



1047 Richmond Road

Planning Rationale + Design Brief Addendum
Official Plan Amendment and Zoning By-law Amendment
July 17, 2023



Prepared for Fengate Asset Management

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Project History, Summary of Changes, and Design Brief

Project History

Initial Submission

The initial Official Plan Amendment and Zoning By-law Amendment application submission proposed:

- / Three (3) towers of 36-storeys, 38-storeys, and 40-storeys heights atop a connected six-storey podium;
- / A total of 1,343 residential units ranging from studios to three-bedroom units;
- / A total of 1,347 square metres of commercial space at-grade fronting Richmond Road and New Orchard Avenue;
- / A total of 762 underground parking spaces and 672 bicycle parking spaces within three levels of underground parking;
- / Vehicular access to the site via New Orchard Avenue North along the northern and eastern edges of the site;
- / Both private amenity spaces via balconies and terraces, as well as a central communal outdoor amenity spaces; and,
- / A 1,015 square metre public park at the corner of Richmond Road and New Orchard Avenue North, connecting the New Orchard Light Rail Transit (LRT) Station to the subject site.

First review comments were received from City Staff in April 2022. In response to the comments provided, the applicant team worked through numerous iterations of a site plan. Comments provided by Planning, Urban Design, Parks, and other key City Staff members were considered in the redesign and are summarized below.

- / Exploration of the subject site as a two-tower site, versus a three-tower site;
- / Refining the podium forms to introduce heights, active streetscapes and stepping consistent with Secondary Plan policies;
- / Additional refinement of ground floor commercial spaces to frame the proposed park and Richmond Road;
- / Exploration of a central privately owned publicly accessible space (POPS) space, integrated with the proposed parkland area and outdoor at-grade communal amenity spaces;
- / A site specific and contextual analysis of programming and activation opportunities for the park, POPS, and outdoor communal amenity spaces, as well as green street activation along Richmond Road and New Orchard Avenue; and,
- / Relocation of the drive aisle and pick-up/drop off locations for each building, introducing lay-bys along New Orchard Avenue and along a northern drive aisle.

Summary of Changes

In response to comments provided since April 2022 and subsequent discussions with City Staff, significant revisions have been made to the proposed development and site design for this submission, including:

- / Elimination of the 36-storey tower resulting in two remaining towers of 40-storeys (closest to the New Orchard LRT Station), and 38-storeys (in the eastern part of the site). This revision has resulted in a reduction of 191 residential dwelling units.
 - o Stepbacks are provided from Richmond Road atop the third storey to a podium height of six storeys. with additional stepbacks provided atop the ground floor central to the site.
 - o Townhouses have been introduced to increase unit diversity and add architectural interest at the base of both tower podiums.
- / A six storey mixed-use mid-rise building has been introduced central to the site along Richmond Road, framing the streetscape and providing a setback atop the third storey;
 - o The podiums have been setback 3 metres from Richmond Road to create a wider public realm, increasing opportunities for meaningful landscaping and street trees.

- / Commercial spaces are proposed along Richmond Road and facing the new proposed park, at a total of 859 square metres. Active entrances are provided along Richmond Road and abutting the park to these uses; and,
- / The centrally located, open, and accessible outdoor amenity spaces remain to provide porosity through out the site. POPS have been introduced to the east and west of the mid-rise building to allow public access and pedestrian movement into and through the site, while the 1,013 square metre public park remains at the corner of Richmond Road and New Orchard Avenue.

Project Overview

The proposed development contains 3 buildings, connected underground with a parking garage. There are two towers of 38-storeys and 40-storeys heights atop six-storey podiums, with frontage onto both Richmond Road and New Orchard Avenue North, along with a six-storey mid-rise building along Richmond Road central to the site. A total of 1,152 residential units are proposed, ranging from studios to three-bedroom units, including 181 barrier free units. The proposed development contains a total of 689 underground parking spaces within three levels of underground parking, 621 for residential use and 68 visitor parking spaces. Vehicular access to the site will be provided from New Orchard Avenue North along the northern edge of the site. The proposed development contains an internal pick-up and drop-off area accessed via a drive aisle from the north, which will be designed to also function as a central outdoor amenity space. Privately owned publicly accessible space (POPS) is provided to the east and west of the mid-rise building, totaling an area of approximately 311 square metres.

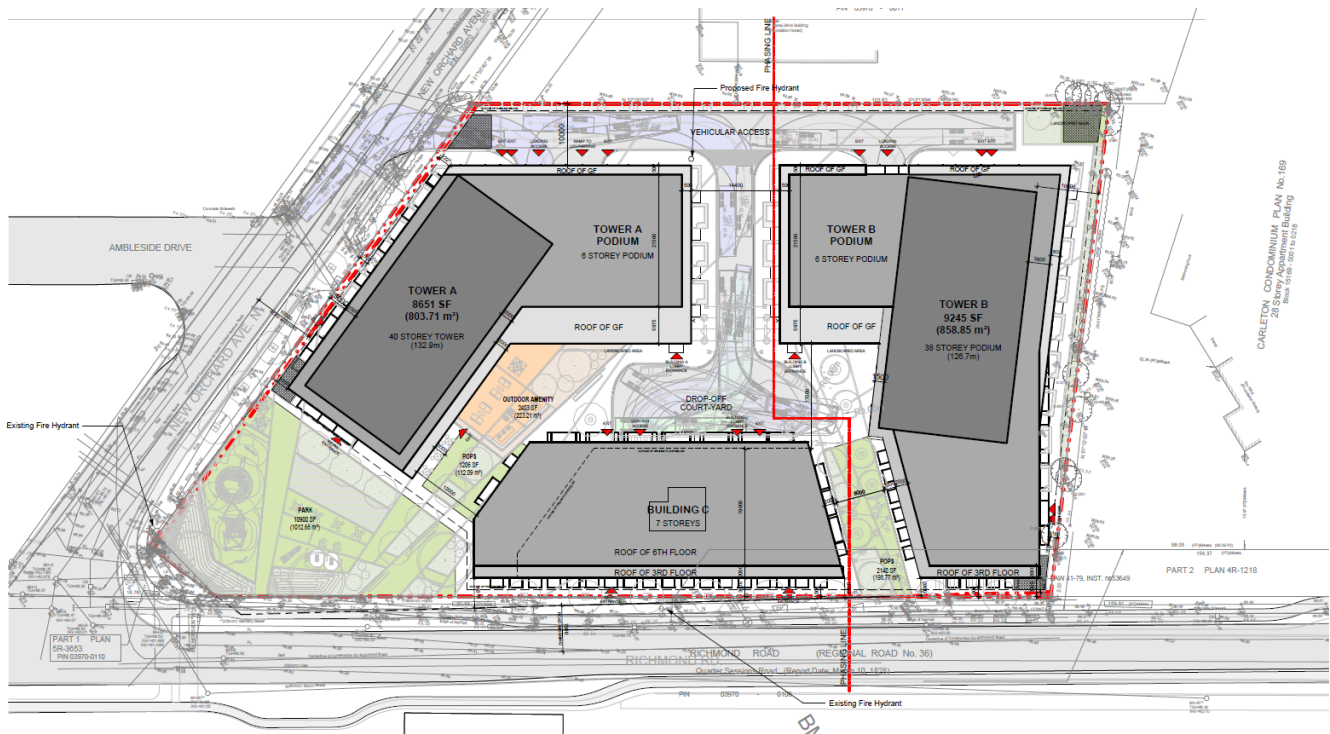


Figure 1: Site Plan for 1047 Richmond Road

The ground floor of the development is proposed to contain a total of 859 square metres of commercial space intended to accommodate retail tenants, with active entrances along Richmond Road and facing the park. A total of 726 bicycle parking spaces are provided for residential use. Residential amenity spaces are proposed internally within the podium, as well as via private balconies and communal at-grade spaces. A 1,013 square metre public park area is proposed at the corner of Richmond Road and New Orchard Avenue North, connecting the New Orchard Light Rail Transit (LRT) Station to the subject site.

