

**PLANNING RATIONALE REPORT: SITE PLAN CONTROL
APPLICATION**

**2380 2396 CLEROUX CRESCENT
BRIDOR DEVELOPMENTS
CITY OF OTTAWA**

**PREPARED BY: P H ROBINSON CONSULTING
JUNE 2021**

[P. H. Robinson Consulting](#)
[Urban Planning, Consulting and Project Management](#)

This report has been prepared on behalf of Bridor Developments. in support of a Site Plan Control Application for their lands at 2380 2396 Cleroux Crescent in the Blackburn Hamlet area of Ottawa. The legal description of the property is Part of Lot 11, Concession 3 (Ottawa Front), former Geographic Township of Gloucester, now in the City of Ottawa (P.I.N 04403-0310 and 04403-0311). The property is comprised of two whole lots on a Plan of Subdivision and they have not merged on title.

The property is located on the west side of Cleroux Crescent (with Cleroux Crescent being considered to be a north south street) and as it is a corner property it also has frontage on the north side of Orient Park Drive (see Location plan in the Appendix Section).

Lands to the north and east contain a school property and to the west of the property are residential properties on Autumn Hill Crescent. (see photo immediately below of the school property)



School property (Norman Johnson Secondary School) located opposite to the subject property (to the north)

To the south are residential properties on Orient Park Drive and to the south there is a three storey plus basement senior citizen home (Blackburn Seniors Residence) . (see photos on the following page. 1st photo is of homes on Orient Park backing on to the property in close proximity to the vehicular entrance to the site from Orient Park. 2nd and 3rd photos are views of the Seniors residence across from the subject property. 2nd photo is a view of the Senior residence

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from Cleroux and 3rd photo is a view of the Seniors residence from Orient Park opposite to the subject property).





The lands at 2380 and 2396 Cleroux have approximately 104 m of frontage (343') on the west side of Cleroux and frontage of approximately 69 m (227') on the north side of Orient Park Drive. The north lot line is approximately 141 m wide (463'). The west lot line is approximately 79 m wide (262'). Due to the fact that the two frontage streets don't meet at a 90 degree angle, the overall shape of the lot is irregular with a total lot area of 8373 m² (90.127 ft² - 0.84 ha). See the attached survey plan (in the Appendix section) for further details.

The property contains two single family homes with one home having its access to Cleroux and the other to Orient Park Drive. (2380 and 2396 Cleroux Crescent. See photos on following page)

The intent of the development application is to build two 3 storey rental apartment buildings with underground and surface parking. (see Site Plan and Underground Parking Plan in the Appendix Section). The proposal calls for 40 rental units in each building for a total of 80 units (there will be thirty 1 bedroom units and ten 2 bedroom units per building).

An area for bicycle storage will be provided in the underground parking level of each of the buildings and also some bicycle parking at ground level for each building. It is proposed that each building will have 23 dedicated parking spaces for bicycles with 17 being underground spaces and 6 being surface spaces.



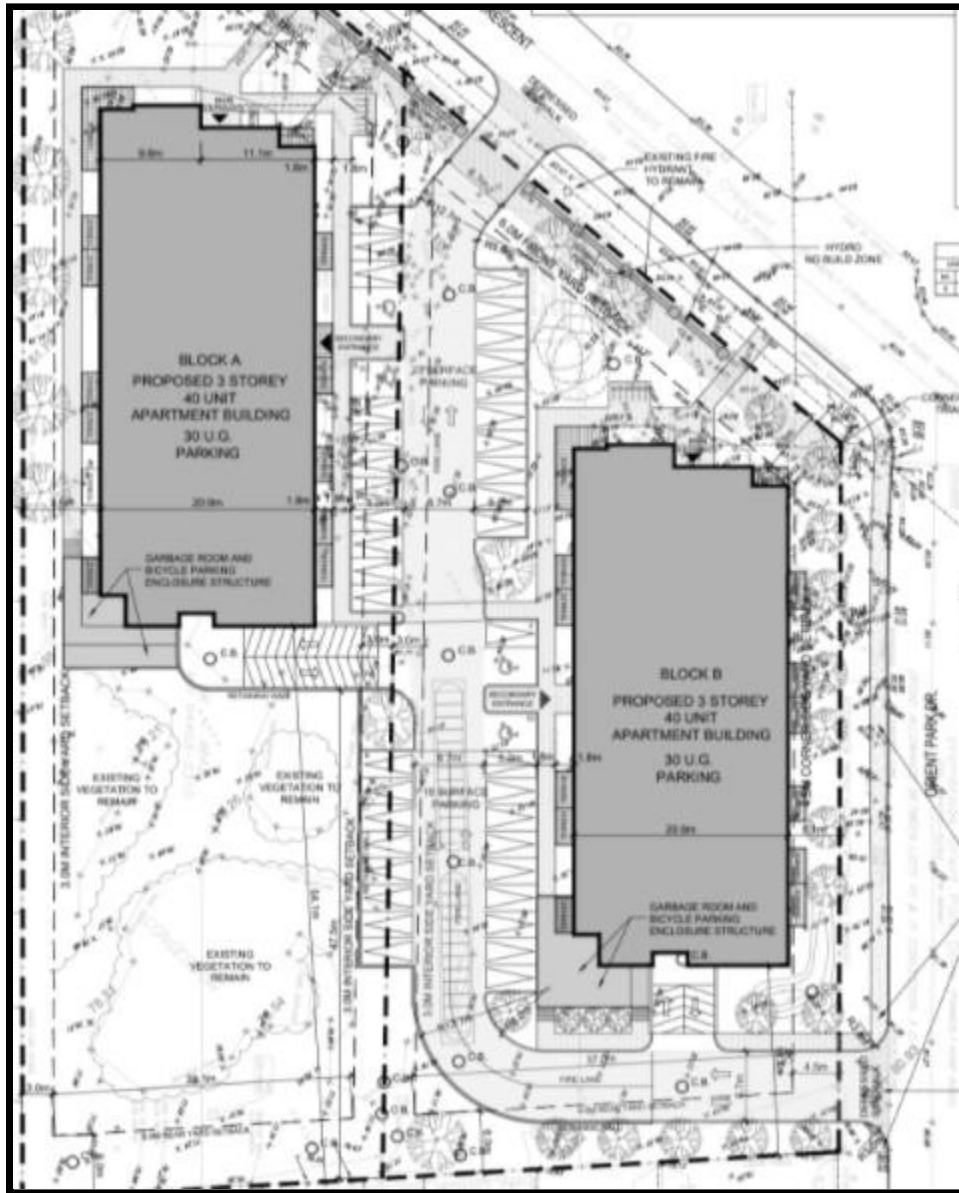
2380 Cleroux Crescent



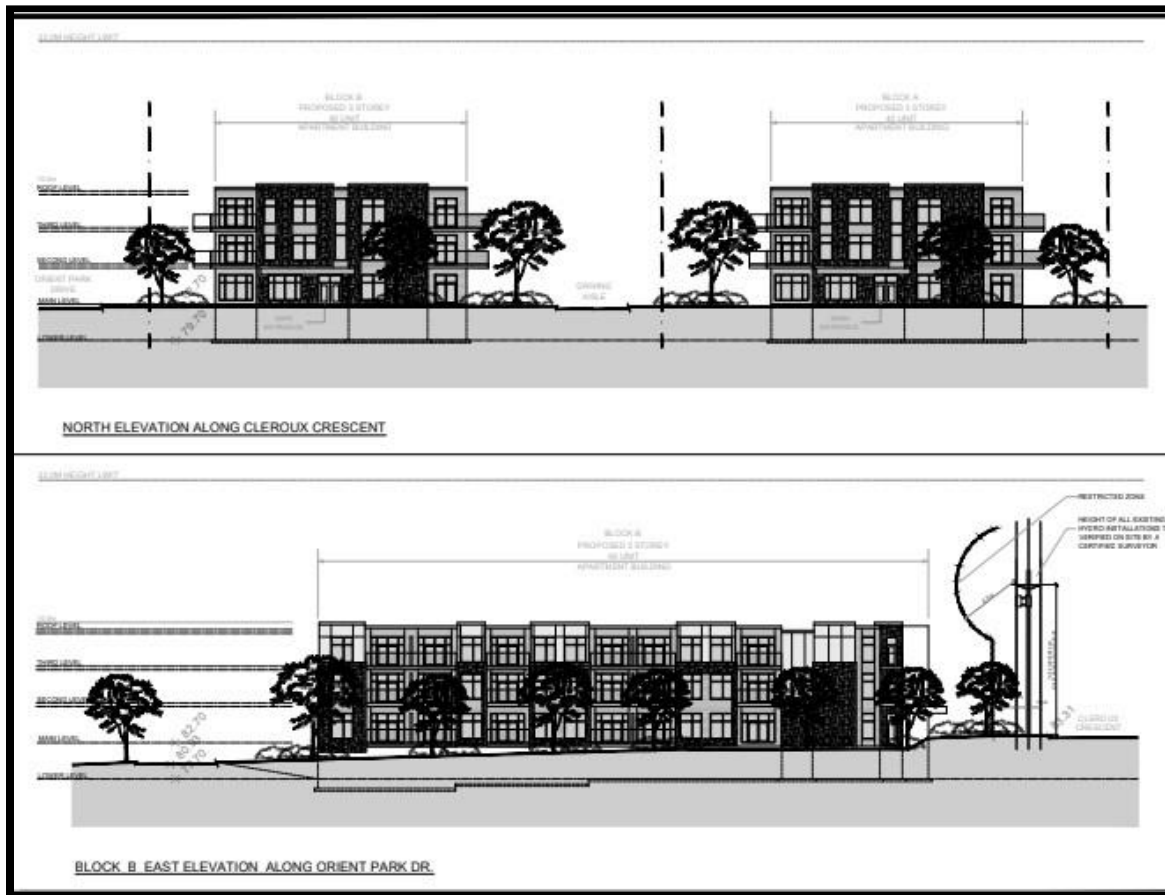
2396 Cleroux Crescent

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The buildings exterior design are characterized by balconies, extensive glazing with canopies facing Cleroux Crescent and this allows for a residential feel and a human scale to the streetscape that results from these two new buildings. The building entrances are connected to the city sidewalks via interior walkways.



Site Plan (see the site plan architectural elevation and rendering plans in the Appendix Section and the Design Brief filed with this application)



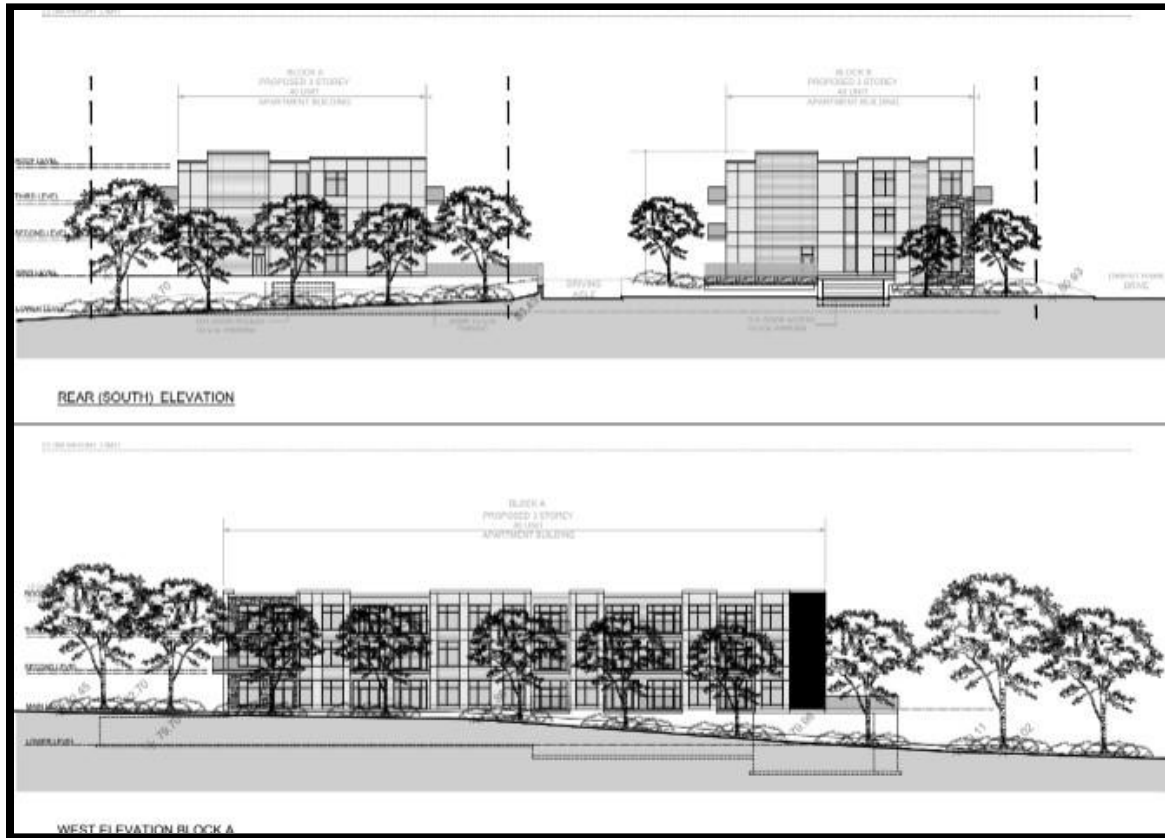
Elevations along Cleroux Crescent (top image) and elevation along Orient Park Drive (bottom image)

The access to the underground parking area will be through a ramp adjacent to the west walls of both buildings. Internal staircases and an elevator will connect the underground parking level with the residential levels of the building. The underground parking level also contains the bicycle and garbage rooms for each building. There will also be direct access to the parking from the exterior at the west and east sides of the building due to the grades of the property.

