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**377 – 381 Winona Avenue**

**Planning Rationale**



Prepared for: Azure Winona Inc.

Engineering excellence.

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Liveable landscapes.

**377-381 Winona Avenue**

**Ottawa, Ontario**

**Planning Rationale**

**in support of**

**Minor Zoning By-law Amendment &**

**Site Plan Control Applications**

Prepared For:

**Azure Winona Inc.**

Prepared By:

**NOVATECH**

Suite 200, 240 Michael Cowpland Drive  
Ottawa, Ontario  
K2M 1P6

October / 27 / 2022

Novatech File: 122087  
Ref: R-2022-187

October 27, 2022

City of Ottawa  
Planning, Real Estate and Economic Development Department  
110 Laurier Avenue West, 4<sup>th</sup> Floor  
Ottawa, ON, K1P 1J1

**Attention: Nader Kadri, Planner II, Development Review, Central**

**Reference: Minor Zoning By-law Amendment and Site Plan Control Applications  
377-381 Winona Avenue  
Our File No.: 122087**

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The following Planning Rationale has been prepared in support of Minor Zoning By-law Amendment and Site Plan Control applications to facilitate the development of the property at 377-381 Winona Avenue (the “Subject Property”).

The Subject Property is within the Inner Urban Transect and is designated Mainstreet Corridor on Schedule B2 of the 2021 Official Plan adopted by Council on November 24<sup>th</sup>, 2021. The Subject Property is within the Evolving Neighbourhood Overlay of the 2021 Official Plan. The Subject Property is located in the Westboro Village Planning Area Sector of the Richmond Road / Westboro Secondary Plan. The property is zoned Traditional Mainstreet (TM) and is located within the Mature Neighbourhoods Overlay in the City of Ottawa’s Zoning By-law 2008-250.

It is proposed to construct a six-storey, mixed-use building on the Subject Property. The building will include two commercial units on the ground floor and 60 residential dwelling units. A minor rezoning application is required to provide relief from zoning provisions on the Subject Property to decrease the minimum required corner side yard setback along Winona Avenue, to decrease the minimum required parking spaces for the proposed development and to increase the maximum permitted driveway width providing access to the underground parking garage.

A one-level underground parking garage will provide 18 parking spaces for residents and visitors. The parking spaces will be accessible via a driveway and ramp off Picton Avenue. Walkways from Winona Avenue and Picton Avenue are proposed along the north and west property lines. The proposed walkways will facilitate pedestrian and bicycle access off Winona Avenue to building entrances and bicycle parking located along Winona Avenue and within the building. Outdoor amenity areas are provided along Winona Avenue and in the interior side yard. A terrace with common access for residents is proposed on the roof of the building. No impacts to traffic or existing land uses surrounding the Subject Property are anticipated as a result of the proposed development. A concurrent site plan control application is being filed as part of the submission package to facilitate development on the Subject Property.

This Planning Rationale examines the location and context of the Subject Property, the planning policy and regulatory framework applicable to the site and makes recommendations on the Minor Zoning By-law Amendment and Site Plan Control applications required to facilitate the proposed development on the Subject Property.

Should you have any questions regarding any aspect of these applications please feel free to contact me at your earliest convenience.

Yours truly,

**NOVATECH**

A handwritten signature in black ink, appearing to read 'JKelly', written in a cursive style.

Jeffrey Kelly, MCIP RPP  
Project Planner

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## **1.0 INTRODUCTION**

Novatech has prepared this Planning Rationale in support of Minor Zoning By-law Amendment and Site Plan Control applications for the property at 377-381 Winona Avenue (the “Subject Property”). The proposed development consists of a six-storey, mixed-use building with two commercial units located on the ground floor and 60 residential units. 18 underground parking spaces are provided for the proposed development. Pedestrian access is proposed from Winona Avenue and along Picton Avenue. The proposed development will require demolition of existing buildings, modifications to site grading, drainage and landscaping on the property.

The Subject Property is within the Inner Urban Transect and is designated Mainstreet Corridor on Schedule B2 of the 2021 Official Plan adopted by Council on November 24<sup>th</sup>, 2021. The Subject Property is within the Evolving Neighbourhood Overlay of the 2021 Official Plan. The Subject Property is located in the Westboro Village Planning Area Sector of the Richmond Road / Westboro Secondary Plan. The property is zoned Traditional Mainstreet (TM) and is located within the Mature Neighbourhoods Overlay in the City of Ottawa’s Zoning By-law 2008-250.

This Planning Rationale will demonstrate that the proposed Minor Zoning By-law Amendment and Site Plan Control applications will:

- Be consistent with the policies of the Provincial Policy Statement (2020);
- Conform to the policies of the adopted City of Ottawa Official Plan (2021);
- Conform to the policies of the adopted Richmond Road/Westboro Secondary Plan;
- Adhere to relevant Urban Design Guidelines applicable to the Subject Property;
- Establish appropriate Zoning standards for the Subject Property; and
- Maintain compatibility with the surrounding uses and community.

1.1 Description of Subject Property

Figure 1. Aerial Photo of Subject Property

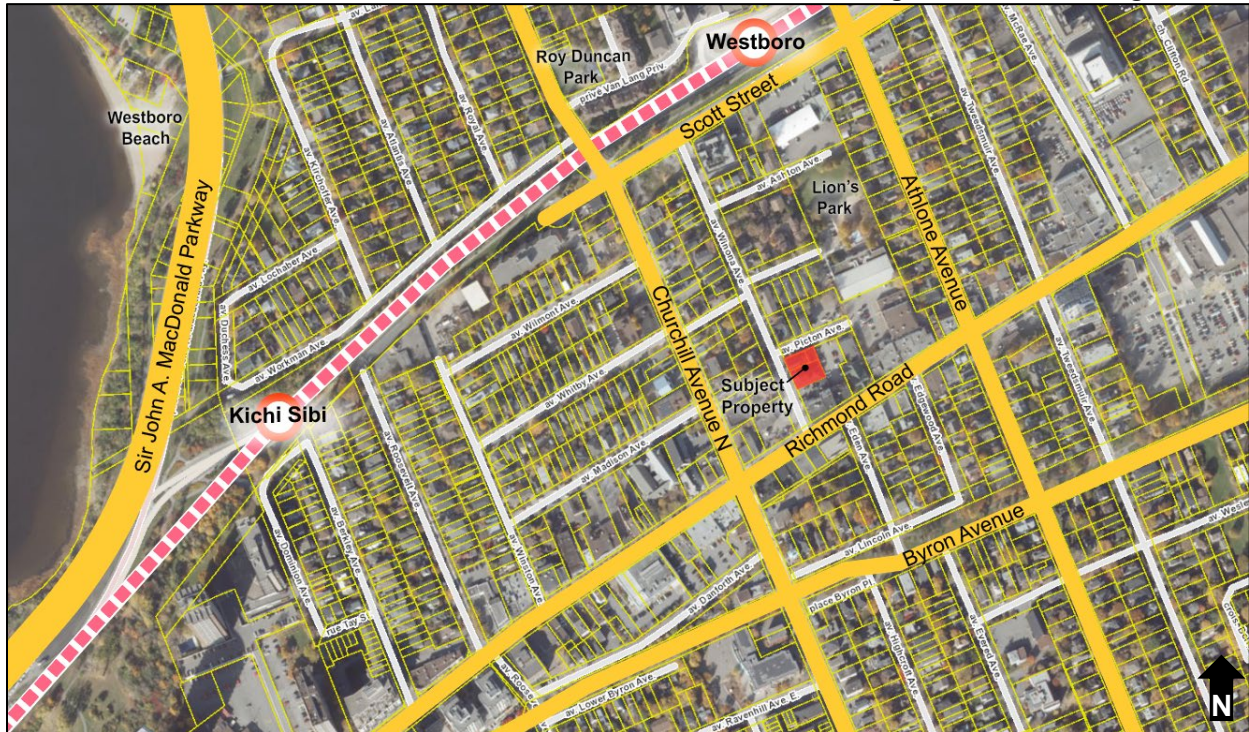


The Subject Property is located in Ward 15 (Kitchissippi) in the City of Ottawa. The Subject Property is located in an area bounded by Scott Street to the north, Richmond Road to the south, Athlone Avenue to the east, and Churchill Avenue North to the west (see Figure 1). The Subject Property is currently occupied by two low-rise mixed residential and commercial use buildings. The Subject Property is a corner lot with an approximate area of 960 square metres, approximately 31 metres of frontage on Winona Avenue, and approximately 30 metres of frontage on Picton Avenue.



