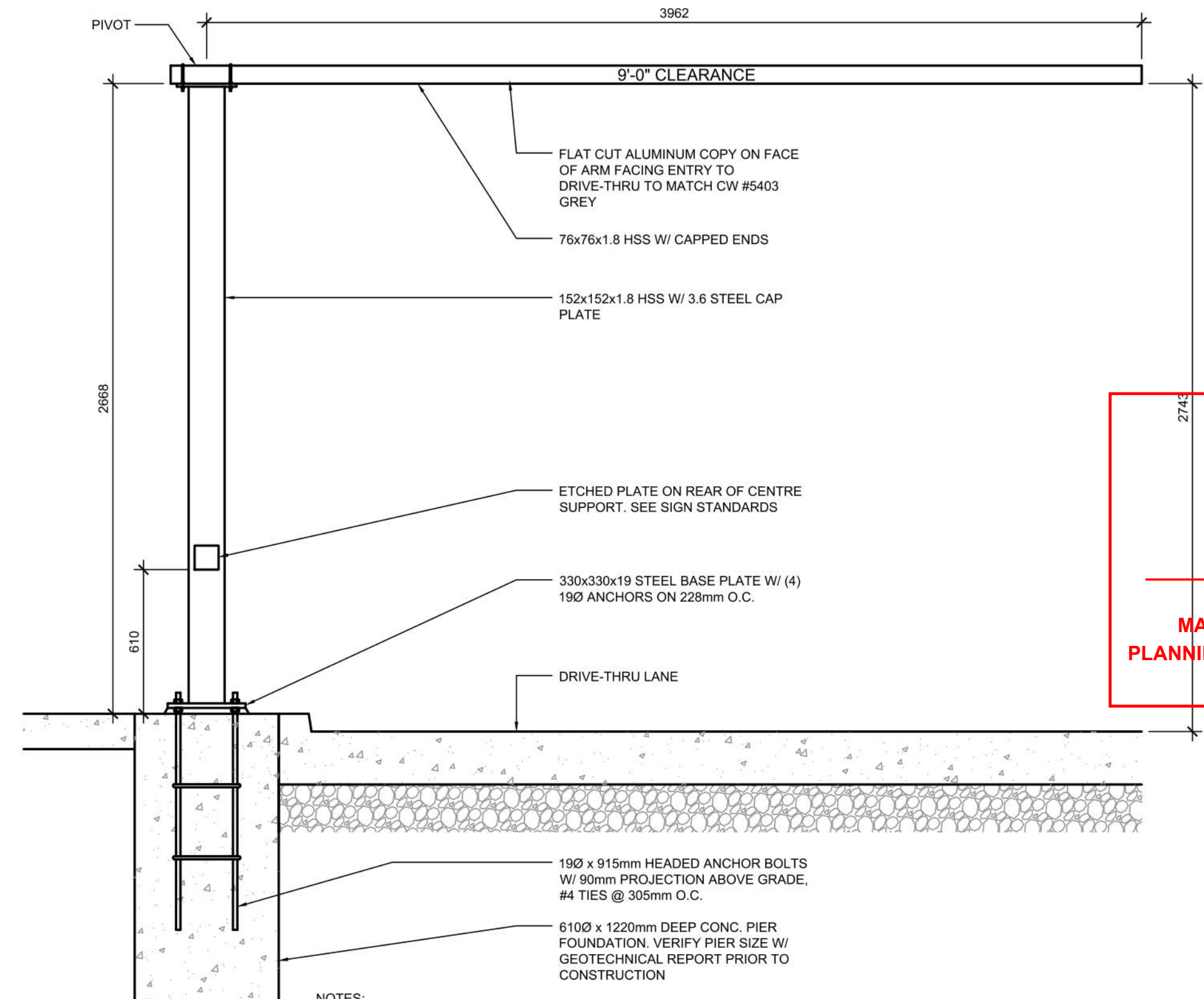
**NOTE:**  
SIGNS PROVIDED AND INSTALLED BY CFA SIGNAGE CONTRACTOR

**3 POLE MOUNTED DIRECTIONAL SIGNAGE 1**  
A105 N.T.S.



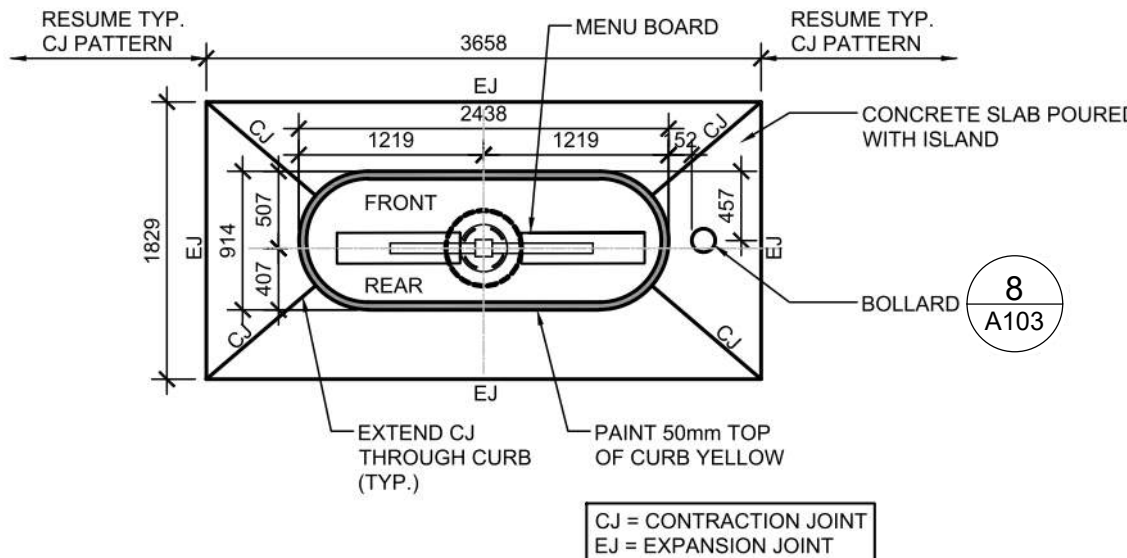
**NOTE:**  
SIGNS PROVIDED AND INSTALLED BY CFA SIGNAGE CONTRACTOR

**7 POLE MOUNTED DIRECTIONAL SIGNAGE 2**  
A105 N.T.S.

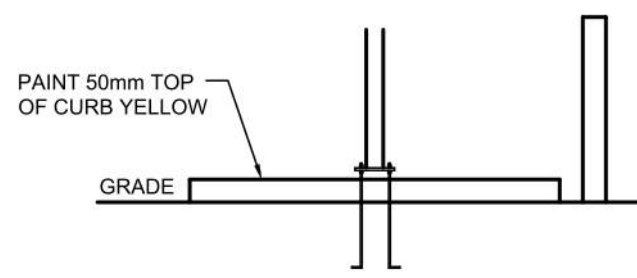


- NOTES:**
1. ENTIRE CLEARANCE BAR & HARDWARE TO BE POWDER COATED QPC P-820 MATTE BLACK FINISH.
  2. CLEARANCE BAR ARM TO ROTATE WHEN STRUCK & RETURN TO ORIGINAL POSITION. COORDINATE W/ ARCHITECT & STRUCTURAL ENGINEER.
  - 3.

