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1166 Bank Street Planning Rationale



Prepared for: Ambassador Realty Inc.

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1166 Bank Street

Ottawa, Ontario

Planning Rationale

in support of

Minor Zoning By-law Amendment &

Site Plan Control Applications

Prepared For:

Ambassador Realty Inc.

Prepared By:

NOVATECH

Suite 200, 240 Michael Cowpland Drive
Ottawa, Ontario
K2M 1P6

March / 28 / 2023

Novatech File: 122103
Ref: R-2023-049

March 28, 2023

City of Ottawa
Planning, Real Estate and Economic Development Department
110 Laurier Avenue West, 4th Floor
Ottawa, ON, K1P 1J1

Attention: Colette Gorni, Planner I, Development Review, Central

**Reference: Minor Zoning By-law Amendment and Site Plan Control Applications
1166 Bank Street
Our File No.: 122103**

The following Planning Rationale has been prepared in support of Minor Zoning By-law Amendment and Site Plan Control applications to facilitate the development of the property at 1166 Bank Street (the "Subject Property").

The Subject Property is within the Inner Urban Transect and is designated Mainstreet Corridor on Schedule B2 of the City of Ottawa Official Plan. The Subject Property is located within the Evolving Neighbourhood Overlay. The Subject Site is zoned Traditional Mainstreet, Subzone 2, Height limited to 15 metres (TM2 H(15)) and is located within the Mature Neighbourhoods Overlay in the City of Ottawa's Zoning By-law 2008-250.

It is proposed to construct a six-storey, mixed-use building on the Subject Property. The building will include four commercial units on the ground floor and 37 residential dwelling units. A minor rezoning application is required to provide relief from zoning provisions on the Subject Property to increase the maximum permitted building height, to alter the required angular plane for the proposed building from the rear lot line, to reduce the minimum total amenity area requirement, and to reduce the minimum communal amenity area requirement for the proposed development.

A one-level underground parking garage will provide 14 parking spaces for residents and visitors. The parking spaces will be accessible via a driveway and ramp off Grove Avenue. Walkways from Bank Street and Grove Avenue are proposed along the east and south property lines. The proposed walkways will facilitate pedestrian and bicycle access off Bank Street and Grove Avenue to building entrances and bicycle parking located along Grove Avenue and within the building. An indoor amenity area with common access for residents is provided on the fifth floor of the building. No impacts to traffic or existing land uses surrounding the Subject Property are anticipated as a result of the proposed development. A concurrent site plan control application is being filed as part of the submission package.

This Planning Rationale examines the location and context of the Subject Property, the planning policy and regulatory framework applicable to the site and makes recommendations on the Minor Zoning By-law Amendment and Site Plan Control applications required to facilitate the proposed development on the Subject Property.

Should you have any questions regarding any aspect of these applications please feel free to contact me at your earliest convenience.

Yours truly,

NOVATECH

A handwritten signature in black ink, appearing to read 'Jeffrey Kelly', written in a cursive style.

Jeffrey Kelly, MCIP RPP
Project Planner

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1.0 INTRODUCTION

Novatech has prepared this Planning Rationale in support of Minor Zoning By-law Amendment and Site Plan Control applications for the property at 1166 Bank Street (the “Subject Property”). The proposed development consists of a six-storey, mixed-use building with four commercial units located on the ground floor and 37 residential units. 14 underground parking spaces are provided for the proposed development. Pedestrian access is proposed from Bank Street and along Grove Avenue. The proposed development will require demolition of an existing building, modifications to site grading, drainage and landscaping on the property.

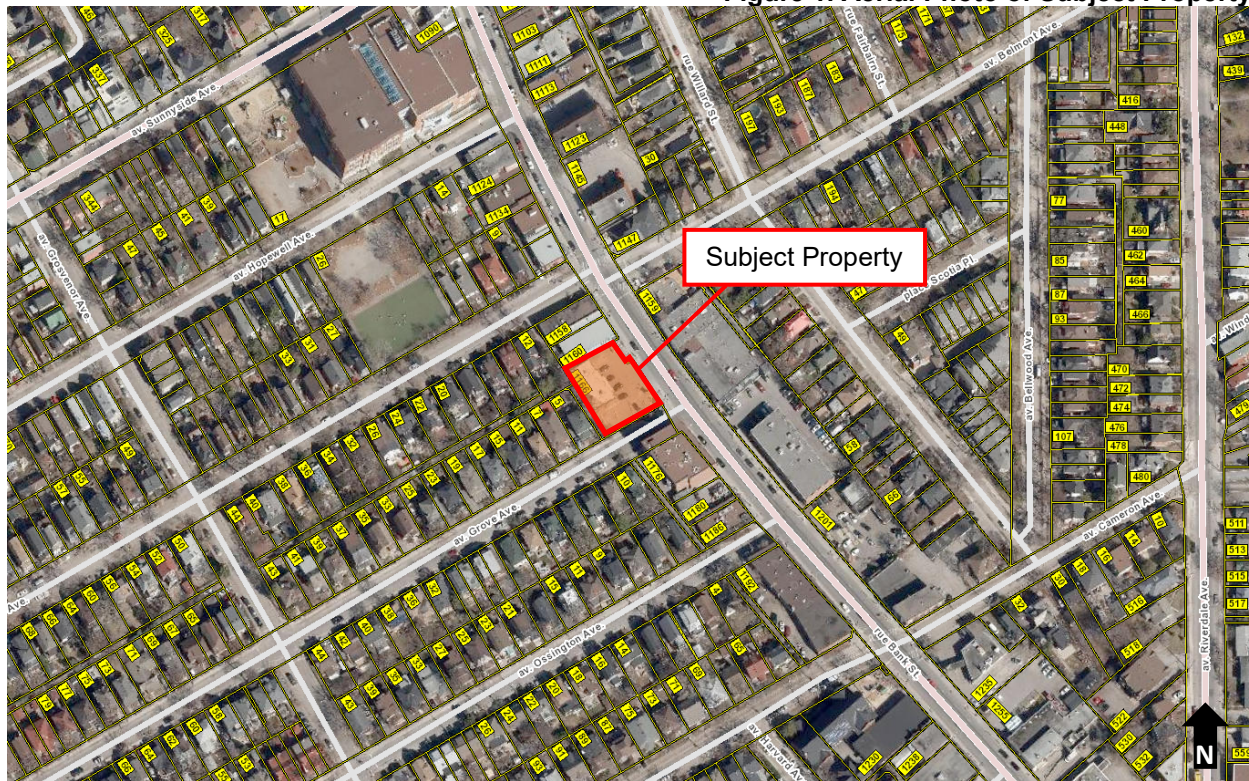
The Subject Property is within the Inner Urban Transect and is designated Mainstreet Corridor on Schedule B2 of the City of Ottawa Official Plan. The Subject Property is within the Evolving Neighbourhood Overlay. The Subject Site is zoned Traditional Mainstreet, Subzone 2, height limited to 15 metres (TM2 H(15)) and is located within the Mature Neighbourhoods Overlay in the City of Ottawa’s Zoning By-law 2008-250.

This Planning Rationale will demonstrate that the proposed Minor Zoning By-law Amendment and Site Plan Control applications will:

- be consistent with the policies of the Provincial Policy Statement (2020);
- conform to the policies of the City of Ottawa Official Plan (2021);
- adhere to relevant Urban Design Guidelines applicable to the Subject Property;
- establish appropriate Zoning standards for the Subject Property; and
- maintain compatibility with the surrounding uses and community.

1.1 Description of Subject Property

Figure 1. Aerial Photo of Subject Property



The Subject Property is located in Ward 17 (Capital) in the City of Ottawa. The Subject Property is located in an area bounded by Glen Avenue to the north, Ossington Avenue to the south, Bank Street to the east, and Grosvenor Avenue to the west (see Figure 1). The Subject Property is currently occupied by a low-rise commercial use building and surface parking. The Subject Property is a corner lot with an approximate area of 1,015 square metres, approximately 36 metres of frontage on Bank Street, and approximately 29 metres of frontage on Grove Avenue.

1.2 Site Location and Community Context

Figure 2: Surrounding Context



The Subject Property is located within the Old Ottawa South neighbourhood. Directly to the south of the Subject Property across Grove Avenue is a four storey mixed-use building containing retail uses at grade with residential uses above. West of the Subject Property is predominantly characterized by a mix of low-rise residential uses. To the north and south along Bank Street are a mix of low-rise retail, restaurant, commercial and office uses. Further to the north is the Rideau Canal and Lansdowne Park. The Rideau River, Riverside Drive, and Billings Bridge Shopping Centre are located further to the south (see Figure 2).

Additional details are provided in Figures 3 to 6 and in the descriptions below.

Figure 3: Land uses to the north of the Subject Property



North: To the north of the Subject Property are small-scale retail and commercial uses. Further north, there are a variety of low-rise retail, restaurants and commercial uses.

Figure 4: Land uses to the south of the Subject Property



South: To the south of the Subject Property is a low-rise, mixed-use building. Commercial uses such as retail stores, offices and restaurants characterize the Bank Street corridor.

Figure 5: Land uses to the west of the Subject Property



West: To the west of the Subject Property is a public laneway and low-rise residential uses.

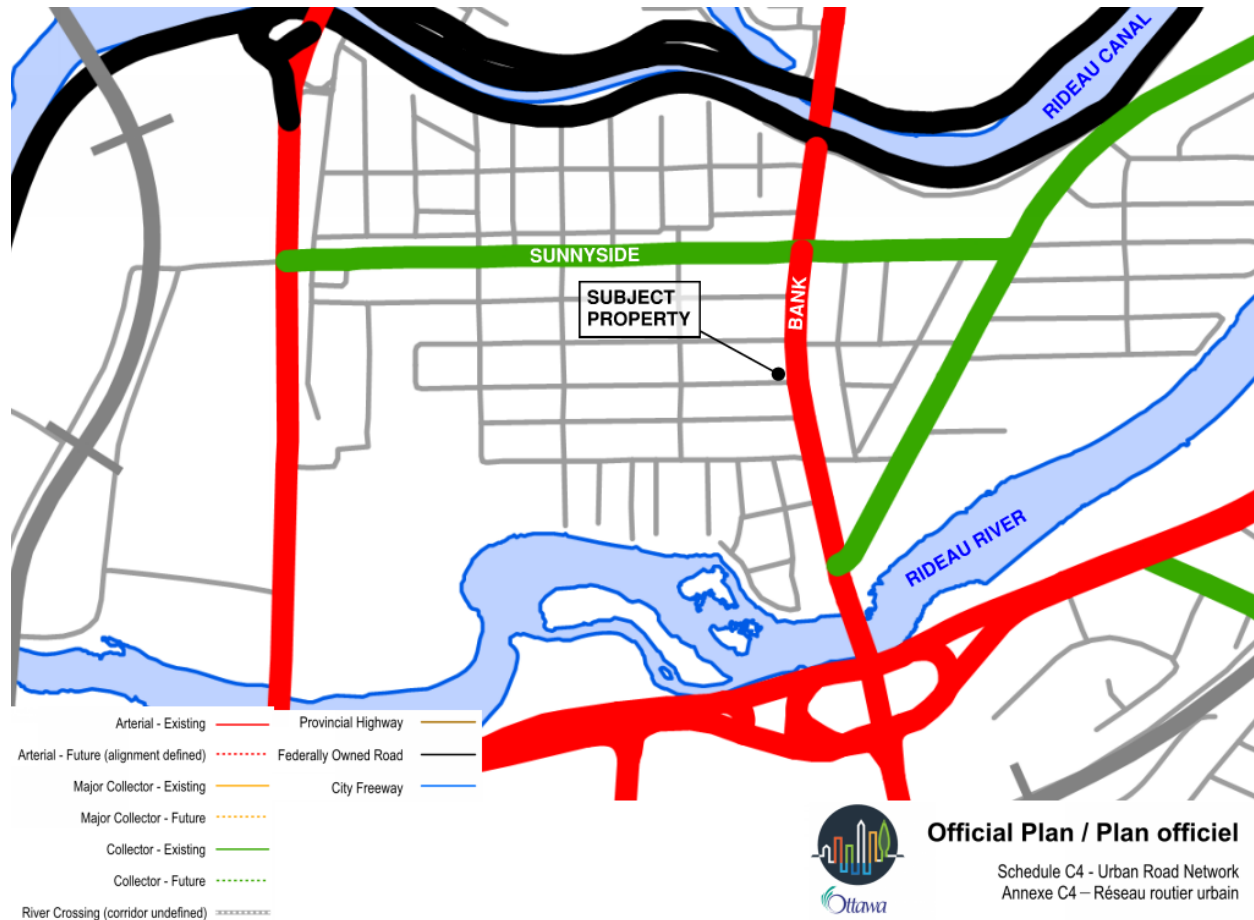
Figure 6: Land uses to the east of the Subject Property



East: Immediately to the east of the Subject Property across Bank Street are a variety of low-rise retail and commercial uses.