The designs call for unit balconies on all sides of the building except for the side above the parking ramps.

There will be landscaped areas along both street frontage of the property, around the edge of the buildings, the perimeter of the property and also a large area at the north west corner of the overall property which abuts nearby residential

properties. These lands will be left in their current state and contain existing



Top image is view of the ‘rear’ of the buildings from the rear of the property along the west property line

vegetation and are located at a lower existing grade than the remainder of the property. The landscaped areas will provide a buffer to the adjacent frontage roads and surrounding properties.

The new buildings will each have a building footprint of approximately 1013 m² (10904 ft²) and a total building area of approximately 3039 m² (32712 ft²) per building. Each building will have a building height of approximately 9.5 m (31.2 ft) .

Below the footprint of each of the proposed buildings will be 1 level of underground parking with 30 parking spaces for a total of 60 underground parking spaces. There are also surface parking spaces provided in the areas between the two buildings bringing the total parking count to 101 units (1.26 spaces/unit).



East elevation of Block A (top image) and west elevation of Block B (bottom image). Interior elevations of buildings from the internal driveway.

In addition to the underground parking spaces, this level will contain the garbage room, some resident locker areas and a dedicated room that contains the majority of the buildings bicycle parking areas.

Background studies addressing soils, environmental issues and civil engineering design are being filed as part of this Site Plan application. A full set of engineering, architectural, surveying and landscaping plans are also being filed with this site plan submission. No major issues or concerns were identified in these studies.

Consultation

We have had a formal pre consultation meeting on December 3 2020 with City Staff and on April 20 we had a video conference meeting with Councillor Dudas to introduce the project to her and to discuss the details of the project.

A meeting with community representatives/neighbours will be held in the near future.

Zoning

Under Zoning By-law 2008-250 the subject lands are zoned R5A H (22) (Residential Fifth Density Zone) . This is a zone which permits the scale of apartment buildings that are being proposed with a maximum building height of 22 metres. The location of the buildings and the density proposed meets the zoning regulations in place for the property. Based on the plans being filed, there will be a need for a minor zoning application to address the parking being provided and the location of some of the surface parking in the ‘front yard’ of the buildings.

The proposed uses of the site are in conformity with the existing zoning and the setback requirements that apply to this site.

The following are the R5A H (22) zone provisions and how the proposed development meets these requirements.

	REQUIREMENT	PROPOSED/PROVIDED
Minimum frontage	18 m	48.68 m/55.88 m
Minimum Lot Area	540 m ²	4517 m ² /3871 m ²
Minimum front yard	6 m	11.2/6.5 m
Minimum interior side yard	3m	7.5 m/12 m
Minimum Rear yard	7.5 m	54.1 m/18.4 m
Maximum Building Height	22 m	9.5 m
Parking	112 spaces	101 spaces
Landscaped Area	30 %	55 %
Common Amenity Area	480 m ²	722 m ²

Note: The total number of provided parking spaces and the location of some of the parking spaces (in the ‘front yard’ of the property facing Cleroux Crescent- 7 surface spaces are in front of the Block B building and would be considered to be front yard parking) will be the subject of a Minor Variance application to be filed near the conclusion of the Site Plan application process.

Provincial Policy Statement

The current Provincial Policy Statement (PPS) came into effect in 2020 and it outlines the key matters of Provincial planning interest with respect to land use planning decisions made by municipal approval authorities. Any decisions that are made by municipal approval authorities must be consistent with the policies of the PPS.

The following are sections of the PPS that are applicable to the proposed development at 2380 2396 Cleroux:

- 1.1.1 Healthy, liveable and safe communities are sustained by:
 - a) promoting efficient development and land use patterns which sustain the financial well-being of the Province and municipalities over the long term; *The proposed development on the site represents an efficient intensification of the site and will not create inefficient land use patterns in the area.*
 - e) promoting cost-effective development standards to minimize land consumption and servicing costs. *The intensification of the site will be achieved through two new 3 storey new buildings on the site and the majority of the site's parking requirements will be through underground parking. We have ensured that an efficient building design has been proposed based on the lot shape which is irregular as the two frontage streets do not meet at a 90 degree angle. The existing infrastructure within the City Right of Way will be connected to the new building.*
- 1.1.3.1 *Settlement areas* shall be the focus of growth and their vitality and regeneration shall be promoted. *The property is within a settlement area of the City of Ottawa.*
- 1.1.3.2 Land Use Patterns within *settlement areas* shall be based on:
 - a) densities and a mix of land uses which: 1) efficiently use land and resources. *This redevelopment of the property is an efficient use of the land and will allow for a sensitive infill development within City height limits for this use and with significant underground parking to ensure that there is a maximum of greenspace maintained on the site.* 2) are appropriate for, and efficiently use, the infrastructure and public service facilities which are planned or available, and avoid the need for their unjustified and/or uneconomical expansion; *The proposed new development on the property will efficiently utilize the existing infrastructure available within the frontage streets.*
- 1.4.3 Planning authorities shall provide for an appropriate range of housing types and densities to meet projected requirements of current and future residents of the regional market area by:

- c) directing the development of new housing towards locations where appropriate levels of infrastructure and public service facilities are or will be available to support current and projected needs. *The proposed low rise apartment building is within an established neighbourhood with nearby infrastructure, public transit on a number of routes within a short walking distance and nearby parks and schools for residents' needs.*
- d) promoting densities for new housing which efficiently use land, resources, infrastructure and public service facilities, and support the use of alternative transportation nodes and public transit in areas where it exists or is to be developed; *This development proposal represents an efficient use of land on the property, allows for passive and active open space areas within the site, utilizes existing infrastructure and is located close to OC Transpo bus routes and other public facilities including recreational facilities and parkland.*

Official Plan Designation – City of Ottawa Official Plan

The site is designated General Urban Area in the Urban Policy Plan of the City of Ottawa Official Plan.

This designation permits the 'development of a full range and choice of housing types to meet the needs of all ages, incomes and life circumstances, in combination with conveniently located employment, retail, service, cultural, leisure, entertainment and institutional uses'.

Key policies within the General Urban Area designation that are applicable to this infill development are:

- 3.6.1.3 When considering a proposal for residential intensification through infill or redevelopment in the General Urban Area, the City will:
- a) Recognize the importance of new development relating to existing community character so that it enhances and builds upon desirable established patterns and built form; *The proposed development will result in a continuation of the residential nature of Orient Park Drive away from Cleroux and of Cleroux Drive further away from Innes Road and will allow for new development on the street that will not create undue negative impact on surrounding properties. The overall residential density and overall maximum building height are within the allowable zoning requirements and due to the proximity of this site to nearby transit and parks and institutional facilities, it represents a sensitive new development to the surrounding neighbourhood. The majority of the provided parking for the development will be in an underground parking level. Landscaping*

along the side, rear and front lot lines will minimize the impact of the new building on adjacent properties and provide some of the total amenity areas for residents of this new development. The proposed development will complement and enhance the street frontage.

- Apply the policies of Sections 2.5.1 and 4.11 of the Official Plan. Note that these sections are related to Urban Design and Compatibility. The proposed infill development meets the compatibility criteria in Section 4.11.2 of the Official Plan:
- 4.11.2.a) Traffic. Road should adequately serve the development with sufficient capacity to accommodate the anticipated traffic generated. *The number of units proposed for this development does not trigger any of the TIA Guidelines and the main vehicular entrance/exit will be via the two frontage streets and they lead to underground parking access points. Cleroux Drive is a road with sufficient vehicular capacity which leads to Innes Road.*
- 4.11.2.b) Vehicular access. The location and orientation of vehicle access and egress should address matters such as the impact of noise, headlight glare and loss of privacy on development adjacent or immediately opposite. *The main vehicular access to the site will be via the underground parking ramp. This will minimize noise and headlight glare to any adjacent residential properties. For exiting cars from the underground parking ramps, there are no impacts on properties to the west on Autumn Hill Crescent and due to the setbacks to homes to the west on Orient Park Drive of over 10 m and also with fencing along the property line the entrance for the visitor parking/surface parking spaces will not create lighting impacts on nearby residential properties.*
- 4.11.2.c) Parking requirements. The development should have adequate on-site parking to minimize the potential for spillover parking on adjacent areas. *The majority of the parking for the proposed buildings will be provided in an underground parking area for each property with also some surface parking to supplement the underground parking being provided. The parking being provided (101 spaces) is slightly less than the City parking requirements under the zoning by-law and this will be the subject of a minor variance application to the Committee of Adjustment . The amount of parking being provided will minimize the potential for any spillover parking in adjacent areas.*
- 4.11.2.d) Outdoor amenity areas. The development should respect the privacy of outdoor amenity areas of adjacent residential units and minimize any undesirable impacts through the siting and design of the buildings and the use of screening, lighting, landscaping or other mitigative design measures. *The outdoor amenity areas are to be located along the frontage streets and a large area to the rear of Block A in an*

- area where existing vegetation is to remain. There will be fencing along the perimeter of the property that will screen the buildings and their amenity areas from adjacent residential properties. All site lighting is to be designed to avoid spillover onto adjacent properties. Note that there is also an indoor amenity area on the ground floor of each building.*
- 4.11.2.e) Loading Areas, Service Areas and Outdoor Storage. The operational characteristics and visual appearance of loading facilities, service areas (including garbage), parking and areas for the outdoor storage of goods or materials should be mitigated using a variety of methods. These uses should be located away from residences where possible. *All garbage areas will be within the building and there will be no outdoor storage. There are no loading areas proposed.*
 - 4.11.2.f) Lighting. The potential for light spill over or glare from any lighting source onto adjacent light-sensitive areas should be avoided or mitigated. *All external site lighting will have sharp cut off lighting to ensure that there will not be spill over or glare onto adjacent properties.*
 - 4.11.2.g) Noise and Air Quality. The development should be located and designed to minimize the potential for significant adverse effects on adjacent sensitive uses. *The mechanical and electrical rooms are to be internal and part of the underground parking area .*

Conclusions

The proposed development of a new 3 storey rental apartment building on the property is consistent with current zoning, Official Plan and Secondary Plan policies for these lands.

The site represents a sensitive infill development that will allow for:

- a well designed contemporary building to be located within an established neighbourhood that will add to the character of the street and the neighbourhood
- a centralized underground parking facility which results in the majority of the provided parking on the site being underground and also will serve as the location for some of the bicycle parking for the development.
- a new development that meets the majority of the current zoning regulations (Minor Variances to be filed regarding the total number of parking spaces being provided and the location of some of these spaces.
- buffering through vegetation and setbacks to adjacent properties .

Through the site plan process there are a number of key engineering studies and drawings required in support of the development. These studies show that the

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site can be serviced through existing service connections along the frontage streets. The overall site drainage and grading has been designed based on current City guidelines and will utilize roof top storage and parking lot ponding in order to control the peak post development flows to the allowable release rate.

Landscaping will be provided along the side, front and rear yards to provide screening and buffering to adjacent properties. The proposed building height of this development is less than the permitted maximum building height as outlined in the zoning by-law and privacy and noise issues will be mitigated through building design

It is our opinion that the proposed development is consistent with the Provincial Policy Statement and the City of Ottawa Official Plan and relevant zoning regulations. It is being proposed at an appropriate scale of development and will be compatible with surrounding land uses and will contribute to the overall housing supply in the community surrounding the property.

The proposed development represents good land use planning that is in the public interest and it is recommended for approval.

Should you have any questions or require anything further, please do not hesitate to contact the undersigned.