1.2 Site Location and Community Context

Figure 2: Surrounding Context



The Subject Property is located within the Westboro neighbourhood. Directly to the south of the Subject Property is Richmond Road and a three storey mixed-use building containing retail and office uses. To the east and west along Richmond Road are a mix of low-rise retail, restaurant, commercial and office uses. West of the Subject Property is predominantly characterized by a mix of low-rise residential uses. The context of the Subject Property to the north along Winona Avenue is predominantly characterized by a mix of low-rise residential uses and small scale commercial uses. Further to the north is Scott Street, the planned Westboro O-Train station and Roy Duncan Park. The planned Kichi Sibi O-Train station, Sir John A. MacDonald Parkway, Ottawa River and Westboro Beach are located further to the west (see Figure 2).

Additional details are provided in Figures 3 to 6 and in the descriptions below.

Figure 3: Land uses to the north of the Subject Property



**North:** To the north of the Subject Property are low-rise residential and small-scale commercial uses. Further north, there are a variety of low-rise residential uses.

Figure 4: Land uses to the south of the Subject Property



**South:** To the south of the Subject Property is a low-rise, mixed-use building. Commercial uses such as retail stores, offices and restaurants characterize the Richmond Road corridor.

Figure 5: Land uses to the west of the Subject Property



West: To the west of the Subject Property are low-rise residential uses.

Figure 6: Land uses to the east of the Subject Property



East: Immediately to the east of the Subject Property is a surface parking lot.

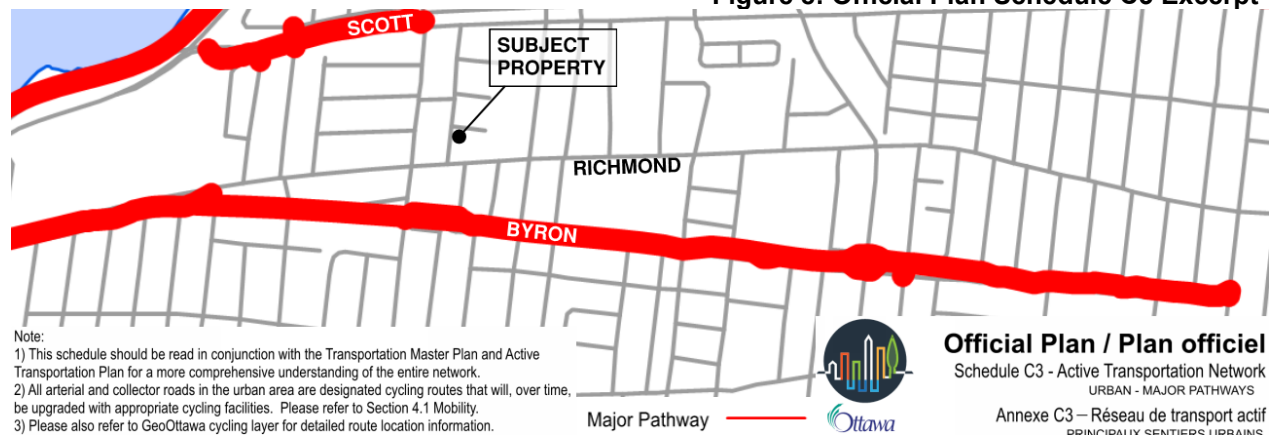
### 1.3 Linkages and Transportation Framework

Figure 7: Official Plan Schedule C4 Excerpt



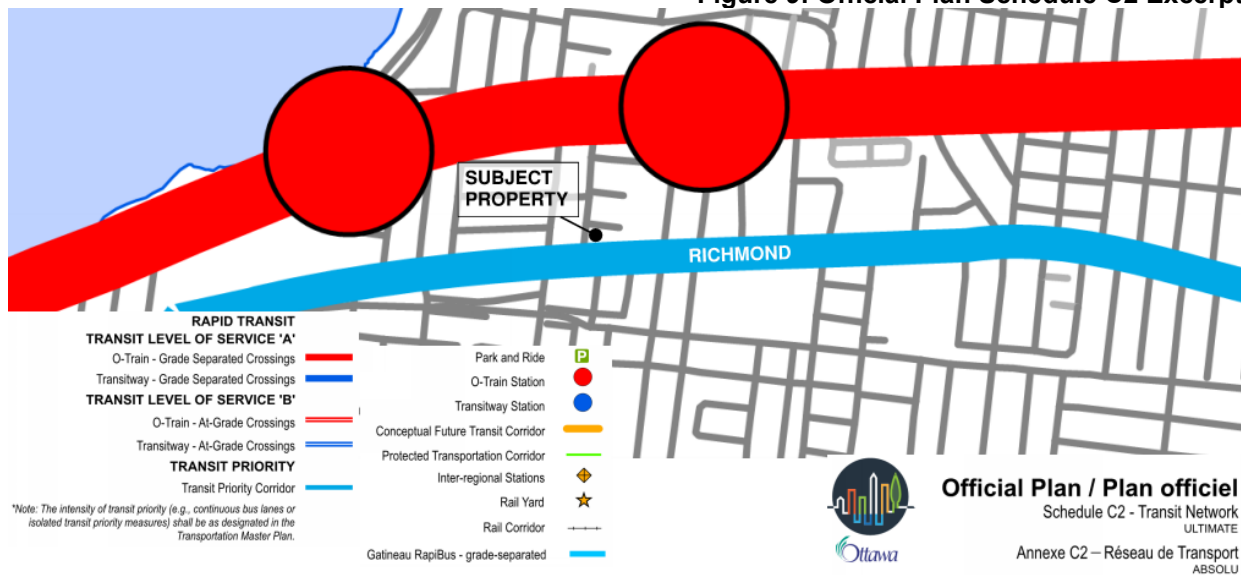
The Subject Property has frontage on Winona Avenue and Picton Avenue. The Subject Property is located north of Richmond Road, south of Scott Street and east of Churchill Avenue North. Richmond Road, Scott Street and Churchill Avenue North are each designated as arterial roads on Schedule C4 of the 2021 Official Plan (see Figure 7). All arterial and collector roads in the urban area are designated cycling routes (see Figure 8).

Figure 8: Official Plan Schedule C3 Excerpt



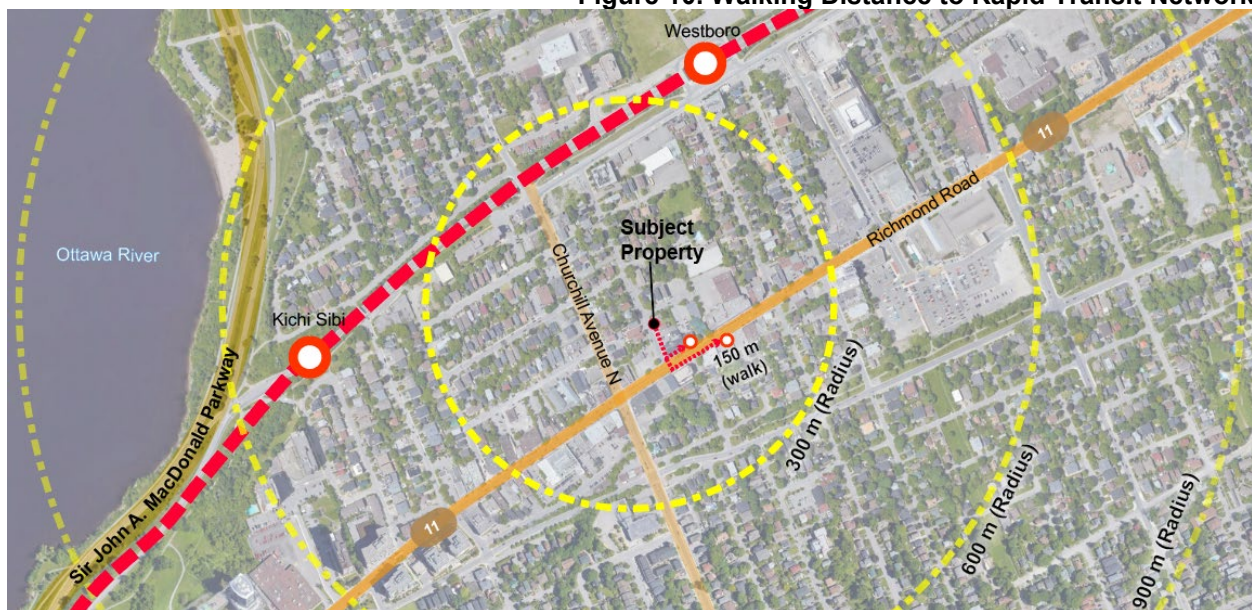
The Subject Property is located to the south of Scott Street and to the North of Byron Avenue which are each designated as Major Pathways on Schedule C3 of the 2021 Official Plan (see Figure 8). There are public sidewalks located along the east and west sides of Winona Avenue. Walkways from Winona Avenue and Picton Avenue are proposed along the north and west property lines to facilitate pedestrian and bicycle access off Winona Avenue to bicycle parking located in the corner side yard and within the building.