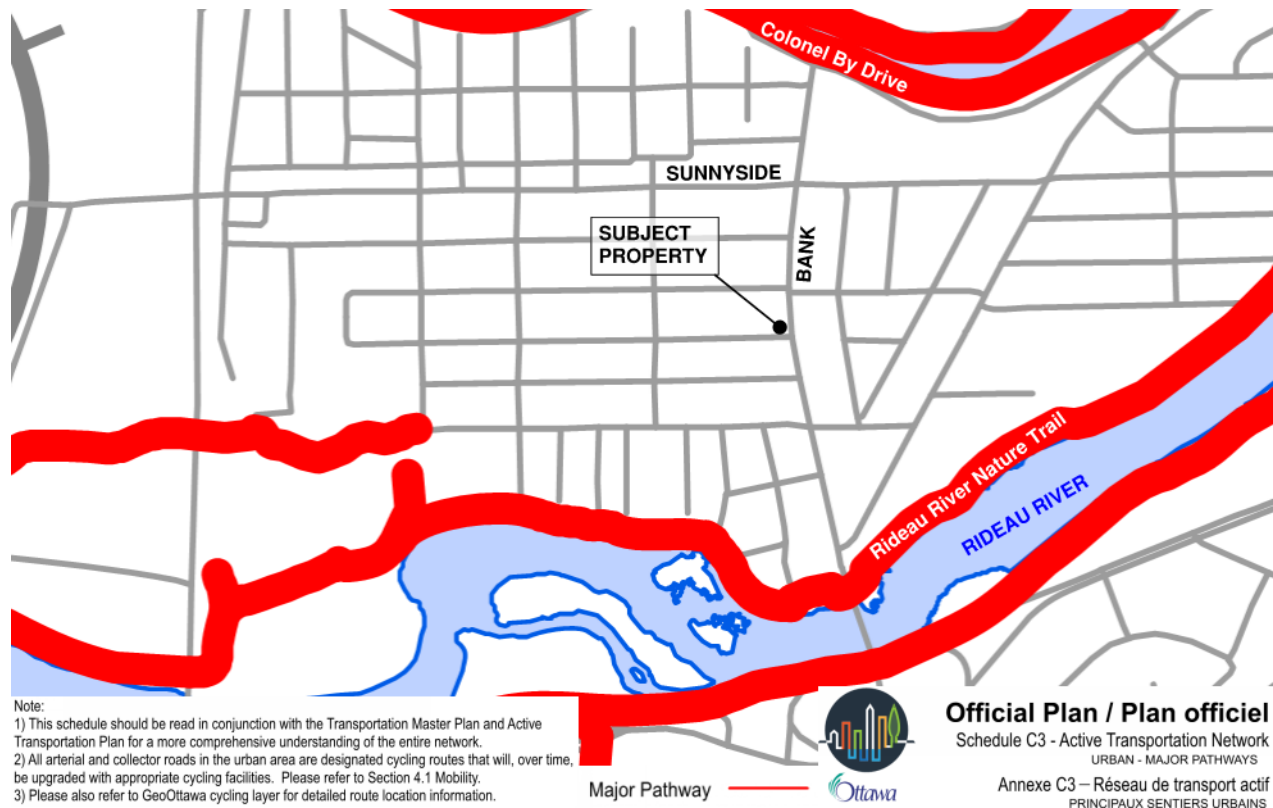
1.3 Linkages and Transportation Framework

Figure 7: Official Plan Schedule C4 Excerpt



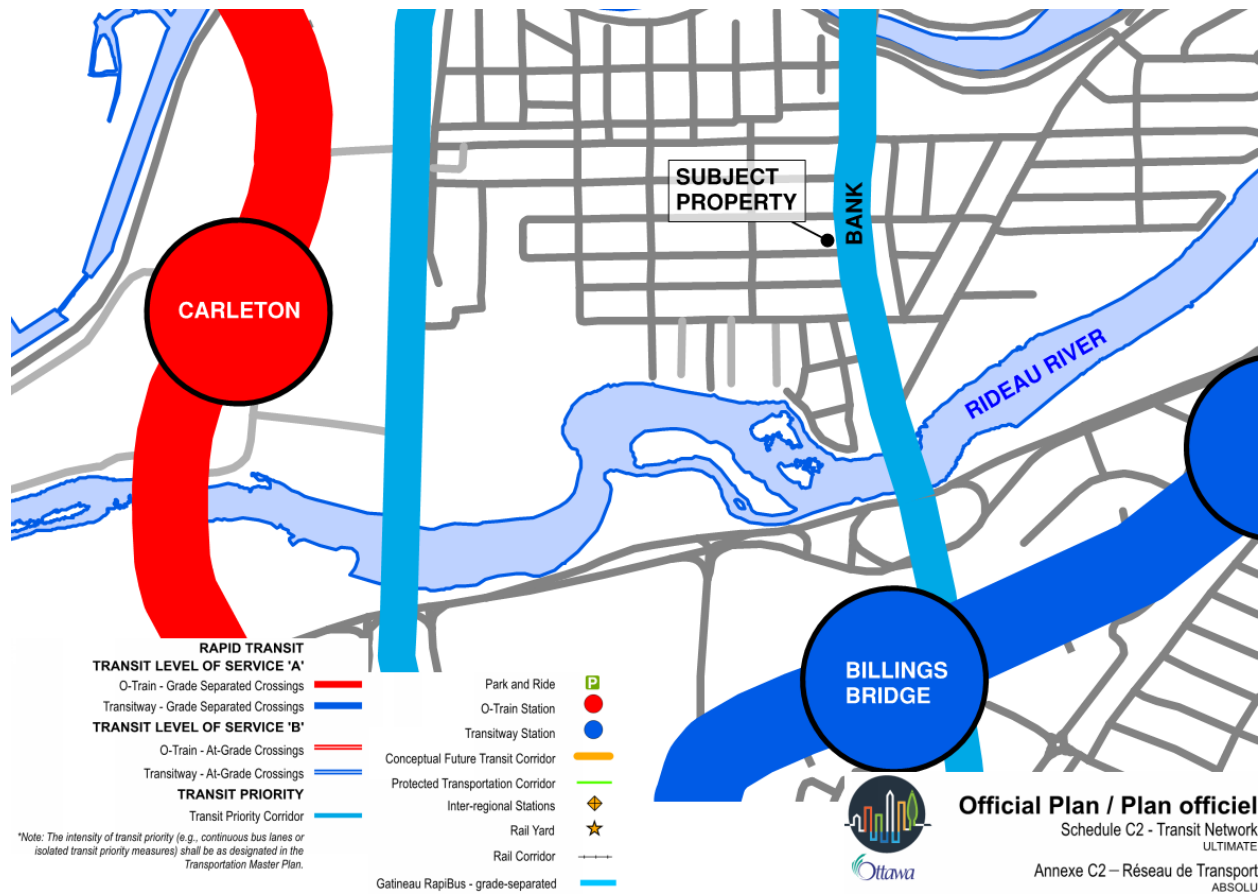
The Subject Property has frontage on Bank Street. The Subject Property is located south of Sunnyside Avenue. Bank Street is designated as an arterial road and Sunnyside Avenue is designated as a collector road on Schedule C4 of the Official Plan (see Figure 7). All arterial and collector roads in the urban area are designated cycling routes (see Figure 8).

Figure 8: Official Plan Schedule C3 Excerpt



The Subject Property is located to the south of Colonel By Drive and to the North of Rideau River Nature Trail which are each designated as Major Pathways on Schedule C3 of the Official Plan (see Figure 8). There are public sidewalks located along Grove Avenue and the east and west sides of Bank Street. Access is proposed off Grove Avenue and Bank Street to facilitate pedestrian and bicycle access to commercial units at grade and to bicycle parking located within the building.

Figure 9: Official Plan Schedule C2 Excerpt



Bank Street is designated as a Transit Priority Corridor on Schedule C2 of the Official Plan. The Carleton O-Train station is located to the west of the Subject Property. The Billings Bridge BRT Station is located to the south of the Subject Property (see Figure 9).

Figure 10. Walking Distance to Transit Network



The Subject Property is located along bus transit route options with connections to rapid transit options in the immediate area (see Figure 10). Bus routes 6 and 7 service the Subject Property directly with a bus stop provided in front of the property along Bank Street and on the south side of Grove Avenue across from the Subject Property. Bus routes 6 and 7 are designated as frequent routes offering high frequency bus service along major roads on the OC Transpo network.

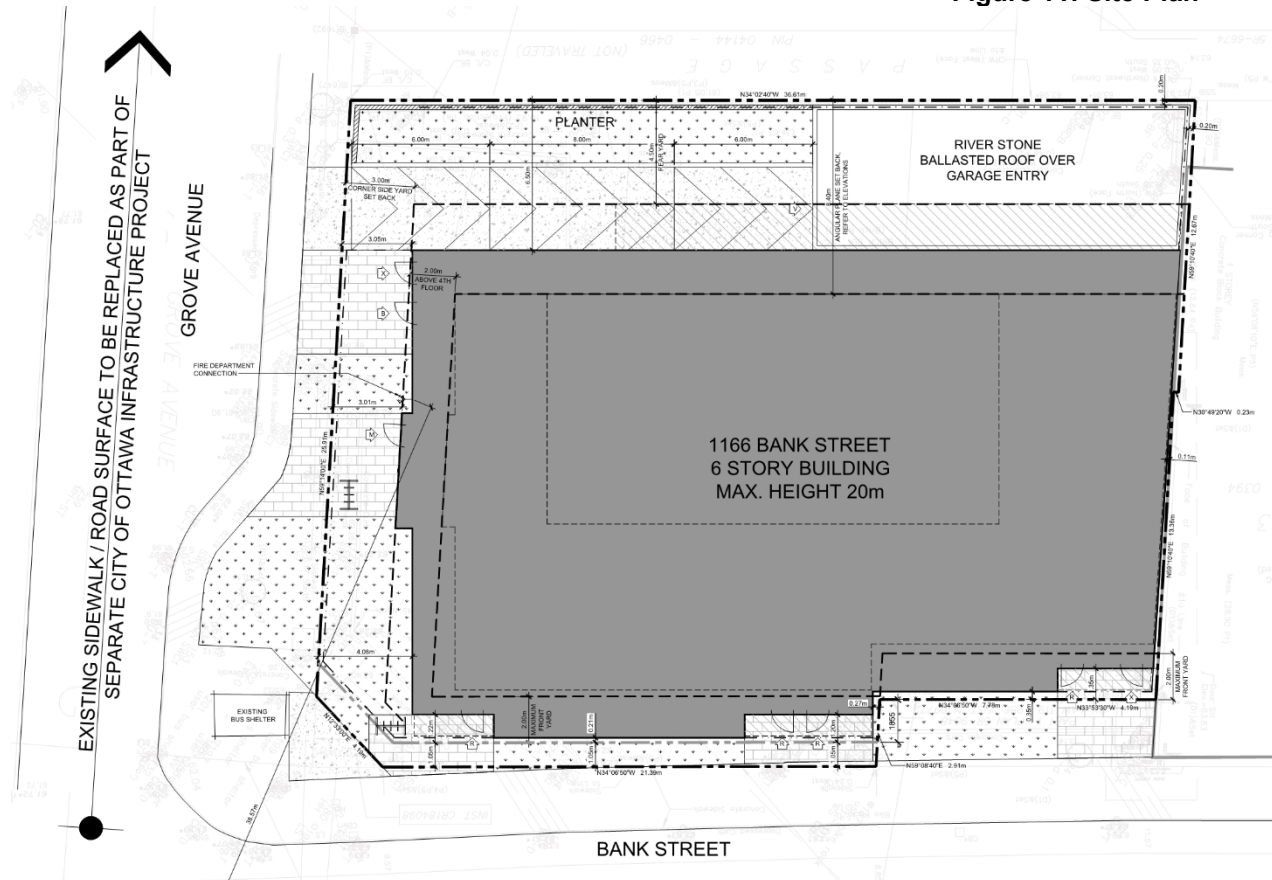
2.0 DEVELOPMENT PROPOSAL

It is proposed to develop a six-storey, mixed-use building on the Subject Property. The proposed building will have four commercial units on the ground floor. 37 residential units will be provided ranging in size from 1-bedroom to 2-bedroom plus den apartments. The principal access to the commercial and residential units will be provided off Grove Avenue. 14 underground motor vehicle parking spaces are provided for residents and visitors with access off Grove Avenue. Bicycle parking spaces are provided along Grove Avenue and within the building on the ground level and underground parking level.