P H Robinson Consulting



Paul Robinson RPP

APPENDIX

LOCATION PLAN

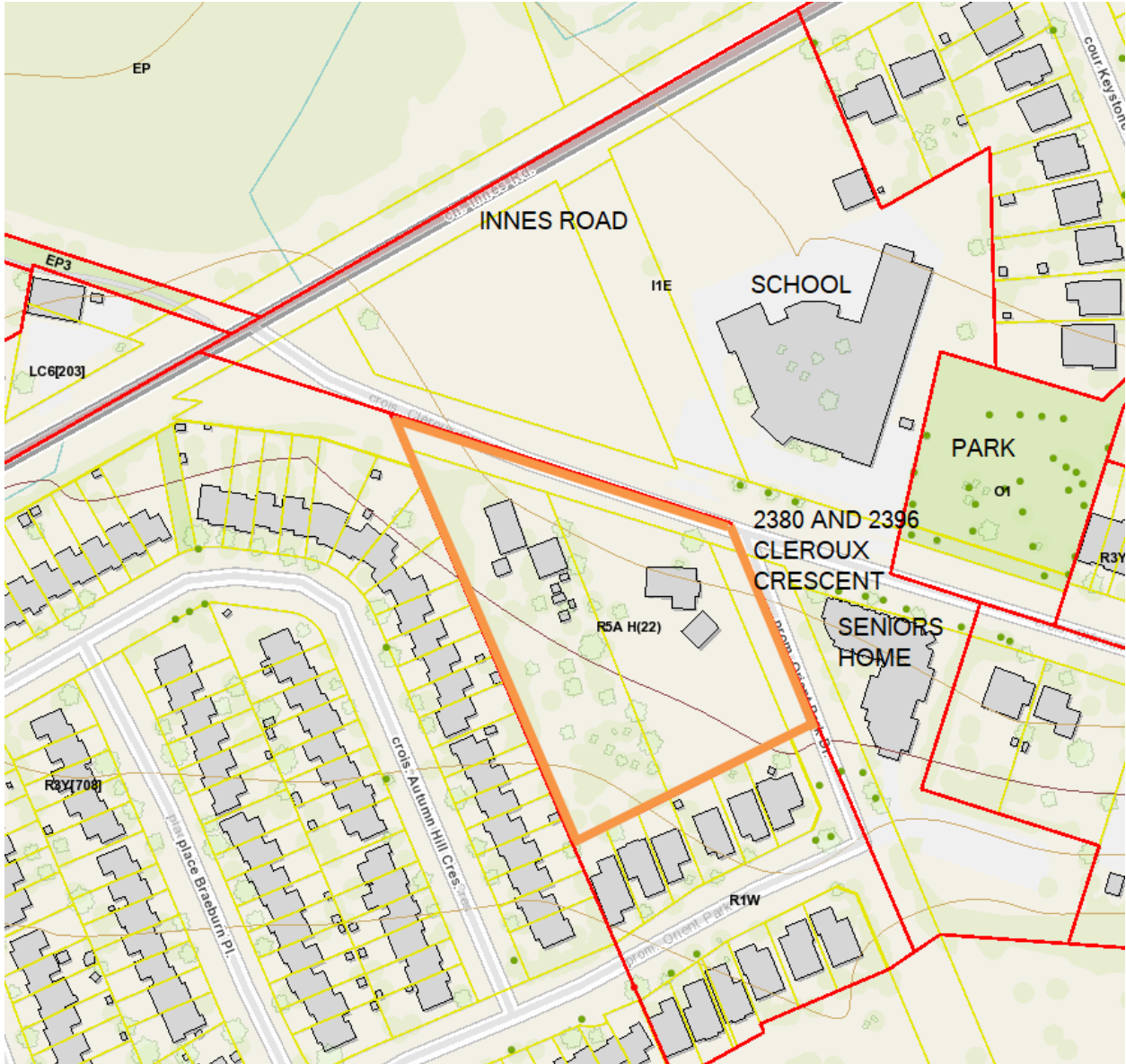
SURVEY PLAN

SITE PLAN

UNDERGROUND PARKING PLAN AND FLOOR PLANS

ARCHITECTURAL PLANS AND RENDERING OF THE BUILDINGS

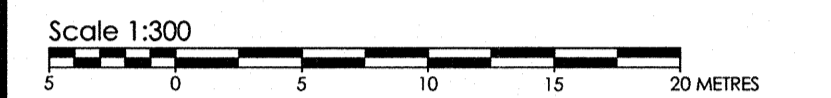
LANDSCAPE PLAN



LOCATION PLAN

TOPOGRAPHIC PLAN OF SURVEY

**PART OF LOT 11
CONCESSION 3 (OTTAWA FRONT)**
(GEOGRAPHIC TOWNSHIP OF GLOUCESTER)
CITY OF OTTAWA



METRIC CONVERSION

DISTANCES AND COORDINATES SHOWN ON THIS PLAN ARE IN METRES AND CAN BE CONVERTED TO FEET BY DIVIDING BY 0.3048

GRID SCALE CONVERSION

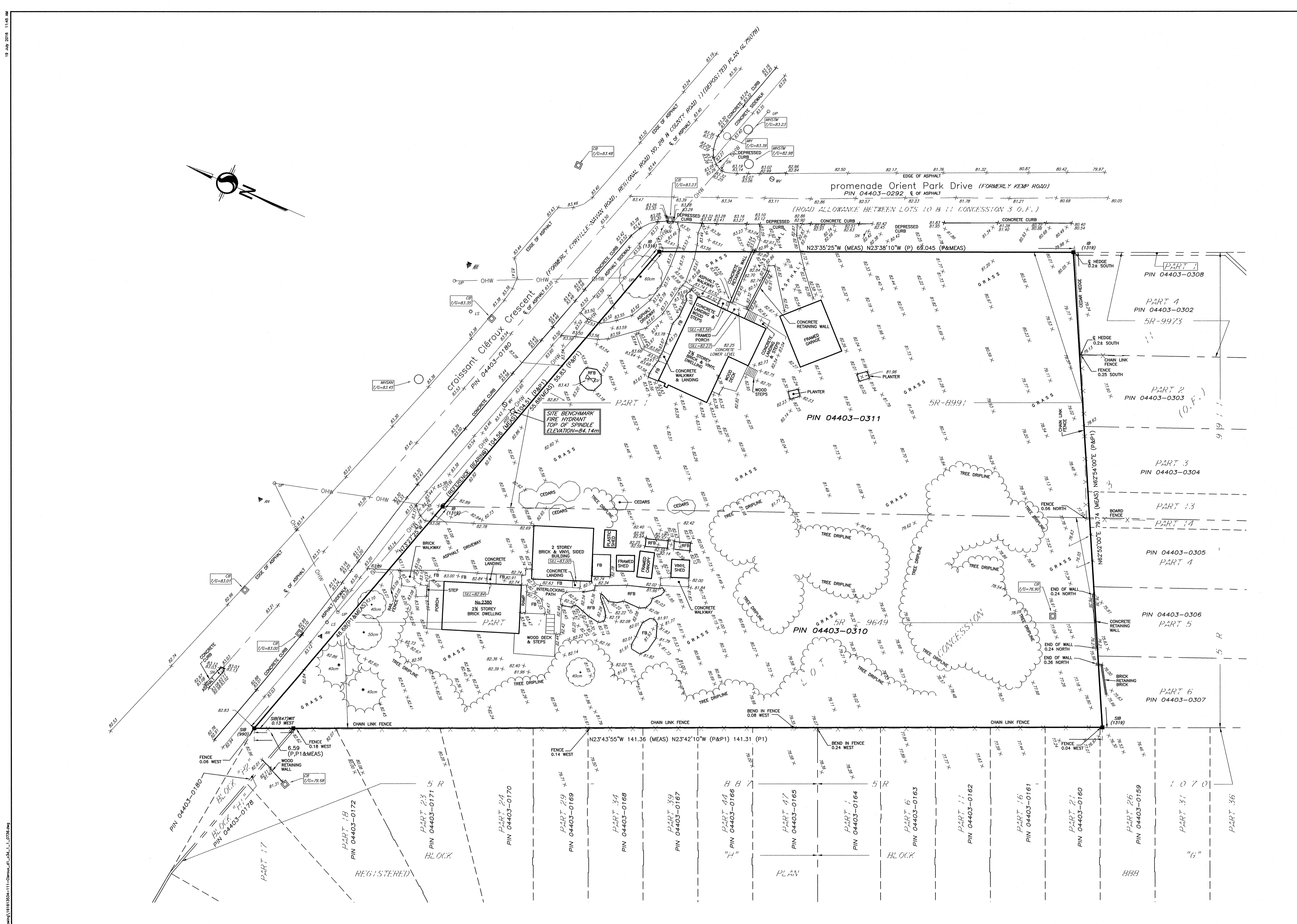
DISTANCES ARE GROUND AND CAN BE CONVERTED TO GRID BY MULTIPLYING BY THE COMBINED SCALE FACTOR OF 0.99996.

BEARING NOTE

BEARINGS HEREON ARE ASTRONOMIC AND ARE REFERRED TO THE SOUTHERLY LIMIT OF CLEROUX CRESCENT AS SHOWN ON PLAN SR-9649, HAVING A BEARING OF N73°27'25"W.

ELEVATION NOTE

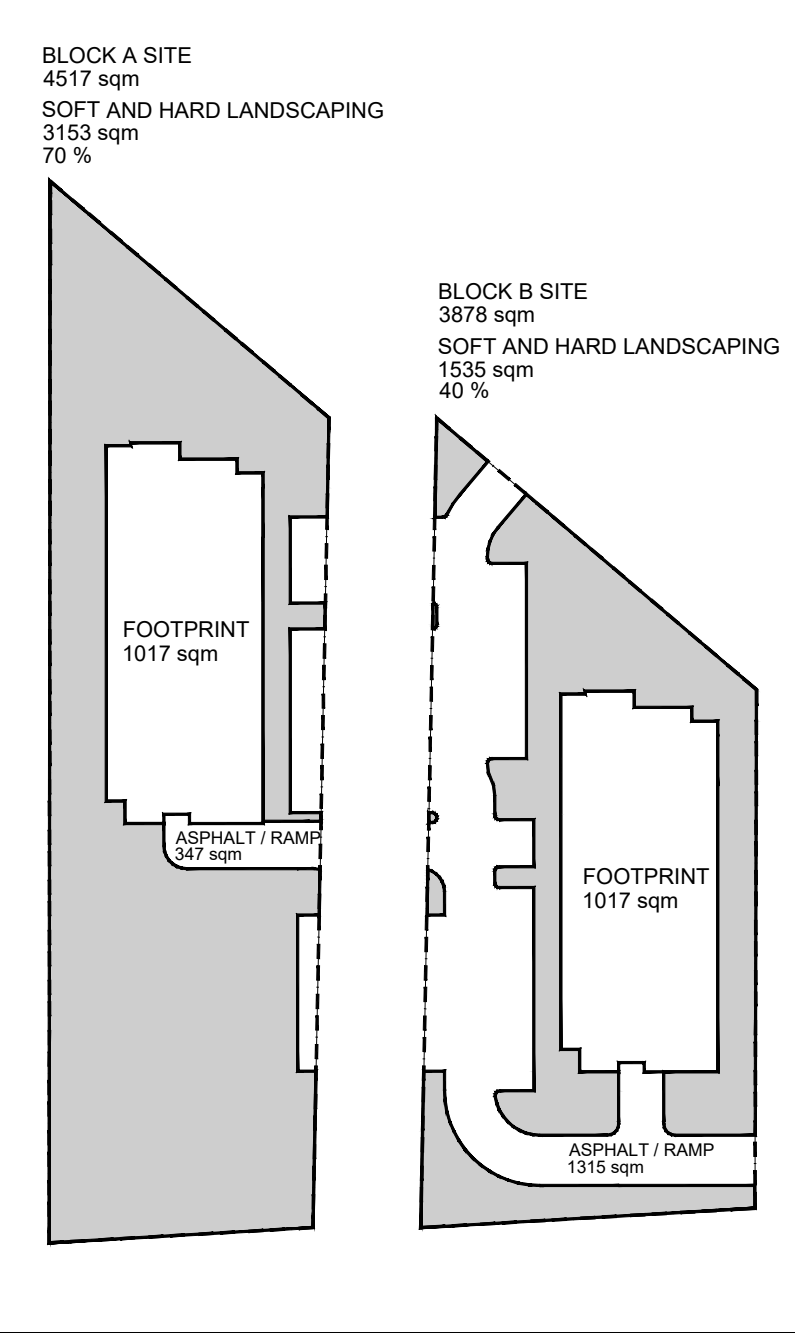
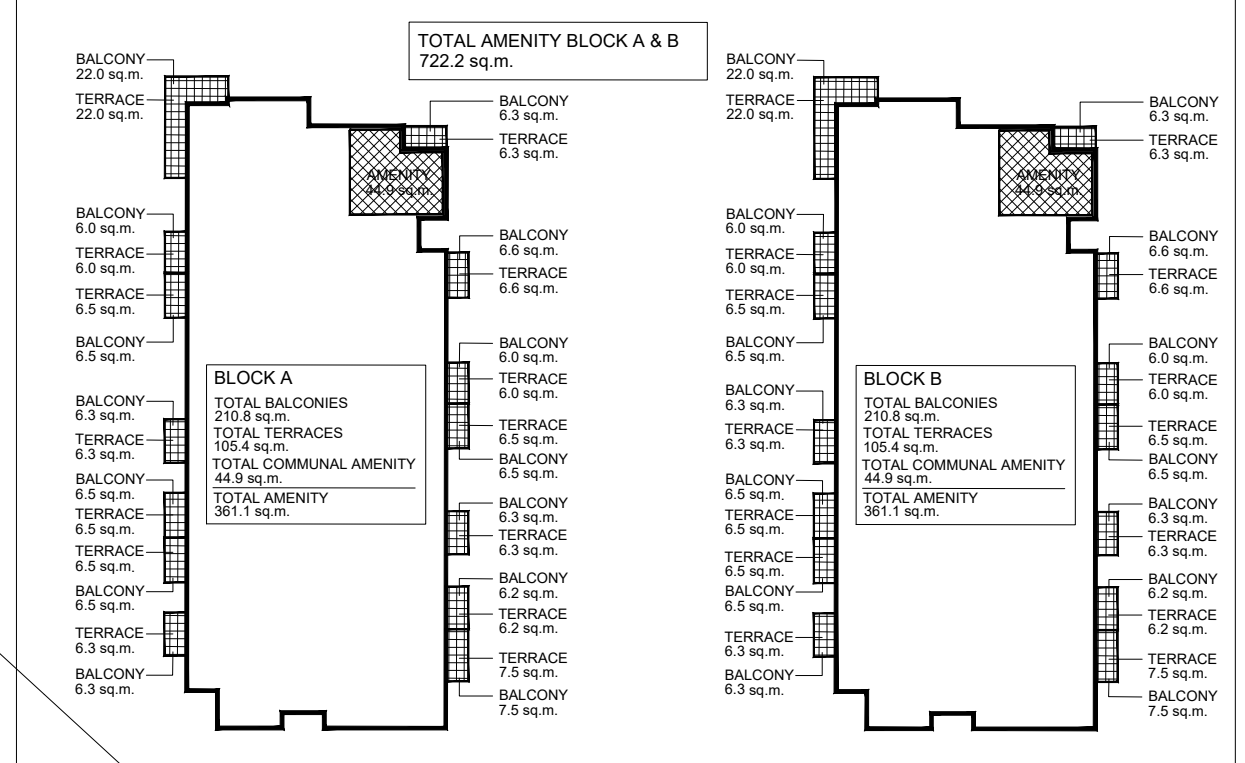
ELEVATIONS SHOWN HEREON ARE GEODETIC (CGVD:1928:1978) AND ARE DERIVED FROM NCC VERTICAL CONTROL MONUMENT No.217 HAVING AN ELEVATION OF 85.00.