Design Brief

Massing & Scale

The proposed development includes two mixed-use high-rise buildings (referred to as Buildings A and B) which comprise of both residential and commercial spaces. Additionally, the proposed development includes a mid-rise, mixed-use building facing Richmond Road. Building A is a 40-storey tower and Building B is a 38-storey tower, each atop a six-storey podium. Building C is a six-storey mid-rise. The tower portions of the proposed development step back from the podium to ensure a pedestrian scale and active, inviting frontages via commercial and residential spaces framing Richmond Road and New Orchard Avenue North. Additional setbacks are provided atop the third storey where podiums face Richmond Road. The podium built form provides a scale that will encourage pedestrian activity along Richmond Road and New Orchard Avenue, transitioning to the public realm in a way which enhances the streetscape. The dedicated park space at the southwest corner of the site, along with POPS and outdoor amenity spaces connecting to a central courtyard encourage pedestrian movement at-grade, integrating the public and private realm throughout the site and into the surrounding community.



Figure 2: Rendering of 1047 Richmond Road looking northwest

The proposed development provides a consistent architectural vocabulary as a unified development. The mass of each building has been divided into smaller volumes ensuring porosity throughout the site, distinguished by the difference in treatment from solid materials, transparent glass, and the rhythm created by the balconies. The architectural expression of the towers is carried down into the podium, where vertical fins change in thickness to create a fading effect and movement. This vertical articulation is continued through the mid-rise building for cohesion, creating an overall site volume which appears less solid. Increased glazing reveals a ground floor which achieves more visibility to enhance the public realm and ensure active entrances.

Building Design and Transition

The proposed development provides a building and tower design which is appropriate for the size of the site and provides adequate transitions to surrounding high-rise developments and the low-rise neighbourhood across

Richmond Road, the Byron Linear Park, and Byron Avenue. The proposed development is oriented in a way that minimizes impacts on existing neighbourhoods to the south, with an at-grade condition that strongly links pedestrians and public realm movement from Richmond Road to an internal courtyard, proposed to contain the main access to the lobbies of the three residential buildings.

Towers A and B are located atop a six-storey podium and are setback from all lot lines, Richmond Road, and New Orchard Avenue. The podiums have been designed with stepping to ensure appropriate transition to the public realm and activate the streetscape. A mid-rise built form has been introduced to the site, additionally stepped at the third storey to maintain a consistent streetwall along Richmond Road. The ground floor of the mid-rise building will accommodate active entrances to commercial spaces along Richmond Road while tower podiums contain both commercial spaces and indoor amenity areas that will contribute to the overall animation of the site. The commercial spaces have been designed to retain flexibility and attract tenants which will contribute significantly to the animation of the public realm. Mechanical penthouses have been integrated within the volume of towers to diminish their appearance and enhance the skyline of the area. Amenity spaces, including private balconies and terraces, are also integrated within the volumes, behind the grids which ensures a clean and orderly aesthetic of each tower. This design intention has been carried through to the mid-rise building. Loading spaces and access to underground parking is located from the north of the site, via a drive aisle from New Orchard Avenue, while the central courtyard will allow access to vehicle pickup and drop off areas, and access for emergency vehicles.

The demonstration of angular plane is included below. The angular plane is measured from a point 11 metres high, representing the current maximum permitted height of the residential zone south across Richmond Road, the linear park, and Byron Avenue. As demonstrated, the angular plane crosses the proposed tower and does not align with the angular plane as illustrated in the City of Ottawa's Urban Design Guidelines for High-rise Buildings. However, considering the orientation and high-rise context of the subject site relative to the low-rise neighbourhood, the application of the angular plane may not be appropriate, with several other transition measures integrated into the design mitigating potential and perceived impacts on the surrounding context. Further, it is important to measure that the conformity to the angular plane policies and guidelines against the broader policy objectives discussed within this addendum.

Recognizing that the intent of the angular plane is to reduce the impacts of massing towards the low-rise area, the tower placement and orientation proposed at the subject site minimizes the impacts on the adjacent residential area in ways that a development with less height and similar density could not. Transition measures, including at-grade setbacks, amenity spaces, tower stepbacks, and active frontages have been provided to frame Richmond Road while interfacing with abutting properties. Further, the location of the proposed development is generally north of these low-rise areas, which generally eliminates impacts caused by shadows. The application of the angular plane may be considered less relevant considering the distance and relationship of the proposed development to the residential areas. The interfacing residential is over 50 metres away from the closest tower and its rear yard amenity area is effectively screened from the interfacing residence mitigating overlook and privacy concerns, furthermore, the dual roadways, pathways, and transit corridor within the separation distance between the residential and the tower produce noise and visual separation conditions which serve to accentuate this separation in a manner that a landscaped setback would not. Finally, the elimination of one of the proposed towers allows for substantial separation distances generally ranging from 56.2 metres to 74.8 metres between the towers and setback distances ranging from 24.8 metres to 25.3 metres from Richmond Road resulting in increased openness to the sky and a pedestrian experience along Richmond Road that will be almost exclusively defined by the interfacing podiums. Development goals within proximity to transit, how the density is experienced by the public realm, and surrounding build form context are key considerations to be considered when applying angular plane criteria to high-rise development.