A Minor Zoning By-law Amendment application is required to facilitate the proposed development on the Subject Property. Site-specific amendments for the Subject Property are required to incorporate the required relief from zoning provisions to increase the maximum permitted building height, to alter the required angular plane for the proposed building from the rear lot line, to reduce the minimum total amenity area requirement and to reduce the minimum communal amenity area requirement for the proposed development.

A detailed Site Plan Control application is required to facilitate development on the Subject Property and is being filed as part of this submission. The proposed development will require demolition of the existing building, modifications to site grading, drainage and landscaping. The proposed site plan is attached as Appendix A (see Figure 11).

Figure 11: Site Plan



3.0 PLANNING POLICY AND REGULATORY FRAMEWORK

3.1 Provincial Policy Statement

The Provincial Policy Statement (2020) provides policy direction on land use planning and development matters of provincial interest. The PPS was issued under the authority of Section 3 of the Planning Act and came into effect on May 1, 2020. All decisions affecting planning matters “shall be consistent with” policies issued under Section 3 of the Planning Act.

Section 1.1 of the PPS provides policies to manage and direct land use to achieve efficient and resilient development. Policy 1.1.1 states:

- “1.1.1 Healthy, liveable and safe communities are sustained by:*
- a) promoting efficient development and land use patterns which sustain the financial well-being of the Province and municipalities over the long term;*

- b) *accommodating an appropriate affordable and market-based range and mix of residential types (including single-detached, additional residential units, multi-unit housing, affordable housing and housing for older persons), employment (including industrial and commercial), institutional (including places of worship, cemeteries and long-term care homes), recreation, park and open space, and other uses to meet long-term needs;*
- c) *avoiding development and land use patterns which may cause environmental or public health and safety concerns;*
- d) *avoiding development and land use patterns that would prevent the efficient expansion of settlement areas in those areas which are adjacent or close to settlement areas;*
- e) *promoting the integration of land use planning, growth management, transit-supportive development, intensification and infrastructure planning to achieve cost-effective development patterns, optimization of transit investments, and standards to minimize land consumption and servicing costs;*
- f) *improving accessibility for persons with disabilities and older persons by addressing land use barriers which restrict their full participation in society;*
- g) *ensuring that necessary infrastructure and public service facilities are or will be available to meet current and projected needs;*
- h) *promoting development and land use patterns that conserve biodiversity; and*
- i) *preparing for the regional and local impacts of a changing climate”*

The PPS defines “intensification” as:

“the development of a property, site or area at a higher density than currently exists through:

- a) *redevelopment, including the reuse of brownfield sites;*
- b) *the development of vacant and/or underutilized lots within previously developed areas;*
- c) *infill development; and*
- d) *the expansion or conversion of existing buildings”*

The proposed mixed-use development on the Subject Property will promote the efficient use of land through redevelopment of an underutilized site within the City’s urban area. The proposed infill development is an example of intensification. The mixed-use building will add 37 dwelling units to a site that is comprised of a convenience store with surface parking. The proposed development will enhance the Bank Street corridor through provision of ground-floor retail and commercial uses fronting on Bank Street. The location of the development along a Mainstreet is well suited to intensification with bus stops on frequent transit routes located immediately in front of the proposed building and on the south side of Grove Avenue at Bank Street. Residents of the building will have access to several restaurants, retail stores, and other amenities along Bank Street. The proposed building has direct access at grade to the sidewalks along Grove Avenue and Bank Street. The proposed building design limits potential barriers to accessibility for pedestrians.

Section 1.1.3 of the PPS defines and lays out policies for settlement areas. The Subject Property is considered part of a settlement area, as it is within the Urban Area for the City of Ottawa. Policy 1.1.3.1 states: “*Settlement areas shall be the focus of growth and development.*” Policy 1.1.3.2 states:

“1.1.3.2 Land use patterns within settlement areas shall be based on densities and a mix of land uses which:

- a) efficiently use land and resources;*
- b) are appropriate for, and efficiently use, the infrastructure and public service facilities which are planned or available, and avoid the need for their unjustified and/or uneconomical expansion;*
- c) minimize negative impacts to air quality and climate change, and promote energy efficiency;*
- d) prepare for the impacts of a changing climate;*
- e) support active transportation;*
- f) are transit-supportive, where transit is planned, exists or may be developed;
and*
- g) are freight-supportive.”*

The Minor Zoning By-law Amendment and Site Plan Control applications will facilitate the development of a six-storey, mixed-use building on the Subject Property. The proposed development is consistent with Policy 1.1.3.1 and 1.1.3.2 of the PPS as it represents an efficient use of an existing fully serviced property and existing municipal infrastructure. The property is within proximity of designated cycling routes along Bank Street and Sunnyside Avenue and promotes active transportation opportunities for residents, commercial tenants and customers. The Subject Property is within immediate proximity of transit stops at the intersection of Grove Avenue and Bank Street located along routes 6 and 7 on the frequent transit network. The proposed development is transit supportive.

Policy 1.1.3.3 states:

“1.1.3.3 Planning authorities shall identify appropriate locations and promote opportunities for transit-supportive development, accommodating a significant supply and range of housing options through intensification and redevelopment where this can be accommodated taking into account existing building stock or areas, including brownfield sites, and the availability of suitable existing or planned infrastructure and public service facilities required to accommodate projected needs.”

The Subject Property is underutilized and can accommodate intensification and redevelopment. The proposed development is situated in an appropriate location to establish transit-supportive development. The proposed six-storey, mixed-use building on the Subject Property will increase the City’s housing supply and provide a range of housing options within proximity to the existing active transportation routes and transit stops located along the frequent transit network.

Policy 1.1.3.4 states:

“1.1.3.4 Appropriate development standards should be promoted which facilitate intensification, redevelopment and compact form, while avoiding or mitigating risks to public health and safety.”

The proposed development is subject to the City of Ottawa’s Site Plan Control process as a more intensive mixed-use development is proposed on the Subject Property.

Section 1.3 of the PPS provides policy direction for employment areas and includes the following policy:

“1.3.1 Planning authorities shall promote economic development and competitiveness by:

- d) encouraging compact, mixed-use development that incorporates compatible employment uses to support liveable and resilient communities, with consideration of housing policy 1.4;”*

The proposed mixed-use building is consistent with policy 1.3.1 of the PPS as it will provide a mix of uses, including employment along the Bank Street corridor. The proposed development allows residents access to shopping and commercial uses, as well as potential employment opportunities.

Section 1.4 of the PPS sets out policies for housing. Policy 1.4.3 states:

“1.4.3 Planning authorities shall provide for an appropriate range and mix of housing options and densities to meet projected market-based and affordable housing needs of current and future residents of the regional market area by:

- b) permitting and facilitating:*
 - a. all housing options required to meet the social, health, economic and well-being requirements of current and future residents, including special needs requirements and needs arising from demographic changes and employment opportunities; and*
 - b. all types of residential intensification, including additional residential units, and redevelopment in accordance with policy 1.1.3.3;*
- c) directing the development of new housing towards locations where appropriate levels of infrastructure and public service facilities are or will be available to support current and projected needs;*
- d) promoting densities for new housing which efficiently use land, resources, infrastructure and public service facilities, and support the use of active transportation and transit in areas where it exists or is to be developed;*
- e) requiring transit-supportive development and prioritizing intensification, including potential air rights development, in proximity to transit, including corridors and stations; and”*

The proposed six-storey, mixed-use building on the Subject Property is an example of residential intensification directed to an area where appropriate levels of infrastructure and public service facilities exist to support the needs of the community. The proposed development will make efficient use of underutilized land and be supportive of active transportation and transit in an area where suitable infrastructure is in place. The Subject Property is located in an area where intensification is appropriate and supports the use of transit given proximity to the existing active transportation corridors and direct access to bus stops along the frequent transit network. Commercial uses located on the ground-floor fronting on Bank Street will encourage walking and cycling in the area.

Section 1.6.6 of the PPS sets out policies for Sewer, Water and Stormwater. Policy 1.6.6.2 of the PPS states: *“Within settlement areas with existing municipal sewage services and municipal water services, intensification and redevelopment shall be promoted wherever feasible to optimize the*

use of the services.” The proposed development on the Subject Property represents intensification from the current use. The proposed development of a mid-rise, mixed-use building has been designed to align with and optimize existing municipal infrastructure capacity and avoid the requirement for servicing upgrades on the Subject Property.

Section 1.6.7 of the PPS sets out policies for transportation systems. Policy 1.6.7.4 states: *“A land use pattern, density and mix of uses should be promoted that minimize the length and number of vehicle trips and support current and future use of transit and active transportation.”* The proposed development of a six-storey, mixed-use building with limited off-street motor vehicle parking provided on the Subject Property will introduce a level of density that promotes the viability of active transportation and transit use and contributes to minimizing the required length and number of vehicle trips for residents.

Section 1.6.8 of the PPS sets out policies for transportation and infrastructure corridors. Policy 1.6.8.3 states: *“New development proposed on adjacent lands to existing or planned corridors and transportation facilities should be compatible with, and supportive of, the long-term purposes of the corridor and should be designed to avoid, mitigate or minimize negative impacts on and from the corridor and transportation facilities.”* The Subject Property is located south of Sunnyside Avenue and abuts Bank Street. Sunnyside Avenue and Bank Street are each designated as cycling routes on Schedule C3 and Schedule C4 of the 2021 Official Plan. Transit stops along routes 6 and 7 of the frequent transit network are located immediately abutting the Subject Property. The proposed development of a six-storey, mixed-use building on the Subject Property will be compatible with the existing use of the Bank Street corridor, will be transit-supportive and designed to create no negative impacts on the existing or planned function of transportation corridors in the area.

