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Planning Rationale – Minor Rezoning Application

2983-3052-3079 Navan Road Development



Planning Rationale – Minor Rezoning Application

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Planning Rationale – Minor Rezoning Application

1.0 Introduction

1.1 Background

This Planning Rationale has been prepared by J.L. Richards & Associates Limited (JLR) on behalf of our client, 12714001 Canada Inc. and H&H Gas Orleans Inc., in support of the Minor Rezoning Application for Blocks 15, 16 and 17 of the larger subdivision application at 2983-3052-3079 Navan Road, which is found in Chapel Hill South of Orleans, in the City of Ottawa’s east end. See Figure 1 for the site location.

The land is currently a draft approved plan of subdivision and pre-servicing site works for the overall subdivision are underway. The lands are designated Suburban as shown in the City of Ottawa Official Plan Appendix A (approved November 4, 2022). Further, the subject property is designated Evolving Neighbourhood within the Suburban Transect as seen in the City of Ottawa Official Plan Suburban (East) Transect, Schedule B8.

Block 15 and 17 were subject to Zoning By-law Amendment No. 2024-378 (Area A and B shown on Figure 2), which rezoned Block 15 from GM[2546] H(14.5) to GM[2974] H(16.0) and Block 17 from DR to GM[2546] H(14.5). Block 16 remains zoned as GM[2546] H(14.5); this exception permits the car wash and gas station. Table 1 provides a summary of the changes that are being requested.

Block 15 is located at the north-eastern side of the site (see Figure 1) and consists of two low-rise (four story) buildings (C & D), Building D being residential and Building C being mixed-use with commercial space at-grade. Block 15 is currently in the Site Plan Application process (D07-12-24-0129) and has been deemed complete by the City. In reviewing the supporting studies and plans, it was noted that the proximity of the garbage removal pad to Brian Coburn Boulevard does not meet the by-law’s requirements. As such, an amendment to the by-law is required for the site plan application to comply and be approved. The intent is to increase the proposed buffering to provide additional screening in support of the requested reduction.

Block 16 is located at the eastern side of the site (see Figure 1) and consists of a gas station, commercial building, and car wash. Block 16 is currently in the Site Plan Application process (D07-12-24-0119) and has been deemed complete by the City. In reviewing the supporting studies and plans, it was noted that the car wash does not meet the setback requirements for abutting a lot with a residential use. As such, an amendment to the by-law is required for the site plan application to comply and be approved. Similarly, use of a noise fence and increased landscaping will be utilized to buffer the yard and the noise has all been directed away from the proposed residences.

Block 17 is located at the south end of the site (see Figure 1) and consists of two low-rise residential buildings (Buildings E & F). Block 17 is currently in the Site Plan Application process (D07-12-24-0120) and has been deemed complete by the City. In reviewing the supporting studies and plans, it was noted that the setback between the parking area and the pond at the north of the site does not comply to the minimum landscape requirements in the by-law. As such, an amendment to the by-law is required for the site plan application to comply and be approved. The rear yard provision also requires a small reduction, of 0.5 metres to permit the proper development of the internal access to the underground parking structure and to allow for the development of this garage footprint.

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Table 1 Rezoning Table

Navan Road Minor Rezoning Table City of Ottawa Zoning By-law 2008-250 Exceptions 2546 as amended; 2974; 2975				
Block # Reference	By-law Section	Required	Provided	Relief Requested
15	110(3)(a)	All outdoor refuse collection and refuse loading areas contained within or accessed via a parking lot must be located at least 9.0 metres (m) from a lot line abutting a public street.	The garbage removal pad at 6579mm (6.579m) from the lot line.	Relief of 2.5m is required.
15	Table 187(e)(iii)	Minimum rear yard setback for a residential use building is 7.5m	The rear yard at Building D has a setback of 4.5m.	Relief of 3.0m is required.
16	Exception 2546	Minimum required setback for all yards abutting a lot containing a residential use is 7.5m	The car wash is less than 3.0m from the rear lot line abutting a mixed-use zone containing residential buildings.	Relief of 4.5m is required.
17	110(1) Table 110(III)	Landscape buffer of 1.5m required for parking lots with 10 – 100 parking spaces.	Buffer is 0.5m	Relief of 1.0m to allow for 0.5m buffer for area abutting the storm pond.
17	Table 187 (e)(iii)	Minimum rear yard setback for a residential use building is 7.5m.	The rear yard setback (between Building F and Page Road) is 7.0m.	Relief of 0.5m required.