**5 DRIVE-THRU CLEARANCE BAR**  
A105 N.T.S.



**ISLAND CURB PLAN**



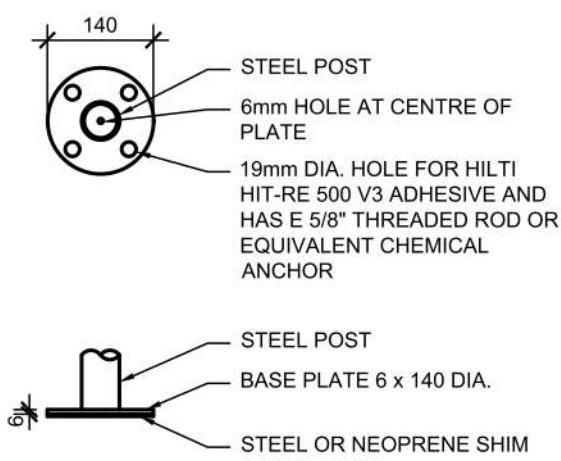
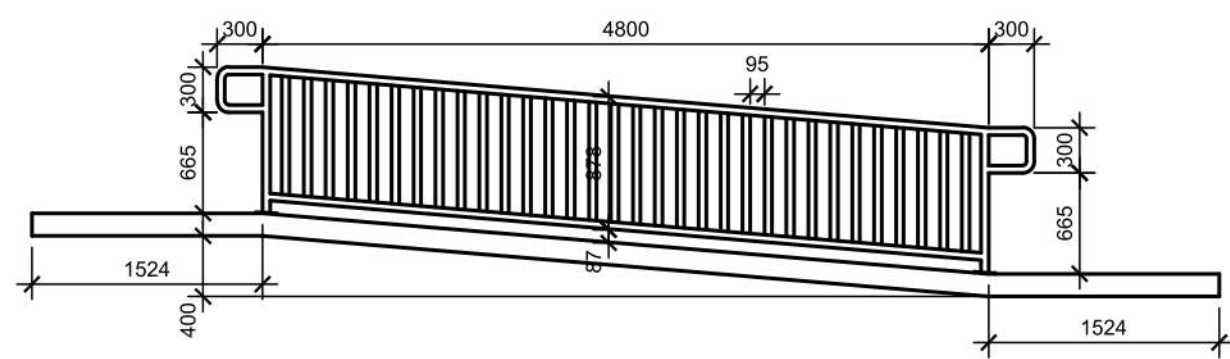
**ISLAND CURB SECTION**

**2 DRIVE-THRU ORDER POINT ISLAND CURB**  
A105 N.T.S.



**NOTE:**  
ALL DIRECTIONAL SIGNAGE TO COMPLY WITH THE ONTARIO TRAFFIC MANUAL AND LOCAL STANDARDS.

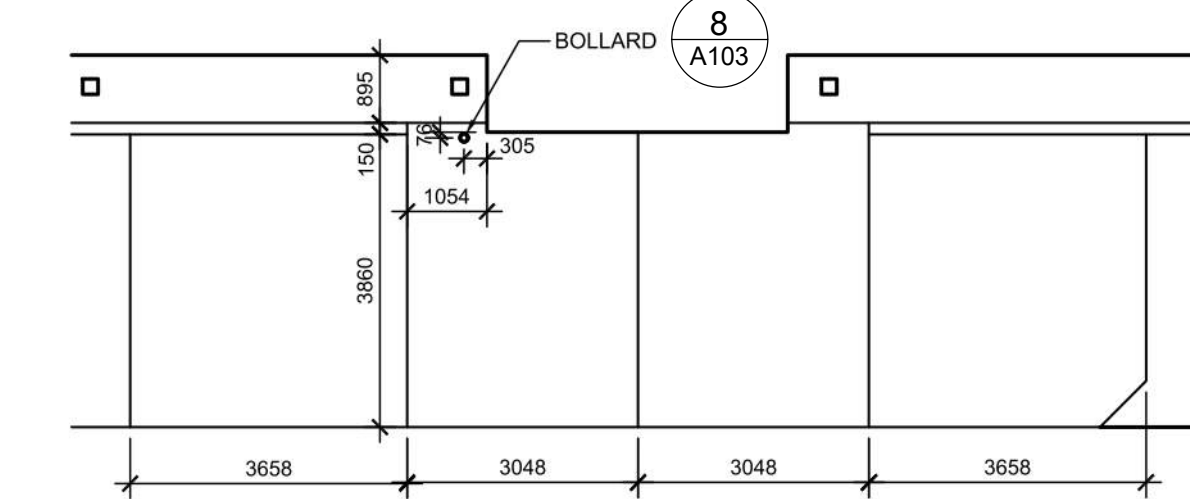
**6 DIRECTIONAL SIGNAGE**  
A105 N.T.S.



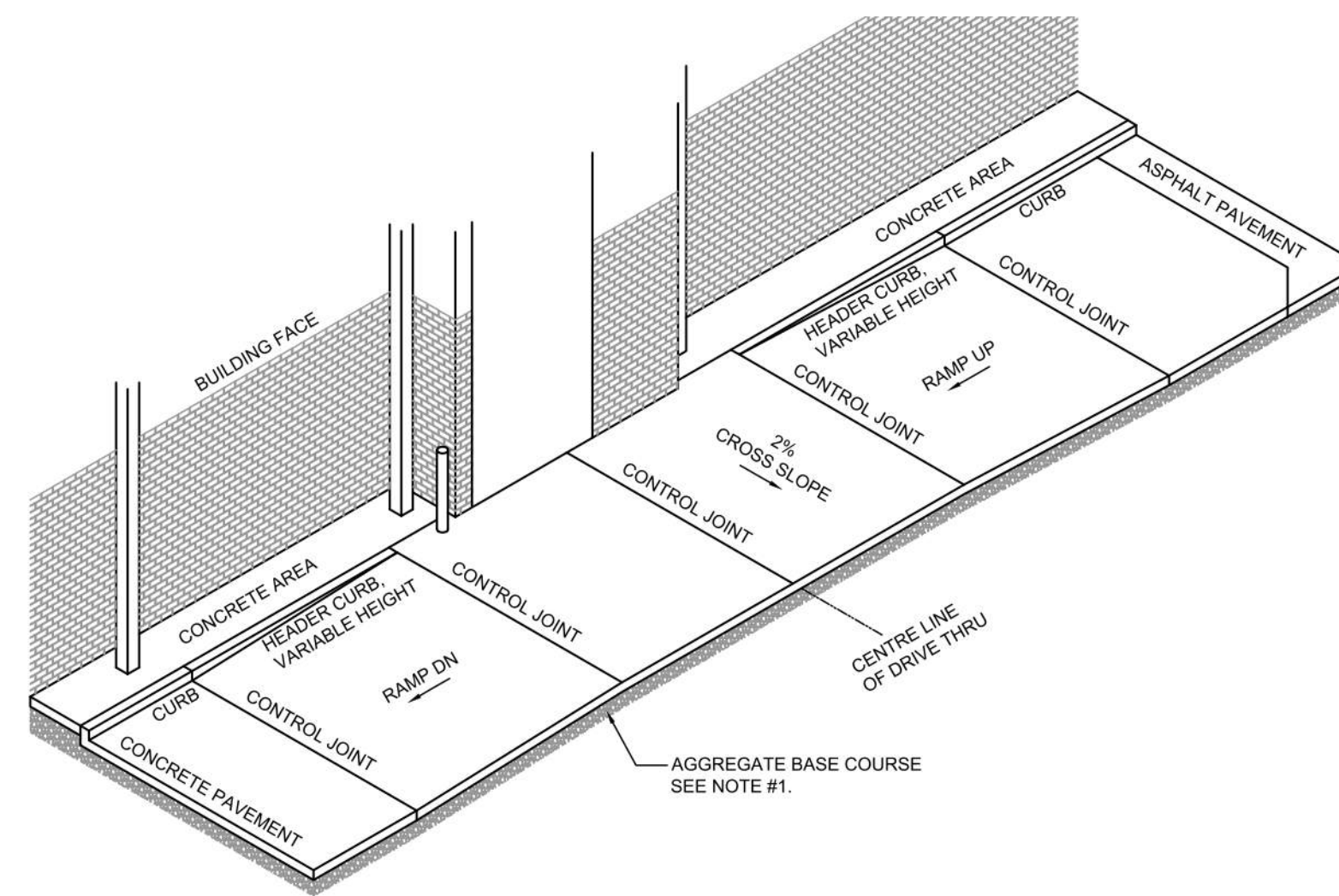
**7 BARRIER FREE RAMP & HANDRAIL DETAIL**  
C9.3 SCALE 1:50