Section 1.8 of the PPS provides policy direction related to energy conservation, air quality, and climate change. Policy 1.8.1 states:

“1.8.1 Planning authorities shall support energy conservation and efficiency, improved air quality, reduced greenhouse gas emissions, and preparing for the impacts of a changing climate through land use and development patterns which:
e) *encourage transit-supportive development and intensification to improve the mix of employment and housing uses to shorten commute journeys and decrease transportation congestion;”*

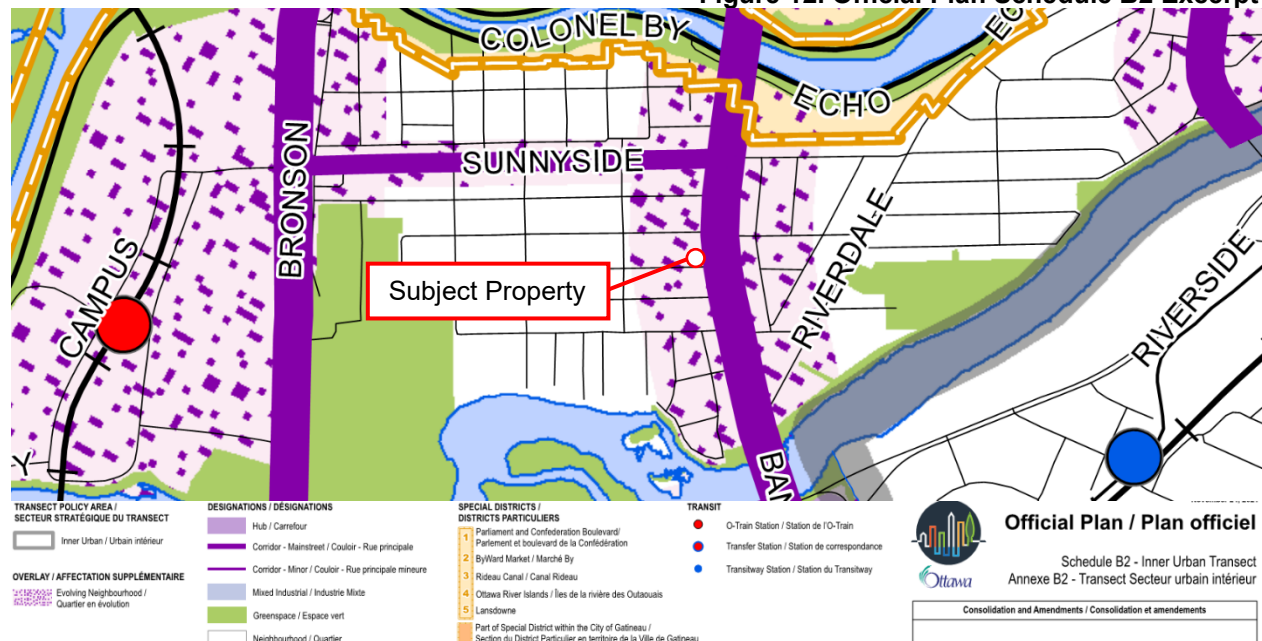
The proposed development meets the climate change objectives and policies of the PPS by providing intensification in proximity to active transportation routes and rapid transit. The introduction of ground-oriented commercial uses and proximity to bus stops on frequent transit routes increases opportunities for residents to fulfill most of their daily needs by walking, cycling, or taking transit.

The proposed Minor Zoning By-law Amendment and Site Plan Control applications are consistent with the policies of the Provincial Policy Statement.

3.2 City of Ottawa Official Plan

The City of Ottawa Official Plan (the “Official Plan”) was adopted by City Council on November 24th, 2021. The Official Plan received final approval, as modified by the Minister of Municipal Affairs and Housing on November 4th, 2022. For the purposes of this Planning Rationale, the Official Plan dated November 24, 2021 was used for reference.

Figure 12. Official Plan Schedule B2 Excerpt



3.2.1 Land Use Designation

The Subject Property is designated “Mainstreet Corridor” and “Evolving Neighbourhood Overlay” in the Inner Urban Transect on Schedule B2 of the Official Plan (see Figure 12).

Section 5.2 of the Official Plan sets out general policies and guidance for proposed development within the Inner Urban Transect.

Section 5.2.1, Policy 4 of the Official Plan states that:

- “4) The Inner Urban Transect shall continue to develop as a mixed-use environment, where:
- a) Hubs and a network of Mainstreets and Minor Corridors provide residents with a full range of services within a walking distance from home, in order to support the growth of 15-minute neighbourhoods;
 - b) Small, locally oriented services may be appropriately located within Neighbourhoods;
 - c) Existing and new cultural assets are supported, including those that support music and nightlife;
 - d) Larger employment uses are directed to Hubs and Corridors; and

e) Increases in existing residential densities are supported to sustain the full range of services noted in Policy a).”

Policies of the Official Plan direct areas designated “Mainstreets” to accommodate increased residential growth and mixed-use development that support a wide variety of housing types and services for residents in proximity to home. The proposed development of a mixed-use building on the Subject Property is consistent with policies of the Official Plan for Mainstreet Corridors within the Inner Urban Transect.

Section 5.2.3, Policy 2 of the Official Plan states that:

“2) Along Mainstreets, permitted building heights are as follows, subject to appropriate height transitions, setbacks, and angular planes:

- a) On sites that front on segments of streets whose right-of-way (after widening requirements have been exercised) is 30 metres or greater as identified in Schedule C16 for the planned street context, and where the parcel is of sufficient size to allow for a transition in built form massing, not less than 2 storeys and up to High-rise;
- b) On sites that front on segments of streets whose right-of-way is narrower than 30 metres, generally up to 9 storeys except where a secondary plan or area-specific policy specifies different heights; and
- c) In all cases:
 - i) The wall heights directly adjacent to a street, and the heights of the podiums of High-rise buildings, where permitted, shall be proportionate to the width of the abutting right of way, and consistent with the objectives in the urban design section on Mid-rise and High-rise built form in Subsection 4.6.6, Policies 7), 8) and 9); and
 - ii) The height of such buildings may be limited further on lots too small to accommodate an appropriate height transition.”

The Subject Property is designated as Mainstreet on Schedule B2 of the Official Plan. The Bank Street right-of-way is less than 30 metres in width abutting the Subject Property. A building height of up to 9 storeys on the Subject Property would generally be supported by policies of the Official Plan.

Section 5.6.1 of the Official Plan provides policy direction for Built Form Overlays, including the Evolving Neighbourhoods Overlay.

Section 5.6.1.1, Policy 1 of the Official Plan states that:

“1) The Evolving Overlay will apply to areas that are in a location or at stage of evolution that create the opportunity to achieve an urban form in terms of use, density, built form and site design. These areas are proximate to the boundaries of Hubs and Corridors as shown in the B-series of schedules of this Plan. The Evolving Overlay will be applied generally to the properties that have a lot line along a Minor Corridor; lands 150 meters from the boundary of a Hub or Mainstreet designation; and to lands within a 400-metre radius of a rapid transit station. The Overlay is intended to provide opportunities that allow the City to reach the goals of its Growth Management Framework for intensification through the Zoning By-law, by providing:

- a) Guidance for a gradual change in character based on proximity to Hubs and Corridors.
- b) Allowance for new building forms and typologies, such as missing middle housing;
- c) Direction to built form and site design that support an evolution towards more urban built form patterns and applicable transportation mode share goals; and
- d) Direction to govern the evaluation of development.

The Subject Property is suitable for intensification being located within 150 metres from the Bank Street Mainstreet Corridor. The proposed development of a six-storey, mixed-use building on the Subject Property is consistent with City objectives to establish a more urban community character and to create opportunities for infill and intensification within walking distance of bus stops along frequent transit routes in the Inner Urban Transect.

Section 5.6.1.1, Policy 2 of the 2021 Official Plan states that:

“2) Where an Evolving overlay is applied:

- a) *The Zoning By-law shall provide development standards for the built form and buildable envelope consistent with the planned characteristics of the overlay area, which may differ from the existing characteristics of the area to which the overlay applies; and*
- b) *The Zoning By-law shall include minimum-density requirements as identified in Table 3a, and permissions to meet or exceed the density targets of Table 3b.”*

For Mainstreet Corridors, the required minimum density is 120 dwelling units per net hectare. The proposed development exceeds this requirement, as it provides approximately 372 dwelling units per net hectare.

Section 5.6.1.1, Policy 6 of the 2021 Official Plan states that:

“6) Zoning By-law development standards and development on lands with an Evolving Overlay should generally include built form and site design attributes that meet most of the urban characteristics described in Table 6 in Section 5, and where suburban attributes are retained, that these do not structurally impede the achievement of a fully urban site design over time.”

The urban characteristics outlined in Table 6 of Section 5 of the 2021 Official Plan are as follows:

- *Shallow front yard setbacks and in some contexts zero front yards with an emphasis on built-form relationship with the public realm*
- *Principal entrances at grade with direct relationship to public realm*
- *Range of lot sizes that will include smaller lots, and higher lot coverage and floor area ratios*
- *Minimum of two functional storeys*
- *Buildings attached or with minimal functional side yard setbacks*
- *Small areas of formal landscape that should include space for soft landscape, trees and hard surfacing*
- *No automobile parking, or limited parking that is concealed from the street and not forming an integral part of a building, such as in a front facing garage*

The proposed development has a 0.23 metre front yard setback after road widening requirements and a three metre corner side yard setback to establish a strong interface with the public realm along Bank Street and Grove Avenue. To create an active street frontage, principal entrances to

the commercial and residential portions of the building are located facing Bank Street and Grove Avenue complete with soft and hard landscaping, street trees and patio areas for residents and visitors. The principal entrances are at grade with the street level to provide direct access and reduce accessibility concerns for residents and visitors. The proposed development has limited off-street parking located entirely underground. The entrance to the parking garage is located to the south extent of the building along Grove Avenue and is separated from the principal entrances and active frontages along Bank Street and Grove Avenue.

Section 6.2 of the 2021 Official Plan sets out functional policies related to Corridor designations that are to be applied to each Transect on a site-specific basis.

Section 6.2.1, Policy 1 of the 2021 Official Plan states that:

“The Corridor designation applies to any lot abutting the Corridor, subject to:

- a) *Generally, a maximum depth of:*
 - i) *In the case of Mainstreet Corridors, a maximum depth of 220 metres from the centreline of the street identified as a Mainstreet Corridor;*

The Subject Property has frontage on Bank Street. The Subject Property is located entirely within 220 metres of the centreline of Bank Street, designated as Mainstreet Corridor in the 2021 Official Plan. The Mainstreet Corridor designation applies to the entire area of the Subject Property.

Section 6.2.1, Policy 2 of the 2021 Official Plan states that:

“2) Development within the Corridor designation shall establish buildings that locate the maximum permitted building heights and highest densities close to the Corridor, subject to building setbacks where appropriate. Further, development:

- a) *Shall ensure appropriate transitions in height, use of land, site design and development character through the site, to where the Corridor designation meets abutting designations;*
- b) *May be required to provide public mid-block pedestrian connections to nearby streets or abutting designations;”*

Policies of the 2021 Official Plan direct new development to locate the maximum permitted building heights and highest densities close to the corridor along Bank Street. Policies of the 2021 Official Plan direct new development to ensure compatibility with surrounding land use designations through appropriate design and transitions in height and land use. Step backs provided above the fourth storey along Bank Street, Grove Avenue and in the rear yard help to facilitate the transition in height between the proposed development and existing low-rise buildings to the north, south and west. The proposed six-storey building height complements the permitted building heights of abutting lands to the north, south and east. The proposed development on the Subject Property is designed to provide appropriate transition to surrounding land uses and built form and create a comfortable pedestrian environment along Bank Street and Grove Avenue.

Section 6.2.1, Policy 3 of the 2021 Official Plan states that:

“3) Corridors will generally permit residential uses and such non-residential uses that integrate with a dense, mixed-use urban environment. The City may require through the Zoning By-law and/or development applications to amend the Zoning By-law:

- a) Commercial and service uses on the ground floor of otherwise residential, office and institutional buildings with a strong emphasis on uses needed to contribute to 15-minute neighbourhoods;*
- b) Residential and/or office uses on the upper floors of otherwise commercial buildings; and/or*
- c) Minimum building heights in terms of number of storeys to ensure multi-storey structures where uses can be mixed vertically within the building.”*

The proposed development provides a mix of uses within the building. The Subject Property is well located within walking distance from a number of commercial, employment, and recreational amenities. The proposed mixed-use development will help contribute to the establishment of 15-minute neighbourhoods by providing the residential density needed to support commercial uses on the ground floor and within the surrounding area.

3.2.2 Growth Management Framework

Section 3 of the 2021 Official Plan sets out policies to direct growth toward target areas for intensification. The majority of projected growth between 2018 and 2046 is directed within the urban boundary, representing 93% of all new development. 47% of the growth allocation is directed to take place within existing built-up areas inside the urban boundary where services are available or can be easily provided for new development to accommodate the creation of jobs, housing and increased transit use.

Section 3.2, Policy 3 of the adopted Official Plan identifies “Corridors” as one of the target areas designated for residential and employment intensification in the City of Ottawa.

The Subject Property is designated as Mainstreet Corridor and is located within the Inner Urban Transect on Schedule B2 of the 2021 Official Plan. The Subject Property is located within a land use designation targeted for intensification. The proposed development of a six-storey, mixed use building on the Subject Property is consistent with the growth objectives of the 2021 Official Plan.