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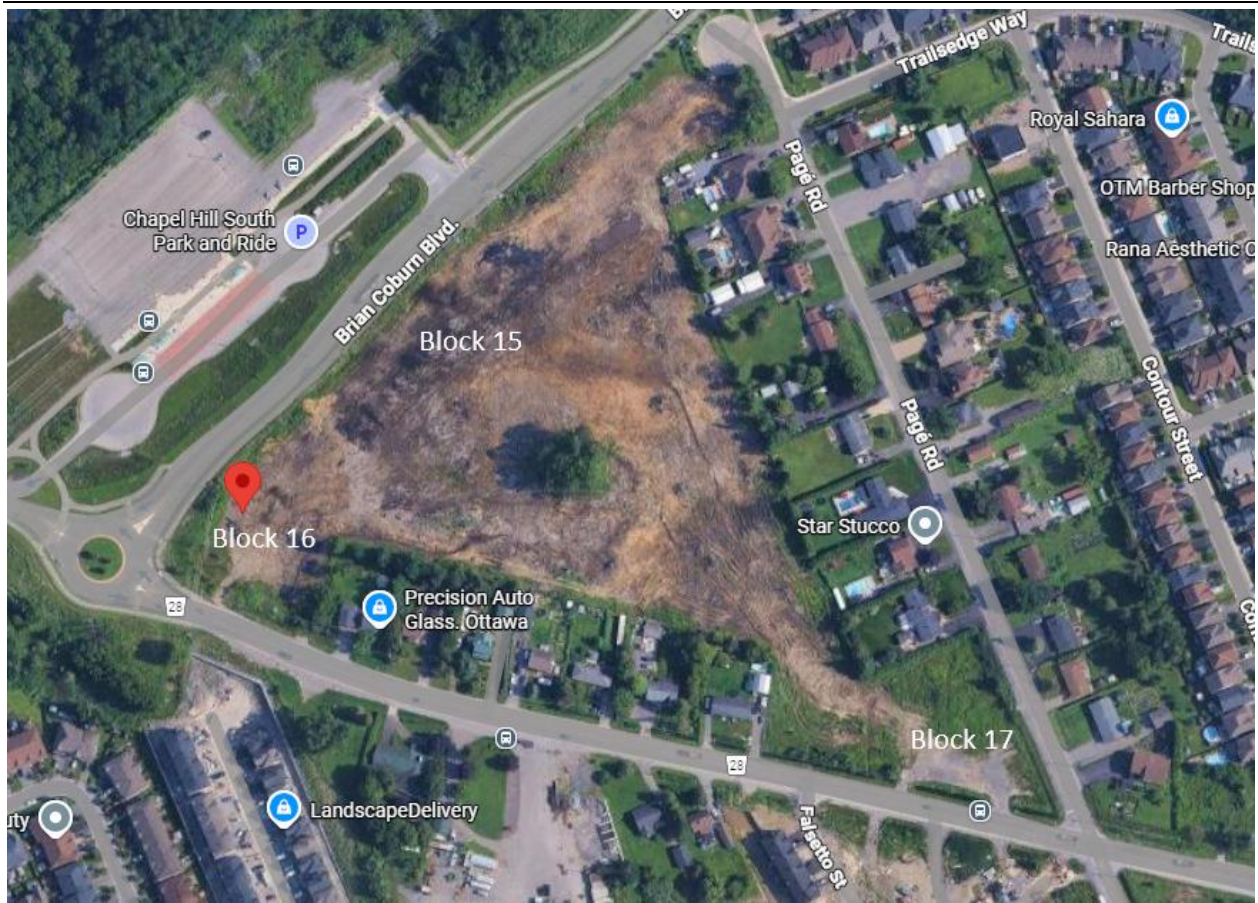


Figure 1 Aerial View of Site and Approximate Location of Blocks

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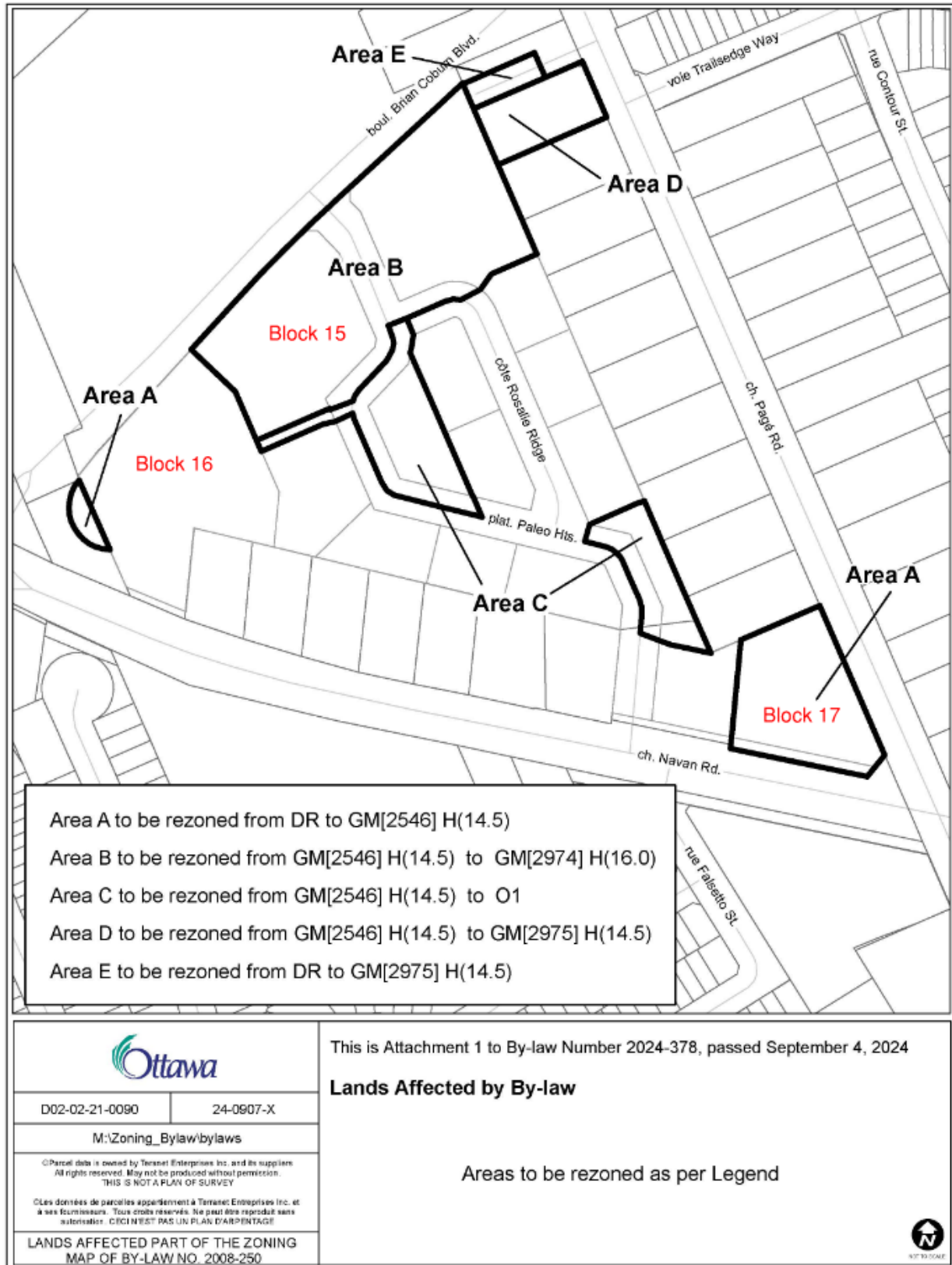


