



1020 & 1070 March Road

Planning Rationale
Minor Zoning By-law Amendment
September 18, 2025



Prepared for Mattamy Homes

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Executive Summary

Fotenn Planning + Design (“Fotenn”) has been retained by Mattamy Homes (“Mattamy”) to submit this Planning Rationale in support of a Minor Zoning By-law Amendment application for the property municipally known as 1020 & 1070 March Road (“the subject lands”) in the Kanata North community of the City of Ottawa.

The purpose of the Zoning By-law Amendment application is to modify the Flood Plain Overlay to reflect developments in the Shirley's Brook Tributary 2 catchment approved by the Mississippi Valley Conservation Authority (MVCA) on the basis of approved finished (as-built) grading plans prepared by Stantec. Developments in the catchment include but are not limited to: channel realignment, changes in land use, the construction of infrastructure (e.g., roadways, culverts, stormwater management facilities), and changes in catchment areas.

1.1 Application Summary

In support of this application to adjust the Flood Plain Overlay, the following materials are attached:

- / Application form;
- / KNUFA Shirley's Brook Tributary 2 Flood & Erosion Hazard Mapping Revisions report, prepared by Novatech, dated March 14, 2025, revised August 22, 2025;
- / Flood Plain Shapefiles, prepared by Novatech, dated July 15, 2025;
- / Grading Plans, prepared by Stantec Consulting Ltd., revision #11 dated June 6, 2026;
- / Site Grading Plans 1-7, prepared by Stantec Consulting Ltd., dated May 6, 2025; and
- / Written confirmation from the MVCA accepting the Flood & Erosion Hazard Mapping Revisions.
 - o The MVCA's update to the flood plain mapping is in progress.

MVCA has accepted the flood and erosion hazard mapping revisions and is in the process of mapping the regulatory flood and regulation limit lines. This Zoning By-law Amendment application requests the Flood Plain Overlay in Comprehensive Zoning By-law 2008-250 be adjusted to match the new mapping.

2.0

Subject Lands and Surrounding Context

The subject lands, 1020 and 1070 March Road, are located in the City's Urban Boundary, within the Kanata North Urban Expansion Area (Figure 1). The subject lands have historically been used for agriculture and are bounded by March Road and an abandoned railway corridor owned by the Canadian National Railway (CNR). As the subject lands' orientation to cardinal directions is somewhat irregular, within the following report it is assumed that March Road is a north-south arterial road and the west boundary of the subject lands, and the CNR is the east boundary. The subject lands front onto March Road for 458.8 metres and have a total area of approximately 40 hectares. Figure 2 below shows an aerial view of the subject lands and the surrounding area.

The legal descriptions for the subject lands are as follows:

1020 March Road:	PIN 04527-1016 - Part of Lot 13 Concession 4 (March); Parts 1 and 2 on Plan 4R-34291
1070 March Road:	PIN 04527-1017 – Part of Lot 13 Concession 4 (March); Parts 3 and 4 on Plan 4R-34291



Figure 1: Subject lands identified on the Kanata North Urban Expansion Area (KNUEA) Context and Location map from Kanata North Community Design Plan