Figure 9: Official Plan Schedule C2 Excerpt



Richmond Road is designated as a Transit Priority Corridor on Schedule C2 of the 2021 Official Plan. The planned Westboro and Kichi Sibi O-Train stations are located to the north of the Subject Property (see Figure 9).

Figure 10. Walking Distance to Rapid Transit Network



The Subject Property is located within 600 metres radius of the planned Westboro and Kichi Sibi O-Train stations (see Figure 10). Several bus transit route options are available in the immediate area. Bus route 11 services the Subject Property directly with a bus stop provided on the north and south sides of Richmond Road between Winona Avenue and Athlone Avenue. Bus routes 50 and 153 service the Subject Property with bus stops provided on the east and west sides of Churchill Avenue North. Bus route 11 is designated as frequent route offering high frequency bus service along major roads on the OC Transpo network.

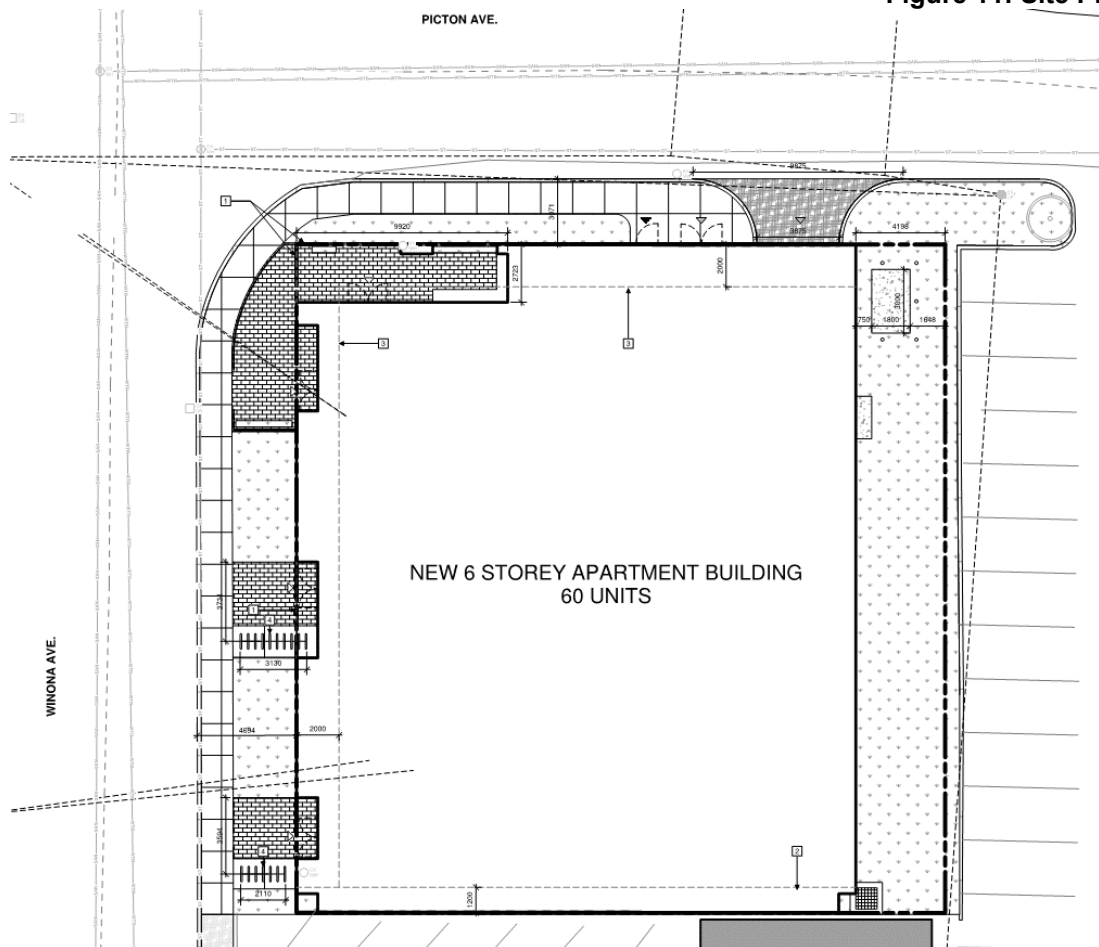
## 2.0 DEVELOPMENT PROPOSAL

It is proposed to develop a six-storey, mixed-use building on the Subject Property. The proposed building will have two commercial units on the ground floor. 60 residential units will be provided ranging in size from studio to 2-bedroom apartments. The principal access to the commercial and residential units will be provided off Winona Avenue. 18 underground motor vehicle parking spaces are provided for residents and visitors with access off Picton Avenue. Bicycle parking spaces are provided along Winona Avenue and within the building on the ground level and underground parking level.

A Minor Zoning By-law Amendment application is required to facilitate the proposed development on the Subject Property. Site-specific amendments for the Subject Property are required to provide relief from zoning provisions to decrease the minimum required corner side yard setback along Winona Avenue, to decrease the minimum required residential parking spaces for the proposed development and to increase the maximum permitted driveway width to access the underground parking garage.

A detailed Site Plan Control application is required to facilitate development on the Subject Property and is being filed as part of this submission. The proposed development will require demolition of the existing buildings, modifications to site grading, drainage and landscaping. The proposed site plan is attached as Appendix A (see Figure 11).

Figure 11: Site Plan



### 3.0 PLANNING POLICY AND REGULATORY FRAMEWORK

#### 3.1 Provincial Policy Statement

The Provincial Policy Statement (2020) provides policy direction on land use planning and development matters of provincial interest. The PPS was issued under the authority of Section 3 of the Planning Act and came into effect on May 1, 2020. All decisions affecting planning matters “shall be consistent with” policies issued under Section 3 of the Planning Act.

Section 1.1 of the PPS provides policies to manage and direct land use to achieve efficient and resilient development. Policy 1.1.1 states:

*“1.1.1 Healthy, liveable and safe communities are sustained by:*

- a) promoting efficient development and land use patterns which sustain the financial well-being of the Province and municipalities over the long term;*
- b) accommodating an appropriate affordable and market-based range and mix of residential types (including single-detached, additional residential units, multi-unit housing, affordable housing and housing for older persons), employment (including industrial and commercial), institutional (including places of worship, cemeteries and long-term care homes), recreation, park and open space, and other uses to meet long-term needs;*
- c) avoiding development and land use patterns which may cause environmental or public health and safety concerns;*
- d) avoiding development and land use patterns that would prevent the efficient expansion of settlement areas in those areas which are adjacent or close to settlement areas;*
- e) promoting the integration of land use planning, growth management, transit-supportive development, intensification and infrastructure planning to achieve cost-effective development patterns, optimization of transit investments, and standards to minimize land consumption and servicing costs;*
- f) improving accessibility for persons with disabilities and older persons by addressing land use barriers which restrict their full participation in society;*
- g) ensuring that necessary infrastructure and public service facilities are or will be available to meet current and projected needs;*
- h) promoting development and land use patterns that conserve biodiversity; and*
- i) preparing for the regional and local impacts of a changing climate”*

The PPS defines “intensification” as:

*“the development of a property, site or area at a higher density than currently exists through:*

- a) redevelopment, including the reuse of brownfield sites;*
- b) the development of vacant and/or underutilized lots within previously developed areas;*
- c) infill development; and*
- d) the expansion or conversion of existing buildings”*

The proposed mixed-use development on the Subject Property will promote the efficient use of land through redevelopment of an underutilized site within the City’s urban area. The proposed

infill development is an example of intensification. The mixed-use building will add 60 dwelling units to a site that currently has only two dwellings and will enhance the Richmond Road corridor through ground-floor commercial uses fronting on Winona Avenue. The location of the development along a Mainstreet is well suited to intensification and it is within walking distance of the planned Kichi Sibi and Westboro LRT Stations. Residents of the building will have access to several restaurants, stores, and other amenities along Richmond Road. The proposed building has direct access at grade with the sidewalk along Winona Avenue and Picton Avenue. The proposed building design limits potential barriers to accessibility for pedestrians.