LEGEND		FOUND MONUMENTS		HORIZONTAL CONTROL MONUMENT		CONCRETE MONUMENT		HYDRO METER		MAINTENANCE HOLE INVERT		POST		TRAFFIC SIGNAL LIGHT	
■	DENOTES	○ NW	FOUND MONUMENTS	△ HCM	HORIZONTAL CONTROL MONUMENT	●	CONCRETE MONUMENT	○	HYDRO METER	○	MAINTENANCE HOLE INVERT	○	POST	○	TRAFFIC SIGNAL LIGHT
□	DENOTES	○ NW	FOUND MONUMENTS	○ NW	HORIZONTAL CONTROL MONUMENT	○	DOUBLE CB	○	HYDRO TRANSFORMER	○	MAINTENANCE HOLE SANITARY	○	SCLP	○	MARKER BELL UNDERGROUND
□	DENOTES	○ NW	FOUND MONUMENTS	○ NW	HORIZONTAL CONTROL MONUMENT	○	CB MANHOLE	○	HAND WELL	○	MAINTENANCE HOLE STORM	○	SN	○	MARKER CABLE UNDERGROUND
□	DENOTES	○ NW	FOUND MONUMENTS	○ NW	HORIZONTAL CONTROL MONUMENT	○	DOUBLE CB MANHOLE	○	FIRE HYDRANT	○	MAINTENANCE HOLE TRAFFIC	○	SW	○	MARKER GAS UNDERGROUND
□	DENOTES	○ NW	FOUND MONUMENTS	○ NW	HORIZONTAL CONTROL MONUMENT	○	SIDE INLET CB	○	MAILBOX	○	MONITORING WELL	○	SICD	○	MARKER OIL UNDERGROUND
□	DENOTES	○ NW	FOUND MONUMENTS	○ NW	HORIZONTAL CONTROL MONUMENT	○	GAS MARKER	○	NEWS PAPER BOX	○	MONITORING WELL	○	TR BELL	○	UTILITY POLE
□	DENOTES	○ NW	FOUND MONUMENTS	○ NW	HORIZONTAL CONTROL MONUMENT	○	PIPE FLANGE (GAS)	○	MONITORING PIN	○	TERMINAL BOX - CABLE	○	TR CHY	○	VALVE BOX
□	DENOTES	○ NW	FOUND MONUMENTS	○ NW	HORIZONTAL CONTROL MONUMENT	○	POLE GUYWIRE	○	MONITORING PIN UNIDENTIFIED	○	LIGHT STANDARD ORNAMENTAL	○	TRC	○	VALVE CHAMBER
□	DENOTES	○ NW	FOUND MONUMENTS	○ NW	HORIZONTAL CONTROL MONUMENT	○	GAS SERVICE REGULATOR	○	MAINTENANCE HOLE BELL	○	OBSERVATION WELL	○	TRD	○	WATER VALVE
□	DENOTES	○ NW	FOUND MONUMENTS	○ NW	HORIZONTAL CONTROL MONUMENT	○	GAS VALVE	○	MAINTENANCE HOLE BELL	○	PARKING METER	○	TRP	○	WELL
□	DENOTES	○ NW	FOUND MONUMENTS	○ NW	HORIZONTAL CONTROL MONUMENT	○	LIGHT STANDARD HYDRO	○	MAINTENANCE HOLE FIBRE OPTIC	○	PULL BOX	○	TRV	○	FLOWER BED
□	DENOTES	○ NW	FOUND MONUMENTS	○ NW	HORIZONTAL CONTROL MONUMENT	○		○	MAINTENANCE HOLE HYDRO	○	PILLAR	○	TRV	○	RAISED FLOWER BED
□	DENOTES	○ NW	FOUND MONUMENTS	○ NW	HORIZONTAL CONTROL MONUMENT	○		○		○		○	TRV	○	

SURVEYOR'S CERTIFICATE
I CERTIFY THAT:
1. THIS SURVEY AND PLAN ARE CORRECT AND IN ACCORDANCE WITH THE SURVEYS ACT, THE SURVEYORS ACT AND THE REGULATIONS MADE UNDER THEM.
2. THE SURVEY WAS COMPLETED ON THE 18TH DAY OF JULY, 2016.

July 19/16
DATE
BRIAN J. WEBSTER
ONTARIO LAND SURVEYOR



BLOCK A				BLOCK B			
UNIT TYPE	1B	2B	TOTAL	UNIT TYPE	1B	2B	TOTAL
M.	6	18	24	M.	5	20	25
	16	40			15	40	

SITE STATISTICS

ZONING: R5A H(22)

SITE AREA:
 BLOCK A: 4517 M²
 BLOCK B: 3871 M²
 TOTAL: 8388 M²

SETBACK REQUIREMENTS

FRONT YARD SETBACK
 REQUIRED: 6.00 M
 PROVIDED: BLOCK A 11.20 M, BLOCK B 6.50 M

INTERIOR SIDE SETBACK (ABUTTING R3Y ZONE)
 REQUIRED: 3.00 M
 PROVIDED: BLOCK A 7.50 M

REAR YARD SETBACK
 REQUIRED: 6.00 M
 PROVIDED: BLOCK A 54.10 M, BLOCK B 18.40 M

HEIGHT LIMITATIONS
 REQUIRED: 22.0 M
 PROVIDED: BLOCK A 9.5 M, BLOCK B 9.5 M

BUILDING FOOTPRINT
 BLOCK A: 1013 M²
 BLOCK B: 1013 M²
 TOTAL: 2526 M²

BUILDING GFA
 BLOCK A: 3039 M²
 BLOCK B: 3039 M²
 TOTAL: 6078 M²

LANDSCAPE
 BLOCK A: 3153 sq.m. (70%)
 BLOCK B: 1539 sq.m. (40%)
 TOTAL: 4692 sq.m. (55%)

RESIDENTIAL UNIT COUNT
 BLOCK A: 40
 BLOCK B: 40
 TOTAL: 80

ASPHALT AREA AND RAMPS
 BLOCK A: 347 sq.m.
 BLOCK B: 1315 sq.m.
 TOTAL: 1662 sq.m.

AMENITY AREA
 REQUIRED: 6.0sq.m. per unit
 80 units x 6.0 = 480 sq.m.

BLOCK A
 TOTAL BALCONIES: 210.8 sq.m.
 TOTAL TERRACES: 105.4 sq.m.
 TOTAL COMMUNAL AMENITY: 44.9 sq.m.
 TOTAL AMENITY: 361.1 sq.m.

BLOCK B
 TOTAL BALCONIES: 210.8 sq.m.
 TOTAL TERRACES: 105.4 sq.m.
 TOTAL COMMUNAL AMENITY: 44.9 sq.m.
 TOTAL AMENITY: 361.1 sq.m.

TOTAL AMENITY BLOCK A & B
 722.2 sq.m.

PARKING STATISTICS

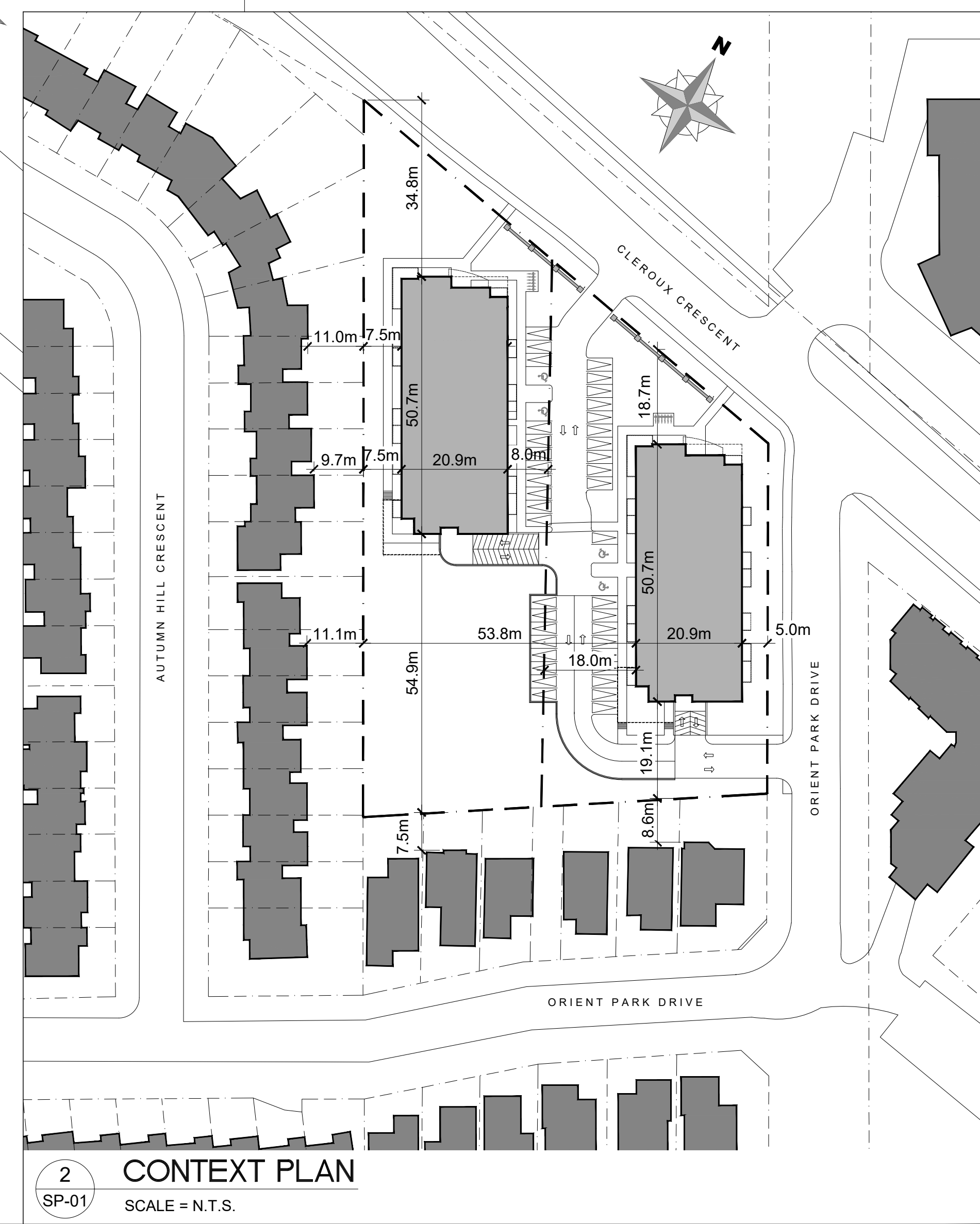
BLOCK A
 REQUIRED: 1.2 PER UNIT + 0.2 VISITOR PER UNIT
 40 UNITS X 1.4 = 56 PARKING SPACES
 PROVIDED: UNDERGROUND 30 SPACES