Section 3.2, Policy 10 of the 2021 Official Plan sets out residential density targets that apply to Corridors and states: *“The residential density and proportion of large household dwelling targets as shown on Schedules B1 through B8 are established in Table 3a for Hubs and Mainstreet Corridors and Table 3b for Neighbourhoods and Minor Corridors.”*

The Subject Property is designated as Mainstreet Corridor in the 2021 Official Plan. Policies of the 2021 Official Plan establish a minimum area-wide density requirement of 120 people and jobs per gross hectare and a minimum a residential density requirement of 120 dwellings per net hectare for proposed developments in the Inner Urban Transect.

The Subject Property has a gross area of approximately 1,015 square metres. A mixed-use development is proposed with commercial uses on the ground level and residential units located above. At a rate of 1 resident per unit, the Subject Property would provide 37 residents. Additional

residents are anticipated, and jobs would be created through the commercial components of the proposed development. The density of the proposed development is approximately 370 people and jobs per gross hectare. The proposed development would exceed the required minimum density targets for people and jobs per gross hectare in the 2021 Official Plan.

37 dwelling units are proposed on the Subject Property, which has a net buildable area of approximately 710 square metres. The density of the proposed development is approximately 535 dwelling units per net hectare, which exceeds the minimum requirement and is consistent with residential density targets for Mainstreets in the 2021 Official Plan.

3.2.3 City-wide Policies

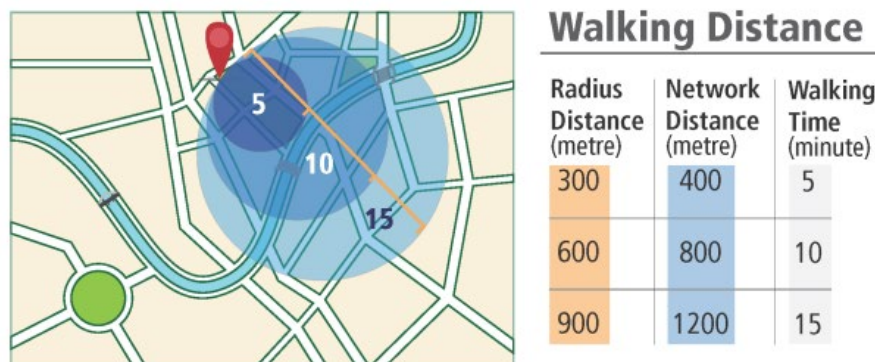
Section 4 of the 2021 Official Plan sets out city-wide policies to be considered where all new development is proposed.

Section 4.1.2, Policy 1 of the 2021 Official Plan sets out the framework for establishing 15-minute neighbourhoods across the City and states that:

“1) In general, this Plan equates a walking time of:

- a) 5 minutes to be equivalent to a radius of 300 metres, or 400 metres on the pedestrian network;*
- b) 10 minutes to be equivalent to a radius of 600 metres, or 800 metres on the pedestrian network; and*
- c) 15 minutes to be equivalent to a radius of 900 metres or 1,200 metres on the pedestrian network.”*

Figure 13 – Official Plan Figure 11



The Subject Property is located fully within 300 metres radius and within 400 metres network distance of transit stops along frequent bus routes 6 and 7, located south of the Subject Property along Grove Avenue. Frequent bus route 7 provides direct access to the Carleton rapid transit station. The proximity of the proposed development to a range of transit options in the immediate area represents a walking distance of 5 minutes or less for residents to access frequent street transit options from their home (see Figure 13).

3.2.4 Urban Design

Section 4.6 of the adopted Official Plan sets out policies related to Urban Design and is intended to promote design excellence in Design Priority areas, encourage innovation in site planning and building design, support the objectives of Corridors, Hubs and Neighbourhoods and enable integration of new development with existing communities.

Section 4.6.5, Policy 2 of the 2021 Official Plan states that:

“Development in Hubs and along Corridors shall respond to context, transect area and overlay policies. The development should generally be located to frame the adjacent street, park or greenspace, and should provide an appropriate setback within the street context, with clearly visible main entrances from public sidewalks. Visual impacts associated with above grade utilities should be mitigated.”

The proposed development has been designed to frame Bank Street and Grove Avenue with an appropriate building height, setback, and building materials. The active frontage on Bank Street provides direct pedestrian connections to the Bank Street Corridor. The variation of building materials along the Bank Street façade and building setbacks above the fourth storey along Bank Street, Grove Avenue and in the rear yard provides a visual transition, complements neighbouring low-rise buildings and provides a human-scaled environment for pedestrians. The use of a variety of colours for façade materials, providing a significant proportion of glazing, and soft landscape treatments surrounding the building at grade level creates a welcoming pedestrian condition at the building base along Bank Street and Grove Avenue and contributes to the commercial streetscape along Bank Street.

Section 4.6.6, Policy 7 of the 2021 Official Plan states that:

“7) Mid-rise buildings shall be designed to respond to context, and transect area policies, and should:

- a) Frame the street block and provide mid-block connections to break up large blocks;*
- b) Include a base with active frontages, and a middle portion that relates to the scale and character of the surrounding buildings, or, planned context;*
- c) Be generally proportionate in height to the width of the right of way as illustrated in the Figure below, with additional height permitted in the Downtown Core Transect;
and*
- d) Provide sufficient setbacks and step backs to:
 - i) Provide landscaping and adequate space for tree planting;*
 - ii) Avoid a street canyon effect; and*
 - iii) Minimize microclimate impacts on the public realm and private amenity areas.”**

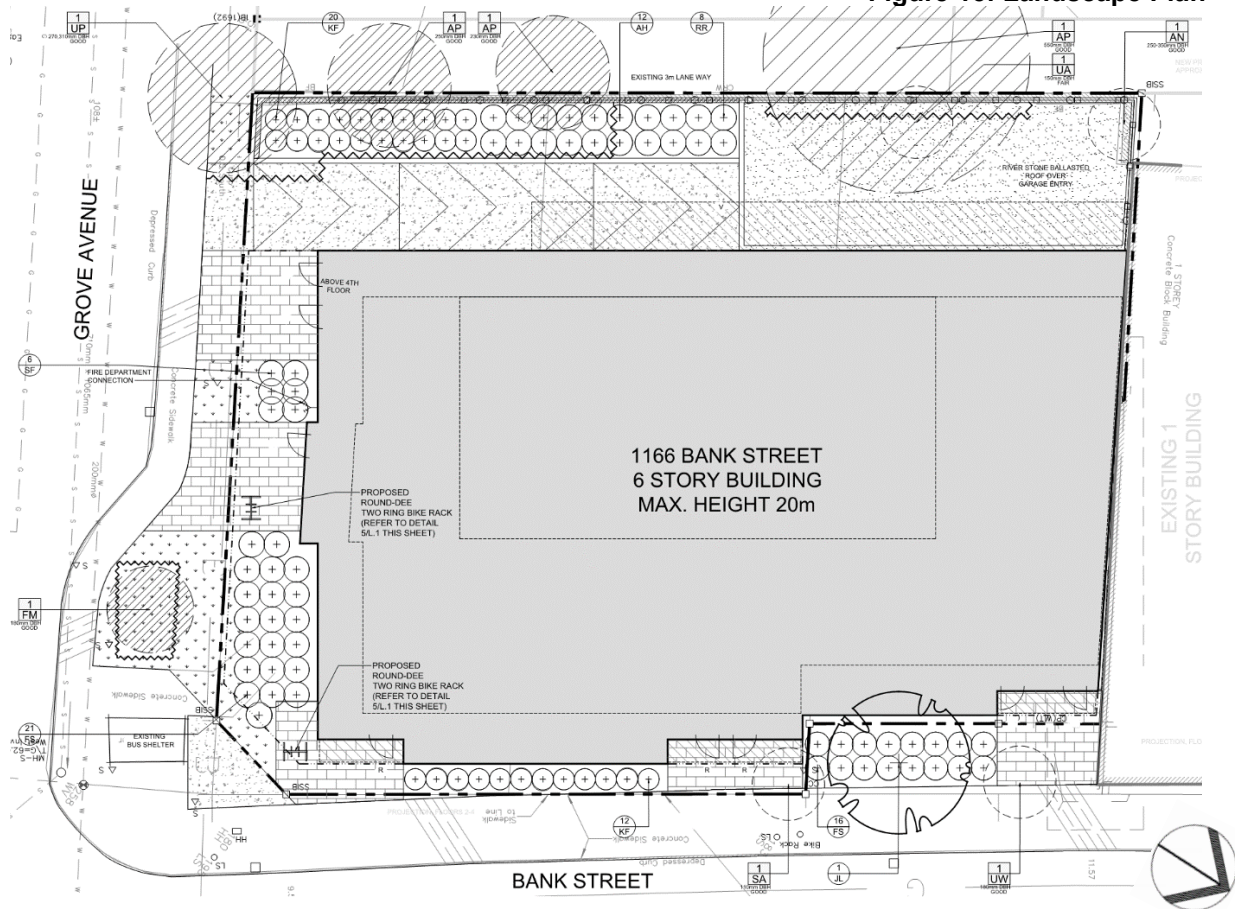
The proposed development of a mid-rise, mixed-use building on the Subject Property has been designed to frame Bank Street and Grove Avenue (see Figure 14). The principal building entrance to access the residential lobby from the sidewalk is provided off Grove Avenue. Entrances to ground floor commercial and retail units are provided off Bank Street to establish an active street frontage. The building has been designed with a distinct base comprised of generous glazing and landscape elements where feasible. The base and middle portion of the building is primarily clad with limestone and red brick with ventilated ceramic and corrugated metal panel elements above. The building is stepped back above the fourth storey with a transition to light

coloured panelling on levels five and six. An appropriate building setback, materials and façade articulation is provided that defines the building entrances and maintains the general scale and character of the surrounding community. Appropriate space for hard and soft landscaping treatments along Bank Street and Grove Avenue help to improve the interface with the public realm. The building is adequately set back from the street.

Figure 14: View of Building Looking South (Bank Street)



Figure 15: Landscape Plan



Section 4.6.5, Policy 3 of the 2021 Official Plan states that:

“Development shall minimize conflict between vehicles and pedestrians and improve the attractiveness of the public realm by internalizing all servicing, loading areas, mechanical equipment and utilities into the design of the building, and by accommodating space on the site for trees, where possible. Shared service areas, and accesses should be used to limit interruptions along sidewalks. Where underground parking is not viable, surface parking must be visually screened from the public realm.”

Direct pedestrian access to the Subject Property is provided from the principal building entrance to the public sidewalk along Grove Avenue. Entrances to grade level commercial patio spaces, and a **secondary resident access** are provided along Bank Street. **A combination of soft and hard landscaping treatments and street trees are provided along Bank Street and Grove Avenue where possible. A soft landscape area with ornamental grasses and shrubs in a raised planter is provided in the rear yard along the west edge of the Subject Property to provide screening from adjacent properties (see Figure 15).** Motor vehicle access to an underground garage is provided off Grove Avenue for the proposed development. Providing motor vehicle access to the Subject Property that is separated from Bank Street reduces the potential for vehicle conflicts and improves the safety, accessibility, and movement of pedestrians and bicycles to and from the Subject Property and is consistent with policies of the 2021 Official Plan.

3.2.5 Review of Development Applications

The 2021 Official Plan requires a range of studies to be included as part of a complete development application to meet the objectives of the Official Plan. The appropriate policies, related studies, and plans were identified through a pre-application consultation meeting with City staff at the beginning of the application review process.

Required studies and plans identified as relevant have been prepared in support of the proposed development. Detailed and technical information can be obtained by reviewing the respective documents.

Relating to Section 4.7.1 – Stormwater Management and Site Servicing

Policy 6 of Section 4.7.1 requires redevelopment applications to implement site, grading, building, and servicing design measures. As part of this application, a Site Servicing Plan, Site Grading Plan, and Erosion and Sediment Control Plan have been completed by EXP Services Inc., dated February 7, 2023.

Policy 8 of Section 4.7.1 requires proof of sufficient stormwater management and drainage system as a condition of Site Plan Control approval. A Servicing and Stormwater Management Report was completed by EXP Services Inc. dated February y, 2023. The Servicing and Stormwater Management Report examined the potential water quality and quantity impacts of the proposed development and how each will be addressed through the proposed development. The proposed stormwater design and recommendations are consistent with the policies of Section 4.7.1 of the 2021 Official Plan.