Section 1.1.3 of the PPS defines and lays out policies for settlement areas. The Subject Property is considered part of a settlement area, as it is within the Urban Area for the City of Ottawa. Policy 1.1.3.1 states: “*Settlement areas shall be the focus of growth and development.*” Policy 1.1.3.2 states:

*“1.1.3.2 Land use patterns within settlement areas shall be based on densities and a mix of land uses which:*

- a) efficiently use land and resources;*
- b) are appropriate for, and efficiently use, the infrastructure and public service facilities which are planned or available, and avoid the need for their unjustified and/or uneconomical expansion;*
- c) minimize negative impacts to air quality and climate change, and promote energy efficiency;*
- d) prepare for the impacts of a changing climate;*
- e) support active transportation;*
- f) are transit-supportive, where transit is planned, exists or may be developed;  
*and**
- g) are freight-supportive.”*

The Minor Zoning By-law Amendment and Site Plan Control applications will facilitate the development of a six-storey, mixed-use building on the Subject Property. The proposed development is consistent with Policy 1.1.3.1 and 1.1.3.2 of the PPS as it represents an efficient use of an existing fully serviced property and existing municipal infrastructure. The property is within proximity of designated cycling routes along Churchill Avenue North and Richmond Road and promotes active transportation opportunities for residents. The Subject Property is located within 600 metres walking distance of the planned Westboro and Kichi Sibi O-Train stations to the north and within 150 metres walking distance of transit stops located along route 11 of the frequent transit network. The proposed development is transit supportive.

Policy 1.1.3.3 states:

*“1.1.3.3 Planning authorities shall identify appropriate locations and promote opportunities for transit-supportive development, accommodating a significant supply and range of housing options through intensification and redevelopment where this can be accommodated taking into account existing building stock or areas, including brownfield sites, and the availability of suitable existing or planned infrastructure and public service facilities required to accommodate projected needs.”*



The Subject Property is underutilized and can accommodate intensification and redevelopment. The proposed development is situated in an appropriate location to establish transit-supportive development. The proposed six-storey, mixed-use building on the Subject Property will increase the City's housing supply and provide a range of housing options within proximity to the planned Westboro and Kichi Sibi O-Train stations, existing active transportation routes and transit stops located along the frequent transit network.

Policy 1.1.3.4 states:

*"1.1.3.4 Appropriate development standards should be promoted which facilitate intensification, redevelopment and compact form, while avoiding or mitigating risks to public health and safety."*

The proposed development is subject to the City of Ottawa's Site Plan Control process as a more intensive mixed-use development is proposed on the Subject Property,

Section 1.3 of the PPS provides policy direction for employment areas and includes the following policy:

*"1.3.1 Planning authorities shall promote economic development and competitiveness by:*

- d) encouraging compact, mixed-use development that incorporates compatible employment uses to support liveable and resilient communities, with consideration of housing policy 1.4;"*

The proposed mixed-use building is consistent with policy 1.3.1 of the PPS as it will provide a mix of uses, including employment in proximity to the Richmond Road corridor. The proposed development allows residents access to shopping or commercial uses, as well as potential employment opportunities.

Section 1.4 of the PPS sets out policies for housing. Policy 1.4.3 states:

*"1.4.3 Planning authorities shall provide for an appropriate range and mix of housing options and densities to meet projected market-based and affordable housing needs of current and future residents of the regional market area by:*

- b) permitting and facilitating:*
  - a. all housing options required to meet the social, health, economic and well-being requirements of current and future residents, including special needs requirements and needs arising from demographic changes and employment opportunities; and*
  - b. all types of residential intensification, including additional residential units, and redevelopment in accordance with policy 1.1.3.3;*
- c) directing the development of new housing towards locations where appropriate levels of infrastructure and public service facilities are or will be available to support current and projected needs;*
- d) promoting densities for new housing which efficiently use land, resources, infrastructure and public service facilities, and support the use of active transportation and transit in areas where it exists or is to be developed;*

- e) requiring transit-supportive development and prioritizing intensification, including potential air rights development, in proximity to transit, including corridors and stations; and"

The proposed six-storey, mixed-use building on the Subject Property is an example of residential intensification directed to an area where appropriate levels of infrastructure and public service facilities exist to support the needs of the community. The proposed development will make efficient use of underutilized land and be supportive of active transportation and transit in an area where suitable infrastructure is in place. The Subject Property is located in an area where intensification is appropriate and supports the use of transit given proximity to the planned Westboro and Kichi Sibi O-Train stations, existing active transportation corridors and walking distance of bus stops along the frequent transit network. Commercial uses located on the ground-floor fronting on Winona Avenue will encourage walking and cycling in the area.

Section 1.6.6 of the PPS sets out policies for Sewer, Water and Stormwater. Policy 1.6.6.2 of the PPS states: "Within settlement areas with existing municipal sewage services and municipal water services, intensification and redevelopment shall be promoted wherever feasible to optimize the use of the services." The proposed development on the Subject Property represents intensification from the current use. The proposed development of a mid-rise, mixed-use building has been designed to align with and optimize existing municipal infrastructure capacity and avoid the requirement for servicing upgrades on the Subject Property.

Section 1.6.7 of the PPS sets out policies for transportation systems. Policy 1.6.7.4 states: "A land use pattern, density and mix of uses should be promoted that minimize the length and number of vehicle trips and support current and future use of transit and active transportation." The proposed development of a six-storey, mixed-use building with limited off-street motor vehicle parking provided on the Subject Property will introduce a level of density that promotes the viability of active transportation and transit use and may contribute to minimizing the required length and number of vehicle trips for residents.

Section 1.6.8 of the PPS sets out policies for transportation and infrastructure corridors. Policy 1.6.8.3 states: "New development proposed on adjacent lands to existing or planned corridors and transportation facilities should be compatible with, and supportive of, the long-term purposes of the corridor and should be designed to avoid, mitigate or minimize negative impacts on and from the corridor and transportation facilities." The Subject Property is located north of Richmond Road and east of Churchill Avenue North. Churchill Avenue North and Richmond Road are each designated as cycling routes on Schedule C3 and Schedule C4 of the 2021 Official Plan. Transit stops along route 11 of the frequent transit network are within walking distance of the Subject Property. The proposed development of a six-storey, mixed-use building on the Subject Property will be compatible with the existing use of the Richmond Road and Churchill Avenue North corridors, will be transit-supportive and designed to create no negative impacts on the existing or planned function of transportation corridors in the area.