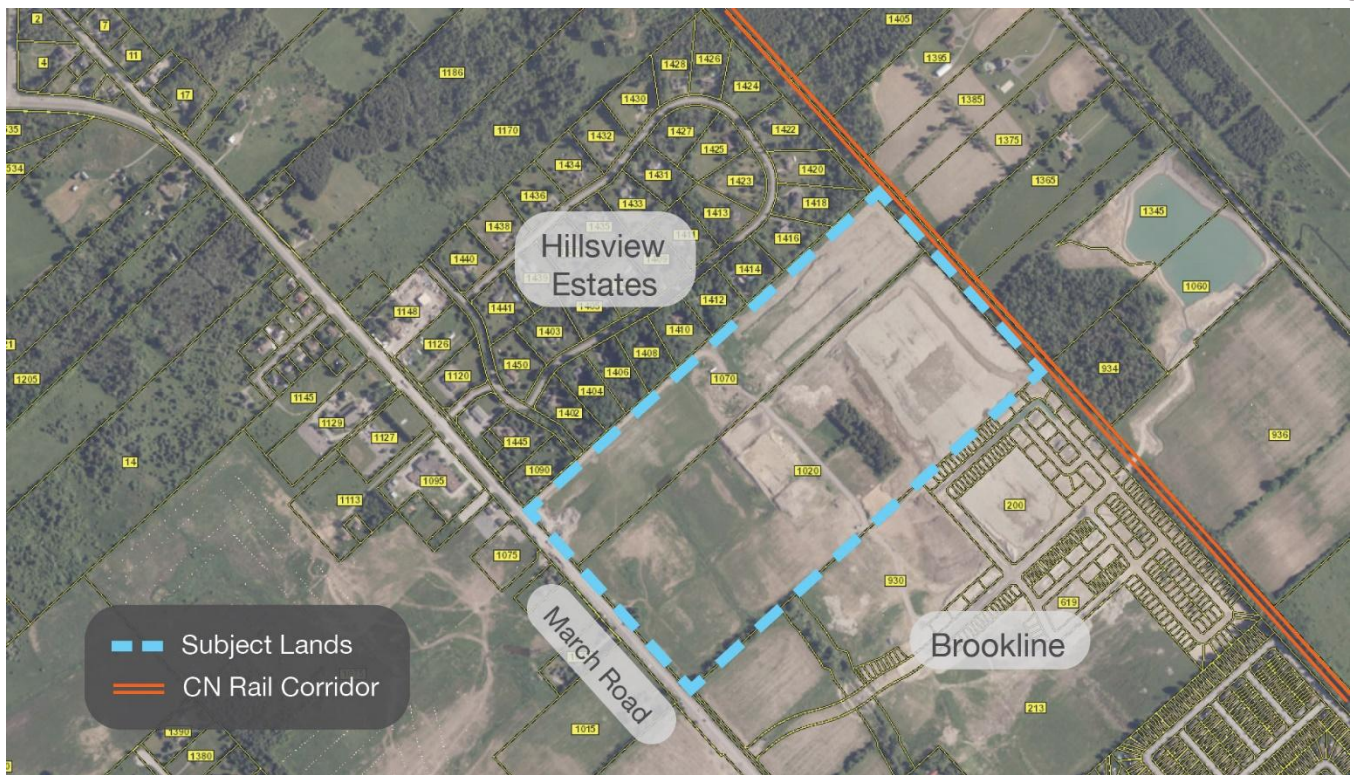


Figure 2: Aerial view of subject lands and surrounding land parcels

The requested Zoning By-law Amendment is required to permit development on a portion of the subject lands, specifically within Phase 1B of Mattamy's Northwoods development (see Figure 3 below). The Northwoods development consists of a 39.5-hectare mixed-use subdivision with predominantly residential development consisting of 348 detached dwellings and 473 townhouse units for a total of 821 dwelling units. Table 1 below shows the land uses proposed and the respective amount of land dedicated to each use as well as the residential unit breakdown.



Figure 3: Draft Phasing Plan prepared by Mattamy, June 2024

Table 1 Land use of Northwoods development

Proposed Uses	Number of Residential Units	Number of Lots or Blocks	Area in Hectares
Residential			
Single-Detached	348	348	12.09
Townhouse	473	91	8.05
Non-Residential			
Institutional	0	1	2.84
Commercial	TBD (mixed-use blocks)	2 (mixed-use blocks)	2.95
Parks and Open Space		9 (1 park, 8 trails/paths)	2.15
Roads		17	10.22
Other		1 (Natural Heritage site)	1.17
TOTAL	821	439	39.467

The subject lands were rezoned on February 26, 2025 (D02-02-19-0090) to permit the proposed development, with the exception of the modification of the flood plain overlay, which was still ongoing at the time of the rezoning. The proposed development also received draft Plan of Subdivision approval (D07-16-19-0020) in April 2023.

As shown on Figure 4 below, the extent of the existing flood plain is illustrated with a blue hatch. The existing flood plain primarily extends into Phase 1B of the Phasing Plan included in Figure 3 above. Figure 5 below displays an excerpt of the final Grading Plan (GP-4) that shows the realignment of Shirleys Brook Tributary 2. The full Grading Plans can be found in the application package. Figure 6 below displays the updated 100-year flood plain in relation to the subject lands' property boundary.

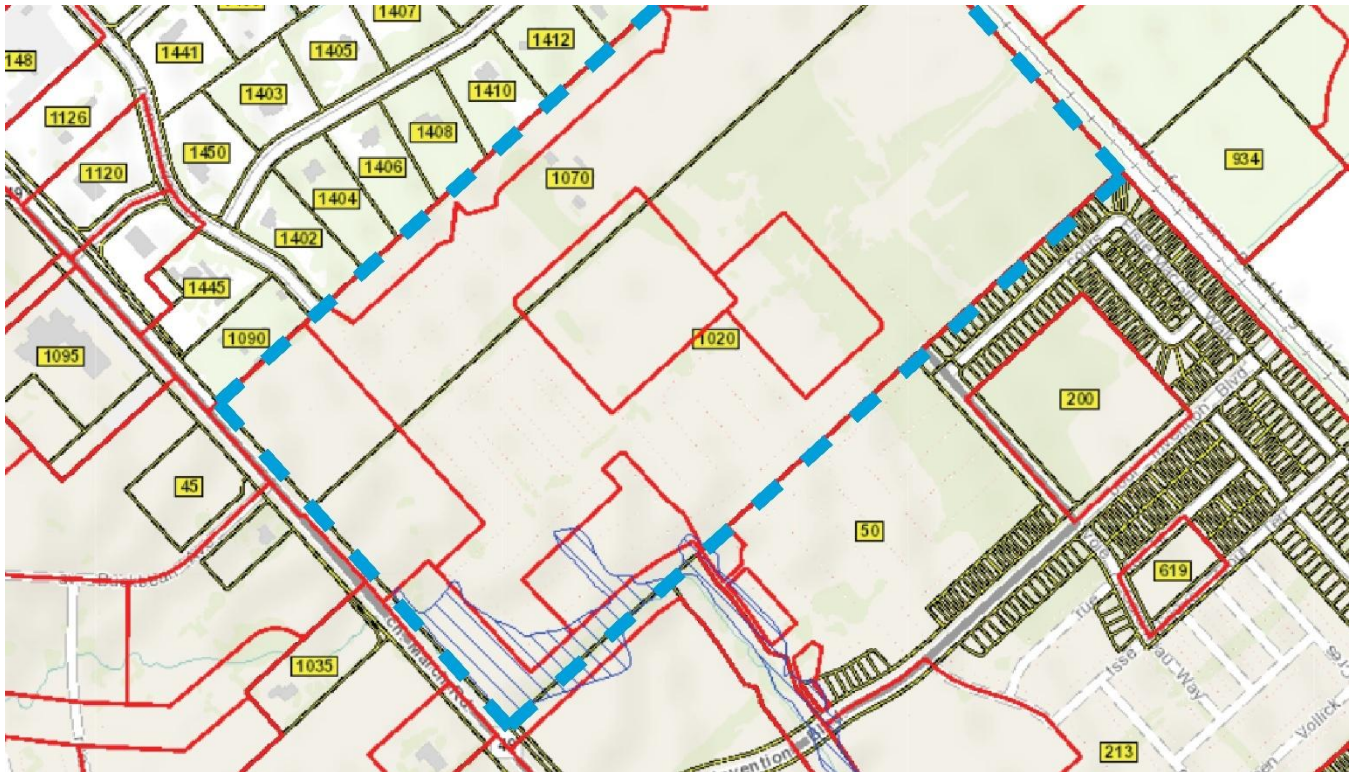


Figure 4: Existing Zoning Map illustrated the extent of the current Flood Plain Overlay (Section 58).