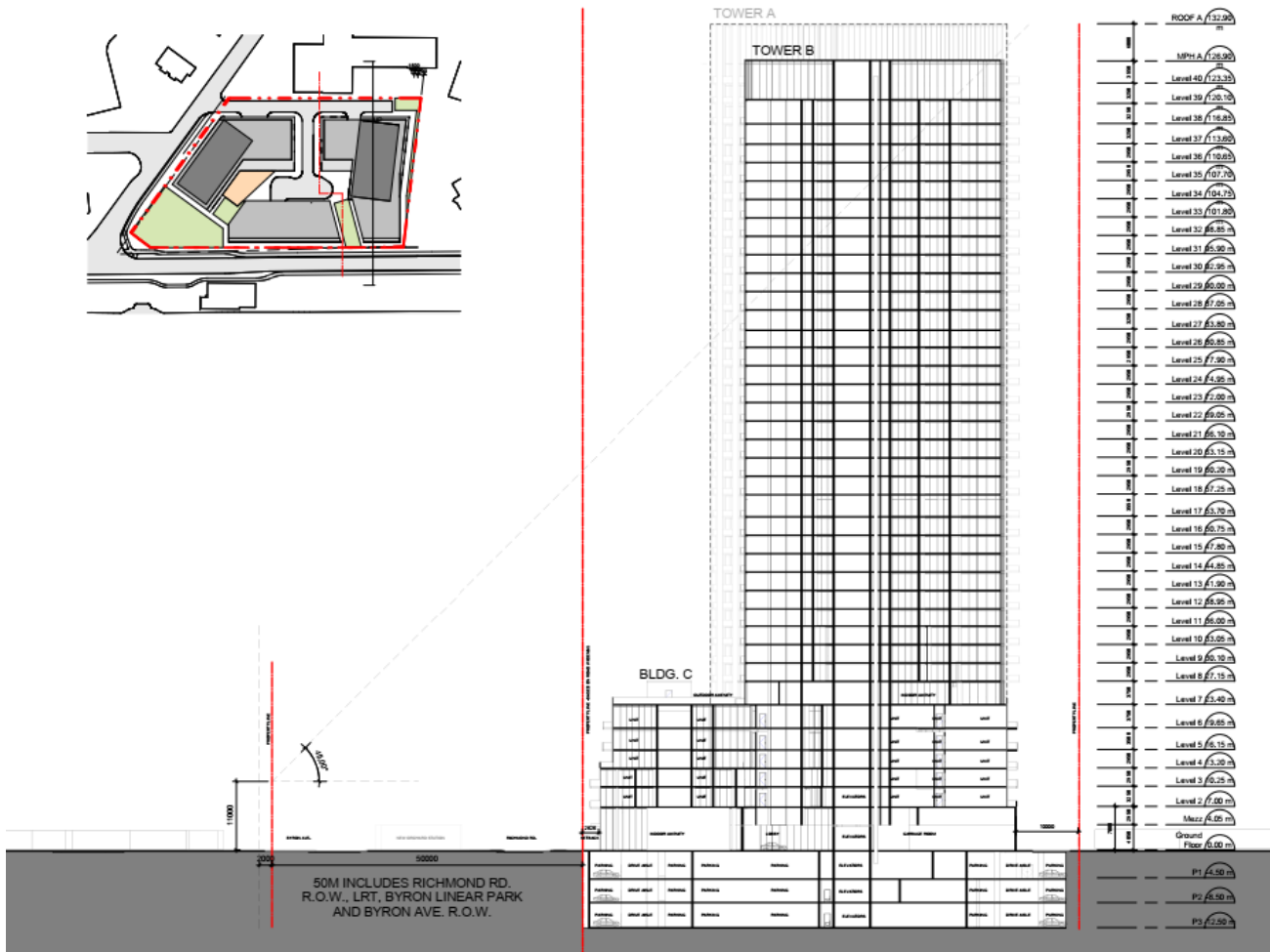


Figure 3 - Angular Plane Diagram

Public Realm

The proposed development has been designed to include open spaces accommodating pedestrian movement and flow, with active entrances and high percentage glazing along Richmond Road and New Orchard Avenue for retail and commercial spaces. The proposed development contributes to the high animation of the pedestrian realm, setting strong precedent for this location along Richmond Road.

Primary commercial entrances will ensure pedestrian use along all public frontages, activating Richmond Street and the parkland area. The treatment of the building podiums and at-grade spaces with fenestration provides visual transparency and improves safety for pedestrians in the area. Commercial space design has been designed to remain flexible, allowing a broad range of tenancies and the ability to program adjacent outdoor spaces to synergize with adjacent commercial activity. Additional public realm improvements, including the introduction of a new street tree canopy and hard landscaping elements, are provided to enhance the frontage along the main roads and activate the streetscape and connection to the public park at the south corner. The design details will be further refined through the planning process and Site Plan Control application.



Figure 4: Rendering of 1047 Richmond Road, looking northeast from future LRT corridor

The site will contain 1,013 square metres of public parkland located at the corner of Richmond Road and New Orchard Avenue North, connecting inward from the LRT and adjacent public streets to the site. The interface between neighbourhood and proposed buildings has been carefully considered, where residential units at grade, ensure a strong and safe interface with the existing pathways and pedestrian connections. The scale of the podium contributes to the transition between the mid-rise building and towers with the surrounding area. A setback from Richmond Road atop the third storey will contribute to the pedestrian scale along Richmond Road and the parkland interface. Landscaping and street trees are provided along these frontages, creating a pleasant, safe, and comfortable environment for pedestrians.

Residential entrances to the main buildings are internal to the site via the courtyard to avoid disruption between public and private uses. The access and egress to the underground parking and loading areas have been located at the west side of the site, avoiding any interruptions in the frontage while reducing conflicts between vehicles, pedestrians, and cyclists.

Amenity Areas

The building is proposed to contain a total of 10,551 square metres of indoor and outdoor amenity spaces. The 223 square metre outdoor communal amenity space is proposed central to the site, connecting the parkland and POPS areas from Richmond Road to the central courtyard area. Indoor amenity spaces are located on ground floor and second floor of Buildings A and B, while additional common indoor amenity spaces are located on the seventh storey, paired with expansive outdoor amenity spaces atop the Tower A and B podiums (551 square metres and 782 square metres, respectively). Private balconies and terraces are proposed throughout the development among all buildings, totaling 6,818 square metres of area.

In addition to these private amenity areas and POPS, a 1,013 square metre public park is provided in the generous setback at the corner of Richmond Road and New Orchard Avenue North. This public park will contribute a new

public outdoor space and transition the proposed development to the surrounding public realm and New Orchard LRT Station.

Parking

The proposed development will contain three levels of underground parking with a total of 689 parking spaces, accessed via the west side of the site. The drive aisles to access this parking garage have been intentionally located to the rear of the site to avoid interruptions with pedestrian activity along Richmond Road, supported by Secondary Plan policies to locate access on side streets. Vehicle circulation has been internalized to the site, where a central courtyard provides pick up and drop off areas to residential entrances, in addition to accommodating pedestrian and cycling movement. This area will be designed to minimize the presence of automobiles and prioritize the pedestrian experience in this space.

Accessibility and Sustainability

The proposed development has incorporated preliminary sustainability measures such as accessibility, site connections, bicycle parking, and bird friendly guidelines into the design, as discussed below. As the redevelopment is refined through the Design Review and Site Plan process so to will be the sustainability considerations.

- / **Accessibility:** 15% of the residential units are proposed to be provided as accessible, barrier-free units. These units have been designed to include zero step entrances, wider doorways, and clear passages to washrooms and bedrooms.
- / **Site Connections:** Pedestrian pathways have been incorporated into landscape design along all site frontages and from courtyard to the public park and adjacent roadways. The sidewalks and walkways will be continuous, universally accessible, barrier-free and will be in accordance with City of Ottawa Official Plan policies, Accessibility for Ontarians with Disabilities Act & City of Ottawa Standards.
- / **Bicycle Storage:** Bicycle parking spaces for residential and retail users are provided in weather protected areas, primarily within the underground parking levels, with controlled access / secure enclosures.
- / **Bird Friendly Design Guidelines:** Bird friendly design guidelines have been incorporated in the podium of three buildings as per City of Ottawa Bird Friendly Design Guidelines. A minimum of 90% of the glass within the first 16 metres is proposed to be treated with visual markers. The treatment of the glass within this area will be accordance with the guidelines.
- / **Additional Elements:** The sustainability items related to the landscaping, site layout, minimizing light pollution, irrigation and water management will be considered in future stages as the design advances.

2.0 Planning Rationale Addendum

City of Ottawa Official Plan Review

Since the initial application, the City of Ottawa has adopted a new Official Plan. It was approved by the Ministry of Municipal Affairs and Housing November 4, 2022. The Plan provides a framework for the way that the City will develop until 2046 when it is expected that the City's population will surpass 1.4 million people. The Official Plan directs how the city will accommodate this growth over time and set out the policies to guide the development and growth of the City. This Planning Addendum addresses the policies of this new Official Plan.

Section 3: Growth Management Framework

Section 3 of the Official Plan contains the City's Growth Management Framework, where **Section 3.2** (Intensification) allocates 47% of city-wide dwelling growth to the built-up portion of the urban area and 46% of city-wide dwelling growth to the greenfield portion of the urban area (**Policy 3.2.1**). This policy further states that the target amount of dwelling growth in the urban area that is to occur through intensification is 51% and represents the proportion of new residential dwelling units.