Policy 12 of Section 4.7.1 requires an approved master servicing study, an approved environmental management plan, and a subwatershed study to be included as part of a complete application for a new development in a future neighbourhood. The Subject Property is not located in a future neighbourhood and these studies are not required. City staff requested that a Site Servicing study and plan be included as part of the complete application package. A Servicing and Stormwater Management Report was completed by EXP Services Inc. dated February 7, 2023. The recommendations of the Site Servicing Study conclude that:

“a total of 32,300 L/min (538 L/sec) flow is available from the 7 existing hydrants for firefighting purposes. Therefore, no new hydrants have been proposed for the proposed development.” and that *“the sanitary service will have a capacity of 32.8 L/s and a full flow velocity of 1.21 m/s, which will be sufficient to service the proposed building”*.

The recommendations of the Site Servicing Study are consistent with Section 4.7.1 of the 2021 Official Plan.

Relating to Section 10.1.6 – Contaminated Sites

Policy 1 of Section 10.1.6 requires environmental site assessments and remedial or risk assessment/risk management activities reports to be completed as part of a development application. City staff identified the need for a Phase I Environmental Assessment to be completed as part of the application. A Phase I ESA was completed by Paterson Group dated April 1, 2022.

The recommendations of the Phase I ESA state that:

“Based on the findings of this assessment, it is our opinion that a Phase II – Environmental Site Assessment will be required for the Phase I Property.”

Based on the recommendations of the Phase I ESA, a subsequent Phase II ESA was prepared by Paterson Group, dated June 23, 2022. The recommendations of the Phase II ESA state the following:

“Based on the findings of this assessment, PHC impacted soil was identified in the southern portion of the Phase II Property, in the vicinity of the former on-site underground fuel storage tank nest.

It is our understanding that the Phase II Property is to be redeveloped in the future, thus it is recommended that this contaminated soil be remediated at the time of site redevelopment. This contaminated soil will require disposal at a licensed waste disposal facility.”

The recommendations are consistent with Section 10.1.6. of the 2021 Official Plan.

Relating to Section 10.2.1 – Noise

Policy 2 of Section 10.2.1 requires a Roadway Traffic Noise Study to be prepared as part of a complete application. A Noise Control Detailed Study was completed by EXP Services Inc. dated March 21, 2023. The recommendations of the Noise Control Detailed Study state the following:

“...daytime and nighttime sound levels at the east façade (POR 1) are greater than 65 dBA and 60 dBA, respectively. Whereas, for the north and south façade, the sound levels are between 55 dBA and 65 dBA during daytime and 50 dBA and 60 dBA during nighttime. Based on these results, the applicable warning clauses, building ventilation and component requirements listed on Table 3-2 and Table 3-3, are summarized in Table 5-2 below”

Table 5-2 - Summary of the applicable warning clauses and required AIF for building components

Receptor ID	Applicable Warning Clauses		Centralized Air Conditioning System	Required AIF for Building Components	
	Daytime	Nighttime		Daytime	Nighttime
POR1 (east façade)	Type D	Type D	Required	30	27
POR2 (north façade)	Type C	Type C	Provisioned	OBC	OBC
POR3 (south façade)	Type C	Type C	Provisioned	OBC	OBC
POR4 (south façade, 5 th -6 th storey)	Type C	Type C	Provisioned	OBC	OBC

The recommendations are consistent with Section 10.2.1 of the 2021 Official Plan.

The proposed Minor Zoning By-law Amendment and Site Plan Control applications conform to the policies of the 2021 City of Ottawa Official Plan.

4.0 URBAN DESIGN GUIDELINES

The City of Ottawa's Urban Design Guidelines provide guidance in order to promote and achieve appropriate development within key growth areas throughout the City. Where the Urban Design guidelines apply, not all of the direction provided will apply to the proposed development. The Urban Design Guidelines are non-statutory documents and are intended to supplement the policies and regulations of the Official Plan and Zoning By-law. The Urban Design Guidelines for Development along Traditional Mainstreets have been reviewed for the proposed development.

4.1 Urban Design Guidelines for Development along Traditional Mainstreets

The Urban Design Guidelines for Development along Traditional Mainstreets were approved on May 24, 2006. These guidelines apply across the City to all streets identified as a Traditional Mainstreet in the Official Plan (2003) and provide urban design guidance for new developments. The primary objectives of the guidelines are as follows:

- To promote development that will enhance and reinforce the recognized or planned scale and character of the street
- To promote development that is compatible with, and complements its surroundings
- To achieve high-quality built form and strengthen building continuity along Traditional Mainstreets
- To foster compact, pedestrian-oriented development linked to street level amenities
- To accommodate a broad range of uses including retail, services, commercial uses, offices, residential and institutional uses where one can live, shop and access amenities.

The proposed development adheres to the objectives of the Urban Design Guidelines for Development along Traditional Mainstreets. The proposed six-storey, mixed-use development includes a setback above the fourth storey along Bank Street, Grove Avenue and in the rear yard which contributes to maintaining the existing scale and character of the streetscape and improving pedestrian comfort. The proposed building design provides a transition between the street and adjacent low-rise built forms. Ground floor commercial uses are intended to extend the existing retail character of the Bank Street Corridor along the site frontage. A significant proportion of glazed surfaces and landscape treatments at grade create active frontages designed to provide an open and transparent pedestrian experience and provide direct access to ground floor commercial uses from Bank Street. A separate principal entrance for residents is located along Grove Avenue. The proposed residential density provided on the site will support the surrounding commercial uses, allowing residents to access their daily needs within walking distance of home. All parking on the site is separated from Bank Street with access off Grove Avenue. Off-street parking is located below grade to minimize any potential impact on the street.

5.0 CITY OF OTTAWA ZONING BY-LAW 2008-250

The Subject Property is zoned Traditional Mainstreet, Subzone 2, Height Limited to 15 metres (TM2 H(15)) in the City of Ottawa Zoning By-law 2008-250. The property is also subject to the Mature Neighbourhoods Overlay.

The purpose of the TM2 zone is to:

1. *accommodate a broad range of uses including retail, service commercial, office, residential and institutional uses, including mixed-use buildings but excluding auto-related uses, in areas designated **Traditional Mainstreet** in the Official Plan;*
2. *foster and promote compact, mixed-use, pedestrian-oriented development that provide for access by foot, cycle, transit and automobile;*
3. *recognize the function of Business Improvement Areas as primary business or shopping areas; and*
4. *impose development standards that will ensure that street continuity, scale and character is maintained, and that the uses are compatible and complement surrounding land uses.*

A six-storey, mixed-use building is proposed for the Subject Property. The proposed development will include four ground-floor commercial units and 37 residential dwelling units. The incorporation of ground-floor commercial uses, proximity to the Bank Street corridor and bus stops on the frequent transit network will promote a pedestrian-oriented public realm where residents can access daily needs by walking, cycling, or taking transit. The built form of the building is designed to maintain the scale and character of the area by providing setbacks above the fourth storey facing Bank Street and Grove Avenue to reduce the scale and visual impact of the building from the street. The proposed development is consistent with the purpose of the TM zone.

Section 197 (1) of the Zoning By-law lists a number of permitted non-residential uses for the TM zone including retail store, personal service business, restaurant and office. The proposed commercial units are permitted to be occupied by a variety of retail and employment uses. No specific employment use has been identified for the proposed development. Section 198(2)(a)(i) of the Zoning By-law lists “*dwelling unit*” as a permitted use in the TM2 zone where “*being in a building containing one or more of the permitted non-residential uses*”. The proposed building containing a mix of commercial units at grade with dwelling units above is permitted in the TM2 zone.

Table 1 below summarizes the applicable zoning provisions for the Subject Property.

Table 1: Zoning Review Table

Zoning Provision	Required	Provided
Minimum Lot Area (m ²)	No minimum	1,015 m ²
Minimum Lot Width (m)	No minimum	28.5 m (after road widening)
Max. Front Yard Setback (m)	2 m	0.23 m
Max. Interior Yard Setback (m)	3 m between a non-residential use building or a mixed-use building and another non-residential use building or mixed-use building	0 m

Min. Interior Yard Setback (m)	No minimum	0 m
Min. Corner Yard Setback (m)	3 m, except for any part of a building above 15 metres for which an additional 2 metre setback must be provided	3 m Additional 2 m setback provided on 4 th storey
Minimum Rear Yard Setback (m)	4.5m - rear lot line abutting a public laneway	4.5 m
Minimum Building Height (m)	6.7 metres for a distance of 20 metres from the front lot line as set out under subsection 197(5)	19.5 m
Maximum Building Height (m)	Height Limited to 15.0 m (TM - 20 m / 6 storeys)	19.5m
	Additional setback of 2 meters where building greater than 4 storeys	Additional 2 m setback provided above 4 th storey
Angular Plane	Rear lot line abuts an R3 zone. A 45 degree angular plane is required measured at a height of 15 m from a point 7.5 m from the rear lot line, projecting upwards towards the front lot line.	A 45 degree angular plane is provided measured at a height of 15.6 m from a point 4.5 m from the rear lot line, projecting upwards towards the front lot line.
Minimum Width of Landscaped Area (m)	No Minimum – lot abuts a public laneway (zoned TM)	2.7m (TBC)
Minimum Driveway Width (m)	197(8)(a) – minimum 3m for parking lots with less than 20 spaces	3.6m
Required Total Amenity Area	228 m ² (6 m ² per dwelling unit)	Private Terraces = 53.75m² Lev. 5 (Interior) = 44.44m² Lev.5 (Terrace) = 15.16m² Total = 113.35m²
Required Communal Amenity Area	114 m ² (50% of required total amenity area)	Lev. 5 (Interior) = 44.44m² Lev.5 (Terrace) = 15.16m² Total = 59.6m²
Parking Requirements - Residential		
Minimum Parking Spaces (0.5 spaces per dwelling unit after the first 12 units) + (10% reduction where located below grade)	11 spaces (37 units – 12) = 25 x 0.5 (minus 10%) = 11.25 (11)	11 residential spaces (0.44 spaces per dwelling unit after the first 12 units)

Minimum Visitor Parking Spaces	3 spaces (0.1 spaces per dwelling unit after the first 12 units)	3 visitor parking spaces
Maximum Visitor Parking Spaces	30 spaces	3 visitor parking spaces
Minimum Barrier Free Parking Spaces	1 Type B space (per Section 3.1.2, Table 3 of Accessibility Design Standards)	1 Type B space
Parking Requirements – Non-Residential		
Area Y – Ground Floor/Basement	GFA= 500m ² or less, no off-street parking required	n/a
Bicycle Parking Requirements		
Minimum Bicycle Parking Spaces – Residential	19 (0.5 spaces per dwelling unit)	63 bicycle spaces total
Minimum Bicycle Parking Spaces – Retail / Commercial	2 (1 space per 250 m ² gross floor area)	59 interior bicycle parking 4 exterior bicycle parking

The Subject Property is located in Area Y on Schedule 1A of the Zoning By-law. Section 101 of the Zoning By-law states that ground floor commercial uses located in Area Y which do not exceed a total gross floor area of 500 square metres do not require motor vehicle parking. The combined gross floor area of the proposed ground floor commercial units is less than 500 square metres. No off-street parking is required for commercial units as part of the proposed development.

Section 64 of the Zoning Bylaw regulates permitted projections above the height limit. The provision states that maximum building limits do not apply to the following structures.

- *“Mechanical and service equipment penthouse, elevator or stairway penthouses”*

The proposed development includes a mechanical room located on the roof of the building that is permitted to be located above the height limit. The proposed development fully conforms with provisions of the Zoning By-law for permitted projections.

The proposed development fully conforms with the required front yard, corner side yard, interior side yard and rear yard setbacks, minimum required visitor parking, minimum required bicycle parking and permitted projections. A minor rezoning application is required to establish site specific zoning provisions on the Subject Property to increase the maximum permitted building height, to alter the required angular plane for the proposed building from the rear lot line, to reduce the minimum total amenity area requirement, and to reduce the minimum communal amenity area requirement for the proposed development.

The proposed Minor Zoning By-law Amendment and Site Plan Control application are consistent with the purpose of the Traditional Mainstreet zone and are consistent with the relevant provisions of the City of Ottawa Zoning By-law.