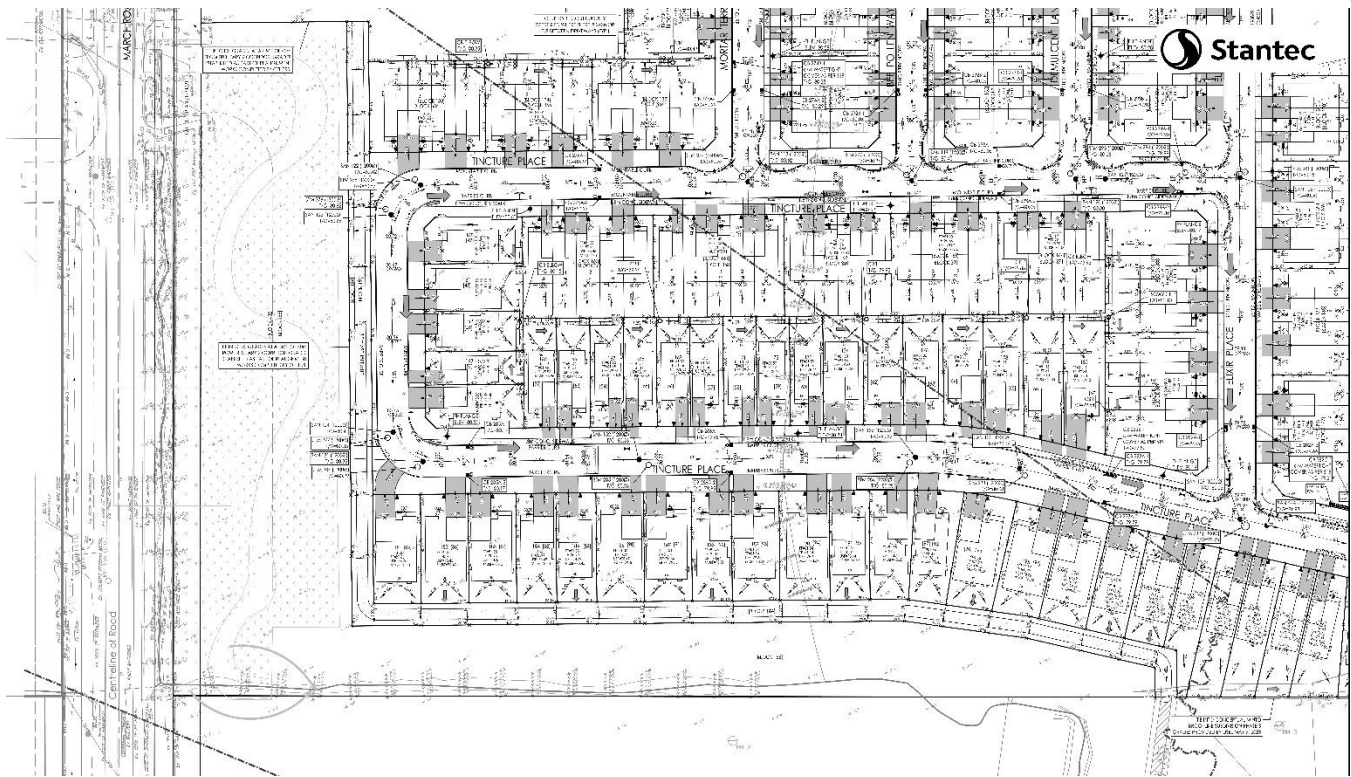


Figure 5: Drawing GP-4 of the finished (as-built) Grading Plan of the subject lands, prepared by Stantec Consulting Inc. (May, 2025).



Figure 6: Extent of updated flood plain. Shapefile of 100-year flood plain prepared by Novatech.

4.0 Policy & Regulatory Framework

4.1 Provincial Planning Statement (2024)

The Provincial Planning Statement (PPS) (2024) is a policy document issued under the *Planning Act* which provides direction on matters of provincial interest related to land use planning and development. The document came into effect October 20, 2024. All decisions on planning matters “shall be consistent with” the PPS. Generally, the PPS 2024 aims to increase the supply and mix of housing in Ontario and to “support a strong and competitive economy that is investment-ready and recognized for its influence, innovation and diversity.” The policies also aim to sustain strong and competitive communities.

5.2 Natural Hazards

Policy 2 of Section 5.2 – Natural Hazards of the PPS states that development shall generally be directed to areas outside of:

- a) hazardous lands adjacent to the shorelines of the Great Lakes - St. Lawrence River System and large inland lakes which are impacted by flooding hazards, erosion hazards and/or dynamic beach hazards;
- b) hazardous lands adjacent to river, stream and small inland lake systems which are impacted by flooding hazards and/or erosion hazards; and
- c) hazardous sites.

The proposed Zoning By-law Amendment aims to revise the flood plain overlay associated with the Shirley’s Brook Tributary 2 watercourse, in recognition of modifications to the watercourse and flood plain. The modified flood plain overlay will reflect the completed revisions and allow for the development to take place on a portion of the subject lands that are no longer located within the flood plain. The proposed development is consistent with the PPS (2024).

4.2 City of Ottawa Official Plan (2022)

The Official Plan for the City of Ottawa was approved on November 4, 2022. The Plan provides a framework for the way that the City will develop until 2046 when it is expected that the City’s population will surpass 1.4 million people. The Official Plan directs how the city will accommodate this growth over time and sets out policies to guide the development and growth of the City.

4.2.1 Transect and Land Use Designation

The subject lands are designated ‘Suburban’ in Schedule A - Transect Policy Areas of the Official Plan (Figure 7 below). Within Schedule B5 - Suburban (West) Transect, the subject lands are largely comprised of a Neighbourhood designation, with a Mainstreet Corridor designation and Evolving Overlay applying near and along March Road, as shown below in Figure 8.