The Official Plan encourages intensification to occur in a variety of built forms and height categories, from low-rise to High-Rise 41+ buildings provided density requirements are met. Minimum densities are intended to establish a minimum starting point for the intensity of development, and maximum building heights are intended to establish a limit to building height (**Policy 3.2.2**). Most of the residential intensification shall focus within 15-minute neighbourhoods, which are comprised of Hubs, Corridors and lands within the Neighbourhood designations that are adjacent to them. Specifically, Corridors are intended to be diverse concentrations of employment, commercial, community and transportation services, in addition to accommodating significant residential opportunities (**Policy 3.2.3**).

Intensification is permitted in all designations where development is permitted considering whether the site has municipal water and sewer services and shall be in conformity with transect and overlay policies as applicable (Policy 3.2.4). More specifically, intensification is permitted and encouraged on brownfield sites to collectively achieve intensification and sustainable and resilient design goals and targets (**Policy 3.2.5**).

Intensification should occur in a variety of dwelling unit floorspace sizes to provide housing choices, including via small-household dwellings (units with up to two bedrooms, typically within apartment built forms) and large-household dwellings (units with three or more bedrooms or an equivalent floor area, typically within ground-oriented built forms) (**Policy 3.2.8**). The Official Plan establishes a residential density and proportion of large household dwelling targets for Mainstreet Corridors, where higher densities are directed closer to Mainstreets, Minor Corridors, rapid transit stations, and major neighbourhood amenities (**Policy 3.2.10**). A minimum residential density requirement for intensification of 120 dwellings per net hectare and a proportion of large household dwellings within intensification minimum of 5%, and target of 10%. Density targets and the overall Growth Management targets shall be implemented in the Zoning By-law (**Policy 3.2.10**).

The proposal is supported by the new Official Plan policies related to growth management and intensification. The subject site is within 100 metres of the New Orchard LRT Station, and the proposal conforms the intensification policies which direct higher densities to rapid transit stations, Mainstreets, and major neighbourhood amenities. The proposal provides a built form which is compact, containing a mix of uses, and that achieves minimum residential density targets for Mainstreets while expanding the housing type and tenure available in the neighbourhood. Through the redevelopment of a former automotive use, brownfield site along a Mainstreet Corridor, the proposal conforms to the intent of the City's intensification policies.

Sections 5 and 6: Inner Urban Transect & Mainstreet Policies

The subject site is located within the Inner Urban Transect and designated "Mainstreet" as shown in Schedule B2 of the new Official Plan.



Figure 5: Schedule B2, Inner Urban Transect, subject site indicated

The Inner Urban Transect existing built form and site design includes both urban and suburban characteristics, however its intended pattern is urban (**Policy 5.2.1.1**) where the City supports the development of large parcels into fully urban districts and integrated neighbourhood centres to achieve this pattern of development (**Policy 5.2.1.2**). The Inner Urban Transect is planned to accommodate mid- to high-density development, subject to proximity and access to frequent street transit or rapid transit and, limits on building heights and massing, as per the underlying functional designation, and the separation of tower elements, established through secondary plans or area-specific policy, the functional designations and urban design policies in Subsection 4.6 (**Policy 5.2.1.3**).

The Inner Urban Transect shall continue to develop as a mixed-use environment, where Mainstreets provide residents with a full range of services within close walking distance and increases in existing residential densities supported to sustain the full range of services (**Section 5.2.1.4**). Along Mainstreets, properties that front on segments of streets whose right-of-way is 30 metres or greater, and where the parcel is of sufficient size to allow for a transition in built form massing, high-rise (40-storey) building heights are supported (**Policy 5.2.3.4**). All buildings along Mainstreets shall have active entrances facing the Mainstreet, regardless of use (**Policy 5.2.3.2**). The Inner Urban area is planned for high-density urban forms where no onsite parking is provided, or where parking is arranged on a common parking garage (**Policy 5.2.1.5**), with motor vehicle parking only required to the extent needed to offset sudden large increases in parking demand (**Policy 5.2.2.3**).

Development within the Corridor designation shall establish buildings that locate the maximum permitted building heights and highest densities close to the Corridor, subject to ensuring appropriate transitions in height, use of land, site design, and development character through the site. Larger redevelopment sites may be required to provide public mid-block pedestrian connections to nearby streets or abutting designations and shall be required to establish an enhanced circulation network throughout the site that prioritizes the needs of pedestrians, cyclists and transit users (for sites larger than one hectare in size) (**Policy 6.2.1.2**).

Both residential uses and non-residential uses are permitted along Corridors, where integrated within a dense, mixed-use urban environment, where the City encourages commercial and service uses on the ground floor and residential uses on the upper floors (**Policy 6.2.1.3**). Development shall address the Corridor and vehicular access shall be provided from the side street (**Policy 6.2.1.4**). The Official Plan permits a mix of uses, where active

commercial or service uses should be located on the ground floor to maintain, extend, or create a continuous stretch of active frontages along a Mainstreet (**Policy 6.2.2.1**).

The proposal conforms to Inner Urban Transect and Mainstreet Corridor policies of the Official Plan. The proposal provides a high-density, urban built form along a Mainstreet Corridor, supported by existing services, infrastructure, and rapid transit while contributing a new mixed-use environment on a large, underutilized, brownfield site. The proposal provides a site configuration and building design which is suitable for the subject site and achieves the Mainstreet Corridor policy objectives related to transition, urban design, vehicle parking, enhanced circulation, active frontages, and the pedestrian realm. Considering the intent of the policy related to right of way width as a determinant of locations for 40-storey buildings, the 38 and 40-storey building heights of the proposed development are appropriate considering the combined and continuous right-of-way along Richmond Road, the Byron Avenue Linear Park, and Byron Avenue is approximately 50 metres, not including additional building setbacks (greater than 30 metres), while the size of the subject site is greater than 1 hectare and is in close proximity to rapid transit (within 100 metres).

Section 4.6: City Wide Policies; Urban Design

Section 4.6 of the Official Plan contains city-wide policy direction related to Urban Design. Urban Design is the process of giving form and context to a city to create the theatre of public life and plays an important role in supporting the City's objectives such as building healthy 15-minute neighbourhoods, growing the urban tree canopy, and developing resilience to climate change. New development should be designed to make healthier, more environmentally sustainable living accessible for people of all ages, genders, and social statuses.

Development in along Corridors shall respond to context, transect area, and overlay policies. The development should generally be located to frame the adjacent street, park, or greenspace, and should provide an appropriate setback within the street context, with clearly visible main entrances from public sidewalks. (**Policy 4.6.5.2**) Further, development shall minimize conflict between vehicles and pedestrians and improve the attractiveness of the public realm by internalizing all servicing, loading areas, mechanical equipment, and utilities into the design of the building, and by accommodating space on the site for trees, where possible (**Policy 4.6.5.3**).

To minimize impacts on low-rise areas, transition in building heights shall be designed in accordance with applicable design guidelines. Transition requirements for high-rise buildings shall be applied between existing buildings of different heights, where the planned context anticipates the adjacency of buildings of different heights, within a designation that is the target for intensification, and between a Corridor and a surrounding low-rise area (**Policy 4.6.6.1**). Specifically, transitions between high-rise buildings and adjacent properties designated as Neighbourhood are to be achieved by providing a gradual change in height and massing, through the stepping down of buildings, and setbacks from the low-rise properties, generally guided by the application of an angular plane (**Policy 4.6.6.2**). Where two or more high-rise buildings exist within the immediate context, new high-rise buildings shall relate to the surrounding buildings and provide a variation in height (**Policy 4.6.6.3**).

High-rise buildings shall be designed to respond to context and transect area policies, and should be composed of a well-defined base, middle and top, with floorplates generally be limited to 750 square metres for residential buildings. Larger floorplates may be permitted with increased separation distances (**Policy 4.6.6.8**). High-rise buildings shall require separation distances between towers to ensure privacy, light and sky views, with maximum separation distances achieved through appropriate floorplate sizes and tower orientation. A 23-metre separation distance is desired, however less distance may be permitted (**Policy 4.6.6.9**).