Figure 2 Zoning By-law Amendment No. 2024-378 Key Map

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1.2 Site Conditions and Surrounding Context

The existing conditions of the subject property can be summarized as follows:

- The lands are vacant and undeveloped.
- The lands are mostly covered in trees.
- The abutting public realm (e.g. multi-use path, street lighting) is constructed.



Figure 3 Surrounding Land Uses

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1.3 Transportation Network

The City of Ottawa Official Plan – Schedule C4 – Urban Road Network, shows that Brian Coburn Blvd. and Navan Road are both Arterial Roads (Figure 4). There is an existing multi-use pathway on Brian Coburn Blvd.



Figure 4 City of Ottawa Official Plan – Schedule C4 – Urban Road Network.

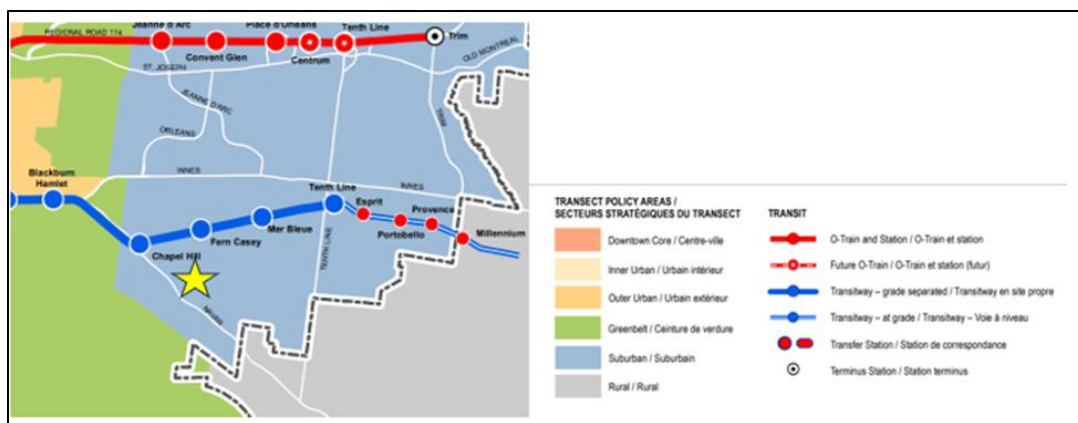


Figure 5 City of Ottawa Official Plan Schedule A showing subject property approximate location within the Suburban area.

As shown in Figure 5 above, the subject property is located in the Suburban Transect Policy Area. The map shows proximity to transitways and other policy areas including the Greenbelt and Rural lands.

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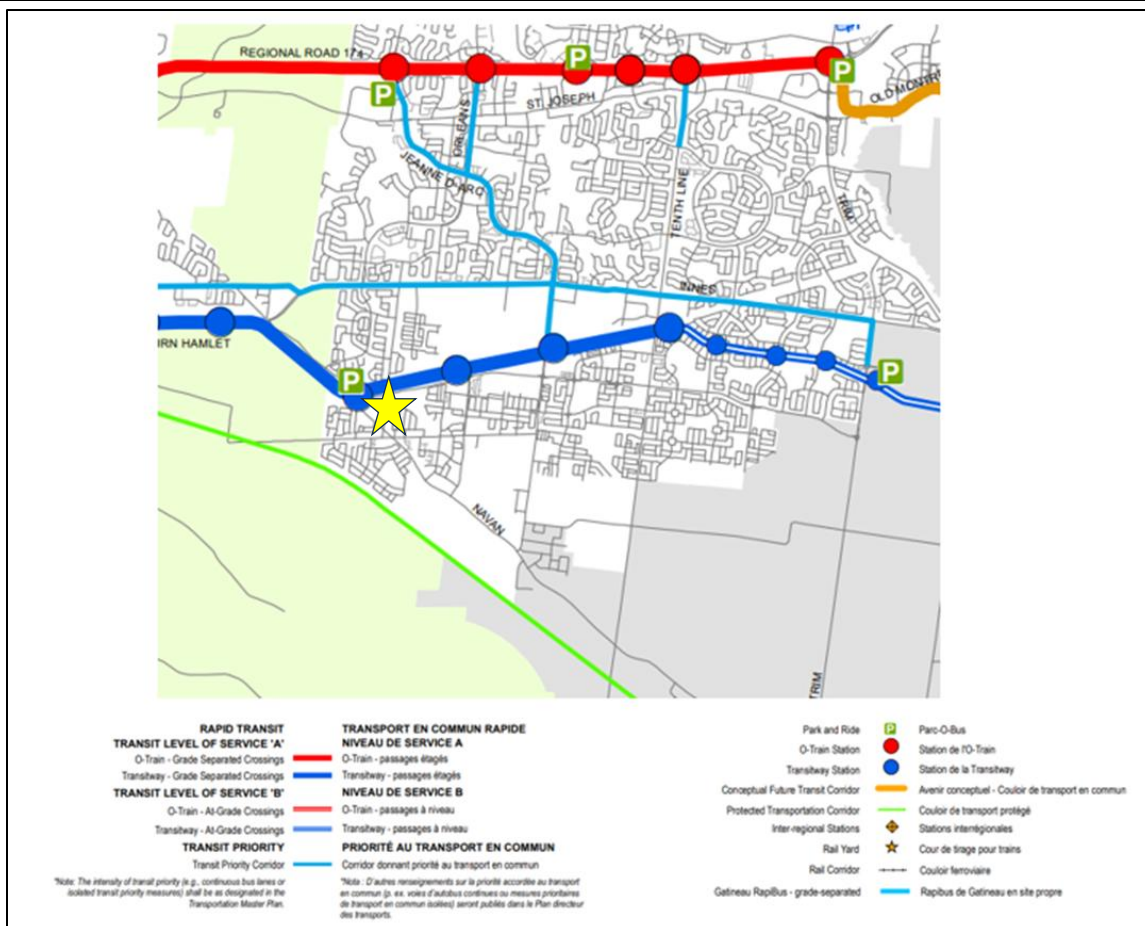