Figure 7 Excerpt from Schedule A - Transect Policy Areas of the Official Plan

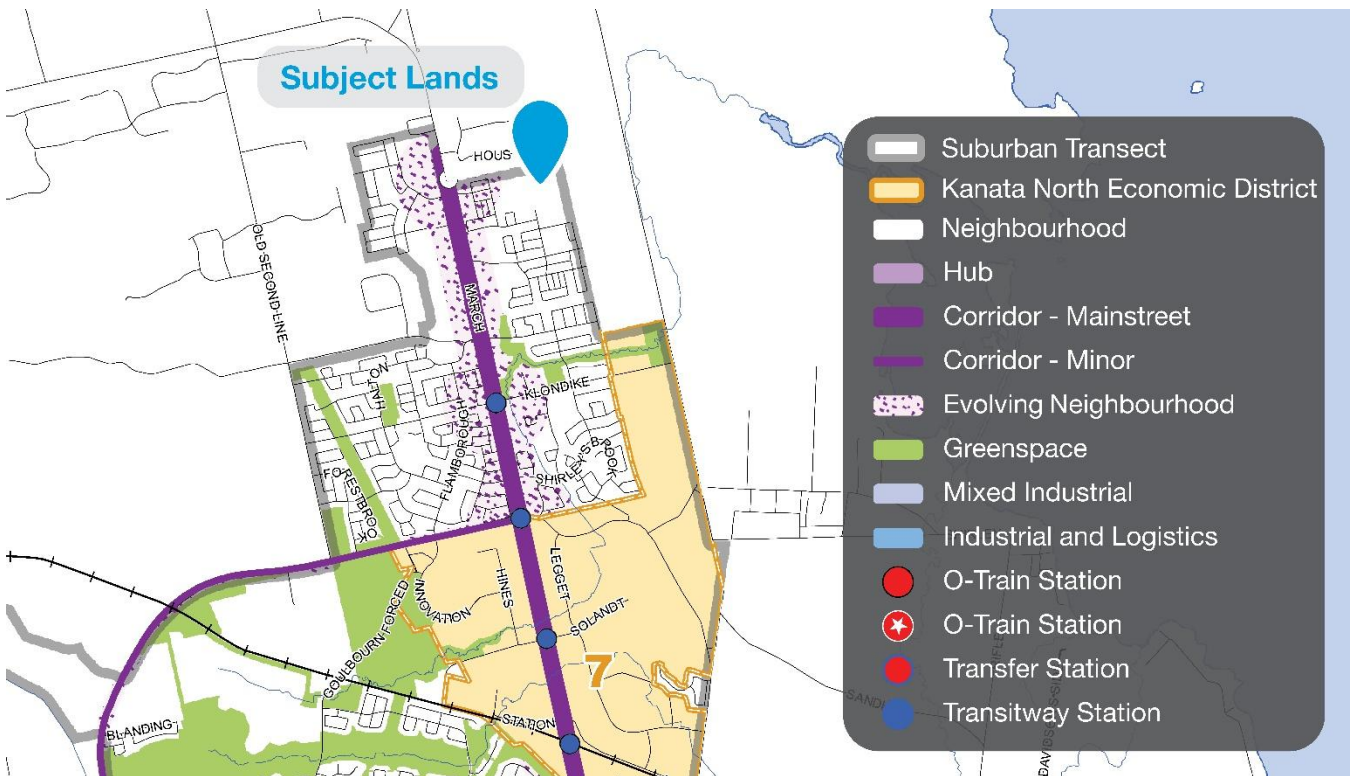


Figure 8 Excerpt from Schedule B5 - Suburban (West) Transect from the Official Plan

4.2.2 Protection of Health and Safety

Section 10 of the Official Plan, Protection of Health and Safety, contains policies to prevent property damage and injury and includes policies for hazards such as flood plains. The following policies pertain to the process for revising the Zoning By-law Flood Plain Overlay:

Policy 10.1.1.1) Development and site alteration shall not be permitted in the 1 in 100 year flood plain or in an erosion hazard area.

Policy 10.1.1.2) Development and site alteration shall not be permitted within areas that would be rendered inaccessible to people and vehicles during times of flooding hazards, erosion hazards and/or dynamic beach hazards, unless it has been demonstrated that the site has safe access appropriate for the nature of development and the natural hazard.

Policy 10.1.1.3) Notwithstanding Policy 1) and 2), some minor development and site alterations may be permitted. Minor development and site alterations are defined as the following:

- d) Passive open spaces which do not affect flood flows;
- e) Minor site alterations which do not result in a negative effect on flooding and which are supported by the appropriate conservation authority

Policy 10.1.1.4) Revisions to the Flood Plain Overlay in the Zoning By-law, or regulation limits in secondary plans or area-specific policies, may be implemented without the need for an Official Plan amendment, when site specific geodetic elevation information prepared by an Ontario Land Surveyor has been accepted by the appropriate conservation authority, or when the City is undertaking updates to the Flood Plain Overlay in the Zoning By-law and concurrence from the appropriate conservation authority has been received.

Policy 10.1.1.5) A requirement for flood plain studies, in support of a development application will be identified in consultation with the City and the appropriate conservation authority. Flood plain studies, submitted to the City, as part of the development review process, shall be supported by appropriate engineering and environmental studies.

The proposed Zoning By-law Amendment to the Flood Plain Overlay conforms with Official Plan policies, including those regarding natural hazards. The changes to the flood plain and erosion hazards undertaken by Mattamy were conducted with permission from the MVCA, and the updated flood plain only impacts lands where passive open space is planned. Access to residential lots is not impacted by the flood plain, and the modification is supported by technical studies. The proposed revision to the Flood Plain Overlay is supported by the MVCA, and therefore does not require an Official Plan Amendment.

4.3 Kanata North Community Design Plan

The Kanata North Community Design Plan (CDP) has been prepared as collaboration between the City of Ottawa and the Kanata North Land Owners Group (KNLOG) to demonstrate how development of the Kanata North Urban Expansion Area (KNUEA) will achieve the requirements of the Official Plan. The CDP was approved by City Council on July 13, 2016.

The CDP provides a level of direction between Official Plan policy and development approvals, and will be used as a guide for the preparation and review of future applications for development.