- RAMP** SECTION 405
- \*ANY PART OF AN ACCESSIBLE ROUTE WITH A SLOPE GREATER THAN 1:20 SHALL BE CONSIDERED A RAMP AND SHALL COMPLY WITH 405\*
  - \*\*THE LEAST POSSIBLE SLOPE SHALL BE USED FOR ANY RAMP.\*\*
  - \*\*\*THE MINIMUM CLEAR WIDTH OF A RAMP SHALL BE 36\"
  - MAXIMUM SLOPE FOR A RAMP SHALL BE 1:12. RISE FOR ANY RAMP RUN SHALL 30\"
- LANDINGS** SECTION 405.7
- \*LEVEL LANDINGS WILL BE PROVIDED AT TOP AND BOTTOM OF EACH RAMP. LANDING LENGTH SHALL BE MIN. 60\"
  - \*\*IF RAMP CHANGES DIRECTION LANDING WILL BE MIN. 60\"
  - \*\*\*IF A DOORWAY IS LOCATED AT A LANDING, THEN THE AREA IN FRONT OR THE DOORWAY SHALL COMPLY WITH 404.2.4 AND 404.3.2.
- HANDRAILS** SECTION 505
- \*\*IF A RAMP HAS A SLOPE GREATER THAN 5% AND A RISE GREATER THAN 6\"
  - SHALL HAVE HANDRAILS ON BOTH SIDES\*
  - \*HANDRAIL WILL BE CONSTRUCTED OF WELDED STEEL PIPE - GRIND SMOOTH AND PAINT TO MATCH DARK BRONZE.
- CROSS SLOPE** SECTION 405.3
- \*THE CROSS SLOPE OF RAMP SURFACES SHALL BE NO GREATER THAN 1:50.
  - \*\*NOWHERE SHALL THE CROSS SLOPE OF AN ACCESSIBLE ROUTE EXCEED 1:50.

**4 BARRIER FREE RAMP & HANDRAIL DETAIL**  
A105 N.T.S.



**8 DRIVE-THRU DETAILS**  
A105 N.T.S.



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7	2023-08-03	ISSUE FOR SPA SUBMISSION 3	MHB
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**PROJECT**  
CHECK-FIL TANGER OUTLETS OTTAWA

3000 PALLADIUM DRIVE, STITTSVILLE, ON K2S 1B9

**SITE DETAILS**

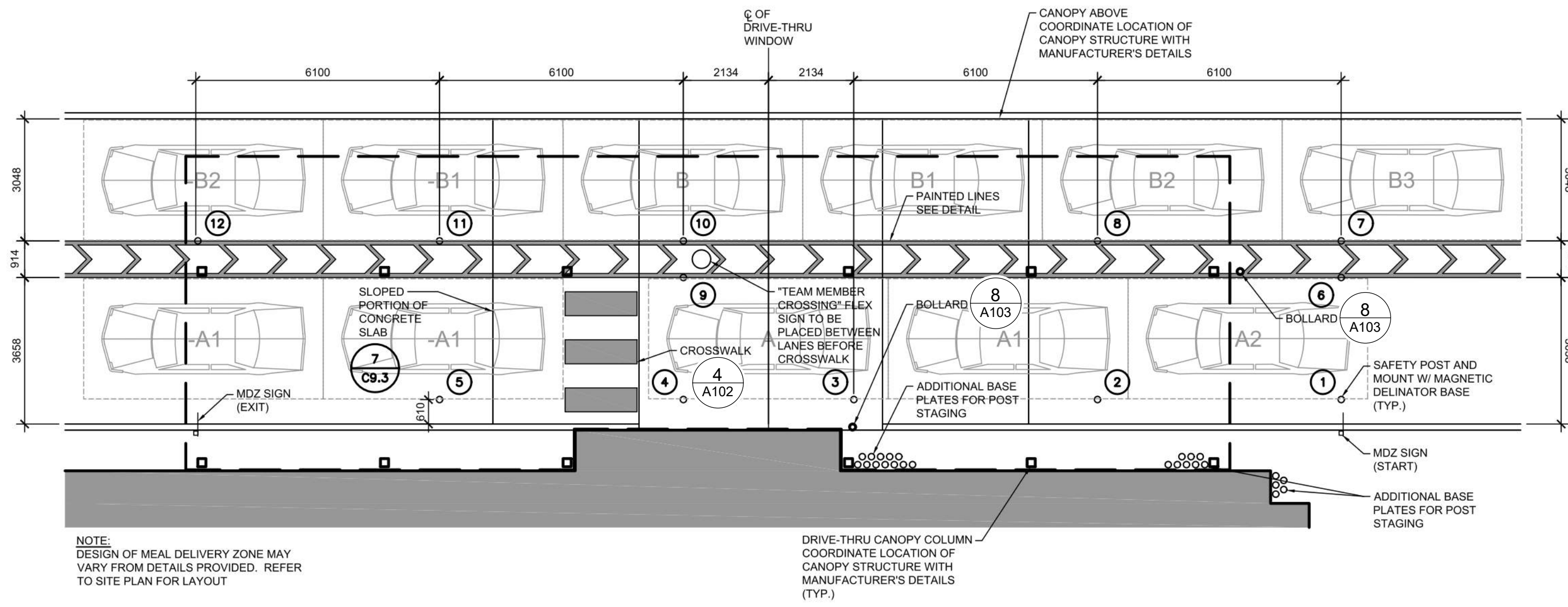
PROJECT NO.	PROJECT DATE
12-258P01	2023-02-14
<b>DRAWN BY</b>	<b>Autor</b>
<b>CHECKED BY</b>	<b>Checker</b>
<b>SCALE</b>	N.T.S.