Figure 6 City of Ottawa Official Plan Schedule C2 Transit Network.

1.4 Conforms to the Official Plan

The proposed amendments conform to the Official Plan including policies for development in the east transect as per policy 5.6.1 which includes the following: *“allow new built forms and more diverse functions of land. The intent is to identify areas that may gradually evolve through intensification to a more urban than suburban built form”*. Policy 2 also makes it clear that:

“2) Where an Evolving Neighborhood Overlay is applied:

a) *The Zoning By-law shall provide development standards for the built form and buildable envelope consistent with the planned characteristics of the overlay area, which may differ from the existing characteristics of the area to which the overlay applies...*”

1.5 Consistent with the Intent of the 2024 Provincial Planning Statement (PPS)

Consistent with intent of the 2024 PPS where the Province has made it clear through Section 2, Policy 2.1.4 municipalities are to provide for: *“an appropriate range and mix of housing options and densities required to meet projected requirements of current and future residents of the regional market area”*. The implementation of this minor zoning amendment will facilitate the implementation of the intent of the PPS.

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1.6 Conforms with the Planning Act

It is our opinion that the approval of this minor rezoning would conform to the requirements of Section 34 of the Act.

2.0 Block 15

2.1 Site Context and Proposed Development

The plan of subdivision is approximately 5.5 hectares (~13.6 acres) of vacant, developable land. Of this, approximately 5,398.92m² (0.54ha) represent the lands for Block 15. The entire subject property fronts two (2) streets, Brian Coburn Boulevard and Navan Road. Access to Block 15 will be from the newly constructed Paleo Heights off Brian Coburn Boulevard. This block has 3 sides that abut streets as Paleo Heights wraps around the site, making it rather unique. The site also abuts the future commercial block.

Block 15 proposes two low-rise (4-storey) apartment buildings, one being mixed use (Building C) and one being all residential (Building D). Building C proposes 36 residential units with commercial space at-grade, and Building D proposes 45 residential units (see Figure 7).

2.2 Required Zoning By-law Amendment

The proposed development on Block 15 includes a shared surface parking area located between two low-rise residential buildings, accommodating 32 parking spaces and a designated garbage removal pad situated at the northeast end of the block (see Figure 8). The garbage removal pad measures 4.41 m by 7.40 m, and the associated site plan illustrates that it is positioned 6.579 m from the lot line abutting the public street.

Section 110(3) of the Zoning By-law requires garbage removal pads to maintain a minimum setback of 9.0 m from any lot line adjacent to a public street. To facilitate the proposed layout, an amendment to the Zoning By-law is requested to permit a reduced setback of 6.579 m, representing a relief of 2.421 m from the required standard.

Although the pad does not meet the full setback requirement, its placement and design ensure that it remains appropriately screened and visually unobtrusive. The Landscape Plan prepared by James B. Lennox & Associates (Figure 9) demonstrates that substantial landscaping, including dense shrub planting, will buffer the enclosure from the adjacent right-of-way. In addition to vegetative screening which consists of over twenty-five shrubs and 3 medium-large sized deciduous trees, a 2.1-m-high wood privacy fence (Figure 10) will fully enclose and shield the garbage area. This fencing provides both functional protection from weather and effective visual screening for users of Brian Coburn Boulevard and the multi-use pathway located between Block 15 and the public street. This siting also ensures that the garbage pick up is done on the private property and does not require use of the street.

The combination of the proposed 6.579 m setback, robust landscape treatment, and high-quality screening features ensures the garbage enclosure will have minimal visual or functional impact on the surrounding public realm. The intent of the Zoning By-law, which is to reduce negative aesthetic and environmental effects associated with waste storage near public streets, is fully maintained. As such, it is our opinion that the requested reduction in setback is consistent with

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the PPS, conforms to the Official Plan and represents good land use planning and supports the orderly development of Block 15.

Here is an expanded, professional (but not overly formal) version of your rationale, written as a single cohesive narrative:

In addition, the rear yard setback shown on Figure 11 was initially designed based on Table 187(e)(i), which permits a 3.0 m rear yard setback for a mixed-use building. At the time, this was an appropriate interpretation because the block is to be developed as a mixed-use development. However, after refining the building types and confirming that Building D is entirely residential, it became clear that it is equally possible to read the by-law to require the application of the standard for a residential rear yard setback of 7.5 m (Table 187(e)(iii) in the by-law). When this is applied to the site, only 4.5 m of rear yard depth can be achieved, resulting in a difference of 3.0 m from the by-law requirement. As a result, relief is required through a zoning amendment to recognize this reduced setback. The site plan shown as Figure 7 shows that the corner of Paleo Heights is 112 degrees, making it a Corner Lot as per the definition in the by-law.

Although the numerical deficiency is greater than originally anticipated, the site context supports allowing this reduction. The rear yard abuts a commercial property (Block 16), not a residential use, and the two sites are already separated by a noise fence and substantial landscaping. The completed noise study confirms that the recommended mitigation measures are effective and that the commercial use will not create adverse impacts on the residential units. With this combination of physical separation, fencing, landscaping, and verified noise attenuation, the reduced setback does not compromise the livability of the proposed units or the compatibility between the two blocks.