Section 1.8 of the PPS provides policy direction related to energy conservation, air quality, and climate change. Policy 1.8.1 states:

*"1.8.1 Planning authorities shall support energy conservation and efficiency, improved air quality, reduced greenhouse gas emissions, and preparing for the impacts of a changing climate through land use and development patterns which:*

- e) encourage transit-supportive development and intensification to improve the mix of employment and housing uses to shorten commute journeys and decrease transportation congestion;”

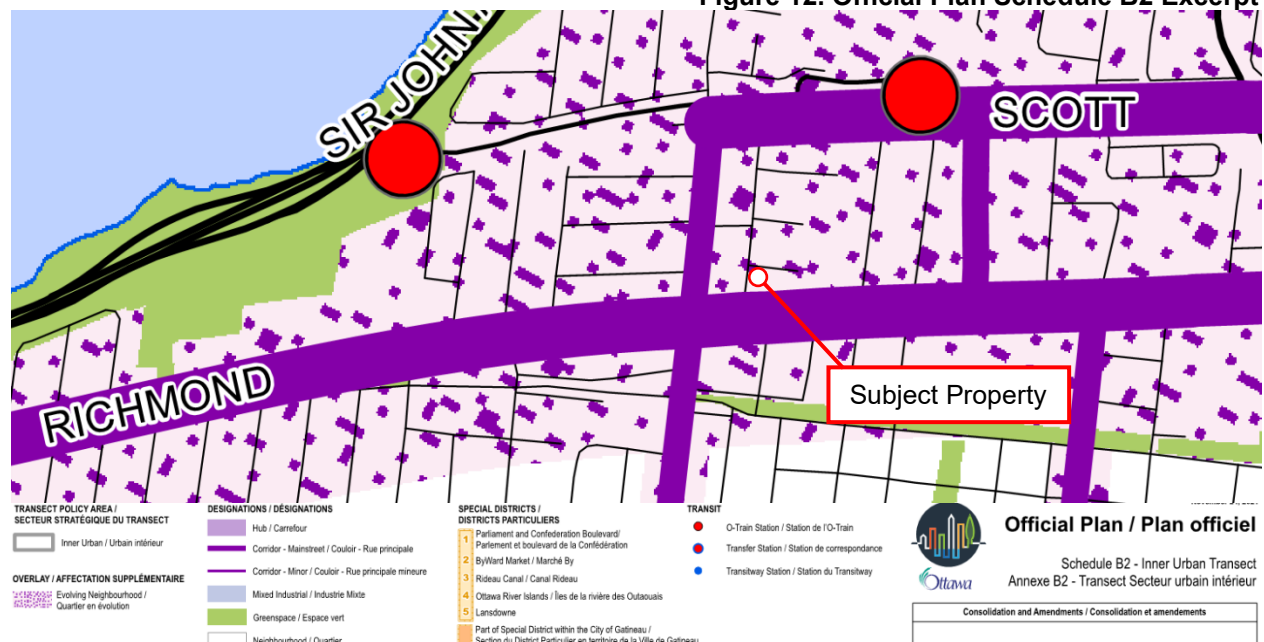
The proposed development meets the climate change objectives and policies of the PPS by providing intensification in proximity to active transportation routes and rapid transit. The introduction of ground-oriented commercial uses and proximity to the Kichi Sibi and Westboro O-Train stations increases opportunities for residents to fulfill most of their daily needs by walking, cycling, or taking transit.

**The proposed Minor Zoning By-law Amendment and Site Plan Control applications are consistent with the policies of the Provincial Policy Statement.**

### 3.2 City of Ottawa Official Plan (2021)

The new City of Ottawa Official Plan (the “2021 Official Plan”) was adopted by City Council on November 24<sup>th</sup>, 2021. The 2021 Official Plan has subsequently been sent to the Minister of Municipal Affairs and Housing and is awaiting a final decision. For the purposes of this Planning Rationale, the 2021 Official Plan dated November 24, 2021 was used for reference.

Figure 12. Official Plan Schedule B2 Excerpt



#### 3.2.1 Land Use Designation

The Subject Property is designated “Mainstreet Corridor” and “Evolving Neighbourhood Overlay” in the Inner Urban Transect on Schedule B2 of the 2021 Official Plan (see Figure 12).

Section 5.2 of the 2021 Official Plan sets out general policies and guidance for proposed development within the Inner Urban Transect.

Section 5.2.1, Policy 4 of the 2021 Official Plan states that:

- “4) The Inner Urban Transect shall continue to develop as a mixed-use environment, where:*
- a) Hubs and a network of Mainstreets and Minor Corridors provide residents with a full range of services within a walking distance from home, in order to support the growth of 15-minute neighbourhoods;*
  - b) Small, locally oriented services may be appropriately located within Neighbourhoods;*
  - c) Existing and new cultural assets are supported, including those that support music and nightlife;*
  - d) Larger employment uses are directed to Hubs and Corridors; and*
  - e) Increases in existing residential densities are supported to sustain the full range of services noted in Policy a).”*

Policies of the 2021 Official Plan direct areas designated “Mainstreets” to accommodate increased residential growth and mixed-use development that support a wide variety of housing types and services for residents in proximity to home. The proposed development of a mixed-use building on the Subject Property is consistent with policies of the 2021 Official Plan for Mainstreet Corridors within the Inner Urban Transect.

Section 5.2.3, Policy 2 of the 2021 Official Plan states that:

- “2) Along Mainstreets, permitted building heights are as follows, subject to appropriate height transitions, setbacks, and angular planes:*
- a) On sites that front on segments of streets whose right-of-way (after widening requirements have been exercised) is 30 metres or greater as identified in Schedule C16 for the planned street context, and where the parcel is of sufficient size to allow for a transition in built form massing, not less than 2 storeys and up to High-rise;*
  - b) On sites that front on segments of streets whose right-of-way is narrower than 30 metres, generally up to 9 storeys except where a secondary plan or area-specific policy specifies different heights; and*
  - c) In all cases:*
    - i) The wall heights directly adjacent to a street, and the heights of the podiums of High-rise buildings, where permitted, shall be proportionate to the width of the abutting right of way, and consistent with the objectives in the urban design section on Mid-rise and High-rise built form in Subsection 4.6.6, Policies 7), 8) and 9); and*
    - ii) The height of such buildings may be limited further on lots too small to accommodate an appropriate height transition.”*

The Subject Property is designated as Mainstreet on Schedule B2 of the 2021 Official Plan. The Winona Avenue right-of-way is less than 30 metres in width abutting the Subject Property. A building height of up to 9 storeys on the Subject Property would generally be supported by policies of the 2021 Official Plan. A secondary plan or area-specific policy may specify different heights. The Subject Property is located within the boundary of the Richmond Road / Westboro Secondary Plan. Policies of the Richmond Road / Westboro Secondary Plan take precedence over policies of the 2021 Official Plan.

Section 5.6.1 of the 2021 Official Plan provides policy direction for Built Form Overlays, including the Evolving Neighbourhoods Overlay.

Section 5.6.1.1, Policy 1 of the 2021 Official Plan states that:

*“1) The Evolving Overlay will apply to areas that are in a location or at stage of evolution that create the opportunity to achieve an urban form in terms of use, density, built form and site design. These areas are proximate to the boundaries of Hubs and Corridors as shown in the B-series of schedules of this Plan. The Evolving Overlay will be applied generally to the properties that have a lot line along a Minor Corridor; lands 150 meters from the boundary of a Hub or Mainstreet designation; and to lands within a 400-metre radius of a rapid transit station. The Overlay is intended to provide opportunities that allow the City to reach the goals of its Growth Management Framework for intensification through the Zoning By-law, by providing:*

- a) Guidance for a gradual change in character based on proximity to Hubs and Corridors.*
- b) Allowance for new building forms and typologies, such as missing middle housing;*
- c) Direction to built form and site design that support an evolution towards more urban built form patterns and applicable transportation mode share goals; and*
- d) Direction to govern the evaluation of development.*

The Subject Property is suitable for intensification being located within 150 metres from the Richmond Road Mainstreet Corridor and within 400-metres radius of the planned Westboro O-Train station. The proposed development of a six-storey, mixed-use building on the Subject Property is consistent with City objectives to establish a more urban community character and to create opportunities for infill and intensification within walking distance of planned rapid transit stations in the Inner Urban Transect.

Section 5.6.1.1, Policy 2 of the 2021 Official Plan states that:

*“2) Where an Evolving overlay is applied:*

- a) The Zoning By-law shall provide development standards for the built form and buildable envelope consistent with the planned characteristics of the overlay area, which may differ from the existing characteristics of the area to which the overlay applies; and*
- b) The Zoning By-law shall include minimum-density requirements as identified in Table 3a, and permissions to meet or exceed the density targets of Table 3b.”*

For Mainstreet Corridors, the required minimum density is 120 dwelling units per net hectare. The proposed development exceeds this requirement, as it provides approximately 760 dwelling units per net hectare.