6.0 PROPOSED ZONING BY-LAW AMENDMENT

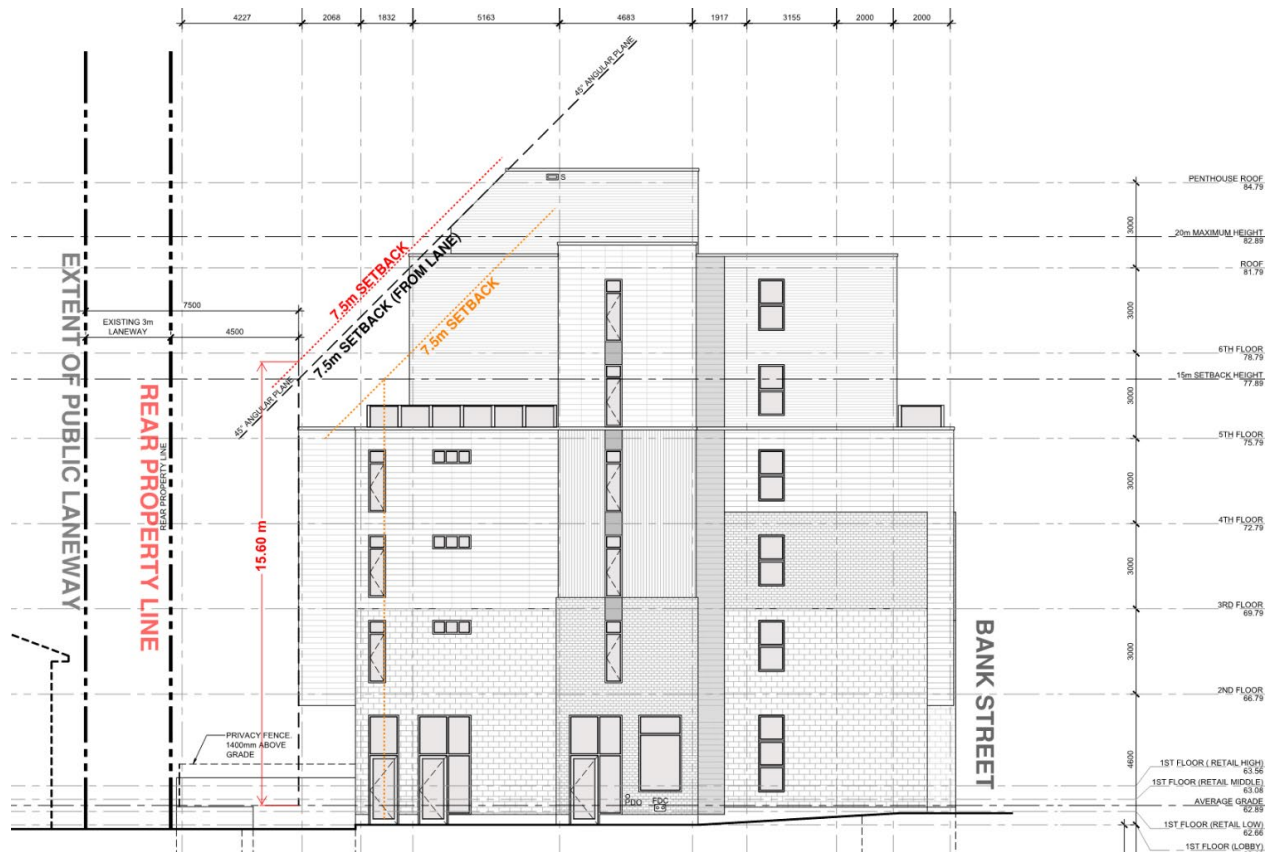
Provisions of the Traditional Mainstreet zone permit building heights of up to 20 metres or six storeys. The existing TM2 H(15) zoning limits the maximum permitted building height to 15 metres on the Subject Property. Relief from the zoning provision is required to increase the maximum permitted building height by five metres to 20 metres to facilitate the proposed development. Policies of the 2021 Official Plan support intensification in areas designated Mainstreet Corridor and permit building heights of up to nine storeys where appropriate transitions can be provided to abutting lots. A six-storey building is proposed on the Subject Property.

Building setbacks are provided above the fourth storey along the front and corner side yard to establish a pedestrian scale along Bank Street and Grove Avenue. Building setbacks provide a built form transition to existing properties to the east across Bank Street and to the south across Grove Avenue. A 4.5 metre setback is provided from the rear lot line in addition to a 3.9 metre setback above the fourth storey to establish a transition to the low-rise neighbourhood to the west. The Subject Property abuts an untravelled public laneway that is heavily vegetated with trees and shrubs along the west property line. The existing condition will be maintained providing a visual separation from the adjacent property to the west.

The Subject Property is located with direct access to bus stops along the frequent transit network and the Bank Street corridor. The Subject Property is located within an appropriate area for increased building heights at a mid-rise scale that support the use of active transportation and public transit. The proposed development of a mid-rise, mixed-use building with a total building height greater than permitted by current height provisions on the Subject Property is appropriate for this location.

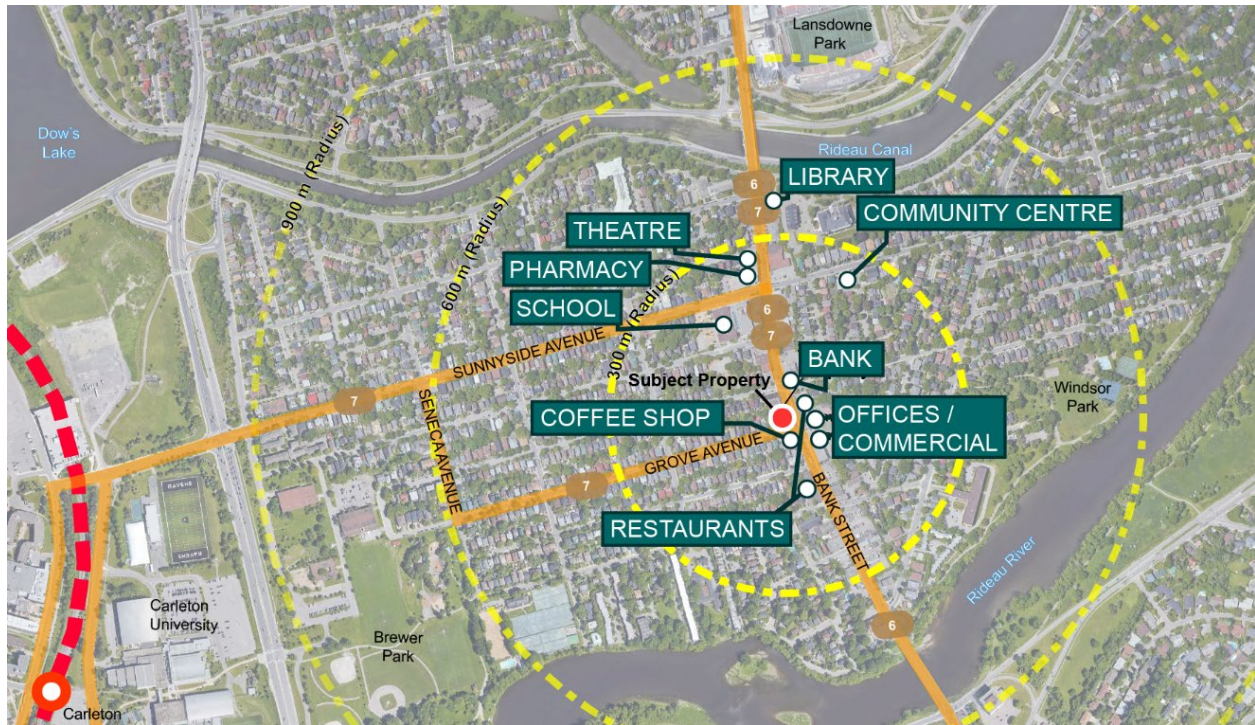
Provisions of the TM2 zone require a 45-degree angular plane be provided for any part of a building from the rear lot line at a distance of 7.5 metres and a height of 15 metres from average grade. Relief from the zoning provision is required to modify where the angular plane is applied by reducing the distance from the rear lot line from 7.5 metres to 4.5 metres and by increasing the height from which the angular plane is measured from 15 metres to 15.6 metres. The Subject Property is approximately 28 metres in lot depth. Due to the shallow depth of the Subject Property, providing a 45-degree angular plane from a setback of 7.5 metres and a height of 15 metres would limit the development potential of the fifth and sixth storeys. The request to apply the angular plane from a reduced distance of 4.5 metres and an increased height of 15.6 metres is required to accommodate the proposed building envelope.

Figure 16. Proposed 45 Degree Angular Plane



Taking into consideration the 3 metre wide untravelled public laneway, the proposed development provides a 7.5 metre setback from the lot line of the residential property to the west (see Figure 16). The existing laneway condition will be maintained to provide a functional setback of 7.5 metres from the proposed building to the abutting residential lot to the west. The untravelled public laneway is heavily vegetated with trees and shrubs providing a visual separation from the adjacent property along the west property line. Establishing a 45-degree angular plane for the proposed building that is provided at a distance of 7.5 metres from the abutting residential lot to the west maintains the intent of the required angular plane and is appropriate for this location.

Figure 17. Amenities within 600m Radius of Subject Property



The Subject Property is within 600 metres radius of a broad range of land uses and amenities that contribute to development of 15-minute neighbourhood, create a walkable community and reduce the need for residents to use private automobiles to access daily needs. Specifically, the Subject Property is located within walking distance of the Rideau Canal, Rideau River, walking paths, parks, retail stores, offices, coffee shops, restaurants, a theatre, banks, pharmacies, schools, a library and a community centre along Bank Street and in the adjacent neighbourhood (see Figure 17).

The combined total amenity area required for the proposed development is 288 square metres. The total required communal amenity area for the proposed development is 144 square metres. The combined total amenity area provided for building residents is 113.35 square metres. The total communal amenity area provided is 59.6 square metres located on the fifth floor of the proposed development. The decision to provide a reduction to the total required amenity area and total required communal amenity area for building residents is in response to the proximity of the proposed development to a range of recreational amenities located within a 15-minute walk of the Subject Property including the Rideau Canal, Rideau River, walking paths, parks and community centres. Based on publicly accessible amenities located in the immediate area, a reduction to the total required amenity area and total required communal amenity area provided on the Subject Property is appropriate for the proposed development.

Section 101, Table 101, Row R15 of the Zoning By-law (Area “Y” – Schedule 1A) sets a minimum parking space rate of 0.5 spaces per dwelling unit for dwelling units in a mixed-use building after the first 12 units. Section 101 (6) (c) of the Zoning By-law states that “*where all parking spaces provided or required for a permitted land use are located below grade in the same building as that land use*” the parking requirement can be reduced by 10%. 11 parking spaces are required for

the proposed residential uses. Section 102, Table 102, Column II of the Zoning By-law (Area “Y” – Schedule 1A) sets a minimum visitor parking space rate of 0.1 spaces per dwelling unit after the first 12 units. Three parking spaces are required for visitor parking.

The combined minimum number of parking spaces required for the proposed development is 14 spaces. A total of three visitor parking spaces are provided. A total of 14 off-street parking spaces are provided within an underground garage. No parking spaces are required for the proposed ground floor retail and commercial uses.

The proposed parking layout, number of resident parking and visitor parking spaces fully conform with all provisions of the zoning by-law.

Section 111, Table 111A, Row (b) of the Zoning By-law sets minimum bicycle parking space rates for the residential component of the proposed development. A rate of 0.5 bicycle parking spaces per dwelling unit is required for residential uses. The minimum number of bicycle parking spaces required for the proposed residential use is 19 spaces.

Section 111, Table 111A, Row (e) of the Zoning By-law sets minimum bicycle parking space rates for the commercial component of the proposed development. A rate of 1 bicycle parking space per 250 square metres of gross floor area is required for retail and commercial uses. A total of four commercial units are proposed. The combined gross floor area of the proposed commercial units is approximately 422 square metres in gross floor area. The minimum number of bicycle parking spaces required for the proposed retail and commercial use is two spaces.

59 bicycle parking spaces are provided within the proposed building and four bicycle parking spaces are provided at the exterior of the building fronting along Bank Street. A total of 63 bicycle parking spaces are provided on the Subject Property, representing an increase of 42 bicycle parking spaces from the minimum required by the Zoning By-law.