Given the nature of the adjoining land use, the existing screening, and the technical evidence showing no negative effects, the reduced rear yard setback continues to meet the intent of the Official Plan and the mixed use nature identified in the Zoning By-law. The reduced yard provides an appropriate interface between the residential building and the neighbouring commercial site while supporting an efficient and functional site layout.

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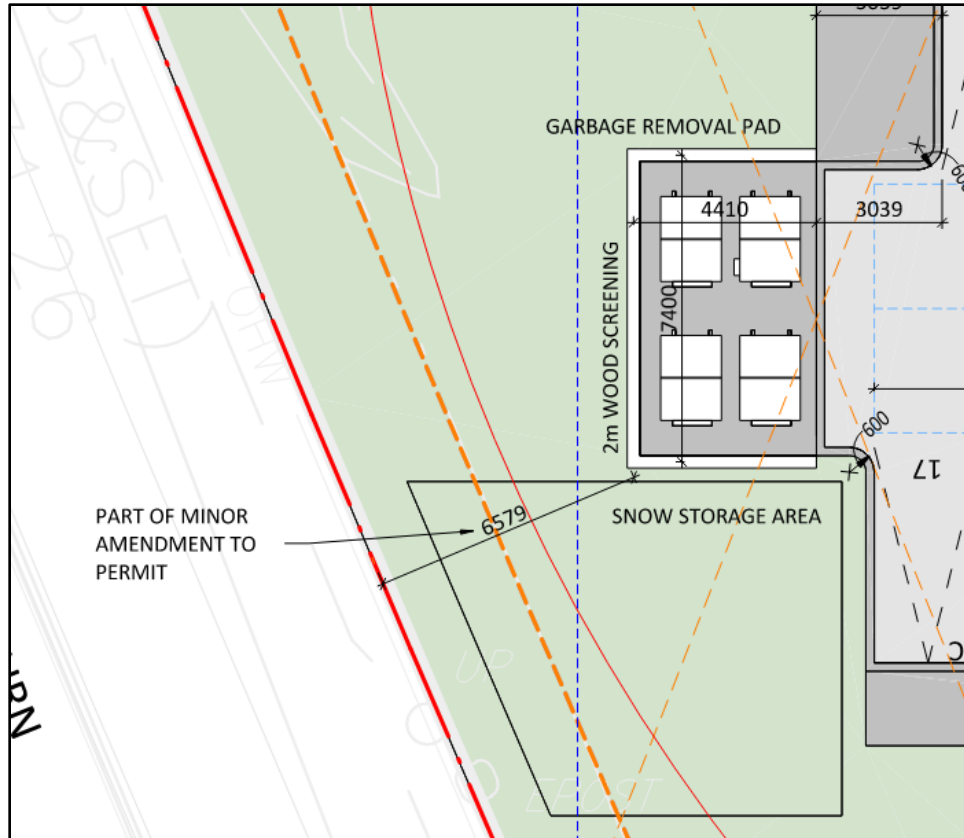


Figure 8 Block 15 Garbage Removal Pad

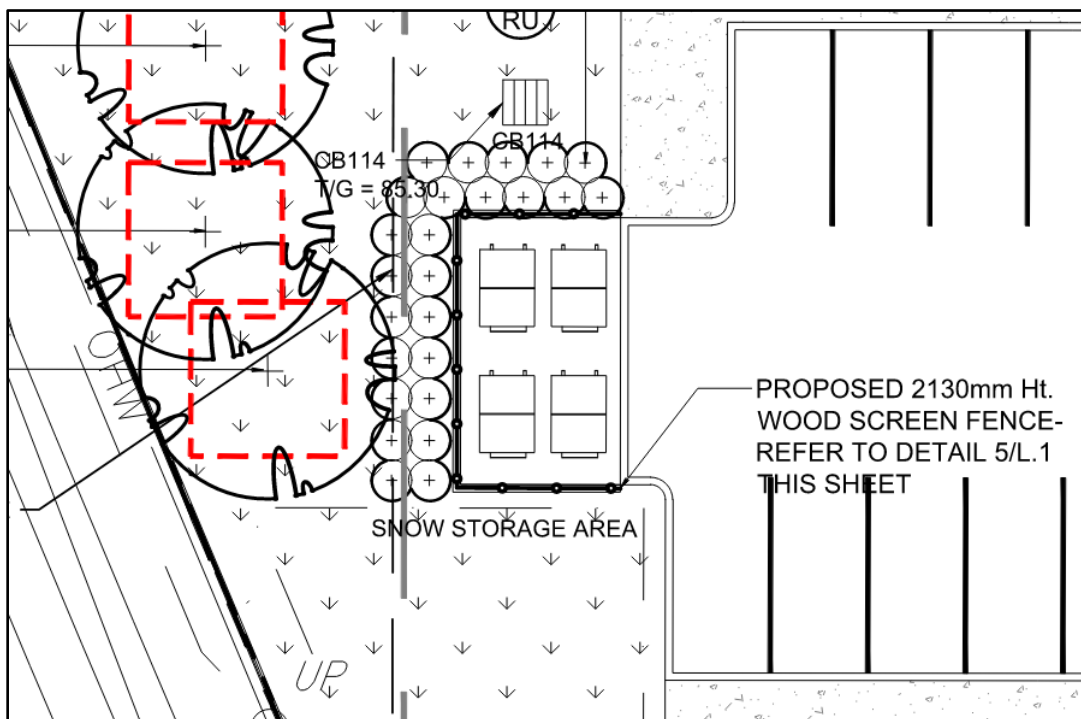


Figure 9 Block 15 Garbage Removal Pad Landscaping and Screening

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3.0 Block 16

3.1 Site Context and Proposed Development

The plan of subdivision is approximately 5.5 hectares (~13.6 acres) of vacant, developable land. Of this, approximately 1.93 acres (~7,717 sq. m) represent the lands for Block 16. The subject property fronts two (2) streets, Brian Coburn Boulevard and Navan Road. Block 16 is zoned for commercial use and contains a gas station, a commercial building with a convenience store and drive-through restaurant, as well as a car wash (see Figure 11).