Section 5.6.1.1, Policy 6 of the 2021 Official Plan states that:

*“6) Zoning By-law development standards and development on lands with an Evolving Overlay should generally include built form and site design attributes that meet most of the urban*

*characteristics described in Table 6 in Section 5, and where suburban attributes are retained, that these do not structurally impede the achievement of a fully urban site design over time.”*

The urban characteristics outlined in Table 6 of Section 5 of the 2021 Official Plan are as follows:

- *Shallow front yard setbacks and in some contexts zero front yards with an emphasis on built-form relationship with the public realm*
- *Principal entrances at grade with direct relationship to public realm*
- *Range of lot sizes that will include smaller lots, and higher lot coverage and floor area ratios*
- *Minimum of two functional storeys*
- *Buildings attached or with minimal functional side yard setbacks*
- *Small areas of formal landscape that should include space for soft landscape, trees and hard surfacing*
- *No automobile parking, or limited parking that is concealed from the street and not forming an integral part of a building, such as in a front facing garage*

The proposed development has zero metre front and corner side yard setbacks to establish a strong interface with the public realm along Winona Avenue and Picton Avenue. To create an active street frontage, principal entrances to the commercial and residential portions of the building are located facing Winona Avenue complete with soft landscaping, trees and patio areas for residents and visitors. The principal entrances are at grade with the street level to provide direct access and reduce accessibility concerns for residents and visitors. The proposed development has limited off-street parking located entirely underground. The entrance to the parking garage is located to the east extent of the building along Picton Avenue and is separated from the principal entrances and active frontages along Winona Avenue.

Section 6.2 of the 2021 Official Plan sets out functional policies related to Corridor designations that are to be applied to each Transect on a site-specific basis.

Section 6.2.1, Policy 1 of the 2021 Official Plan states that:

*“The Corridor designation applies to any lot abutting the Corridor, subject to:*

a) *Generally, a maximum depth of:*

i) *In the case of Mainstreet Corridors, a maximum depth of 220 metres from the centreline of the street identified as a Mainstreet Corridor;*

The Subject Property has frontage on Winona Avenue. The Subject Property is located entirely within 220 metres of the centreline of Richmond Road, designated as Mainstreet Corridor in the 2021 Official Plan. The Mainstreet Corridor designation applies to the entire area of the Subject Property.

Section 6.2.1, Policy 2 of the 2021 Official Plan states that:

*“2) Development within the Corridor designation shall establish buildings that locate the maximum permitted building heights and highest densities close to the Corridor, subject to building setbacks where appropriate. Further, development:*

- a) Shall ensure appropriate transitions in height, use of land, site design and development character through the site, to where the Corridor designation meets abutting designations;
- b) *May be required to provide public mid-block pedestrian connections to nearby streets or abutting designations;”*

Policies of the 2021 Official Plan direct new development to locate the maximum permitted building heights and highest densities close to the corridor along Richmond Road. Policies of the 2021 Official Plan direct new development to ensure compatibility with surrounding land use designations through appropriate design and transitions in height and land use. Step backs provided above the fourth storey help to facilitate the transition in height between the proposed development and low-rise buildings to the north, south and west. The proposed six-storey building height complements the permitted building heights of abutting lands to the south and east. The proposed development on the Subject Property is designed to provide appropriate transition to surrounding land uses and built form and create a comfortable pedestrian environment along Winona Avenue.

Section 6.2.1, Policy 3 of the 2021 Official Plan states that:

“3) Corridors will generally permit residential uses and such non-residential uses that integrate with a dense, mixed-use urban environment. The City may require through the Zoning By-law and/or development applications to amend the Zoning By-law:

- a) *Commercial and service uses on the ground floor of otherwise residential, office and institutional buildings with a strong emphasis on uses needed to contribute to 15-minute neighbourhoods;*
- b) *Residential and/or office uses on the upper floors of otherwise commercial buildings; and/or*
- c) *Minimum building heights in terms of number of storeys to ensure multi-storey structures where uses can be mixed vertically within the building.”*

The proposed development provides a mix of uses within the building. The Subject Property is well located within walking distance from a number of commercial, employment, and recreational amenities. The proposed mixed-use development will help contribute to the establishment of 15-minute communities by providing the residential density needed to support commercial uses on the ground floor and within the surrounding area.

### **3.2.2 Growth Management Framework**

Section 3 of the 2021 Official Plan sets out policies to direct growth toward target areas for intensification. The majority of projected growth between 2018 and 2046 is directed within the urban boundary, representing 93% of all new development. 47% of the growth allocation is directed to take place within existing built-up areas inside the urban boundary where services are available or can be easily provided for new development to accommodate the creation of jobs, housing and increased transit use.

Section 3.2, Policy 3 of the adopted Official Plan identifies “Corridors” as one of the target areas designated for residential and employment intensification in the City of Ottawa.

The Subject Property is designated as Mainstreet Corridor and is located within the Inner Urban Transect on Schedule B2 of the adopted Official Plan. The Subject Property is located within a land use designation targeted for intensification. The proposed development of a six-storey, mixed use building on the Subject Property is consistent with the growth objectives of the 2021 Official Plan.

Section 3.2, Policy 10 of the 2021 Official Plan sets out residential density targets that apply to Corridors and states: *“The residential density and proportion of large household dwelling targets as shown on Schedules B1 through B8 are established in Table 3a for Hubs and Mainstreet Corridors and Table 3b for Neighbourhoods and Minor Corridors.”*

The Subject Property is designated as Mainstreet Corridor in the 2021 Official Plan. Policies of the adopted Official Plan establish a minimum area-wide density requirement of 120 people and jobs per gross hectare and a minimum a residential density requirement of 120 dwellings per net hectare for proposed developments in the Inner Urban Transect.

The Subject Property has a gross area of approximately 960 square metres. A mixed-use development is proposed with commercial uses on the ground level and residential units located at grade and above. At a rate of 1 resident per unit, the Subject Property would provide 60 residents. Additional residents are anticipated, and jobs would be created through the commercial components of the proposed development. The density of the proposed development is approximately 625 people and jobs per gross hectare. The proposed development would exceed the required minimum density targets for people and jobs per gross hectare in the 2021 Official Plan.

60 dwelling units are proposed on the Subject Property, which has a net buildable area of approximately 787 square metres. The density of the proposed development is approximately 760 dwelling units per net hectare, which exceeds the minimum requirement and is consistent with residential density targets for Mainstreets in the 2021 Official Plan.

### **3.2.3 City-wide Policies**

Section 4 of the 2021 Official Plan sets out city-wide policies to be considered where all new development is proposed.

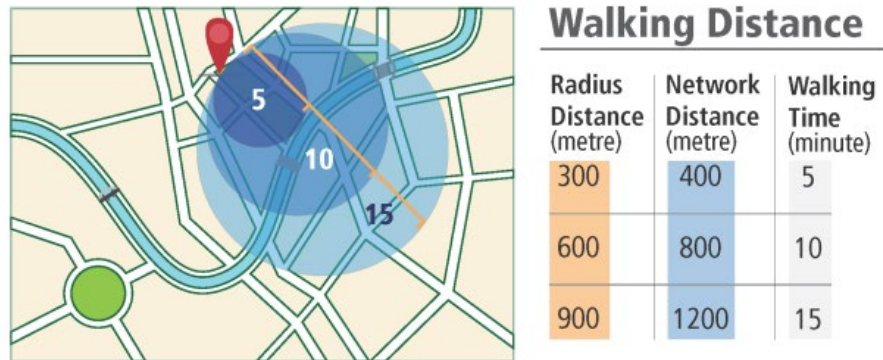
Section 4.1.2, Policy 1 of the 2021 Official Plan sets out the framework for establishing 15-minute neighbourhoods across the City and states that:

*“1) In general, this Plan equates a walking time of:*

- a) 5 minutes to be equivalent to a radius of 300 metres, or 400 metres on the pedestrian network;*
- b) 10 minutes to be equivalent to a radius of 600 metres, or 800 metres on the pedestrian network; and*
- c) 15 minutes to be equivalent to a radius of 900 metres or 1,200 metres on the pedestrian network.”*



Figure 13 – Official Plan Figure 11



The Subject Property is located fully within 600 metres radius and within 800 metres network distance of the planned Westboro and Kichi Sibi O-Train stations. Transit stops along frequent bus route 11 are located south of the Subject Property along Richmond Road. The proximity of the proposed development to a range of transit options in the immediate area represents a walking distance of 10 minutes or less for residents to access rapid transit and frequent street transit from their home (see Figure 13).