**A105** **7**

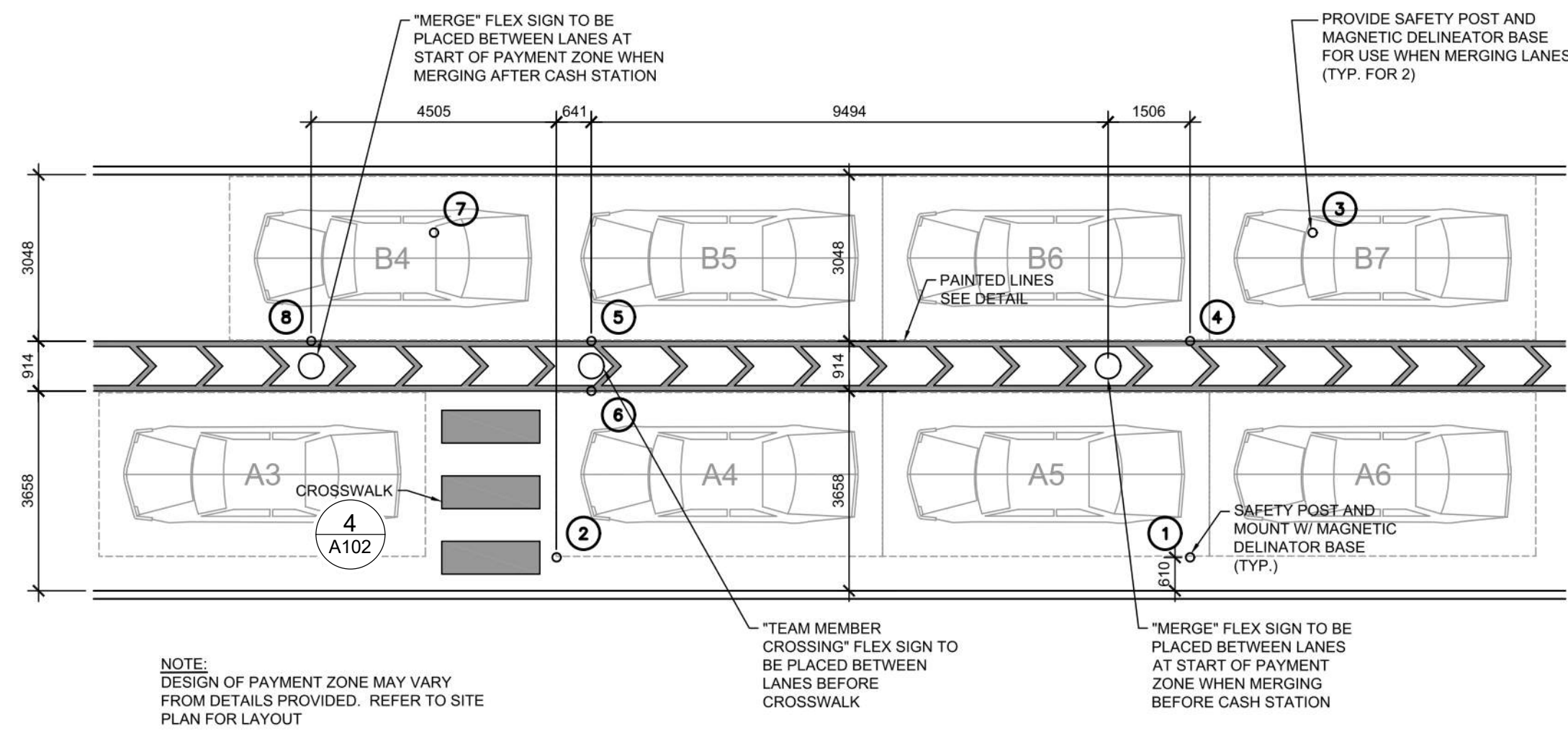


*Allison Hamlin*  
**ALLISON HAMLIN**  
MANAGER (A), DEVELOPMENT REVIEW WEST  
PLANNING, REAL ESTATE & ECONOMIC DEVELOPMENT  
DEPARTMENT, CITY OF OTTAWA

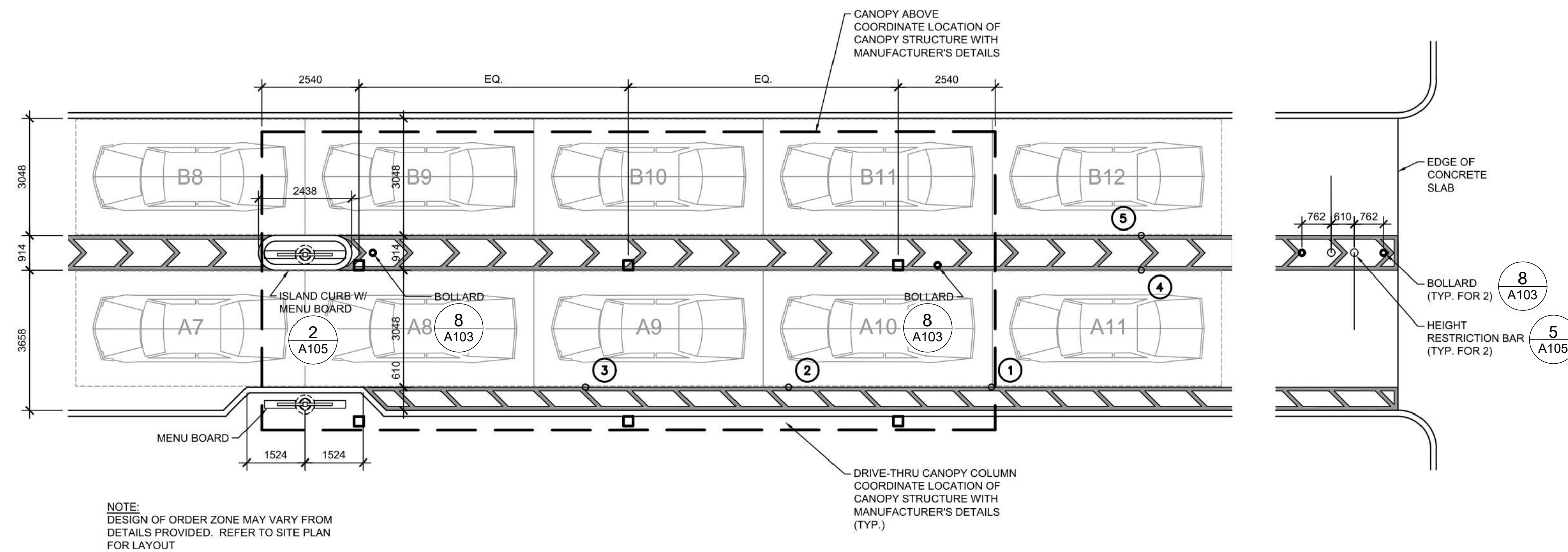
**APPROVED**  
By Allison Hamlin at 1:54 pm, Sep 19, 2023



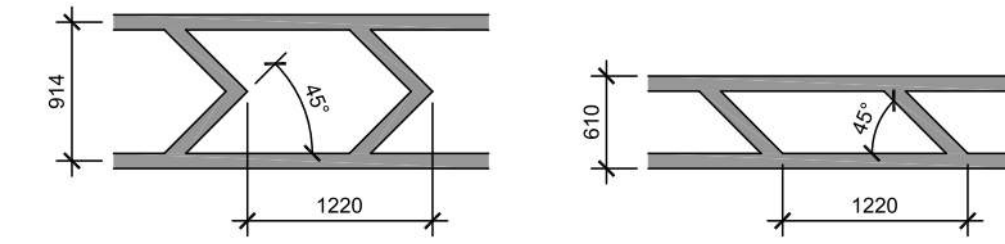
**4 MEAL DELIVERY ZONE - DUAL LANE CANOPY LAYOUT**  
A106 N.T.S.