The details of the built footprint include:

- Commercial Building: 687.4sm / 7,386sf
- Carwash Building: 112.9sm / 1,215sf
 - Total Gross Area: 800.3sm / 8,614sf

3.2 Required Zoning By-law Amendment

The car wash is located adjacent to a lot containing residential use and is therefore subject to the setback requirements established through Exception 2546 of the Zoning By-law. Exception 2546 stipulated that any yard abutting a lot containing a residential use must provide a minimum setback of 7.5m.

The development of the abutting residential block will provide a rear yard of 7.5m. This additional depth of rear yard will also provide for additional separation from the car wash. This residential block is to be developed by the owner as residential rental units.

As illustrated in Figure 12, the proposed car wash provides a setback of 3.03m (3031 mm) from the shared property line. Therefore, a reduction of 4.5m is required to satisfy the 7.5m setback requirement. Despite the numerical reduction, several functional and protective design elements have been incorporated to ensure compatibility with the adjacent residential use:

- A 3-metre landscaped area is provided between the car wash building and the lot line. This landscaped strip offers visual softening, assists in screening the use, and contributes to an enhanced interface between the commercial and residential properties.
- A 2.5-metre-high noise barrier will be installed along the shared property boundary. A stationary noise study completed for the commercial block identified this barrier height as the appropriate mitigation measure required to adequately protect residential receptors from noise generated by the car wash and other commercial activities.
- The combination of the 3-metre setback, landscaping, and engineered noise barrier represents a comprehensive mitigation strategy that effectively addresses potential impacts related to noise, visual intrusion, and general compatibility.

Given these measures, the functional separation between the residential lot and the car wash is maintained, and the intent of the Zoning By-law's 7.5-metre setback requirement — namely, to ensure an appropriate buffer and minimize adverse impacts — is achieved through alternative but equally effective means. The requested setback reduction therefore represents good planning practice, maintains compatibility with surrounding land uses, and does not result in any

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unacceptable impacts on the adjacent residential property. It is our opinion that this minor change is consistent with the intent of the PPS and conforms to the Official Plan.