### 3.2.4 Urban Design

Section 4.6 of the adopted Official Plan sets out policies related to Urban Design and is intended to promote design excellence in Design Priority areas, encourage innovation in site planning and building design, support the objectives of Corridors, Hubs and Neighbourhoods and enable integration of new development with existing communities.

Section 4.6.5, Policy 2 of the 2021 Official Plan states that:

*“Development in Hubs and along Corridors shall respond to context, transect area and overlay policies. The development should generally be located to frame the adjacent street, park or greenspace, and should provide an appropriate setback within the street context, with clearly visible main entrances from public sidewalks. Visual impacts associated with above grade utilities should be mitigated.”*

The proposed development has been designed to frame Winona Avenue and Picton Avenue with an appropriate building height, setback, and building materials. The active frontage on Winona Avenue provides an extension of pedestrian connections to the Richmond Road Corridor. The building stepback and transition of building materials above the fourth storey along Winona Avenue and Picton Avenue complements neighbouring low-rise buildings and provides a human-scaled environment for pedestrians. The use of brick, significant proportion of glazing, and landscape treatments at grade level creates a welcoming pedestrian condition at the building base along Winona Avenue and Picton Avenue and contributes to the commercial streetscape along Richmond Road.

Section 4.6.6, Policy 7 of the 2021 Official Plan states that:

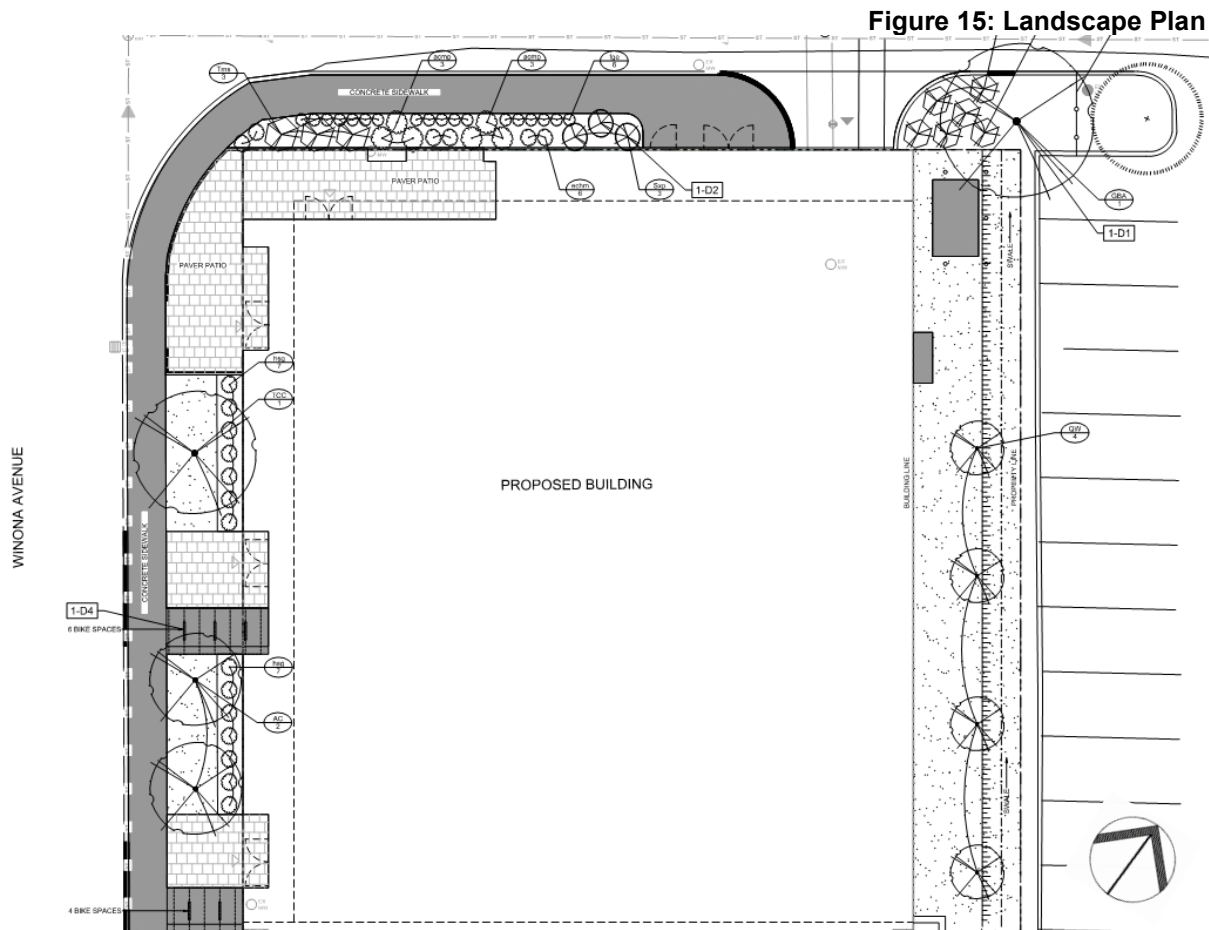
*“7) Mid-rise buildings shall be designed to respond to context, and transect area policies, and should:*

- a) Frame the street block and provide mid-block connections to break up large blocks;*
- b) Include a base with active frontages, and a middle portion that relates to the scale and character of the surrounding buildings, or, planned context;*
- c) Be generally proportionate in height to the width of the right of way as illustrated in the Figure below, with additional height permitted in the Downtown Core Transect; and*
- d) Provide sufficient setbacks and step backs to:*
  - i) Provide landscaping and adequate space for tree planting;*
  - ii) Avoid a street canyon effect; and*
  - iii) Minimize microclimate impacts on the public realm and private amenity areas.”*

The proposed development of a mid-rise, mixed-use building on the Subject Property has been designed to frame Winona Avenue and Picton Avenue (see Figure 14). The principal building entrance is provided off Winona Avenue to access the residential lobby from the sidewalk. Entrances to ground floor commercial and retail units are provided off Winona Avenue to establish an active street frontage. The building has been designed with a distinct base comprised of generous glazing and landscape treatments. The base and middle portion of the building is primarily clad with red brick and is stepped back above the fourth storey with a transition to dark coloured panelling on levels five and six. An appropriate building setback, materials and façade articulation is provided that defines the building entrances and maintains the general scale and character of the surrounding community. Appropriate space for landscaping treatments along Winona Avenue and Picton Avenue help to improve the interface with the public realm. The building is adequately set back from the street. The proposed development of a six-storey, mixed-use building on the Subject Property is consistent with policies of the 2021 Official Plan.

**Figure 14: West Building Elevation (Winona Avenue)**





Section 4.6.5, Policy 3 of the 2021 Official Plan states that:

*“Development shall minimize conflict between vehicles and pedestrians and improve the attractiveness of the public realm by internalizing all servicing, loading areas, mechanical equipment and utilities into the design of the building, and by accommodating space on the site for trees, where possible. Shared service areas, and accesses should be used to limit interruptions along sidewalks. Where underground parking is not viable, surface parking must be visually screened from the public realm.”*

Direct pedestrian access to the Subject Property is provided from the principal building entrances to the public sidewalk along Winona Avenue. Grade level amenity space, including a patio, soft landscaping and trees are provided along Winona Avenue and Picton Avenue. A soft landscape area with trees is provided in the interior side yard along the east edge of the Subject Property for building residents and visitors (see Figure 15). Motor vehicle access to an underground garage is provided off Picton Avenue for the proposed development. Providing motor vehicle access to the Subject Property that is separated from Winona Avenue reduces the potential for vehicle conflicts and improves the safety, accessibility, and movement of pedestrians and bicycles to and from the Subject Property and is consistent with policies of the 2021 Official Plan.