BLOCK B
 REQUIRED: 1.2 PER UNIT + 0.2 VISITOR PER UNIT
 40 UNITS X 1.4 = 56 PARKING SPACES
 PROVIDED: UNDERGROUND 30 SPACES

TOTAL REQUIRED:
 80 UNITS X 1.4 = 112 PARKING SPACES

PROVIDED:
 SURFACE 41 SPACES
 UNDERGROUND 40 SPACES
 TOTAL 101 SPACES (1.26 PER UNIT)
 (4 HANDICAP PARKING SPACES INCLUDED)

- CLIENT:** BRIDOR Developments
- LEGEND:**
- PROPERTY LINE
 - - - PROPERTY SETBACK
 - - - CHW OVERHEAD WIRES
 - - - EXISTING BUILDING
 - ▼ BUILDING ENTRANCE
 - W.P. WALL MOUNTED LIGHT
 - H.P. HYDRO POLE
 - ⊕ F.H. FIRE HYDRANT
 - M.H. MANHOLE
 - C.B. CATCH BASIN
 - RETAINING WALL
 - D.C. DEPRESSED CURB
 - X 100.00 EXISTING GRADE
 - 100.00 PROPOSED GRADE
 - INTERLOCK PAVERS
 - DESIGNATED FIRE ROUTE
 - TERRACE



1 SITE PLAN
SCALE = 1:250

2 CONTEXT PLAN
SCALE = N.T.S.

No.	REVISIONS	BY	DATE
13			
12			
11			
10			
09			
08			
07			
06			
05	FOR REVIEW	PE	MAY 31 2021
04	FOR REVIEW	PE	APR. 15 2021
03	FOR REVIEW	PE	APR. 08 2021
02	FOR REVIEW	PE	DEC. 15 2020
01	FOR REVIEW	PE	NOV. 18 2020

NOT AUTHENTIC UNLESS SIGNED AND DATED

P² concepts
 CONSULTING ENGINEERS

STRUCTURAL
 MECHANICAL
 ELECTRICAL

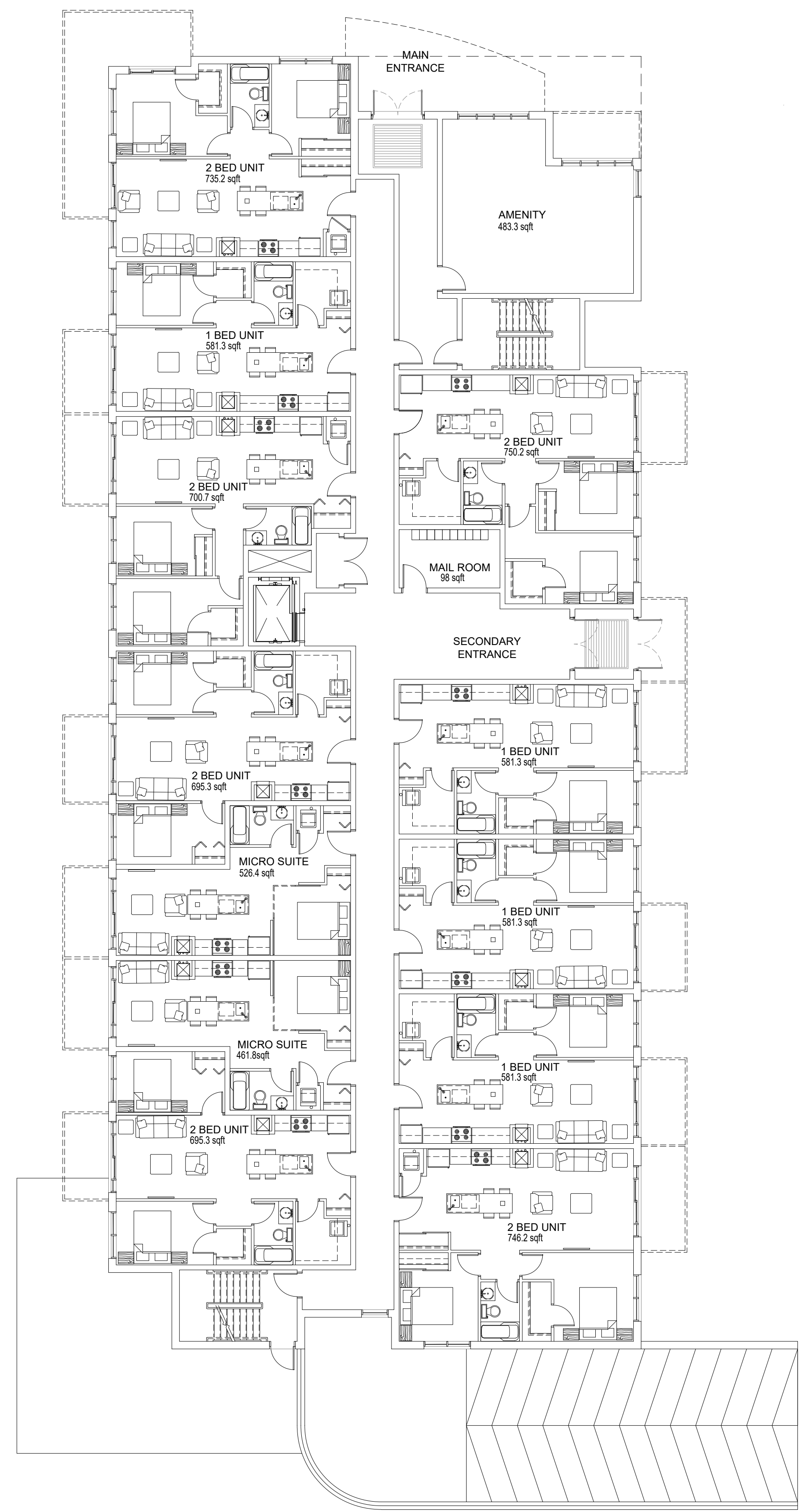
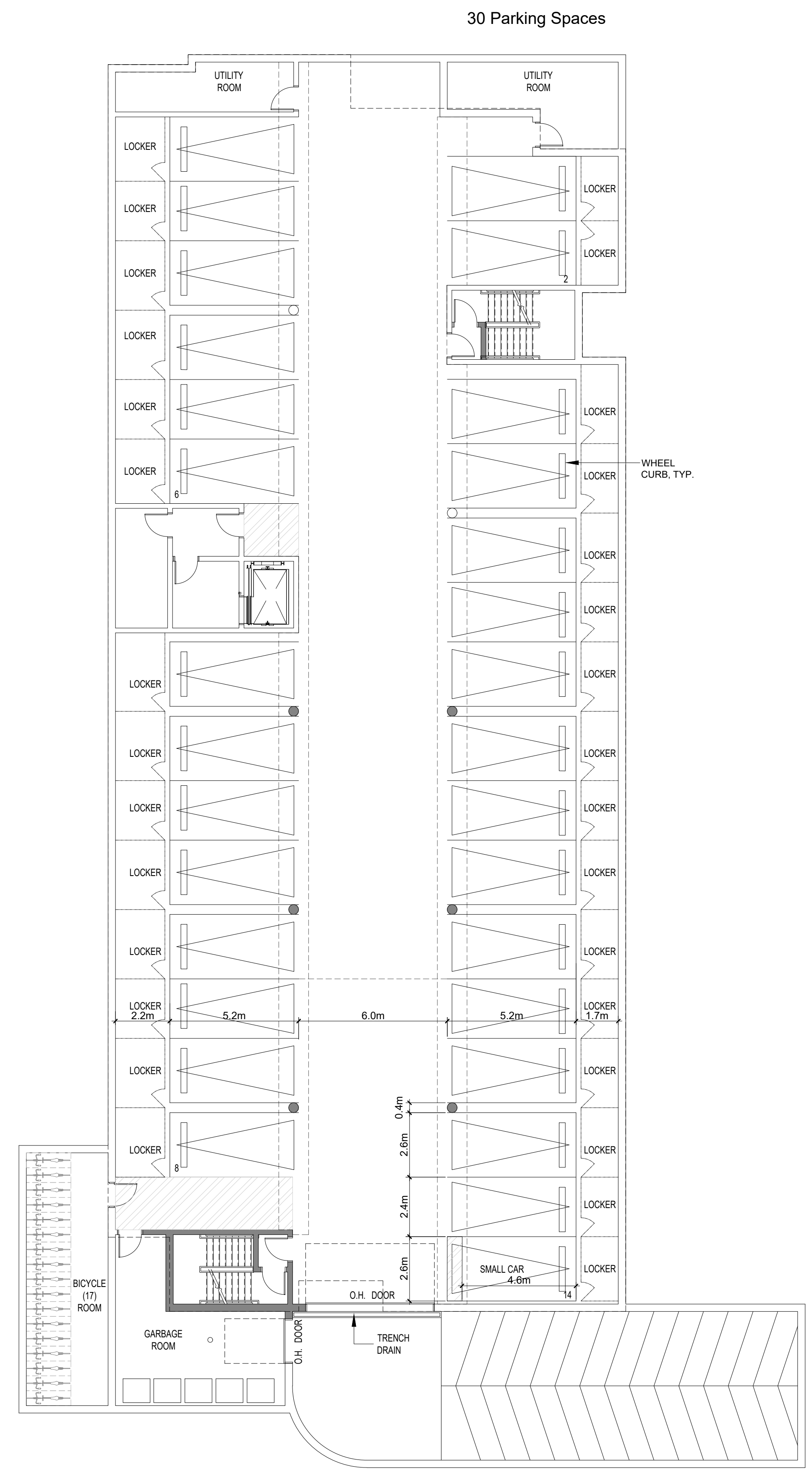
DESIGNED BY: P.E.
 DRAWN BY: P.E.
 APPROVED BY: P.E.

PROJECT: 2308 and 2396 CLEROUX CRESCENT OTTAWA

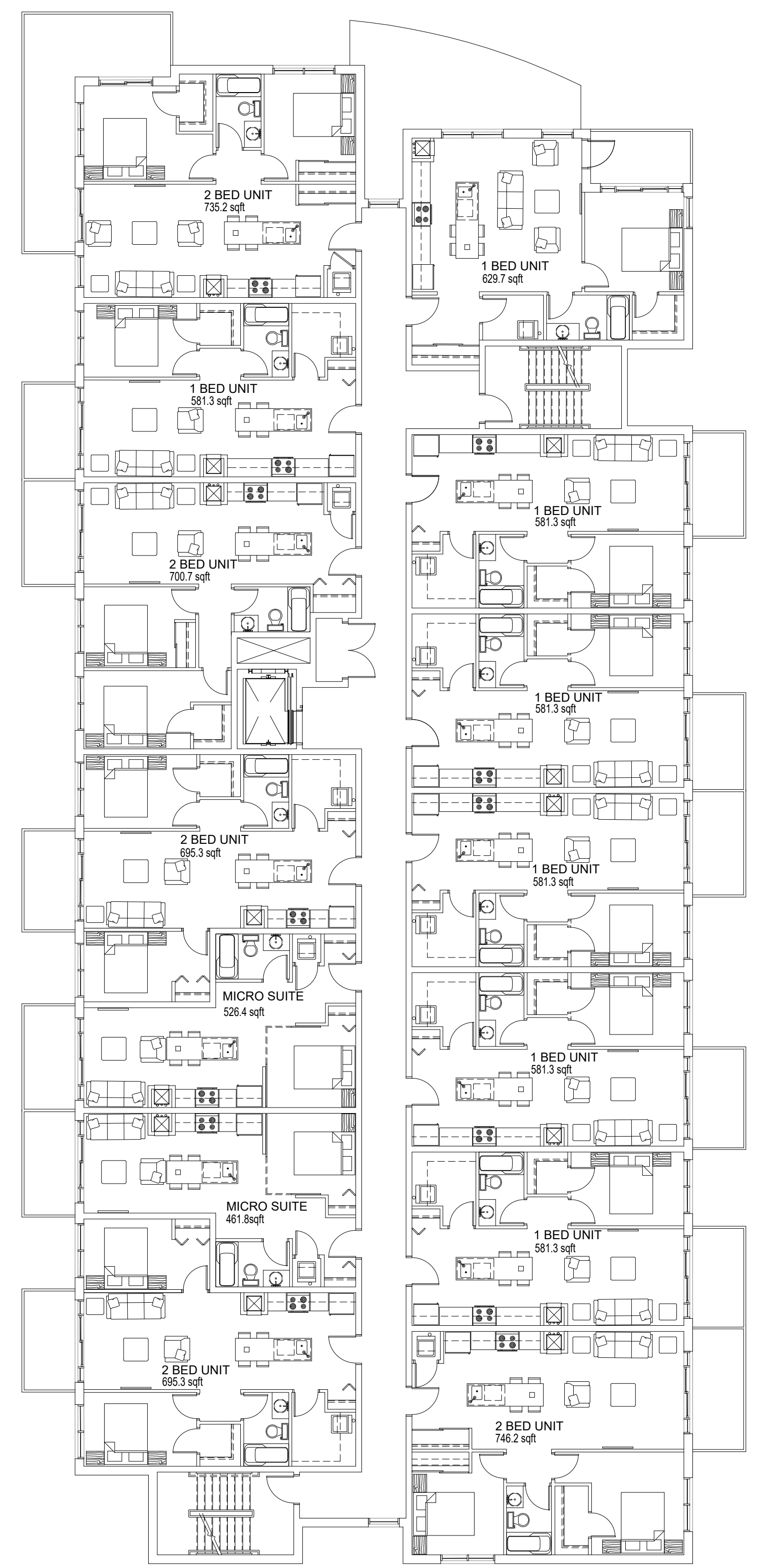
DRAWING TITLE: SITE PLAN

PROJECT NO: 0403
 DATE: NOV., 11, 2020

SP-01



BLOCK A				
UNIT TYPE	MICRO SUITE	1B	2B	TOTAL
LEVEL 1	2	4	6	12
LEVEL 2	2	7	5	14
LEVEL 3	2	7	5	14
TOTAL	6	18	16	40
	15%	45%	40%	