Amenity areas shall be provided in residential development and should serve the needs of all age groups, and consider all four seasons, taking into account future climate conditions. Amenity areas should provide protection from heat, wind, extreme weather, noise and air pollution, and (for indoor amenity areas), be multi-functional spaces (**Policy 4.6.6.4**).

The proposal conforms to the urban design policies of the Official Plan. The proposal provides a built form which responds to its existing and planned context, both along a Mainstreet Corridor and within 100 metres of the New Orchard LRT Station by providing a high-density built form among other high-rise buildings on the lands closest to the rapid transit station. The proposed development is separated between low-rise

residential areas by Richmond Road, the Byron Linear Park and LRT corridor, and Byron Avenue by approximately 50 metres (not including additional setbacks), and provides adequate setbacks from abutting property lines to sites which contain high-rise buildings. The proposal provides varying tower heights with a mid-rise form which relate to the surrounding context, with podium heights designed proportional to the right-of-way, mid-rise form, and tower portions. Although the floorplate sizes slightly exceed 750 square metres, the policy references this generally and contemplates larger floorplates when separation distances exceed 23 metres. The proposal incorporates appropriate design elements, including active frontages, setbacks, stepbacks, tower orientations, separation, floorplate sizes, and materiality which conform to the intent of the Official Plan and Secondary Plan and ensures an improved public realm and appropriate transitions to the surrounding area.

Sherbourne and New Orchard Secondary Plan

The Sherbourne and New Orchard Secondary Plan is a document which provides planning direction to guide future development and redevelopment of lands that are in close proximity to the Cleary and New Orchard LRT Stations. Policies within this document relate to land use, built form, active frontage, the public realm, and density redistribution and provide direction for how the area will evolve and develop with the arrival of the new LRT stations.

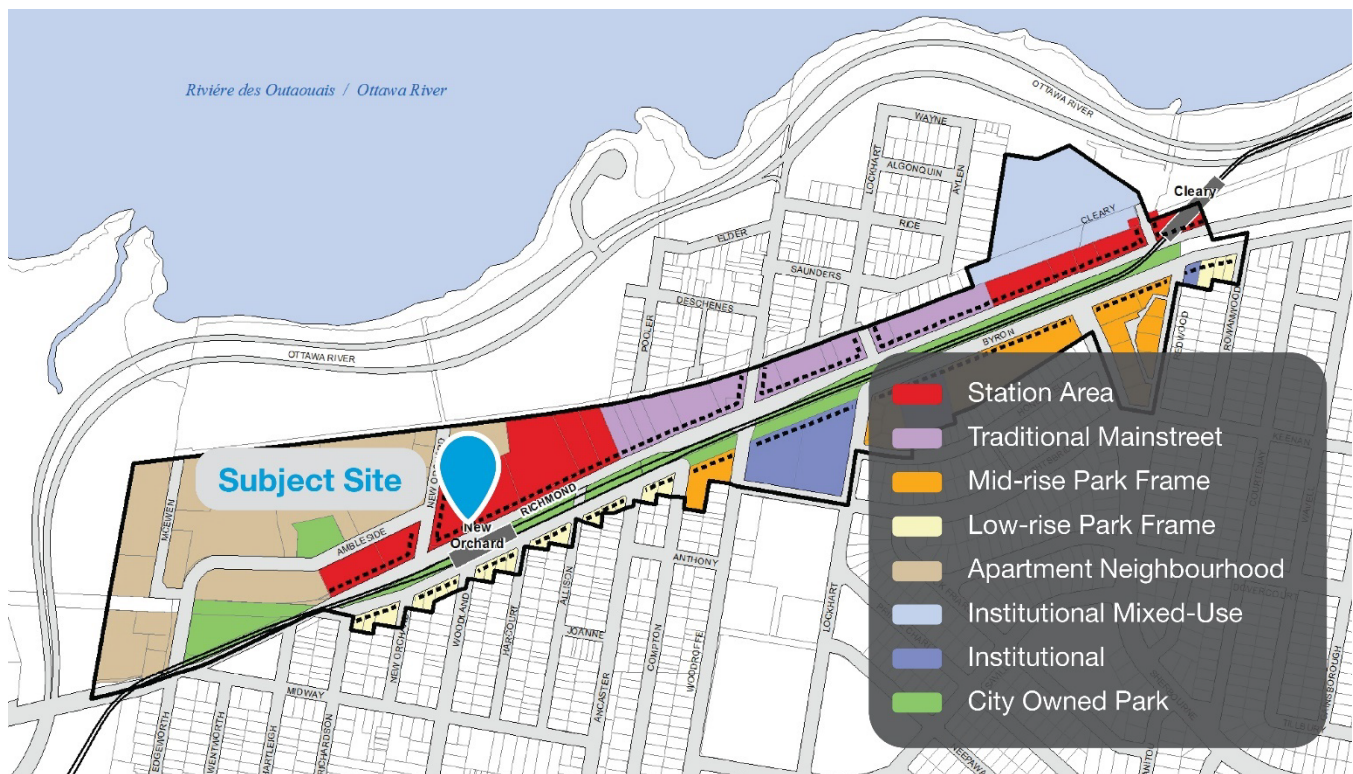


Figure 6: Schedule A, Planning Area and Lane Use, Cleary and New Orchard Secondary Plan, subject site indicated

The subject site is designated as a “Station Area” on Schedule A of the Secondary Plan. Section 3.1 contains policies which require the redevelopment of properties designated Station Area to contain a mix of land uses with a majority or entirety of ground floor space, facing the mainstreet, occupied by retail or commercial uses directly accessed from the sidewalk. curb cuts and the interruption of the sidewalk by vehicular traffic shall be minimized to the greatest extent possible by combining access points, including those leading to loading areas with those leading to parking garages, and locate those access points on side streets in the case of corner lots. All parking is to be provided internal to the site or underground (**Policy 3.1**). Redevelopment in this area is to be with buildings up to 20 storeys in height in accordance with Section 4: Built Form, Policy 6) and Schedule C – Density Redistribution Plan and be in accordance with the above Policy 3.1 (**Policy 3.4**).

Section 4 contains policies for High-Rise Buildings, and limits in the planning area to building heights between 10 to 30 storeys. Where high-rise buildings are identified as appropriate, all of the following provisions are to be met to qualify for a high-rise building:

- a. Lot size generally of 2,000 square metres or more;
- b. Provision of a separation distance between towers of generally 20 metres;
- c. Tower portion of proposed building generally being a minimum of 10 metres from abutting property lines; and,
- d. The podium animating the pedestrian realm, generally forming a continuous street wall for the totality or as close as possible to the totality of the frontage, should relate to the adjacent buildings in massing, height and architectural rhythm (Policy 4.1).

Additionally, Section 4 (**Policy 4.2**) contains provisions required in the design of high-rise buildings, including:

- a. The tower should be set back from the podium faces and should be articulated to break up building mass and allow sky view, sunlight and transition towards the abutting properties;
- b. The tower should be designed to address shadow impacts of adjacent sensitive areas, including low-rise neighbourhood;
- c. Mechanical penthouses should be architecturally integrated into the tower's design and remain consistent with the overall character of the tower;
- d. The tower should be well-proportioned and generally take the podium and tower approach in built form design;
- e. The podium height should be no less than three storeys but no more than six.