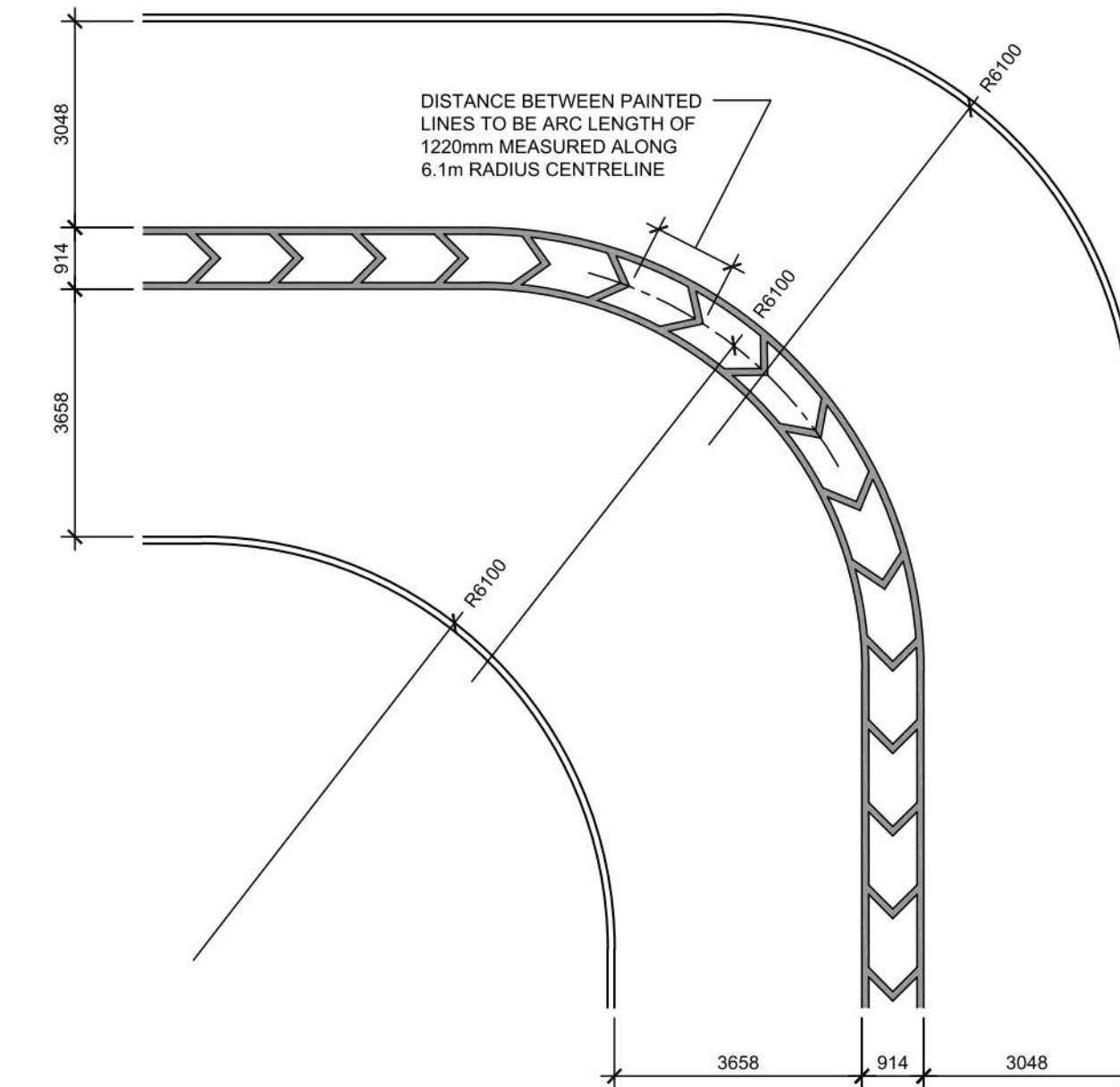
**1 PAYMENT ZONE LAYOUT**  
A106 N.T.S.



**2 ORDER ZONE LAYOUT**  
A106 N.T.S.

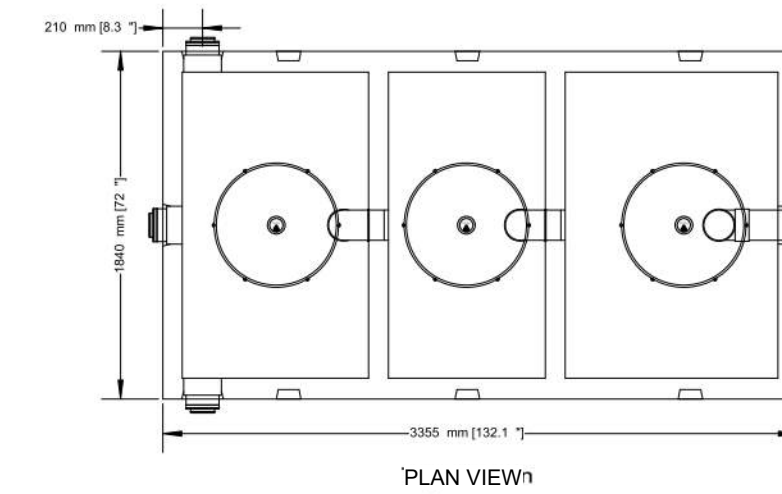


**3 DRIVE-THRU PAINTED LINES**  
A106 N.T.S.



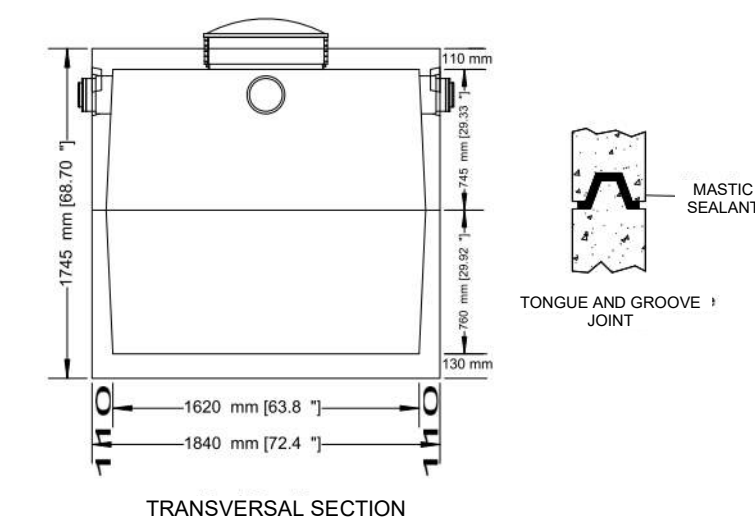
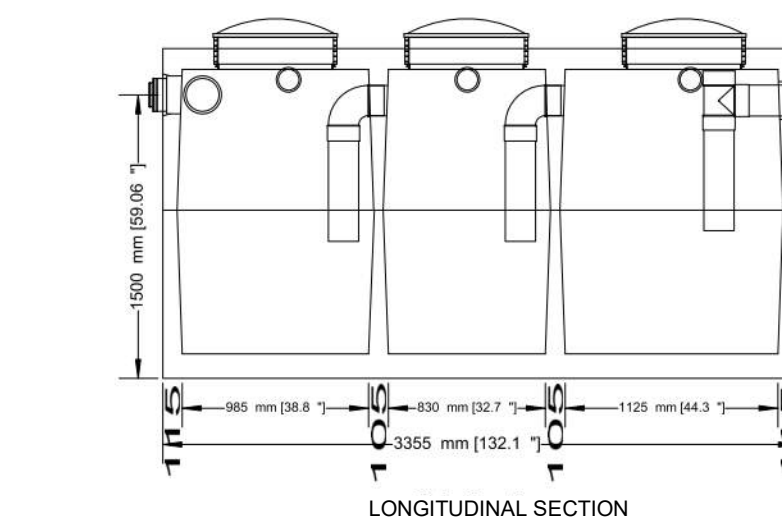
**5 DRIVE-THRU BEND DESIGN**  
A106 N.T.S.