No.	REVISIONS	BY	DATE
13			
12			
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07			
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05			
04			
03	FOR REVIEW	PE	APR. 09 2021
02	FOR REVIEW	PE	FEB. 10 2021
01	FOR REVIEW	PE	DEC. 15 2020

NOT AUTHENTIC UNLESS SIGNED AND DATED

P2
concepts
CONSULTING ENGINEERS

STRUCTURAL
MECHANICAL
ELECTRICAL

DESIGNED BY: P.E.
DRAWN BY: P.E.
APPROVED BY: P.E.

PROJECT
**2308 and 2396
CLEROUX CRESCENT
OTTAWA**

DRAWING TITLE
**BLOCK A
FLOOR PLANS**

PROJECT NO.
0403
DATE
DEC. 11, 2020

A100

22.0M HEIGHT LIMIT

10.0m
ROOF LEVEL

THIRD LEVEL

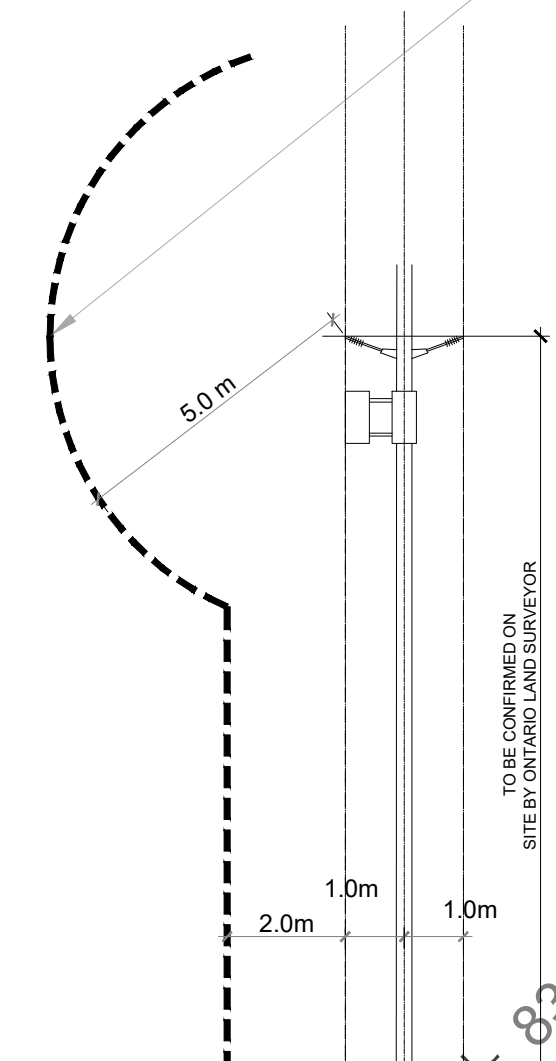
SECOND LEVEL

MAIN LEVEL

82.70

BLOCK B
PROPOSED 3 STOREY
40 UNIT
APARTMENT BUILDING

RESTRICTED ZONE



83.37

CLEROUX
CRESCENT

EAST ELEVATION BLOCK A

22.0M HEIGHT LIMIT

RESTRICTED ZONE

10.0m
ROOF LEVEL

THIRD LEVEL

SECOND LEVEL

MAIN LEVEL

LOWER LEVEL

TO BE CONFIRMED ON
SITE BY ONZAND LAND SURVEYOR

1.0m

1.0m

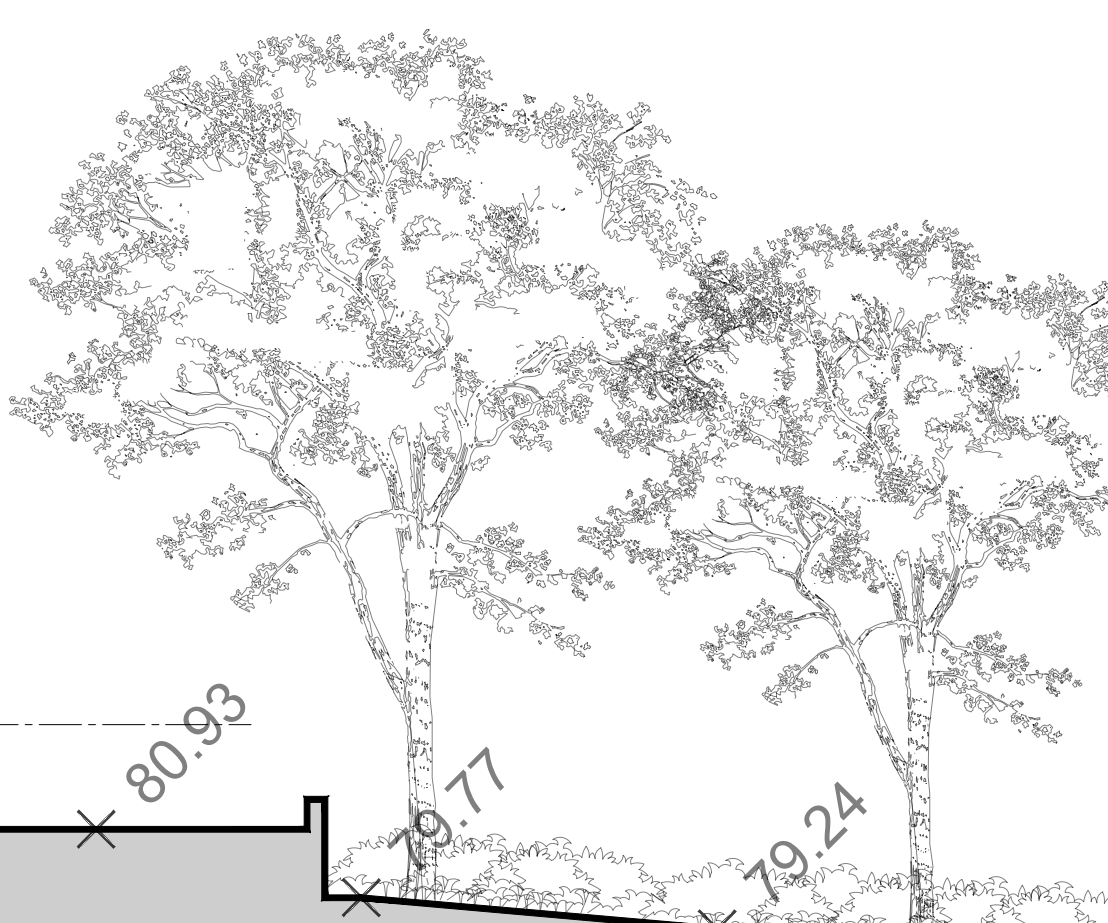
2.0m

82.55

82.70

79.70

BLOCK B
PROPOSED 3 STOREY
40 UNIT
APARTMENT BUILDING



80.93

79.77

79.24

WEST ELEVATION BLOCK B

CLIENT:



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02	FOR REVIEW	PE	APR. 09 2021
01	FOR REVIEW	PE	DEC. 15 2020
No.	REVISIONS	BY	DATE

NOT AUTHENTIC UNLESS SIGNED AND DATED



CONSULTING ENGINEERS

STRUCTURAL

MECHANICAL

ELECTRICAL

DESIGNED BY: P.E. DRAWN BY: P.E. APPROVED BY: P.E.

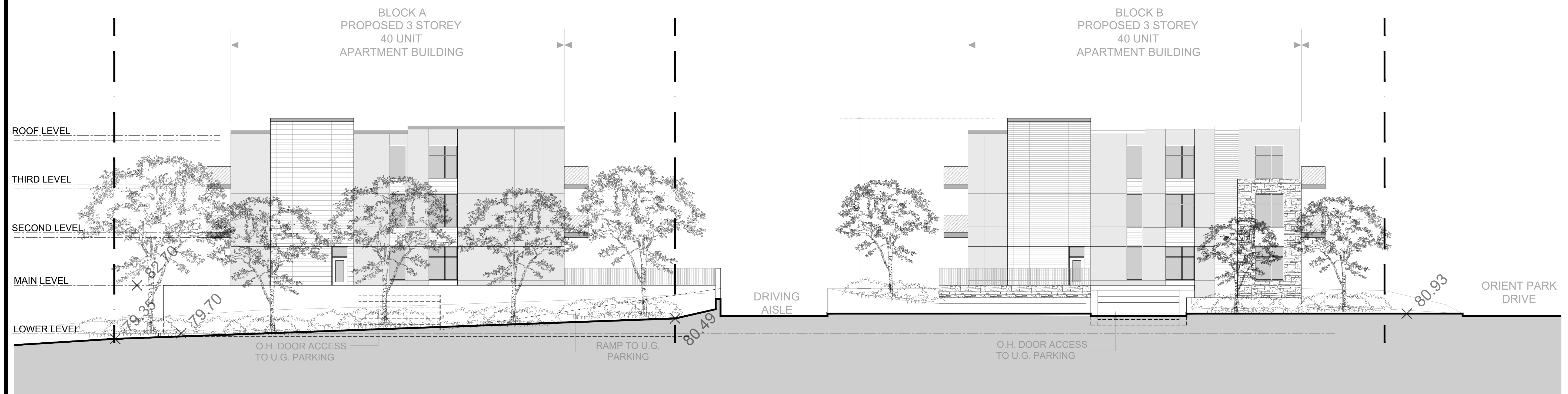
PROJECT
2308 and 2396
CLEROUX CRESCENT
OTTAWA

DRAWING TITLE
ELEVATIONS

PROJECT NO.
0403
DATE
DEC, 11, 2020

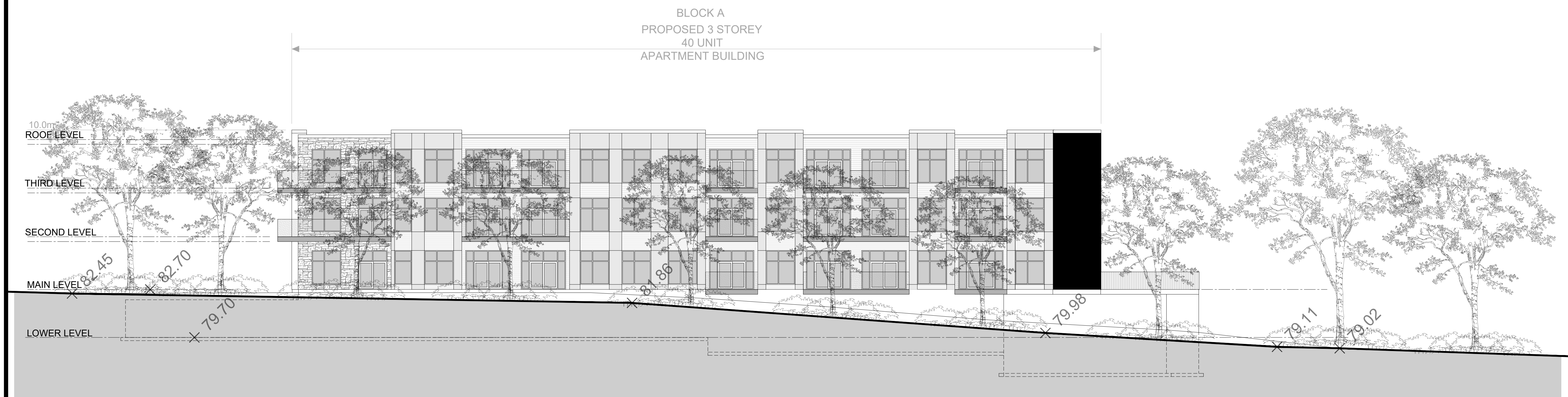
A202

22.0M HEIGHT LIMIT



REAR (SOUTH) ELEVATION

22.0M HEIGHT LIMIT



WEST ELEVATION BLOCK A

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02	FOR REVIEW	PE	APR. 09 2021
01	FOR REVIEW	PE	DEC. 15 2020
No.	REVISIONS	BY	DATE

NOT AUTHENTIC UNLESS SIGNED AND DATED



CONSULTING ENGINEERS

STRUCTURAL

MECHANICAL

ELECTRICAL

DESIGNED BY: P.E. DRAWN BY: P.E. APPROVED BY: P.E.