Section 4 (**Policy 4.4**) further contains policies for Active Frontages and requires all development on the north side of Richmond Road and east side of New Orchard Avenue North adjacent to an Active Frontage Line to be designed to animate the public spaces they face. This can be achieved through various techniques, including incorporating pedestrian-oriented uses and architectural features and details that will enhance pedestrian safety and provide visual interest to enrich pedestrian experience. New development will include all the following considerations:

- a. Provision of transparent glazing and active main entrances for a majority of the ground floor wall;
- b. Use of high-quality building materials on all elevations facing the public realm;
- c. Provision of a continuous built edge along the street or public space, with a provision for varied setbacks where a Privately-Owned Public Space is provided at the ground level;
- d. No exterior surface parking between the building and the street.
- e. No interior parking spaces being located on the ground or upper floors of any mid- or high-rise building;
- f. Provision of individual direct pedestrian access to ground level uses;
- g. Parking internal to a site being screened from view to any public realm space; and,
- h. The public realm includes existing or new trees.

Density Redistribution policies are further described in Section 4, and state that Council may approve a built form design that deviates from the maximum building height provisions described in Section 3: Land Designation Policies, for properties identified on Schedule C - Density Redistribution Plan, provided all the following conditions are met:

- a. The lot is a minimum of 2000 square metres in size;
- b. The floor plate for a residential tower is generally a maximum of 750 square metres in size;
- c. The lot must abut at least two public realms (such as roads paths or greenspaces);
- d. The proposed development does not exceed the maximum height allowance detailed in Schedule C - Density Redistribution Plan;
- e. The proposed development provides a minimum 200 square metres of Privately Owned Public Spaces;
- f. The proposed development achieves the built form principles found in Section 4, as applicable.



Figure 7: Schedule C, Density Redistribution, Cleary and New Orchard Secondary Plan, subject site indicated

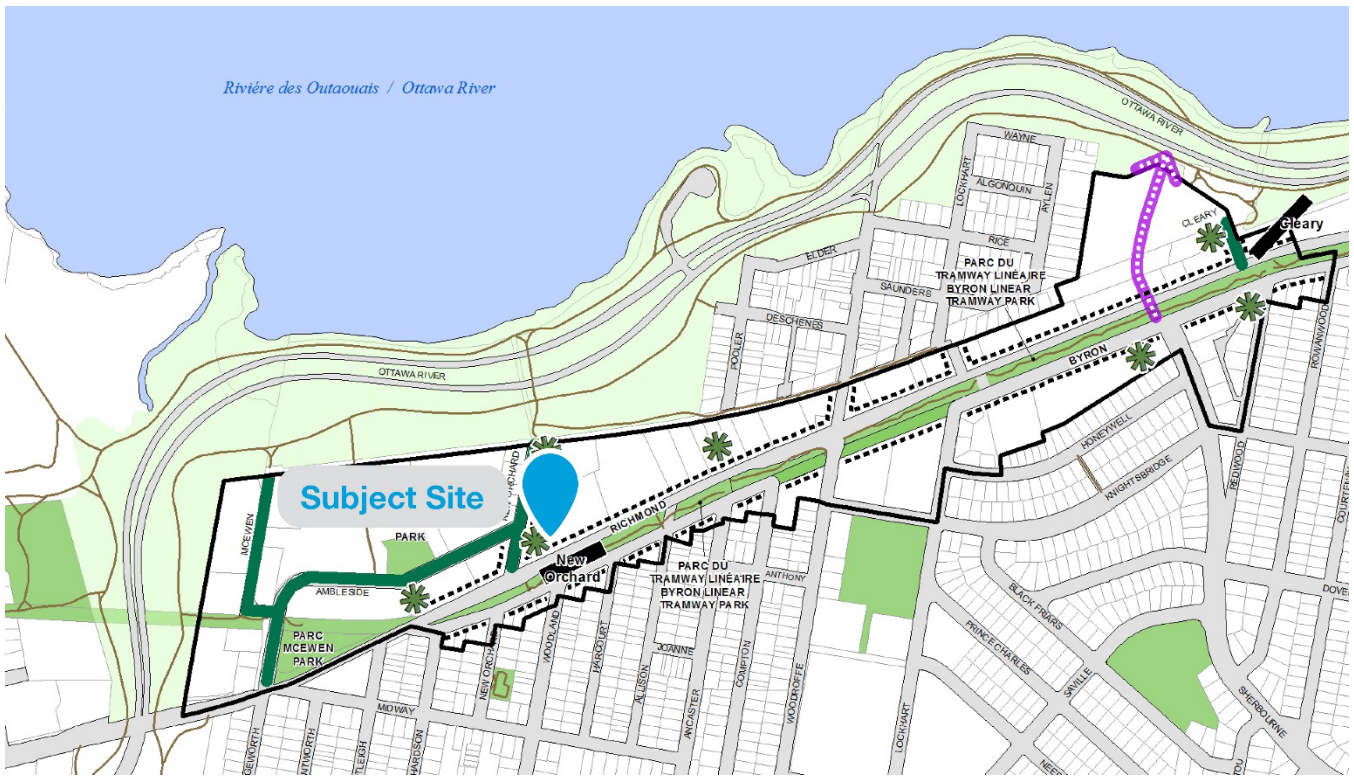


Figure 8: Schedule B, Public Realm, Cleary and New Orchard Secondary Plan, subject site indicated

Section 5 requires that new urban parks and plazas be created in association with redevelopment in areas identified Schedule B, the Public Realm Plan, through the development review process with a priority for securing lands through parkland dedication (**Policy 5.1**).

The proposal meets the intent and objectives of the policies for the Sherbourne and New Orchard Secondary Plan. High-rise buildings are planned to be located within the Station Area. Although the Secondary Plan limits high-rise buildings to heights between 10 and 30-storeys, the 40-storey proposal is appropriate as it conforms to a new definition for the maximum height of high-rise buildings of the new Official Plan. The high-rise proposal provides appropriate setbacks and transitions to abutting property lines, while also providing a floorplate size appropriate for the site considering an increased on-site tower separation of between 56.2 metres to 74.8 metres, which greatly exceeds the policies of the Secondary Plan and applicable urban design guidelines. The proposal provides a mix of land uses, with commercial uses proposed within the ground floor and residential uses above. Vehicle parking is located underground, with pick-up and drop-off locations for all three buildings located internally and away from the public realm. Underground parking and loading are accessed via a shared access from New Orchard Avenue to prevent disruption along the Mainstreet. The proposal has been designed in a manner which incorporates a mid-rise podium, with towers setback to enhance the public realm and maintain the pedestrian scale along each abutting right of way.

Apart from the maximum height of 20-storeys described on Schedule C, the proposal is appropriate conforming to each of the conditions for density redistribution. The high-rise built form is provided on a lot greater than 2,000 square metres in size and provides tower floorplate sizes ranging between 803 square metres and 863 square metres of gross floor area, which are expected to be “generally” a maximum of 750 square metres. The Secondary Plan does not specifically identify how the footprint is to be measured and does not also contemplate how larger sites may be able to meet the objective of the footprint policy with modest increases in tower footprints sizes through increased tower separation distances where standard tower separations and slimmer footprints could accommodate an additional tower while meeting the specifics of the policies but arguably less of the intent.

The proposal provides frontage along two public realms while the tower form allows greater at-grade POPS and public space. A 1,013 square metre public park is provided at the corner of Richmond Road and New Orchard Avenue North, while POPS areas are provided via an internal courtyard and pedestrian connections throughout the site. The density of the proposal is highly supportive of the nearby New Orchard LRT Station, rapid transit in the nearby area, and neighbourhood amenities includes various commercial uses, parks, cycling trails and pedestrian connections.