To facilitate the proposed development of a mid-rise, mixed-use building on the Subject Property, this application requests a site-specific amendment to incorporate the required relief from the zoning provisions for an increase to the maximum permitted building height, to alter the required angular plane for the proposed building from the rear lot line, to reduce the minimum total amenity area requirement, and to reduce the minimum communal amenity area requirement as highlighted in orange on the Zoning Review Table (see Table 1). The site-specific amendment will result in development of a six-storey, mixed-use building located with direct access and opportunities for residents to increase use of transit and active transportation infrastructure in the Old Ottawa South community. The proposed development is within proximity to restaurants, shops and outdoor amenity areas for residents. The requested relief from provisions of the TM zone is appropriate to facilitate the proposed development of a six-storey, mixed-use building on the Subject Property.

7.0 PUBLIC CONSULTATION STRATEGY

Prior to Submission

A formal pre-application consultation meeting was held with City staff and members of the Old Ottawa South Community Association (OOSCA) in November 2021. A public open house was held with members of the OOSCA and the local Councillor on June 23, 2022 to solicit feedback on the proposed development.

Upon Submission

The public will be consulted with regarding the proposed development through the legislated public consultation requirements. This includes a signed posted on the site and the posting of the application on the City's 'DevApps' website. At this time, neighbours will have the opportunity to comment on the proposal.

Immediately following the filing of the application, an information meeting will be coordinated with the Councillor's office. If necessary, a second public meeting will take place to discuss this development application with the community.

Virtual Open House

- Who:** Residents of the community
- Where:** The Open House may be held electronically via Zoom, subject to the necessary COVID-19 protocols.
- When:** Soon after the City's circulation. This is to ensure that members of the public are aware of the project well in advance of any public meeting of Planning Committee.
- City rep.:** The File Lead may wish to attend, depending on the level of interest from the public. This meeting will be coordinated with the Ward Councillor.
- Follow up:** Attendees wishing to receive follow-up information may email Novatech's file lead or the City's file lead. The Project Team will do their best to keep interested citizens informed of significant changes and/or the final submission that will be heard at Planning Committee.

8.0 CONCLUSION

This Planning Rationale has been prepared in support of Minor Zoning By-law Amendment and Site Plan Control applications to facilitate the development of a six-storey, mixed-use building on the Subject Property. The Subject Property is within the Inner Urban Transect and is designated Mainstreet Corridor on Schedule B2 of the 2021 Official Plan. The Subject Property is within the Evolving Neighbourhood Overlay of the 2021 Official Plan. The property is zoned Traditional Mainstreet, Subzone 2, Height Limited to 15 metres (TM2 H(15)) and is located within the Mature Neighbourhoods Overlay in the City of Ottawa's Zoning By-law 2008-250.

The purpose of these applications is to facilitate the proposed development of a six-storey, mixed-use building through establishing site-specific zoning provisions for the Subject Property to increase to the maximum permitted building height, to alter the required angular plane for the proposed building from the rear lot line, to reduce the minimum total amenity area requirement,

and to reduce the minimum communal amenity area requirement for the proposed development. Approval of the proposed development will permit a total of 37 residential dwelling units and four commercial units on the Subject Property. A total of 11 off-street resident parking spaces and three visitor parking spaces are provided in an underground garage. A total of 63 bicycle parking spaces are proposed on the Subject Property.

The proposed mid-rise, mixed-use development is appropriate to support growth objectives and residential intensification targets within the urban area of the City of Ottawa. The proposed Minor Zoning By-law Amendment and Site Plan Control applications will have no negative impacts to natural heritage and features, natural resources, or cultural heritage resources. The requested Minor Zoning By-law Amendment and Site Plan Control applications are consistent with the policies of the Provincial Policy Statement.

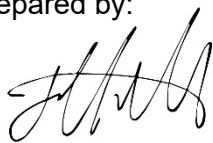
The Minor Zoning By-law Amendment and proposed development conform with the policies of the City of Ottawa Official Plan (2021). The proposal is consistent with the Corridor designation and meets the City’s intensification goals while maintaining a form that is context-sensitive and adds to the existing characteristics of the Old Ottawa South community. The proposed ground-floor commercial uses are consistent with the retail context of the Bank Street corridor and contribute to the City’s objectives to establish 15-minute neighbourhoods. The requested Minor Zoning By-law Amendment establishes appropriate zoning provisions for the proposed mixed-use building and permits development that is compatible with surrounding land uses.

The Minor Zoning By-law Amendment and Site Plan Control applications are appropriate for the development of the Subject Property and represent good land use planning.

Yours Truly,

NOVATECH

Prepared by:



Jeffrey Kelly, MCIP, RPP
Project Planner

Reviewed by:



Murray Chown, MCIP, RPP
Director | Planning & Development

Appendix A:
Site Plan

EXISTING SIDEWALK / ROAD SURFACE TO BE REPLACED AS PART OF SEPARATE CITY OF OTTAWA INFRASTRUCTURE PROJECT

GROVE AVENUE

BANK STREET

1166 BANK STREET
6 STORY BUILDING
MAX. HEIGHT 20m

PLANTER
RIVER STONE
BALLASTED ROOF OVER
GARAGE ENTRY

SITE PLAN LEGEND:

- EXTENT OF REMOVALS
- EXISTING PROPERTY LINE
- PROPERTY LINE AFTER ROAD WIDENING
- SET BACK LINE
- NEW PRIVACY FENCE TO APPROX. 1.4m ABOVE GRADE
- EXTENT OF FOUNDATION BELOW GRADE
- EXTENT OF BUILDING AT GROUND FLOOR
- BUILDING OVERHANGS ABOVE, REFER TO ELEVATIONS
- CONCRETE WALKING / DRIVING SURFACE, REFER TO CIVIL DOCUMENTS
- SOFT LANDSCAPING, REFER TO LANDSCAPING DOCUMENTS
- HARD LANDSCAPING, REFER TO LANDSCAPING DOCUMENTS
- RAISED LANDSCAPING BLOCKS
- LOCATION FOR NEW BICYCLE RACK

EXTERIOR DOOR LEGEND:

- LOCATION OF EXTERIOR DOORS:
- B = BICYCLE STORAGE ROOM
- M = MAIN RESIDENTIAL ENTRANCE
- R = RETAIL UNIT
- V = VEHICLE ENTRANCE / EXIT
- X = EXIT DOOR

GENERAL SITE PLAN NOTES:

PROPERTY BOUNDARY INFORMATION, AND TOPOGRAPHIC INFORMATION DERIVED FROM SURVEY OF LOT 81 PART OF LOTS 81 & 82 REGISTERED PLAN 109830, CITY OF OTTAWA, PREPARED BY ANNIS, O'SULLIVAN, VOLLEBEKK LTD., SIGNED AND DATED FEBRUARY 12, 2022

SITE & BUILDING DATA, 166 BANK STREET:

SITE AREA: 1015.47m²
GROSS FLOOR AREA (CITY OF OTTAWA): 2980.87m²
BUILDING AREA (OBC): 709.60m²

ZONING:
PART 10 - MIXED USE:
TM2 H(15) - TRADITIONAL MAIN STREET SUBZONE 2

SETBACKS:
FRONT: 2m MAX + 2m ABOVE THE 4TH FLOOR
REAR: 4.5m (ABUTS LANEWAY), REFER TO ELEVATIONS FOR ANGULAR SETBACK ABOVE 15m
CORNER SIDE YARD: 3m MIN + 2m ABOVE THE 4TH FLOOR
INTERIOR SIDE YARD: 0m

BUILDING HEIGHT:
AVERAGE GRADE: 62.89m
MAXIMUM HEIGHT: 82.89m (20m)
PROPOSED HEIGHT: 82.39m (19.5m)

RESIDENTIAL PARKING:

TYPE	REQUIRED	PROVIDED
TENANT PARKING	11	11
MIN. VISITOR PARKING	3	3
MAX. VISITOR PARKING	30	3
BARRIER FREE PARKING	1 (TYPE B)	1 (TYPE B)
TOTAL:		14

COMMERCIAL PARKING:

BASEMENT + GROUND FLOOR GFA	REQUIRED	PROVIDED
456.11m ²	0	0

BICYCLE PARKING:

RESIDENTIAL BICYCLE PARKING	COMMERCIAL BICYCLE PARKING	REQUIRED	PROVIDED
19	2	60	8

GARBAGE ROOM REQUIREMENTS:

TYPE	REQUIRED	PROVIDED
GARBAGE	8.55cu YARDS OF CONTAINER	1x 6cu YARD & 1x 4cu YARD CONTAINER
FIBRE RECYCLING	2.30cu YARDS OF CONTAINER	1x 3cu YARD CONTAINER
GLASS, METAL, & PLASTIC RECYCLING	0.67cu YARDS OF CONTAINER	3x 0.25cu YARD (50g) CONTAINERS
ORGANICS	1x 240L GREEN CONTAINER	1x 240L GREEN CONTAINER

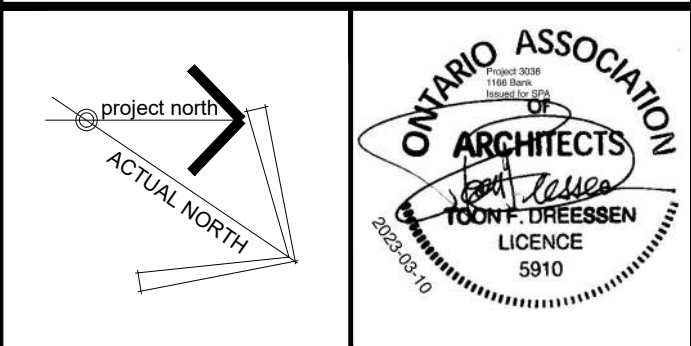
AMENITY AREAS

TYPE	REQUIRED	PROVIDED
TOTAL AMENITY AREA	222m ²	113.35m ²
COMMUNAL AMENITY AREA	MIN. 111m ²	59.60m ²

*CALCULATIONS ARE BASED ON 37 RESIDENTIAL UNITS

- GENERAL NOTES**
- DO NOT SCALE DRAWINGS; ONLY FIGURED DIMENSIONS ARE TO BE USED, WHERE DOUBT EXISTS, FILE REQUEST FOR INTERPRETATION AND REQUEST CLARITY.
 - IT IS THE RESPONSIBILITY OF THE GENERAL CONTRACTOR TO VERIFY DIMENSIONS ON SITE, REPORT DISCREPANCIES TO THE ARCHITECT PROMPTLY.
 - GENERAL CONTRACTOR TO TAKE INTO ACCOUNT CONSTRUCTION TOLERANCE, GENERAL CONTRACTOR TO COORDINATE THE WORK OF DIFFERENT TRADES TO COMPLY WITH DESIGN INTENT.
 - ALL WORK DESCRIBED IN THESE DRAWINGS AND SPECIFICATIONS ARE TO COMPLY WITH THE CURRENT EDITION OF THE ONTARIO BUILDING CODE (2012) OR NATIONAL BUILDING CODE (2010) INCLUDING MOST RECENT AMENDMENTS.
 - DRAWINGS AND SPECIFICATIONS ARE COMPLEMENTARY AND ARE TO BE READ TOGETHER.

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ISSUE RECORD:

NO.	DESCRIPTION	DATE
1	ISSUED FOR SITE PLAN COORDINATION	2022-11-03
2	ISSUED FOR SITE PLAN COORDINATION	2022-12-09
3	ISSUED FOR COORDINATION	2023-01-20
4	ISSUED FOR COORDINATION	2023-02-01
5	ISSUED FOR SITE PLAN CONTROL	2023-03-10

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185 SOMERSET ST. W., OTTAWA, ONTARIO
TEL: 613-233-1104

201-139 WELINGTON ST. WEST OTTAWA ON K1Y 3B8
WWW.ARCHITECTSDCA.COM 613.725.2294

PROJECT TITLE
1166 BANK STREET

DRAWING TITLE
SITE PLAN: NEW WORK

DATE	DRAWN	JOB NO.	DRAWING NO.
OCT. 2022	IC	3038	A101
SCALE	REVIEWED		
1:100	TD/DR		

ARCHITECTURAL