**6000 LITRES (1319 GAL) OIL AND GREASE INTERCEPTOR  
2 PIECE 3 COMPARTMENTS**



- PRECAST INTERCEPTOR IS DELIVERED AS A ONE PIECE UNIT
  - INDIVIDUAL ACCESS LIDS TO EACH COMPARTMENT FOR CLEANOUT
  - THREE COMPARTMENTS SEPARATE WATER FROM ANY INSOLUBLE LIQUID OF SOLIDS HAVING A SPECIFIC GRAVITY LESS THAN THAT OF WATER
  - UNIT MUST BE VENTED IN REGULATION WITH PLUMBING CODES
  - TANKS ARE CAST WITH 35 MPA STRENGTH CONCRETE AT 7 DAYS WITH 5-7% AIR
  - STEEL REINFORCED WITH 10M REBAR, SPACED AT 400MM IN EACH DIRECTION
  - REBAR COVER IS 25MM
  - INTERCEPTORS ARE USED TO PREVENT OIL WASTE FROM ENTERING THE SANITARY SEWER SYSTEM
- TOP HALF WEIGHT.....3157KG (6960LBS)  
BOTTOM HALF WEIGHT.....2784KG (6160LB)  
TOTAL TANK WEIGHT.....5951KG (13120LB)

- NOTES**
1. 150MM FLOW THROUGH PIPES EXTENDING 600MM DOWN
  2. STANDARD THREE 150MM INLETS AS WELL AS ONE OUTLET BAFFLE CONSISTING OF 150MM TEE AND 150 MM PIPE (710MM IN LENGTH) THAT IS READILY ADAPTABLE FOR FILTER
  3. CAST IN PLACE 610MM ID PLASTIC RISER WITH PLASTIC LID SECURED WITH STAINLESS STEEL FASTENERS EXTENDING 100MM ABOVE TOP
- DESIGNED FOR UP TO ONE METRE BURIAL OVER TOP OF TANK -VEHICLE TRAFFIC AND UP TO TWO METRE BURIAL TANKS ARE AVAILABLE UPON REQUEST.
- WWW.NEWMARKETPRECAST.COM | 905-852-6111 | 1-800-283-1297



**6 OIL & GREASE INTERCEPTOR DETAIL**  
A106 N.T.S.

#	DATE	DESCRIPTION	BY
7	2023-08-03	ISSUE FOR SPA SUBMISSION 3	MHB
6	2023-07-18	ISSUE FOR SPA SUBMISSION 3	MHB
5	2023-07-13	ISSUE FOR SPA SUBMISSION 2	MHB
4	2023-06-30	ISSUE FOR SPA SUBMISSION 2	MHB
3	2023-06-14	ISSUE FOR SPA SUBMISSION 2	MHB
2	2023-03-07	ISSUE FOR REVIEW	MHB

**RIO CAN**

**CHICK-FIL TANGER OUTLETS OTTAWA**

3000 PALLADIUM DRIVE, STITTSVILLE, ON K2S 1B9

**SITE DETAILS**

PROJECT NO. 12-258P01	PROJECT DATE 2023-02-14
DRAWN BY Author	CHECKED BY Checker
SCALE N.T.S.	

A106 7



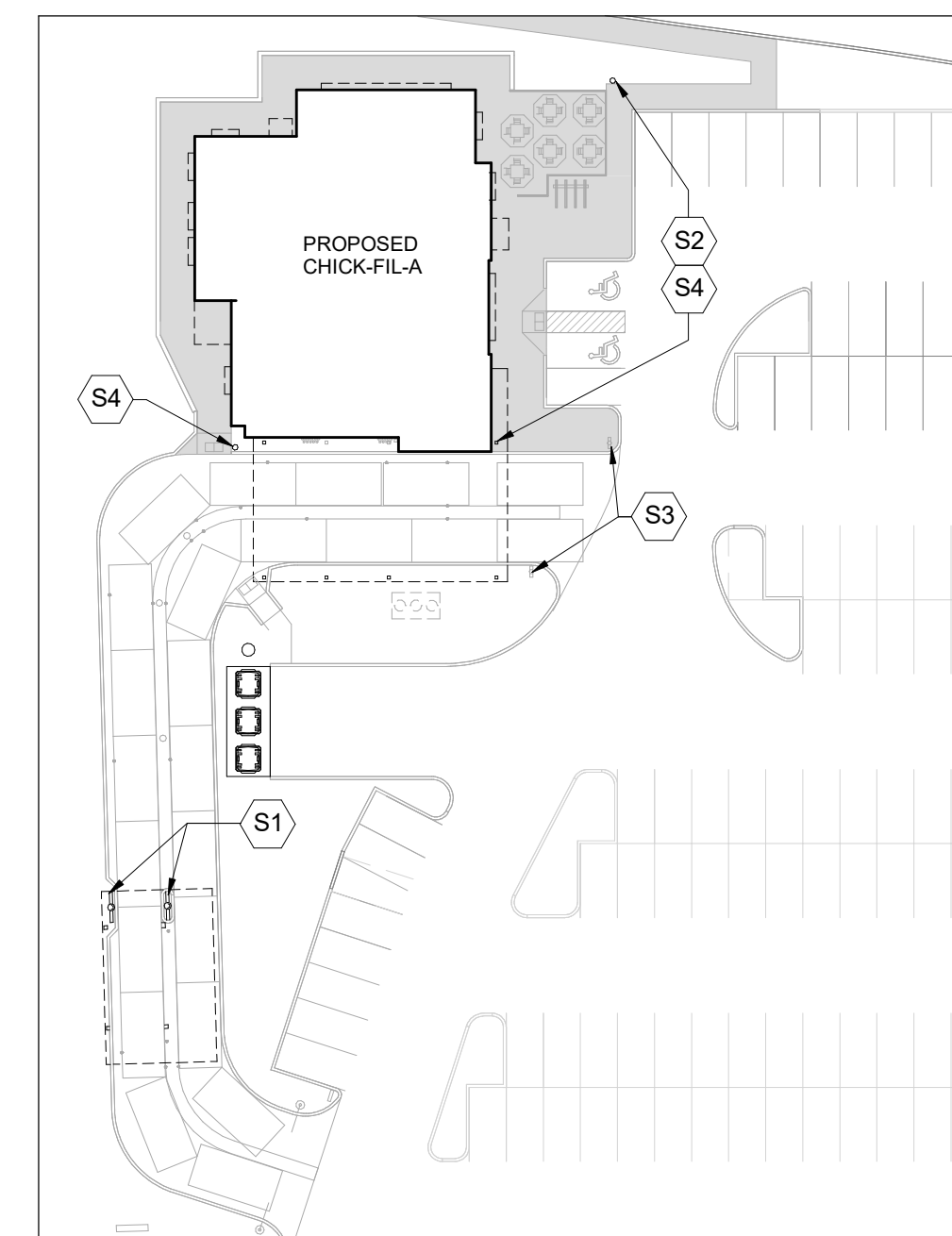
*Allison Hamlin*

**ALLISON HAMLIN**  
MANAGER (A), DEVELOPMENT REVIEW WEST  
PLANNING, REAL ESTATE & ECONOMIC DEVELOPMENT  
DEPARTMENT, CITY OF OTTAWA