The proposal applies the policies of the Secondary Plan related to active frontages, the public realm, and density redistribution. Along the Richmond Road and New Orchard Avenue frontages, commercial uses are provided with active entrances and highly transparent building materials, activating the public realm of the corner site. Direct pedestrian access is provided throughout the site to at-grade uses, where parking is located underground, and vehicle circulation is directed internally and to the rear of the site. The public realm along all frontages is enhanced with new at-grade commercial uses and landscaping.

City of Ottawa Zoning By-law

The subject site is zoned Traditional Mainstreet, with Urban Exception 2494 and a maximum building height of 25 metres applied (TM[2494] H(25)). The Traditional Mainstreet zone is intended to accommodate a broad range of uses including retail, service commercial, office, residential and institutional uses, and mixed-use buildings. This zoning fosters and promotes compact, mixed-use, pedestrian-oriented development that provide for access by foot, cycle, transit and automobile.

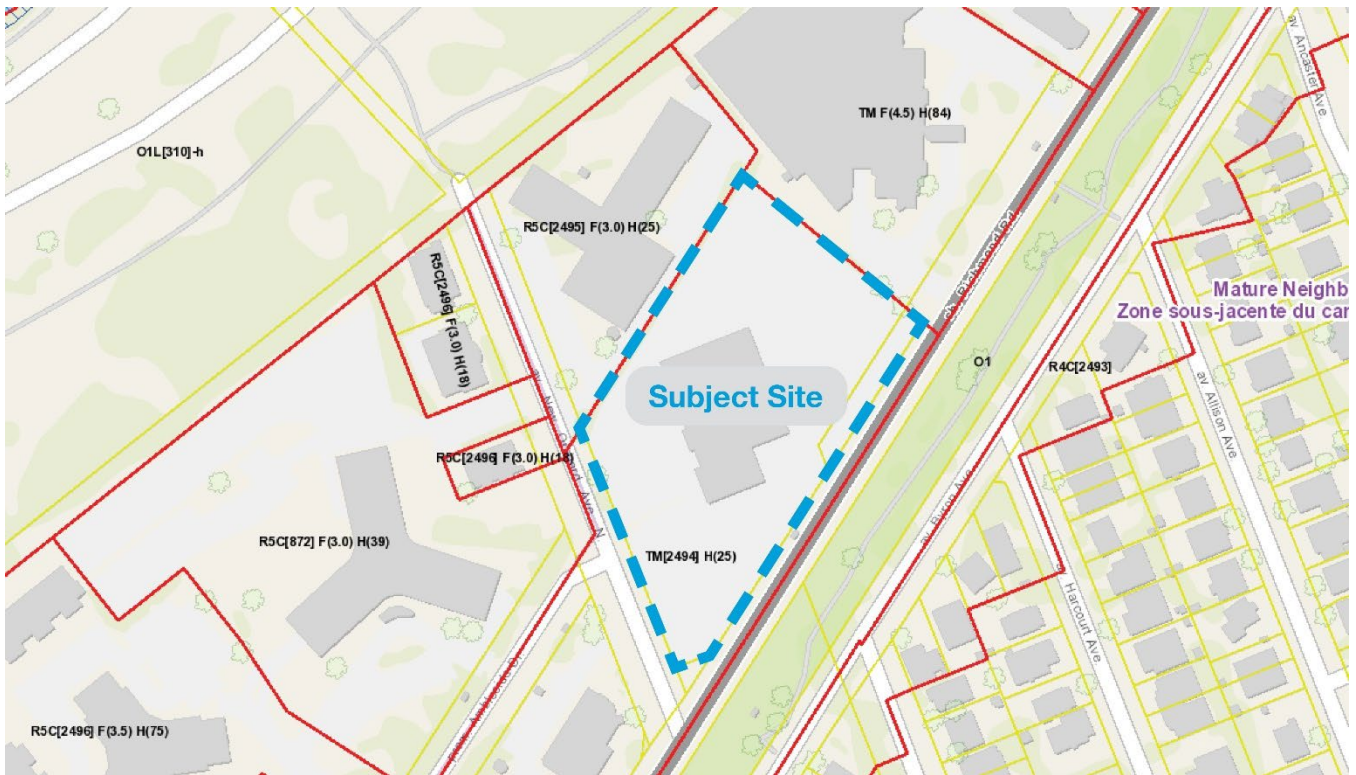


Figure 9: Zoning Map, subject site indicated

Table 1 demonstrates some of the permitted uses on the subject site within the existing TM[2494] H(25) zone:

Table 1: Permitted Uses in the TM[2494] H(25) Zone

Permitted Uses
<p>Residential apartment dwelling, low rise, apartment dwelling, mid rise, bed and breakfast, dwelling units, group home, home-based business, home-based day care, retirement home, rooming house</p> <p>Non-Residential amusement centre, animal care establishment, animal hospital, artist studio, bank, bank machine, catering establishment, cinema, click and collect facility, community centre, community health and resource centre, convenience store, day care, diplomatic mission, emergency service, hotel, instructional facility, library, medical facility, municipal service centre, museum, office, park, parking garage, payday loan establishment, personal brewing facility, personal service business, place of assembly, place of worship, post office, recreational and athletic facility, research and development centre, residential care facility, restaurant, retail food store, retail store, school, service and repair shop, storefront industry, theatre, training centre, urban agriculture</p>

Table 2 provides a summary of the Traditional Mainstreet zoning provisions and other applicable provisions related to the subject site as detailed in the Zoning By-law.

Table 2: Zoning Provision, Required, Provided and Compliance Table

Provision	Required	Provided	Compliance
Minimum Lot Width	No Minimum	80.7 metres	Yes
Minimum Lot Area	No Minimum	10,188m ²	Yes

Provision	Required	Provided	Compliance
Front Yard Setback	No Minimum Maximum: 2 metres	3.0 metres + to new front lot line	No
Interior Side Yard Setback	Maximum: 3 metres between a mixed-use building and another non-residential use building or mixed-use building, except where a driveway is provided, in which case the setback must be a maximum of 6 metres where the driveway leads to a parking area of 20 or more spaces.	Between 2.1 metres and 5.0 metres Future setback compliance to be determined from lot lines abutting parkland dedication	No
Corner Side Yard Setback	3 metres , except for any part of a building above 15 metres for which an additional 2 metre setback must be provided	3.0 metres Stepback above 15 metres: 2.0 metres	Yes
Rear Yard Setback	Abutting a residential zone: 7.5 metres	10 metres	Yes
Minimum Building Height	6.7 metres for a distance of 20 metres from the front lot line	> 6.7 metres	Yes
Maximum Building Height	25 metres Urban Exception [2494] states: where the building height is greater than two storeys or 8 m, whichever is less, between the height of 8 m and 15 m, a building must have an additional setback of at least 5 m than the provided setback from the front lot line and from a side lot line abutting a street and that additional setback is continued from the point where it is provided and zoning mechanism 197(c) in Table 197 does not apply to that additional setback.	Tower A: 127 metres (40 storeys) + 6m Mechanical Penthouse The total height is 133 metres Tower B: 121 metres (38 storeys) + 6m Mechanical Penthouse The total height is 127 metres Mid-Rise Building C: 23.4 metres (6-storeys), + 4m mechanical penthouse. The total height is 27.4 metres Towers provide a stepback from the podium of 2.0m +	No
Minimum Width of Landscaped Area	Abutting a residential zone: 3.0 metres (may be reduced to one metre where a minimum 1.4 metre high opaque fence is provided)	1.3 metres (north property line), with a 1.4 metre fence is to be provided.	Yes
Active Entrances	The façade facing the main street must include at least one active entrance serving each residential or non-residential use occupying any part of the ground floor	Active entrances are provided along Richmond Road	Yes