4.3.1 Vision & Guiding Principles

The Kanata North Community Design Plan (CDP) Section 4.1 envisions the Kanata North Community as a contemporary master-planned community, with a range of housing options and a vibrant, well-defined mixed-use core. Design of this community is rooted in an integrated open space system embracing the existing natural heritage features and structured around the creek corridors. Protection and restoration of Shirley's Brook and its tributaries will both contribute to ensuring the continued function of the natural system, and create a significant amenity for the community.

Section 4.2 of the Kanata North CDP outlines the guiding community principles. Policies related to Shirley's Brook and the flood plain are as follows:

Respect existing significant natural heritage features

- / Seek to minimize negative impacts on the environment and lead towards a restorative approach for natural systems of water, air and soil.
- / Protect and enhance Shirley's Brook and its tributaries.
- / Provide connections to and links between the greenspace network.
- / Consider opportunities to leave parks in a natural state.
- / Recognize the environmental value, active/passive recreational value, and contribution of the natural heritage system to creating a healthy community.

Preservation of natural heritage features on the subject lands are proposed for the subdivision through the tributary realignment.

4.3.2 Land Use Designations

Land Use Designation policies are in Section 4.3 of the Kanata North CDP. The Land Use Plan from the CDP is shown below in Figure 9.

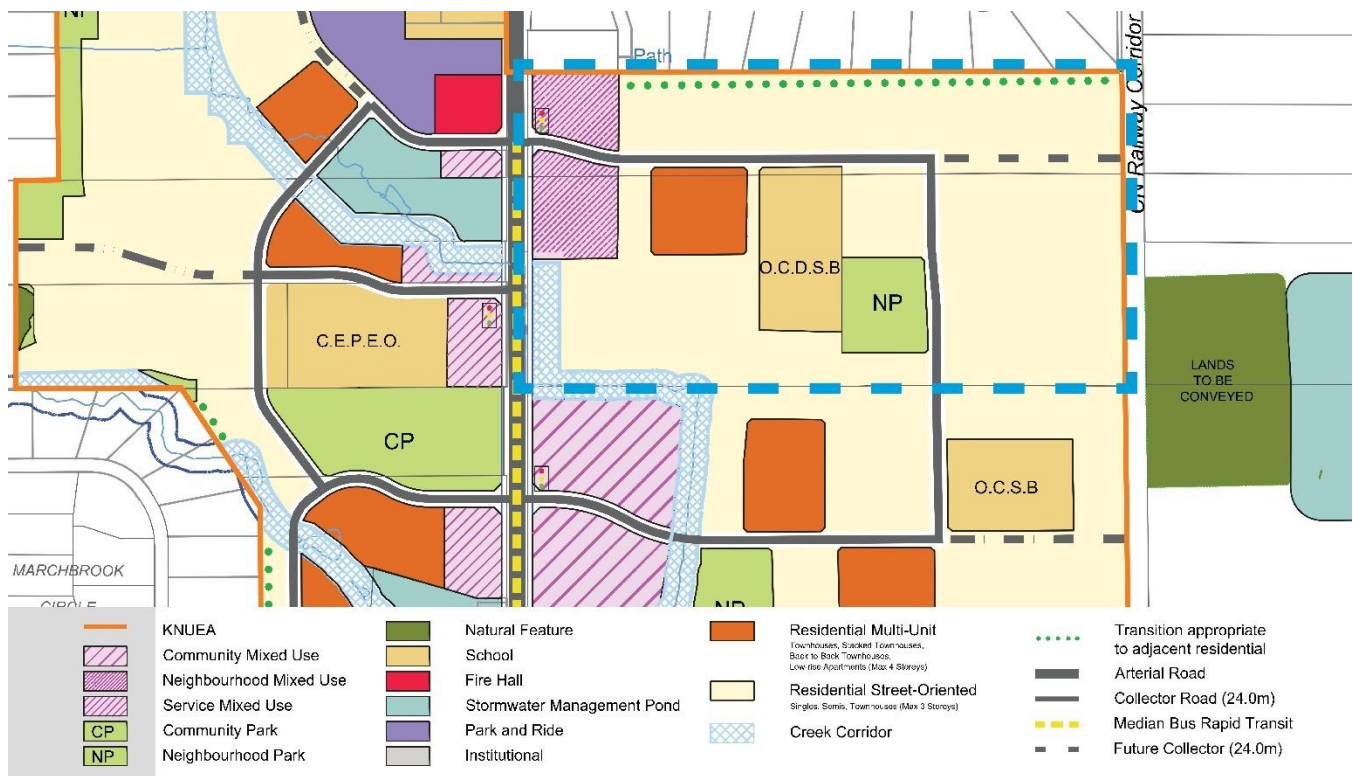


Figure 9: Kanata North Land Use Plan as shown in the Community Design Plan (subject lands outlined in blue)

Creek Corridor

Section 4.3.7 of the CDP states that tributaries 2 and 3 of Shirley's Brook will be protected by a 40-metre-wide corridor providing a 20-metre setback from the centreline of the watercourse. In some locations, an additional 6-metres outside of the creek corridors will accommodate a recreational pathway.

Recreational Pathways

As illustrated on Figure 10 below, Tributary 2 within the subject lands is identified as having a recreational pathway on the interior of the lands. Section 4.3.9 of the Kanata North CDP states that recreational pathways along the creek corridors will provide access to open space, integrate the natural heritage system into the community, and will connect with the existing pathway in the Brookside Community to the south. The policy states that a 6-metre-wide pathway block will be provided outside of the 40-metre creek corridors where pathways are to be provided. Furthermore, recreational pathways along the creek corridors will connect with recreational pathways through the stormwater management blocks.