**APPROVED**  
By Allison Hamlin at 1:55 pm, Sep 19, 2023

### NOTES

1. TAKE ALL FOUNDATIONS DOWN TO ORIGINAL UNDISTURBED CLAYEY SILT OR SILT SOIL OR ENGINEERED FILL CAPABLE OF SAFELY SUSTAINING A BEARING PRESSURE OF 150 kPa (3.5 lb/ft<sup>2</sup>) & 275 kPa (6.0 lb/ft<sup>2</sup>). ENSURE THAT THE MINIMUM FROST DEPTH AS SPECIFIED IN THE SOIL REPORT IS ATTAINED FOR ALL EXTERIOR AND BUILDING PERIMETER FOUNDATIONS, AND ALL INTERIOR FOUNDATIONS IN UNHEATED AREAS. SEE REPORT No. P06671-11 PREPARED BY PATERSON GROUP, DATED MARCH 5, 2023.
2. REFER TO SITE SERVICES PLAN, ELECTRICAL, MECHANICAL DRAWINGS FOR ALL INCOMING SERVICES.
3. ALL FOOTING EXCAVATIONS SHALL BE INSPECTED AND APPROVED BY A COMPETENT SOIL ENGINEER BEFORE PLACING CONCRETE.
4. CENTRE ALL BASE PLATES, PIERS, AND FOOTINGS UNDER COLUMN UNLESS NOTED OTHERWISE ON PLANS.
5. STRUCTURAL STEEL IN CONTACT WITH EARTH SHALL BE PROTECTED WITH MINIMUM 75mm OF CONCRETE OR EQUIVALENT WATERPROOFING.
6. DENOTES INCOMING GAS SERVICE. (G)
7. DENOTES INCOMING ELECTRICAL SERVICE. (E)
8. DENOTES RAIN WATER LEADER. (X)



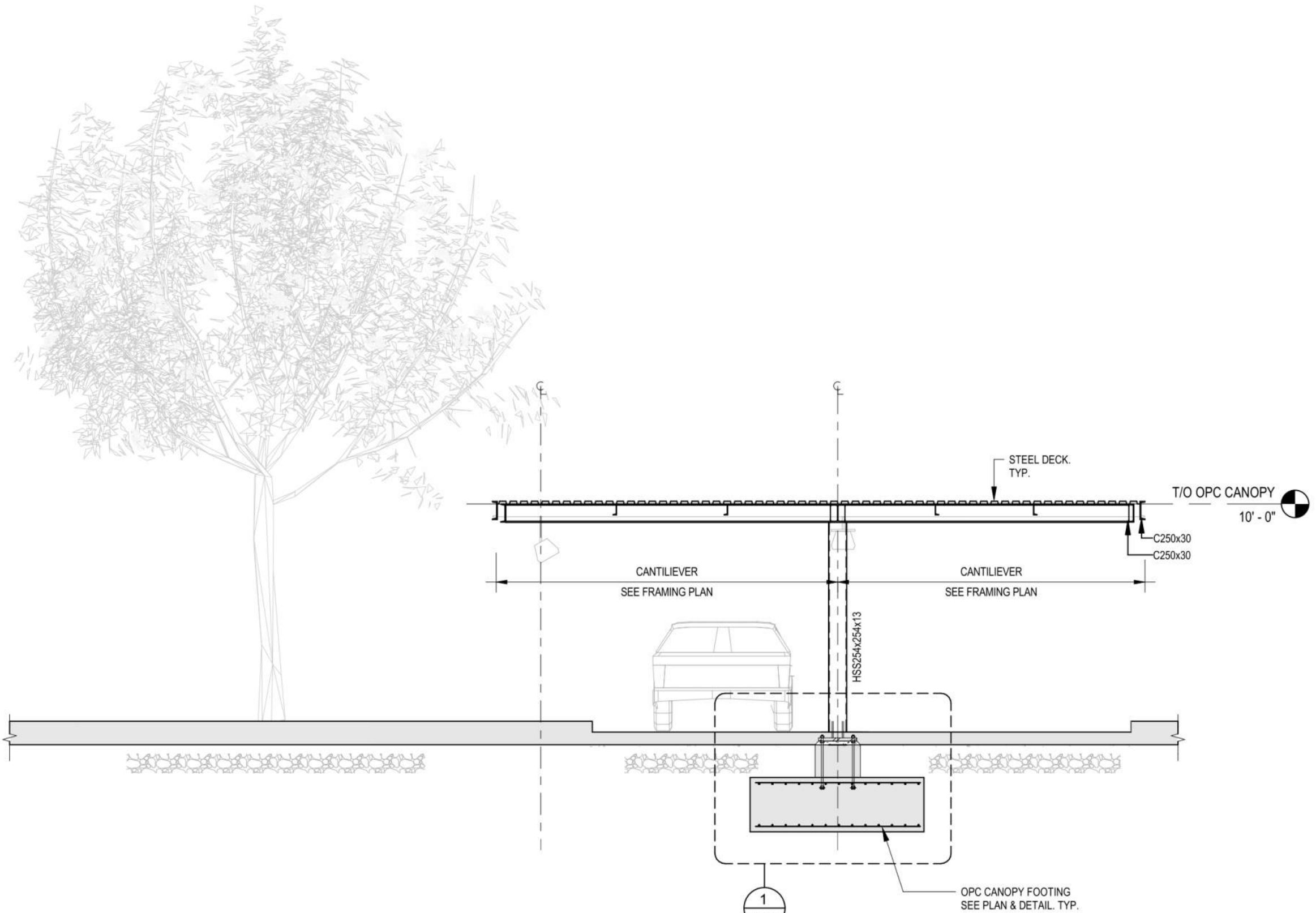
**2 KEY PLAN - OPC CANOPY**  
A107 N.T.S.