Provision	Required	Provided	Compliance
Amenity Space (Section 137)	6.0m ² / unit, 50% require communal 1,152 units x 6.0 = 6,912m ² of amenity space required. 3,456m ² required as communal	Total Amenity Space: 10.551m ² Private (via balconies / terraces): 6,818m ² Communal: 3,733m ²	
Minimum Width of a Private Way (Sec. 131)	6.0 metres	6.0 metres	Yes
Minimum setback for any wall of a residential use building to a private way (Sec. 131)	1.8 metres	Less than 1.8 metres for portions of the central drop off location	No
Permitted Projections above the Height Limit (Sec. 64)	the maximum height limits do not apply to the [mechanical and service equipment penthouses, elevator, or stairway penthouses] that may require a height in excess of maximum height limits in order to serve their intended purpose, unless otherwise specified in the by-law and provided these structures are erected only to such height or area as is necessary to accomplish the purpose they are to serve and that is necessary to operate effectively and safely	Tower A: 127 metres (40 storeys) + 6m Mechanical Penthouse The total height is 133 metres Tower B: 121 metres (38 storeys) + 6m Mechanical Penthouse The total height is 127 metres Mid-Rise Building C: 23.4 metres (6-storeys), + 4m mechanical penthouse. The total height is 27.4 metres	Yes
Permitted Projections into Required Yards (Sec. 65)	Uncovered/Covered Balconies 2 metres, but no closer than 1 metre from the property line	> 1.0 metres from Richmond Road and New Orchard Avenue lot lines	No
High-Rise Zoning Provisions (Sec. 77)	The subject site is not located within Area A or Area B of Schedule 402, therefore the provisions of Section 77 are not applicable	N/A	N/A

Table 3 below describes the Zoning By-law's vehicle and bicycle parking requirements, and how the proposed development complies with these provisions.

Table 3: Parking Provisions, Provided, and Compliance Table

Provision	Required	Provided	Compliance
Minimum Parking (Area Z)	No off-street motor vehicle parking is required to be provided	621 residential parking spaces provided (0.5 / unit)	Yes

Max. Parking near Rapid Transit Station	1.75 per dwelling unit (combined total of resident and visitor parking)	689 residential and visitor spaces combined	Yes
Visitor Parking Required	0.1 spaces / dwelling unit, less the first 12 units to a maximum of 30 visitor parking spaces (per building)	68 visitor spaces provided Tower A: 512 units - 12 x 0.1 = 50 spaces (therefore 30 max) Tower B: 550 units - 12 x 0.1 = 54 spaces (therefore 30 max) Mid-Rise Building C: 90- 12 units = 78 x 0.1 = 8	Yes
Parking Space Dimensions	2.6m width x 5.2m length	2.6m width x 5.2m length	Yes
Bicycle Parking	Residential: 0.5 / dwelling unit 1,151 x 0.5 = 576 spaces required Retail Store: 1.0 / 250m ² commercial space = 3 spaces required (for 859m ² retail space)	726 total bicycle spaces provided	Yes
Bicycle Parking Dimensions	0.6m width x 1.8m length, with a 1.5 metre aisle provided	0.6m width x 1.8m length, with a 1.5 metre aisle provided	Yes

Proposed Official Plan Amendments

Notwithstanding the aforementioned rationale related to the intent of the Secondary Plan policies, a site-specific Official Plan Amendment is proposed to the Sherbourne and New Orchard Secondary Plan as follows:

- / Amend Policy 3.4.a which states that the Station Area adjacent to New Orchard Station contains lands that front onto Richmond Road, New Orchard Avenue and Ambleside Drive. Redevelopment in this area will meet all of the below: a) Be with buildings up to 20 storeys in height in accordance with Section 4: Built Form, Policy 6) and Schedule C – Density Redistribution Plan” to permit high-rise buildings up to 40-storeys at 1047 Richmond Road.
- / Amend Policy 4.1.1 which states that “High-rise buildings are limited in the plan area between 10 to 30 storeys” to permit high-rise buildings up to 40-storeys at 1047 Richmond Road.
- / Amend Schedule C, *Density Redistribution*, by revising the current maximum permitted building height of 20 storeys to allow building heights up to 40 storeys at 1047 Richmond Road.

Proposed Zoning By-law Amendments

The Zoning By-law Amendment application would seek relief from certain provisions of the Zoning By-law on a site-specific basis. A list of zoning by-law amendments has been provided below:

- / **Permitted Use and Maximum Building Heights:** An amendment to the Zoning By-law is sought to permit an “apartment building, high rise” as a permitted use at the subject site with a maximum building height of 127 metres, accommodating the 40-storey proposed development. As described above, both the current and forthcoming City of Ottawa Official Plan and Cleary and New Orchard Area Specific Policy support this use at the subject site. Further, this use is appropriate for the subject site considering its proximity to the New Orchard LRT Station and existing and planned high-rise context. The proposed building height does

not have undue negative impacts on its surrounding context, while the subject site allows sufficient setbacks and transitions to abutting properties.

- / **Front Yard Setback:** The Traditional Mainstreet zone requires a maximum front yard setback of 2.0 metres be provided along the mainstreet, however the development proposes a setback of 3.0 metres. Considering the large size of the site, the amount of density proposed, and existing and planned surrounding context, the proposed increase is appropriate for the site. The proposed mid-rise and podium built forms will frame the street along Richmond Road and provide increased space to the public realm for pedestrian activity, tree planting, and other landscaping. The proposed increase in setback is appropriate for the development of the subject site as it will allow for an improved at-grade condition while still meeting the intent of the Official Plan, Secondary Plan, and Zoning By-law.
- / **Interior Side Yard Setback:** The Traditional Mainstreet zone requires a maximum interior side yard setback of 3.0 metres, however at the widest a setback of 5.0 metres is proposed along the eastern property line. The intent of this provision is to ensure a continuous streetwall along the Traditional Mainstreet. As an existing streetwall does not exist along Richmond Road, the proposed increase in the maximum interior side yard setback is appropriate in facilitating appropriate transition and separation to surrounding properties while still allowing for extensive framing and animation of the Richmond Road frontage through the amount of built form and the proposed park and POPS elements that are designed to fragment the podium mass while offering opportunities for animation and pedestrian respite areas. Further the corner of the building, closest to Richmond Road meets the provision with a setback less than 3 metres.
- / **Minimum Separation from a Building to a Private Way:** Section 131 of the Zoning By-law requires that a private way be a minimum 1.8 metres from a building. The proposed development contains an internal pick-up and drop-off area which for Building C is located closer than 1.8 metres to the building above the ground floor. The proposed reduction to this requirement is appropriate to accommodate the central pick-up and drop-off locations provided for the residential entrance to the mid-rise block, additionally allowing for loading access to be provided centrally and not visible from the public realm, nor Richmond Road and New Orchard Avenue ROWs. As the ground floor meets the requirement there are no concerns from a safety and circulation perspective as the distance at the level of the private way is maintained.
- / **Permitted Projections into Required Yards:** The Zoning By-law requires balconies and uncovered patios project no closer than 1.0 metre from a property line. The proposed development provides balcony projections and at-grade patios which are located within 1.0 metre of future property lines, once ROW protections are accounted for. The proposed amendment is appropriate to allow considering the consideration width of the existing and future ROWs. Further, at-grade patios will provide for more active frontages and enhanced streetscape, ensuring a more urban pattern of development.

Zoning Schedule

A Draft Zoning Schedule has been prepared in support of this Zoning By-law Amendment Application. The Draft Zoning Schedule is intended to set site specific zoning parameters for the proposed development in accordance with the provisions of the Zoning By-law and amendments sought through this application. The Draft Zoning Schedule has been package as part of this resubmission package at assist the City with preparing the amendment.