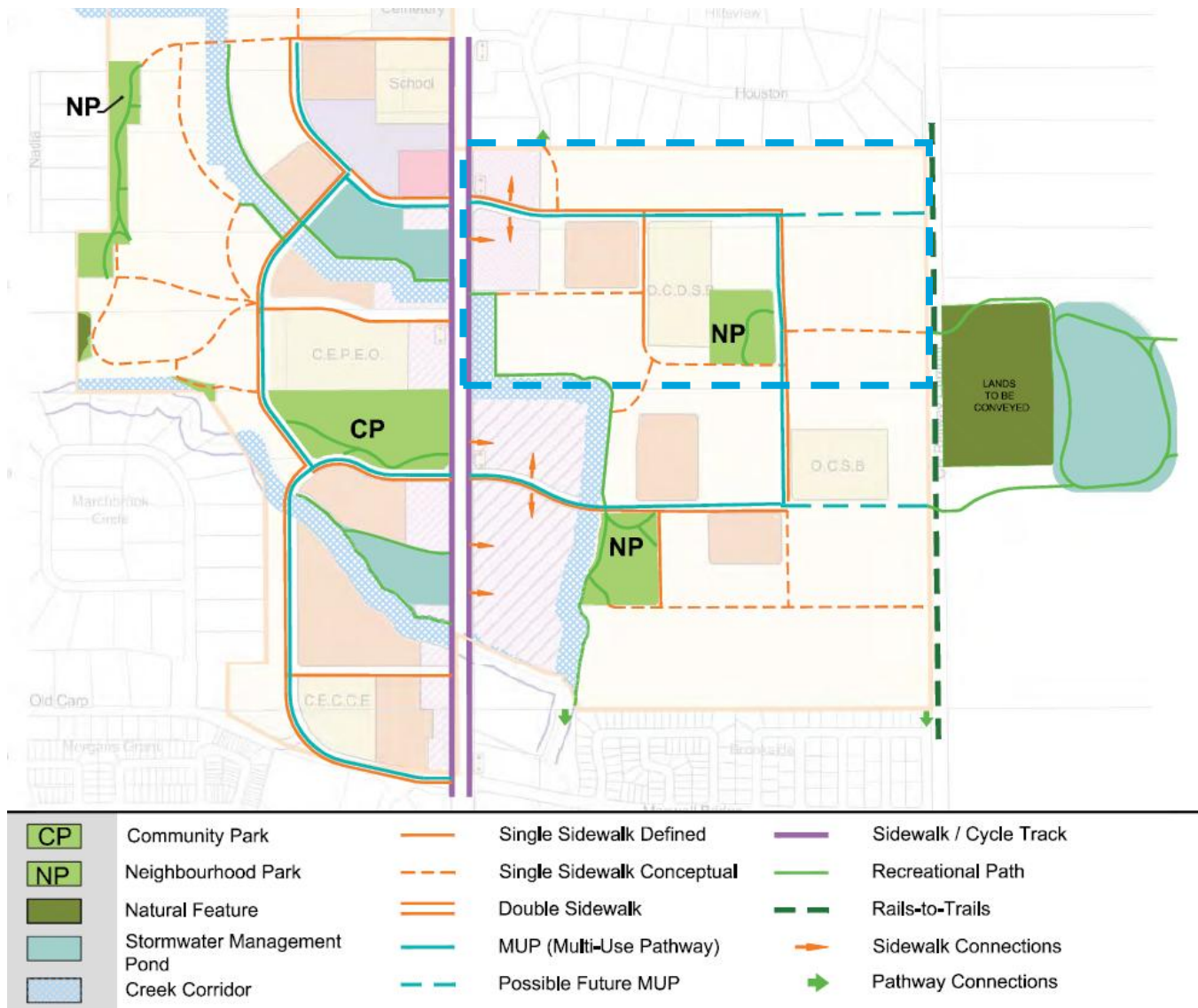


Figure 10: Kanata North Parks and Open Space Plan as shown in the Community Design Plan (subject lands identified in blue)

The proposed development includes a largely naturalized buffer abutting the realigned creek corridor, as well as a recreational pathway that will connect future proposed mixed-use blocks (abutting March Road) to the Brookline neighbourhood to the south.

4.3.3 Environmental Management Plan

Section 5.3 of the Kanata North CDP includes the Recommended Natural Heritage System map (see Figure 11 below). The map identifies Tributary 2 of Shirley's Brook to be realigned to accommodate the March Road widening (illustrated as the red dashed line and green hatched area below). The orange area abutting the tributary indicated lands where rear yard areas are to be directed to the tributary. Two proposed culverts are indicated from Tributary 2 across March Road.