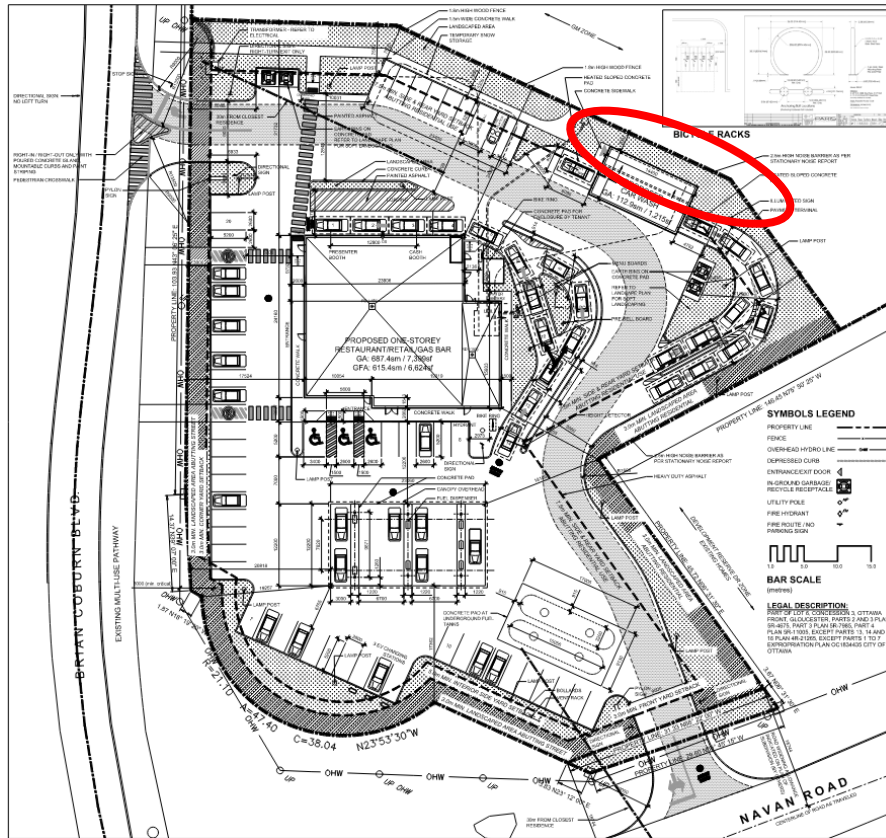


Figure 12 Block 16 Site Plan

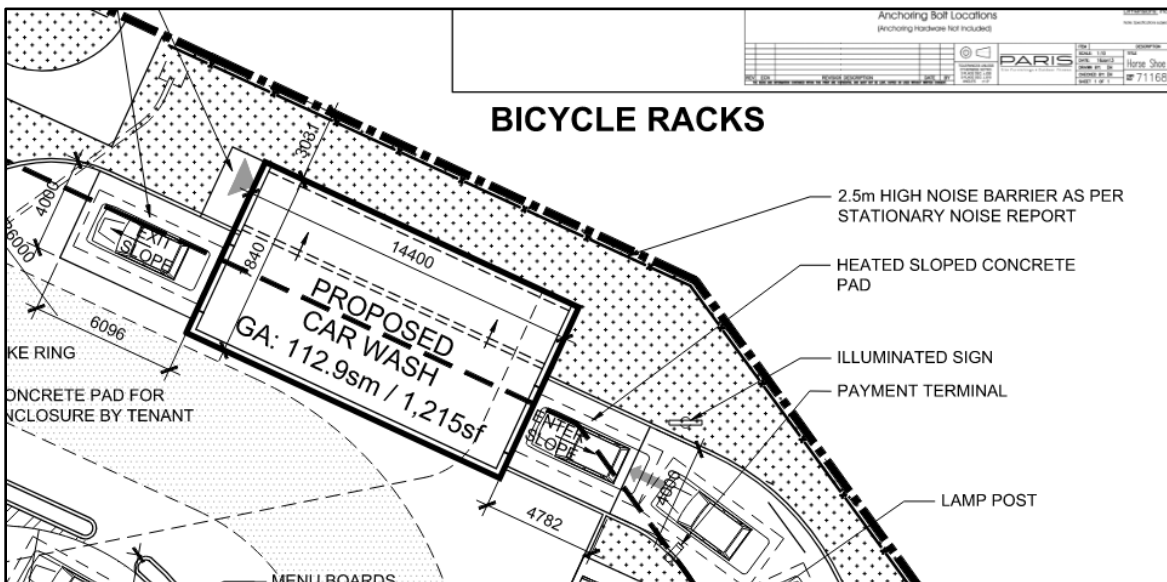


Figure 13 Car Wash Details

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4.0 Block 17

4.1 Site Context and Proposed Development

The subject lands, Block 17, are proposed to have two low-rise residential buildings each consisting of 48 units, building E and F (see Figure 13). The lands are a through corner lot at the intersection of the existing Navan Road and Page Road, with access off of the newly constructed Paleo Heights. There is a shared surface parking lot with 15 parking spaces as well as an additional 100 underground parking spaces.

4.2 Required Zoning By-law Amendment

The surface parking area on Block 15 is situated directly adjacent to the lot line that abuts a stormwater management pond on the neighbouring property. As illustrated in Figure 14, the parking area is separated from the lot line by a 0.5 m landscaped buffer. Section 110(1), Table 110(III) of the City's Zoning By-law requires a minimum 1.5 m landscaped buffer for parking lots containing between 10 and 100 spaces. The proposed 0.5 m buffer therefore represents a deficiency of 1.0 m, requiring relief through a zoning amendment.

In this case, a reduced landscape buffer is appropriate and justifiable due to the unique context of the abutting land use. The adjacent property contains a stormwater management pond—an engineered feature designed for water quantity and quality control—where no buildings, active recreation, or private development can occur. Because the abutting use is environmental infrastructure rather than a sensitive land use such as residential or commercial frontage, the interface does not pose the same compatibility concerns typically addressed through landscaped buffering. There is no expectation of pedestrian activity, privacy, noise attenuation, or aesthetic screening on the pond side of the interface.

Furthermore, stormwater ponds typically include their own substantial vegetative cover, berming, and grading, which inherently soften visual impacts and provide a naturalized edge condition. As a result, the functional purpose of the landscape buffer which is to mitigate visual, noise, and compatibility impacts, is largely achieved by the existing stormwater feature itself. Maintaining the full 1.5 m buffer would not materially improve the quality of the interface between the parking lot and the pond, nor would it enhance public or private realm experience.

In addition, the reduced buffer supports efficient site design and optimal land use. Providing the full 1.5 m buffer would require shifting the parking area, reconfiguring internal circulation, both of which may compromise accessibility, parking supply, or internal site functionality without providing a corresponding planning benefit. The proposed buffer maintains a clear separation between the hard surface and the lot line, while the pond provides an additional and more substantial buffer beyond the property boundary. The alternative would be to move the building closer to Navan Road, which is not recommended as Navan is an Arterial Road with buses. The decreased separation would not be recommended due to the potential for increased noise and lack of usable space between the building and the future wide roadway.

Overall, the requested reduction aligns with good planning practice because:

- The abutting land cannot be developed, eliminating concerns of incompatibility or adverse impacts.
- The stormwater pond already provides natural visual screening and separation, reducing the functional need for a larger planted buffer.

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- The reduced buffer does not create negative environmental or aesthetic impacts, as the character and function of both properties remain intact.

Maintains a good separation from an arterial road.

- The amendment enables efficient site layout without compromising safety, drainage, or the broader intent of the Zoning By-law, which is to minimize impacts between parking areas and sensitive land uses.

In addition, it has been noted that the rear yard setback for Block 17 (see Figure 15), is at 7.1m, whereas the by-law requires 7.5m rear yard setback for a residential use building (Table 187 in the Zoning By-law). As such, 0.5m relief is being requested to permit a reduced rear yard setback of 7.0m for a residential use building.

The rear yard setback for Block 17 is currently shown at approximately 7.1 m, whereas the Zoning By-law requires 7.5 m for a residential building. As the lot line borders Page Road, we're requesting a modest 0.5 m reduction to recognize a rear yard setback of 7.0 m. This is a very minor adjustment that does not affect how the building fits into its surroundings or how it relates to the public street.

Because the rear yard faces Page Road rather than another private property, the typical concerns that rear yard setbacks are meant to address, such as privacy, overlook, or land-use compatibility with adjacent residential lots do not arise here. Page Road already provides a generous separation between the building and any uses across the street. The small reduction in setback will not affect the streetscape, the functionality of the right-of-way, or the experience of pedestrians and drivers along the road. The setback exceeds that of the existing development on the street.

The building still maintains a clear, comfortable distance from the public realm, and there are no servicing, maintenance, or access issues created by shifting the building slightly closer to the lot line. Allowing this reduced setback supports a more efficient site layout without compromising the intent of the Zoning By-law, which is to ensure adequate spacing where it matters. In this case, the planning rationale is strong: the reduction is minimal, it poses no impact to neighbouring properties, and it maintains the overall character and function of the development while respecting the broader objectives of the by-law.