PROJECT

2308 and 2396 CLEROUX CRESCENT OTTAWA

DRAWING TITLE

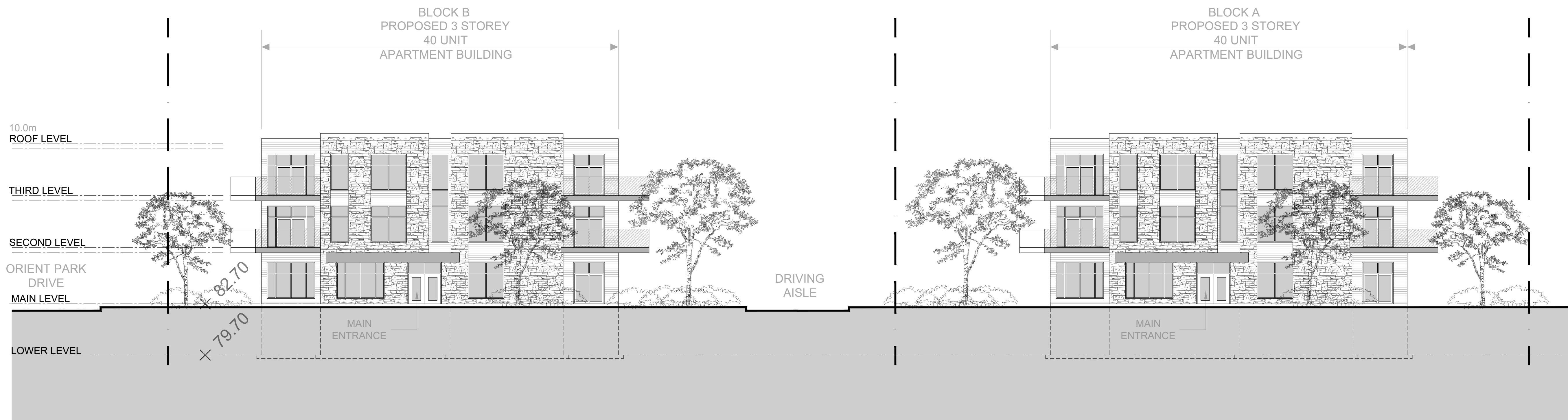
ELEVATIONS

PROJECT NO. 0403

DATE DEC. 11, 2020

A201

22.0M HEIGHT LIMIT



NORTH ELEVATION ALONG CLEROUX CRESCENT

22.0M HEIGHT LIMIT



BLOCK B EAST ELEVATION ALONG ORIENT PARK DR.

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02	FOR REVIEW	PE	APR. 09 2021
01	FOR REVIEW	PE	DEC. 15 2020
NO.	REVISIONS	BY	DATE

NOT AUTHENTIC UNLESS SIGNED AND DATED

CONSULTING ENGINEERS

STRUCTURAL

MECHANICAL

ELECTRICAL

DESIGNED BY: P.E. DRAWN BY: P.E. APPROVED BY: P.E.

PROJECT
2308 and 2396
CLEROUX CRESCENT
OTTAWA

DRAWING TITLE
ELEVATIONS

PROJECT NO.
0403
DATE
DEC, 11, 2020



1 PERSPECTIVE VIEW 1
SCALE: 1/8" = 1'-0"



2 PERSPECTIVE VIEW 2
SCALE: 1/8" = 1'-0"



3 PERSPECTIVE VIEW 3
SCALE: 1/8" = 1'-0"



4 PERSPECTIVE VIEW 4
SCALE: 1/8" = 1'-0"



5 PERSPECTIVE VIEW 5
SCALE: 1/8" = 1'-0"

CLIENT:

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02			
01	FOR REVIEW	PE	DEC. 15 2020
No.	REVISIONS	BY	DATE

NOT AUTHENTIC UNLESS SIGNED AND DATED



CONSULTING ENGINEERS

- STRUCTURAL
- MECHANICAL
- ELECTRICAL

DESIGNED BY: P.E. DRAWN BY: P.E. APPROVED BY: P.E.