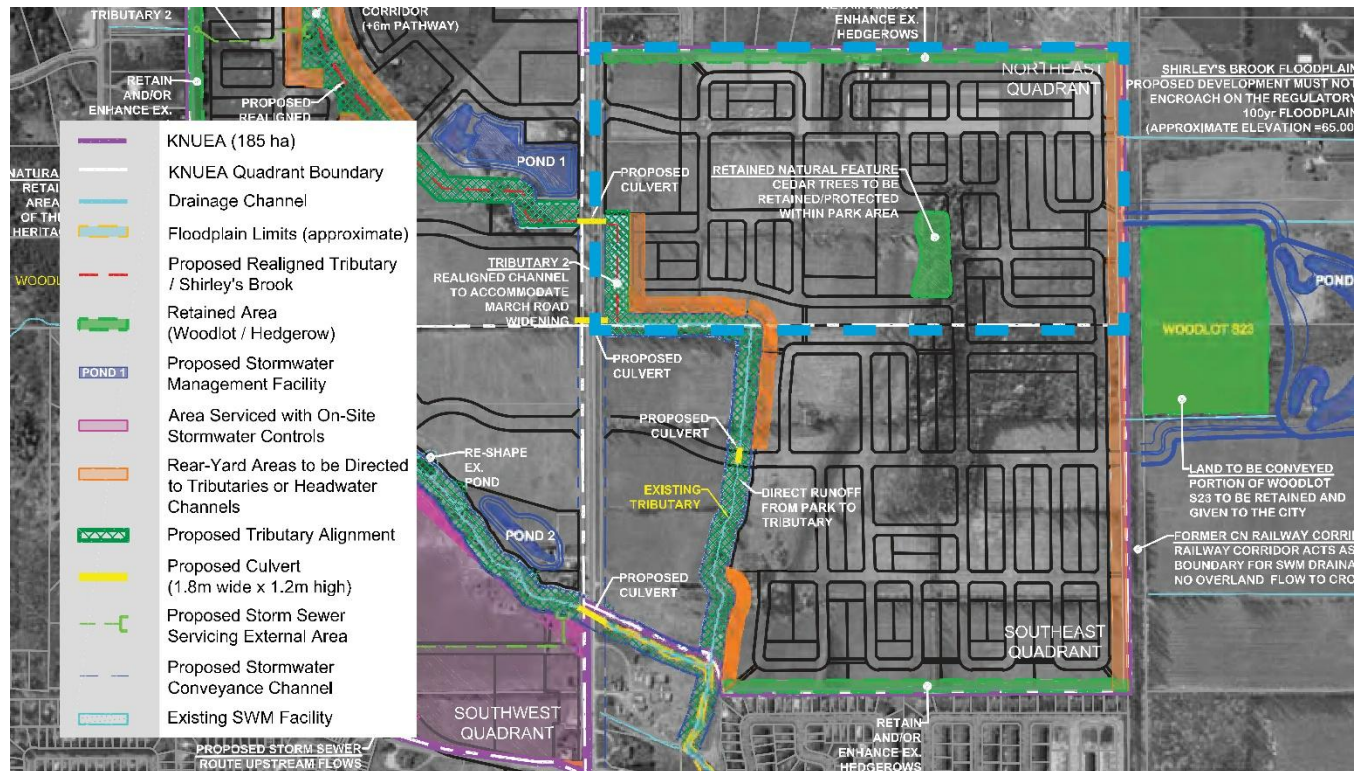


Figure 11: Kanata North CDP Recommended Natural Heritage System. The subject lands are outlined in a dashed blue line.

The realignment of Tributary 2 is in conformity with the CDP direction, incorporating the designated width and buffer between the abutting residential properties.

4.3.4 Design Guidelines

The Kanata North CDP's Design Guidelines (**Section 6.2**) further the City of Ottawa's design guidelines for greenfield development which are presented in Section 3.5 of this document. The CDP design guidelines provide site specific recommendations to help shape Kanata North as a distinctive and unique place.

The proposed Zoning By-law Amendment to lift the Floor Plain Overlay meets several of the CDP's design guidelines, including the following:

Section 6.2.6 Creek Corridor Guidelines

Figure 12 below illustrates a cross-section for creek corridors in the Kanata North CDP including Tributary 2. The proposed development incorporates the following guidelines in the design:

- / Maximize opportunities for window streets along the creek corridors.
- / Provide pathway connections to the creek corridor.

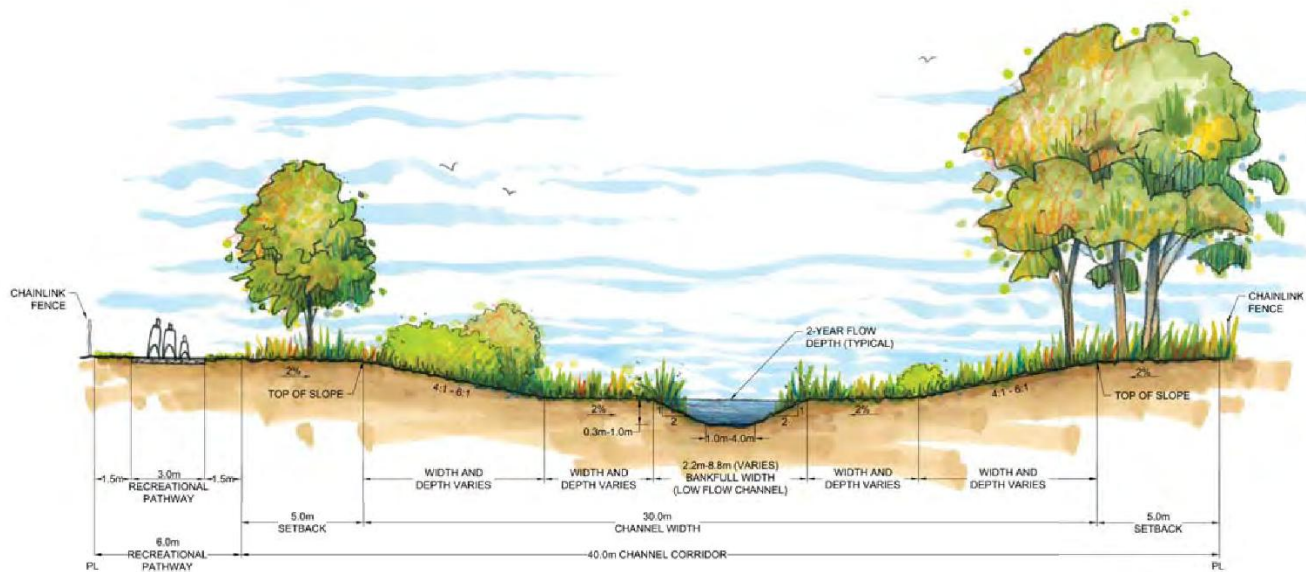


Figure 12: Kanata North CBD Typical Cross-Section of Creek Corridor.

The proposed design aligns with the CDP guidelines. A 3.0 metre asphalt multi-use pathway is proposed abutting the length of Tributary 2 channel corridor on the subject lands with a 1.5 metres buffer on either side creating the recreational pathway as illustrated in Figure 12 above. A window street also runs along a portion of the creek corridor and pathway.

4.4 City of Ottawa Zoning By-law (2008-250)

As illustrated in Figure 13 below, the subject lands are currently zoned General Mixed-Use Subzone 15, Urban Exception 3021(GM15[3021]); Residential Third Density Zone Subzone YY, Urban Exceptions 3018, 3019 and 3020 (R3YY[3018], R3YY[3019] and R3YY[3020]); Environmental Protection Zone (EP); Parks and Open Space Zone (O1); and Minor Institutional Zone (I1A). The southwest portion of the subject lands (including some residentially-zoned lands) and portions of the adjacent subdivision to the south are subject to the Flood Plain Overlay. Figure 14 below illustrates the extent of the existing Flood Plain Overlay on the subject lands.