COLUMN SCHEDULE		
TYPE MARK	SIZE	BASEPLATE THICKNESS
C1	HSS12x12x13	1" THK
C2	HSS15x15x13	1" THK
C3	HSS24x24x13	1" THK

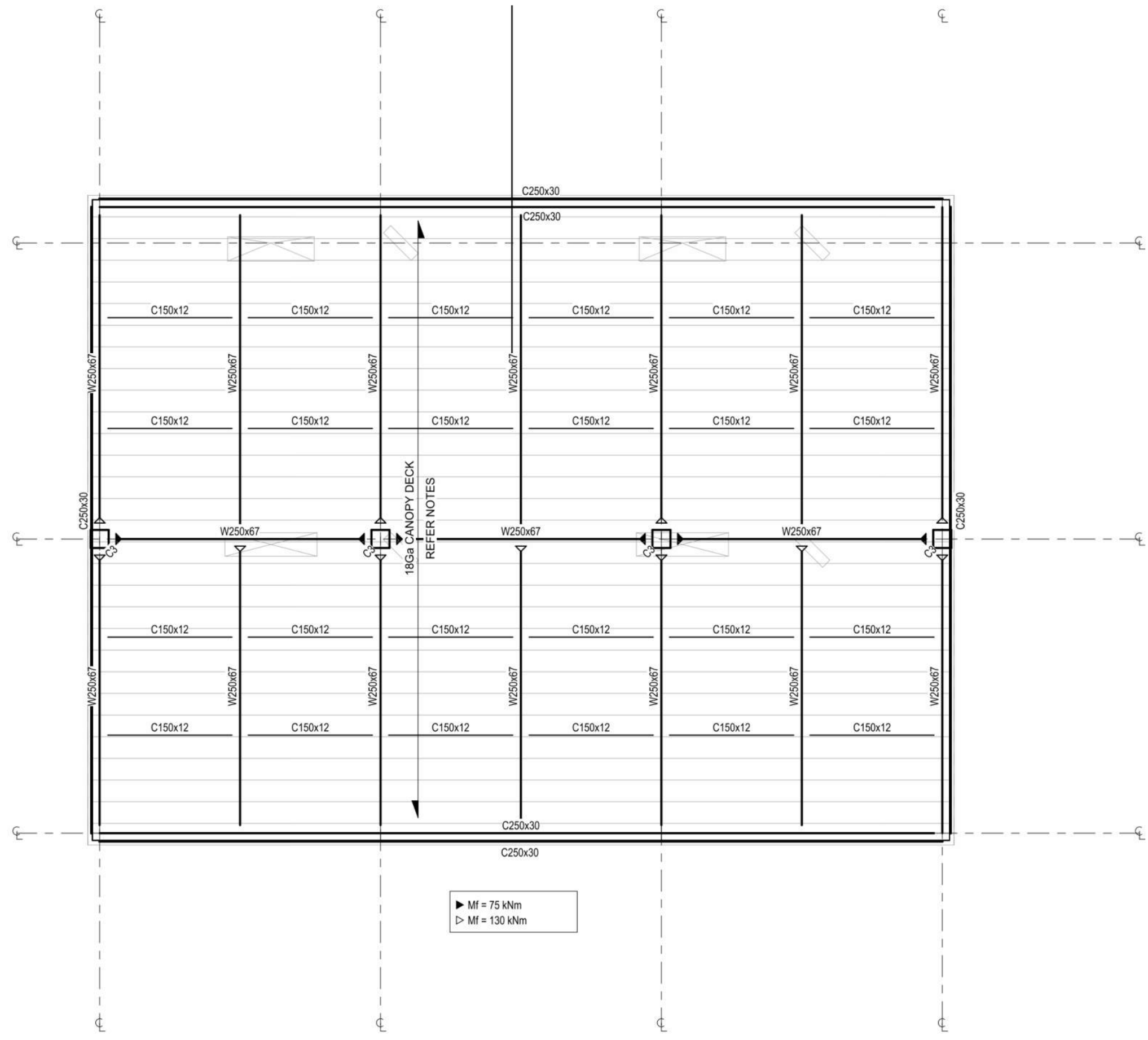
⊂ DENOTES FOOTING

**SITE KEYNOTES:**

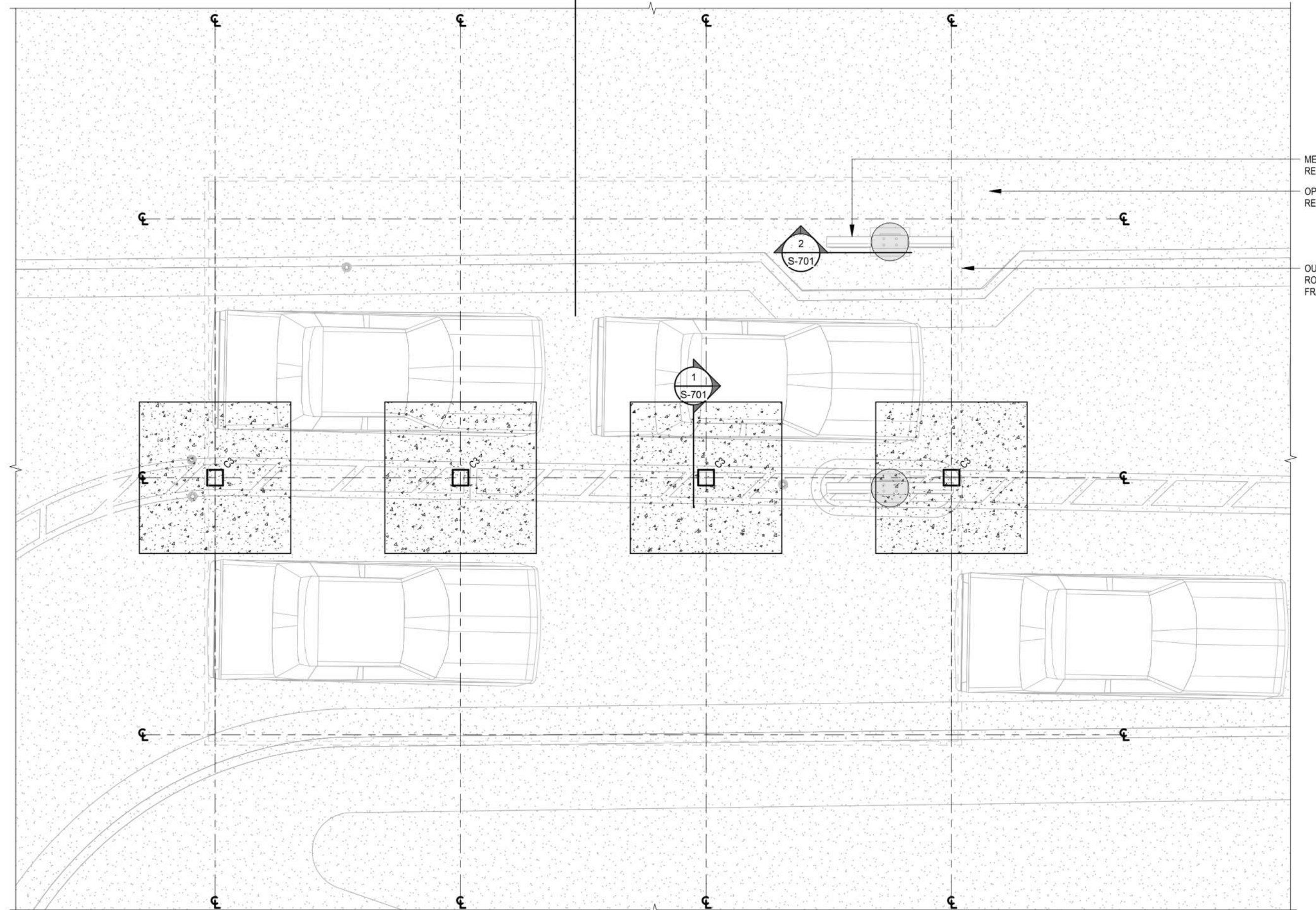
- S1 ORDER MENU SIGN. REFER TO ARCH. AND CIVIL DWGS. SEE 2 / S-701
- S2 FLAG POLE. SEE COORD. EXACT LOCATION WITH CIVIL DWGS. SEE 3 / S-701
- S3 DRIVE THRU ENTRANCE SIGN. REFER TO ARCH. AND CIVIL DWGS. SEE 5 / S-701
- S4 SIGNAGE REFER TO ARCH. AND CIVIL DWGS. SEE 4 / S-701



**3 FRAMING SECTION**  
A107 N.T.S.



**1 OPC CANOPY - FRAMING PLAN**  
A107 N.T.S.



**4 OPC CANOPY - FOUNDATION PLAN**  
A107 N.T.S.

ALL GRADE ELEVATIONS ARE APPROXIMATE AND MUST BE VERIFIED WITH CIVIL DRAWINGS

REFER TO CIVIL, SITE PLAN & ARCHITECTURAL DRAWINGS FOR MORE DETAILS & LOCATIONS OF SIGNAGE

TOP OF CANOPY FOOTINGS TO BE MIN. 16" BELOW TOP OF OPC SLAB ELEVATION REFER TO CIVIL DWGS FOR MORE INFORMATION

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5	2023-07-13	ISSUE FOR SPA SUBMISSION 2	MHB
4	2023-06-30	ISSUE FOR SPA SUBMISSION 2	MHB
3	2023-05-30	ISSUE FOR COORDINATION	MHB



**PROJECT**  
CHICK-FIL TANGER OUTLETS OTTAWA  
3000 PALLADIUM DRIVE, STITTSVILLE, ON K2S 1B9

**DRAWING**  
SITE DETAILS

PROJECT NO.	12-258P01
PROJECT DATE	2023-02-14
DRAWN BY	Author
CHECKED BY	Checker
SCALE	AS NOTED

DRAWING NO.	A107	REV	7
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