PROJECT

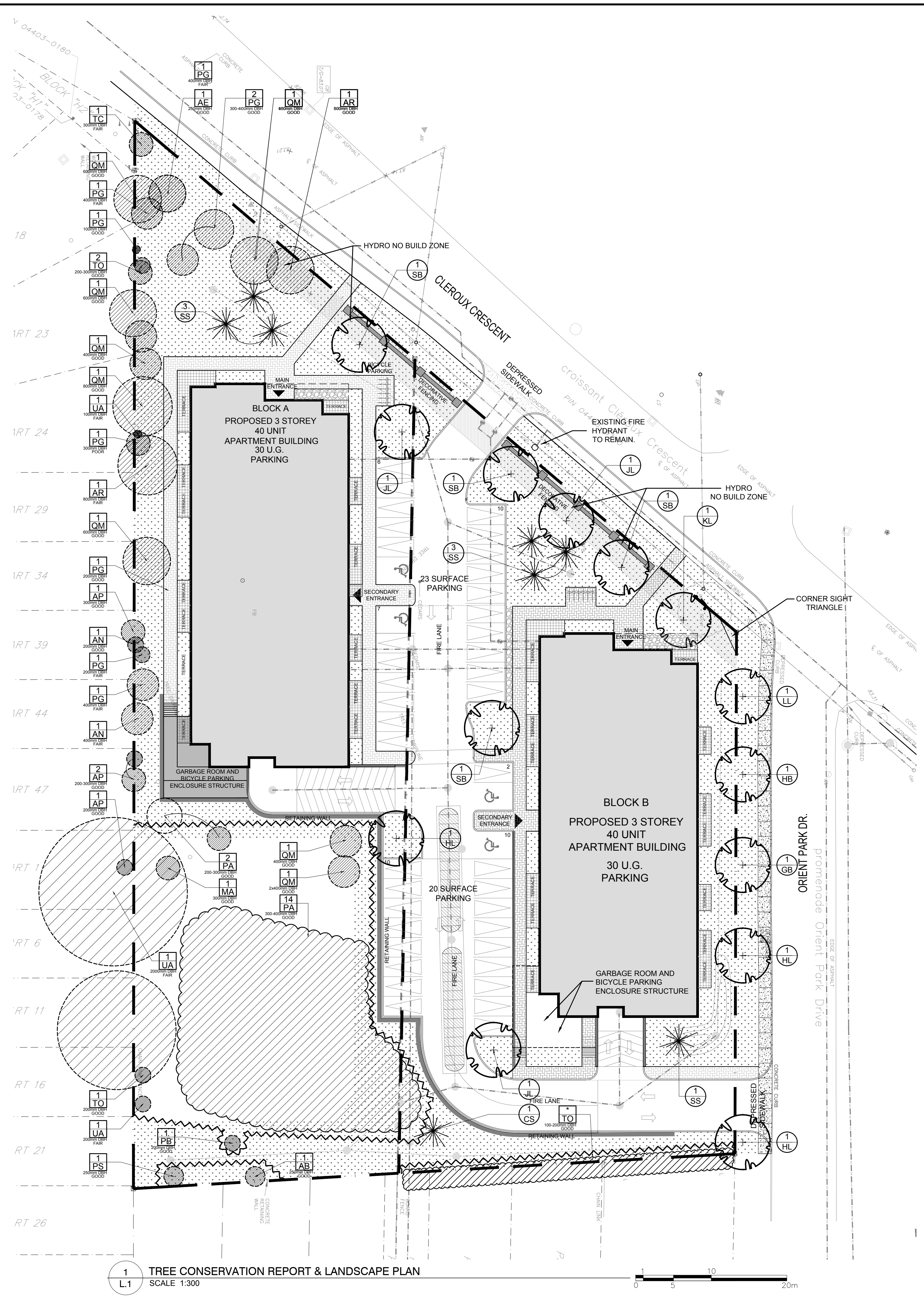
2308 and 2396
CLEROUX CRESCENT
OTTAWA

DRAWING TITLE

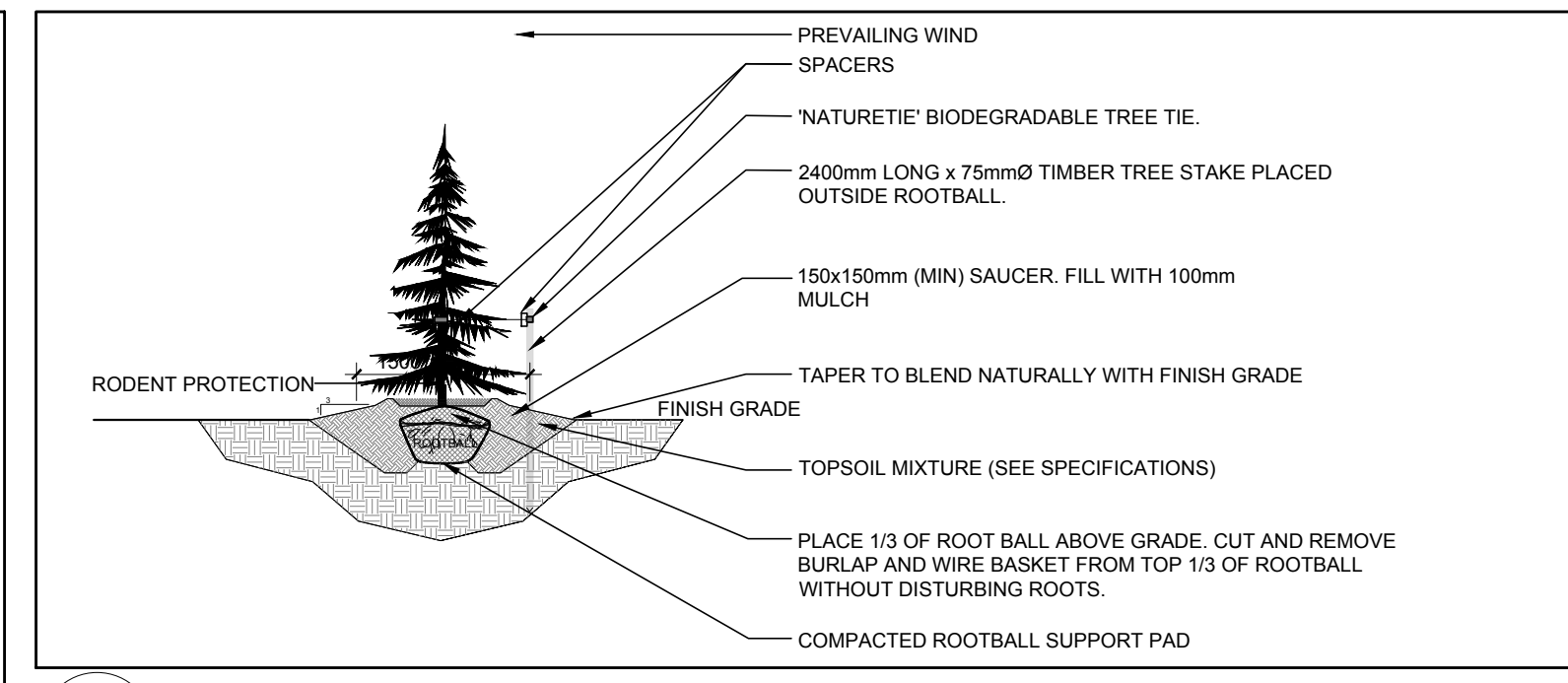
PERSPECTIVE VIEWS

PROJECT NO.
0403
DATE
APR, 09, 2021

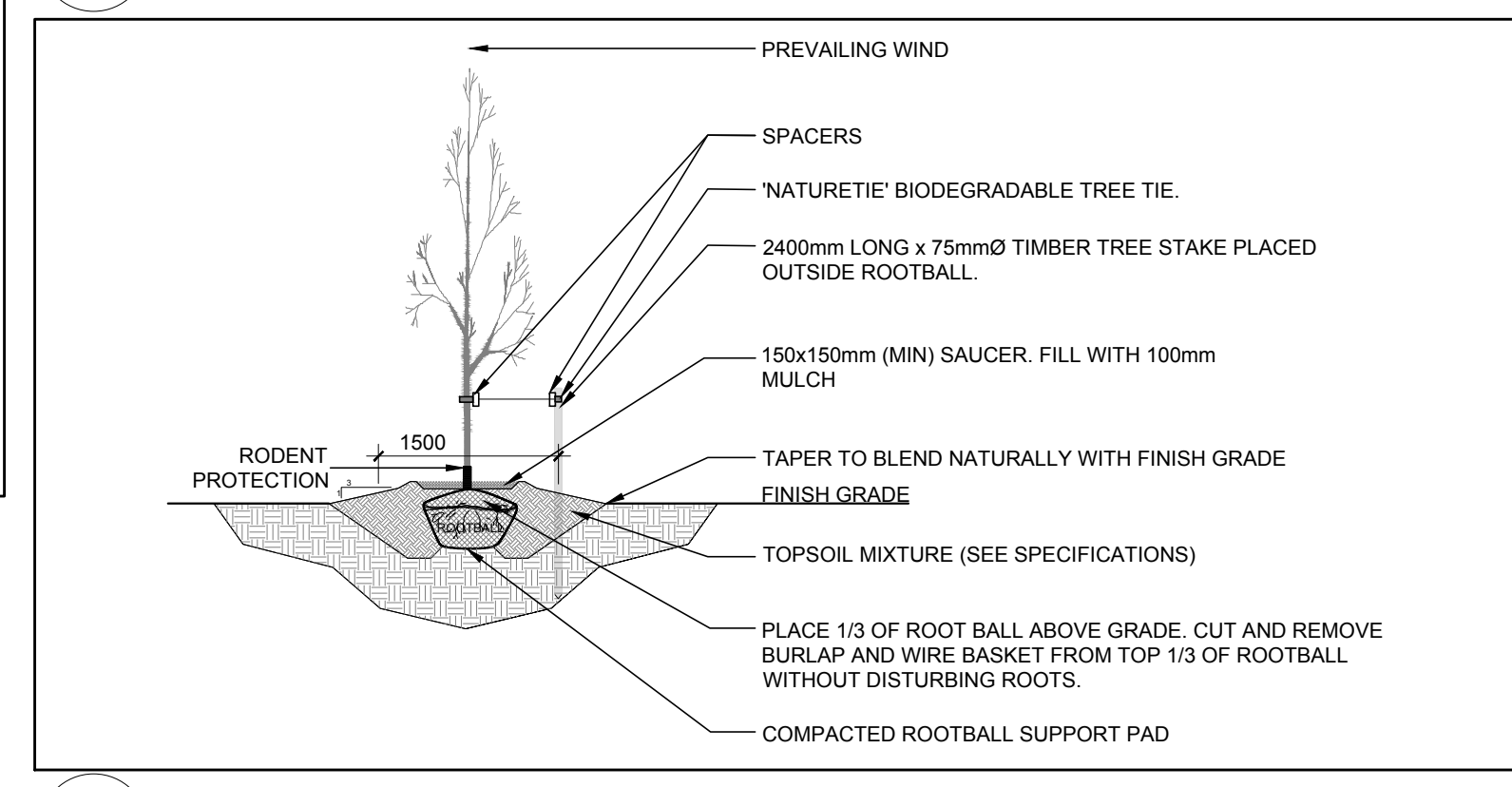
A300



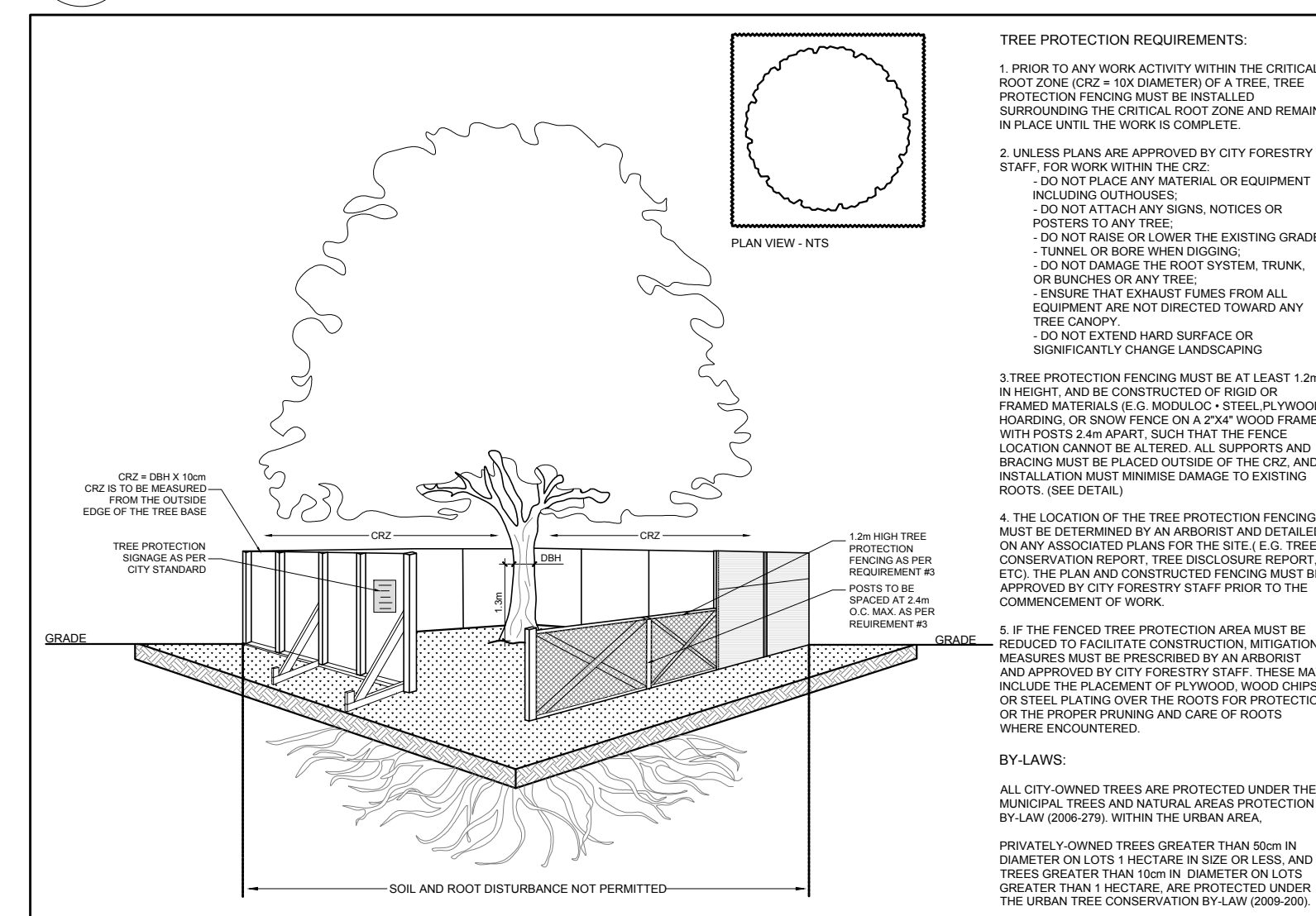
- GENERAL NOTES:**
- It is the responsibility of the appropriate contractor or official to report any errors, omissions or discrepancies on this plan with actual site conditions to the Landscape Architect before proceeding with construction.
 - The contractor is to notify all utility companies and authorities prior to any excavation and ascertain locations of underground services.
 - The contractor is to reinstate all areas and items damaged as a result of construction activity.
 - The contractor is to comply with all pertinent codes and by-laws.
 - The contractor is to maintain a positive surface run-off throughout the entire construction period.
 - The Landscape Architect is not responsible for subsurface conditions.
 - The contractor is to identify all existing trees to remain on site with the Landscape Architect prior to construction.
 - The contractor is to stake the proposed location of all plant material in conjunction with the Landscape Architect prior to excavation.
 - Minimum distances for selected deciduous trees are as follows:
 - Building Foundations 7.5m
 - Sidewalks 1.5m
 - Public Streets 2.5m
 - Underground Infrastructure 2.0m
 - All trees within 1m of underground utility trenches are to be excavated by hand.
 - Remove all protective wrapping from tree trunks after installation.
 - Staking of trees shall only be performed if necessary.
 - Ensure that mulch is pulled back a min. distance of 75mm from base of tree trunk.



2 CONIFEROUS TREE PLANTING
SCALE: NTS



3 DECIDUOUS TREE PLANTING
SCALE: NTS



4 TREE PROTECTION FENCE
SCALE: NTS

EXISTING TREE LIST

KEY	QTY	BOTANICAL NAME	COMMON NAME	SIZE	CONDITION	REMARKS
TREES						
AB	1	Abies balsamea	Balsam Fir	250mm DBH	Good	To remain
AE	1	Aesculus sp.	Chestnut	300mm DBH	Good	To remain
AN	2	Acer negundo	Manitoba Maple	200-400mm DBH	Fair-good	To remain
AR	3	Acer rubrum	Red Maple	500-800mm DBH	Fair-Good	To remain
AP	4	Acer platanoides	Norway Maple	200-400mm DBH	Good	To remain
MA	1	Malus spp.	Crabapple	300mm DBH	Good	To remain
PA	16	Picea abies	Norway Spruce	200-400mm DBH	Good	To remain
PB	1	Pinus banksiana	Jack Pine	200mm DBH	Good	To remain
PG	9	Picea glauca	White Spruce	200-400mm DBH	Poor-Good	To remain
PS	2	Pinus strobus	White Pine	250-400mm DBH	Good	To remain
QM	8	Quercus macrocarpa	Bur Oak	400-800mm DBH	Good	To remain
TC	1	Tilia cordata	Littleleaf Linden	300mm DBH	Fair	To remain
TO	3	Thuja occidentalis	Eastern White Cedar	100-300mm DBH	Good	To remain
UA	3	Ulmus americana	American Elm	100-2000mm DBH	Fair	To remain

* DENOTES NUMEROUS

PROPOSED PLANT LIST

KEY	QTY	BOTANICAL NAME	COMMON NAME	SIZE	CONDITION	REMARKS
TREES						
CS	4	Picea pungens	Colorado Spruce	1.8m Ht.	B&B	
HL	3	Gleditsia triacanthos 'Draves'	Shademaster Honey Locust	60mm cal.	B&B	
JL	3	Syringa reticulata	Japanese Tree Lilac	60mm cal.	B&B	
LL	1	Tilia cordata	Littleleaf Linden	60mm cal.	B&B	
KL	1	Syringa meyeri 'Palibin'	Dwarf Korean Lilac	30mm cal.	Potted	
SB	4	Amelanchier canadensis	Serviceberry	60mm cal.	B&B	
SS	4	Picea omorika	Serbian Spruce	1.8m Ht.	B&B	

BRIDOR Developments

CLIENT: 956 St. Augustin Rd, Ottawa, ON K1R 1W1 Tel: (613) 443-9375

CONSULTANTS: ARCHITECTS: **P2 concepts** 739 RIDGEWOOD AVE., UNIT 201 OTTAWA, ONTARIO, K1V 6A8

CIVIL ENGINEERS: **BLANCHARD LETENDRE ENGINEERING** 787 Notre-Dame St. Suite 42 Ottawa, ON K1R 1W1 Tel: (613) 693-0700

LEGEND

- PROPOSED PRECAST CONCRETE PAVERS TYPE I
- PROPOSED PRECAST CONCRETE PAVERS TYPE II
- PROPOSED DECIDUOUS TREE
- PROPOSED CONIFEROUS TREE
- PROPOSED SOD
- PROPOSED RIVERSTONE MULCH
- EXISTING TREE TO REMAIN
- GROUP OF EXISTING TREES TO REMAIN
- EXISTING TREE TO REMAIN
- TREE PROTECTION FENCE

No.	Issue	Date	ML	JL
1	ISSUED FOR SITE PLAN CONTROL	05/25/2021	ML	JL

JAMES B. LENNOX & ASSOCIATES INC. LANDSCAPE ARCHITECTS
3332 CARLING AVE. OTTAWA, ONTARIO K2H 5A8
Tel. (613) 722-5168 Fax. (613) 343-3942

PROJECT: 3 STOREY APARTMENT BUILDINGS
2308 & 2396 CLEROUX CRESCENT, OTTAWA ON

DRAWING: TREE CONSERVATION REPORT & LANDSCAPE PLAN

SCALE: AS SHOWN
START DATE: FEBRUARY 2021
PROJECT NO.: 21MIS2123

DRAWING NO.: **L.1**
PLOT SIZE ARCH-D