Figure 13: Zoning of subject lands including the surrounding context.

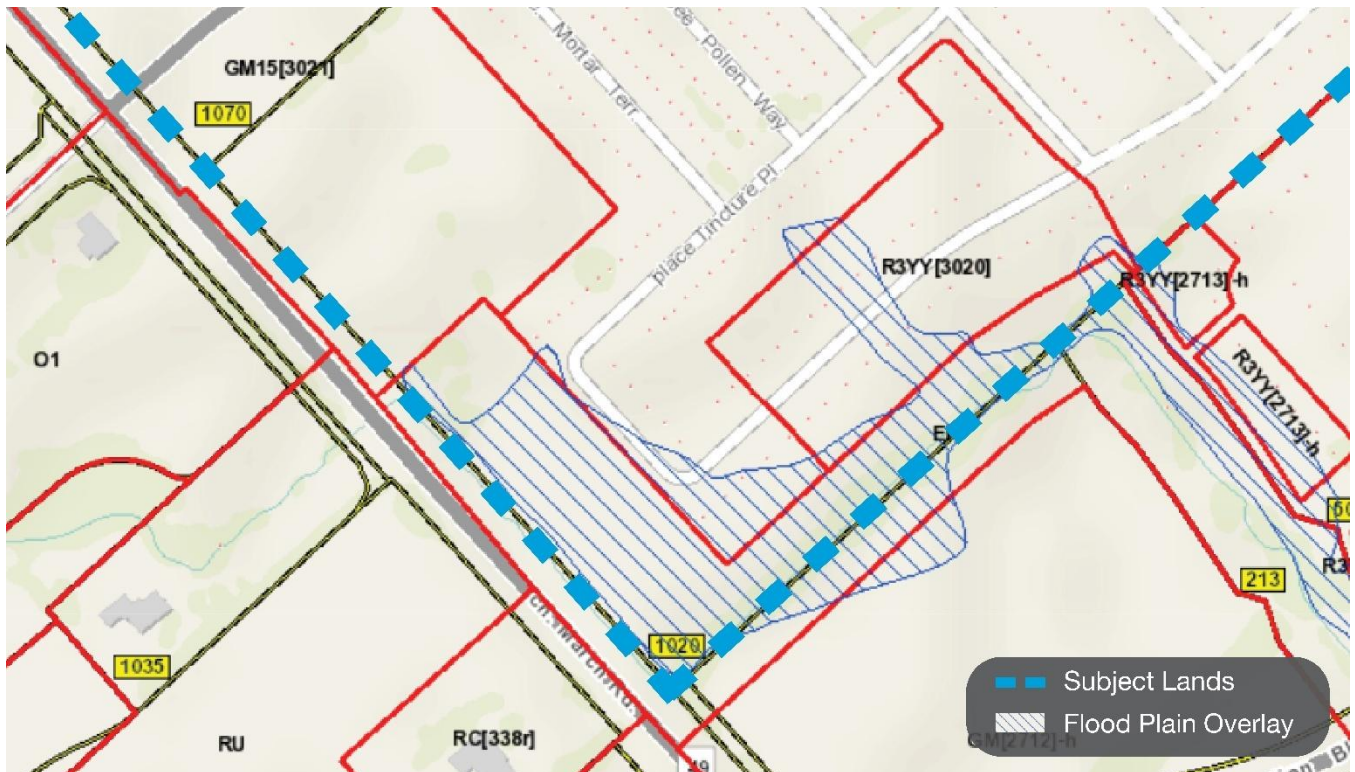


Figure 14: Excerpt of the subject lands zoning illustrating the extent of the current Flood Plain Overlay.

Flood Plain Overlay (Section 58)

The Flood Plain Overlay is intended to restrict development to minimize the threat of injury, contamination, or property damage. Subject to Section 58 of the Zoning By-law, which takes precedence over the provisions of the underlying zone, development of affected lands is limited to:

- / Additions to permitted uses which do not exceed the lesser of 20 square metres or an amount equal to 20 percent of the gross floor area of the building;
- / Buildings or structures that are accessory to permitted uses and do not exceed a gross floor area of 50 square metres and a height of one storey;
- / A change of use from one permitted use to another;
- / One additional dwelling unit that is located fully above grade; or
- / Permitting the following uses whether or not they are permitted by the underlying zone:
 - Marine facility; and
 - Utility installation, subject to approval under the Environmental Assessment Act.

Additional uses, such as cemeteries, daycares, gas bars, and light industrial uses, are not permitted in a flood plain. Development in a flood plain is regulated under the Conservation Authorities Act, which requires a permit from the relevant Conservation Authority in addition to a municipal building permit.

The MVCA has authorized adjustment of the flood plain and erosion hazards, and is in the process of revising their flood plain mapping on the basis of the finished (as-built) grading plan following the developments in the catchment. The adjustment of the Flood Plain Overlay on the subject lands will allow for building permits to be issued for the construction of new dwellings.

Conclusion

It is our professional opinion that the enclosed Minor Zoning By-law Amendment application to adjust the Flood Plain Overlay represents good planning and is in the public interest for the following reasons:

- / The proposed development conforms with the policies in the City of Ottawa Official Plan (2022) and the Kanata North Community Design Plan;
- / The envisioned development on the subject lands has undergone review through the approval of previous development applications (D07-16-19-0020, D02-02-19-0090); and
- / The MVCA confirmed that developments in the Shirley's Brook Tributary 2 catchment have been completed and finished (as-built) grading plans have been reviewed and approved by the MVCA. The MVCA is in the process of updating the regulatory flood and regulation limit lines.

Should you have any questions related to the contents of this letter or the application, please do not hesitate to contact the undersigned.

Sincerely,



Nico Church, MCIP RPP
Senior Planner



Genessa Bates, M.PL
Planner