For these reasons, the proposed 0.5 m landscaped buffer and reduced rear yard setback meet the intent of the Official Plan, is consistent with the PPS, and the change to the by-law and represents good land use planning.

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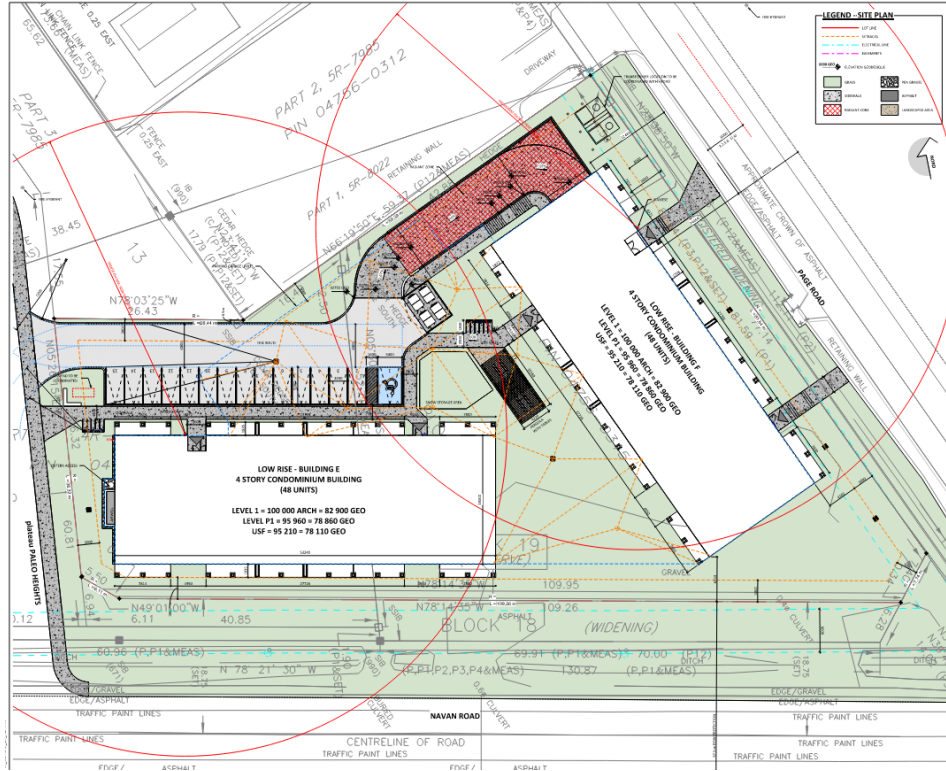


Figure 14 Block 17 Site Plan

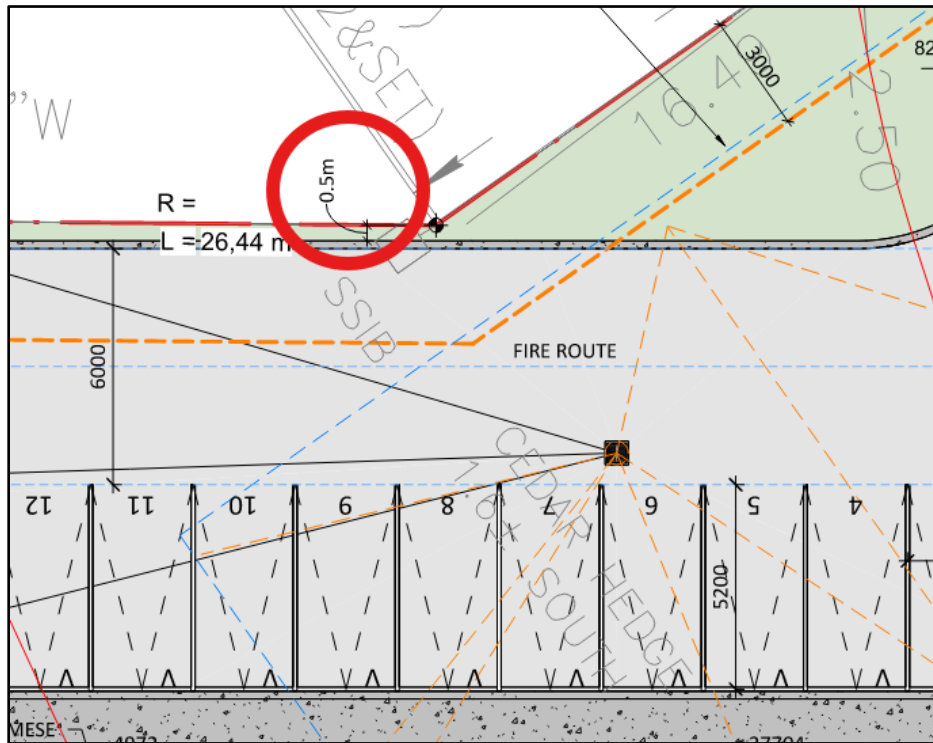


Figure 15 Landscape Buffer Width

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Furthermore, the proposal conforms to the City of Ottawa Official Plan, including the policies of the Suburban Transect and the Evolving Neighbourhood designation. The amendments support the creation of a well-designed, contextually appropriate community that provides a mix of housing, commercial services, and supporting infrastructure. The requested zoning reliefs do not generate adverse impacts and are supported by appropriate design measures, landscaping, and mitigation strategies.

Overall, the proposed Minor Zoning By-law Amendment represents good land use planning, consistent with provincial and municipal policy frameworks, and facilitates the orderly and compatible development of the subject lands. Approval of the amendments is supported and is therefore recommended to the City for approval.

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