### 3.2.5 Review of Development Applications

The 2021 Official Plan requires a range of studies to be included as part of a complete development application to meet the objectives of the Official Plan. The appropriate policies, related studies, and plans were identified through a pre-application consultation meeting with City staff at the beginning of the application review process.

Required studies and plans identified as relevant have been prepared in support of the proposed development. Detailed and technical information can be obtained by reviewing the respective documents.

#### Relating to Section 4.7.1 – Stormwater Management and Site Servicing

Policy 6 of Section 4.7.1 requires redevelopment applications to implement site, grading, building, and servicing design measures. As part of this application, an Existing Conditions, Removals, Lot Grading, Drainage, Servicing, Erosion and Sediment Control Plan has been completed by McIntosh Perry, dated October 12, 2022.

Policy 8 of Section 4.7.1 requires proof of sufficient stormwater management and drainage system as a condition of Site Plan Control approval. A Servicing and Stormwater Management Report was completed by McIntosh Perry dated October 12, 2022. The Servicing and Stormwater Management Report examined the potential water quality and quantity impacts of the proposed development and how each will be addressed through the proposed development. The proposed stormwater design and recommendations are consistent with the policies of Section 4.7.1 of the 2021 Official Plan.

Policy 12 of Section 4.7.1 requires an approved master servicing study, an approved environmental management plan, and a subwatershed study to be included as part of a complete application for a new development in a future neighbourhood. The Subject Property is not located in a future neighbourhood and these studies are not required. City staff requested that a Site Servicing study and plan be included as part of the complete application package. A Servicing and Stormwater Management Report and Plan was completed by McIntosh Perry dated October 12, 2022. The recommendations of the Site Servicing Study conclude that:

*“Based on City guidelines (ISTB-2018-02), the existing hydrants provide adequate (fire) protection for the proposed development.”, and that “the proposed (sanitary) system is sufficiently sized for the development.”*

The recommendations of the Site Servicing Study are consistent with Section 4.7.1 of the 2021 Official Plan.

#### Relating to Section 10.1.6 – Contaminated Sites

Policy 1 of Section 10.1.6 requires environmental site assessments and remedial or risk assessment/risk management activities reports to be completed as part of a development application. City staff identified the need for a Phase I Environmental Assessment to be completed as part of the application. A Phase I ESA was completed for the previous owner by Paterson Group dated September 15, 2021.

The recommendations of the Phase I ESA state that:

*“Based on the results of the Phase I ESA, in our opinion, a Phase II Environmental Site Assessment is required for the Phase I Property”*

Based on the recommendations of the Phase I ESA, a subsequent Phase II ESA was prepared by Paterson Group, dated October 15, 2021. The recommendations of the Phase II ESA state the following:

*“Based on the findings, the VOC impacted groundwater has migrated onto the property from an off-site source. Furthermore, it is expected that the groundwater quality is indicative of a regional groundwater plume and, as a result, it is not possible to remediate the subject property to generic standards. Given that a record of site condition is not required for the redevelopment of the property, it is recommended that a due diligence risk assessment be completed by a toxicology company to develop mitigative measures that may be warranted prior to the future redevelopment of the Phase II – Property.”*

Based on the recommendations of the Phase II ESA a Record of Site Condition is not required for the proposed development. Due to the presence of contaminants beneath the Subject Property, City staff requested that a subsequent Human Health Risk Assessment be submitted with the complete application package. A Human Health Risk Assessment was prepared by Novatox Inc., dated August 2022. The recommendations of the Human Health Risk Assessment state the following:

*“The only practical way to address the assumption would be to perform an indoor air monitoring program, to determine the extent to which modelling reflects real-world conditions. As these are existing residential houses, an indoor air program could be completed to determine whether PCE, TCE and VC is present at sufficient levels to cause of unacceptable risk to occupants of the building. It is also feasible to instrument sub-slab vapour probes through the crawlspace floor and to collect vapour samples from beneath the grade. These concentrations can then be attenuated (using a default factor) to estimate what the potential air levels are within the actual building. This information can be used to further support a weight of evidence for the site and the assessment of potential health risks to building occupants.”*

Based on the recommendations of the Human Health Risk Assessment, a subsequent Air Quality Assessment Program was completed for submission with the complete application package by Paterson Group, dated August 30, 2022. The recommendations of the Air Quality Assessment Program state the following:

*“Based on the findings of the air testing program, cis-1,2-Dichloroethylene, Tetrachloroethylene, Trichloroethylene and Vinyl Chloride were not detected above the laboratory’s method detection limit in either interior air sample. Therefore, the migration of VOC vapours from the groundwater into the residential buildings is considered to be of low risk and does not pose a health concern to the building’s occupants.”*

The recommendations are consistent with Section 10.1.6. of the 2021 Official Plan.

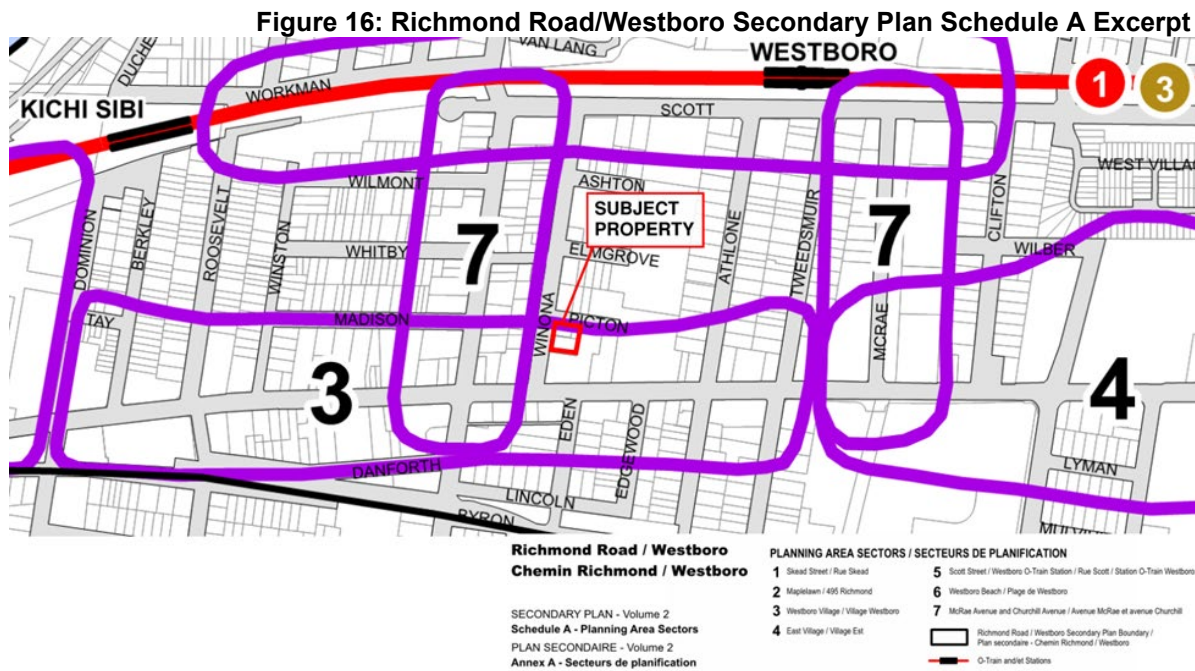
Relating to Section 10.2.1 – Noise

Policy 2 of Section 10.2.1 requires a Roadway Traffic Noise Study to be prepared as part of a complete application. A Roadway Traffic Noise Study was completed by Gradient Wind dated October 12, 2022. The recommendations of the Roadway Traffic Noise Study state the following:

*“The results of the current study indicate that noise levels will range between 60 and 61 dBA during the daytime period (07:00-23:00) and around 59 dBA during the nighttime period (23:00-07:00). The highest noise levels occur along the south façade which is nearest and most exposed to Richmond Road. Since noise levels do not exceed the ENCG plane of window limit of 65 dBA and 60 dBA during the daytime and nighttime periods, respectively, standard building components will be sufficient. The development will require forced air heating with provisions for central air conditioning, however given the nature of the development air condition is expected to be provided. A Type D Warning Clause will be required on all Lease, Purchase and Sale Agreements.”*

The recommendations are consistent with Section 10.2.1 of the 2021 Official Plan.

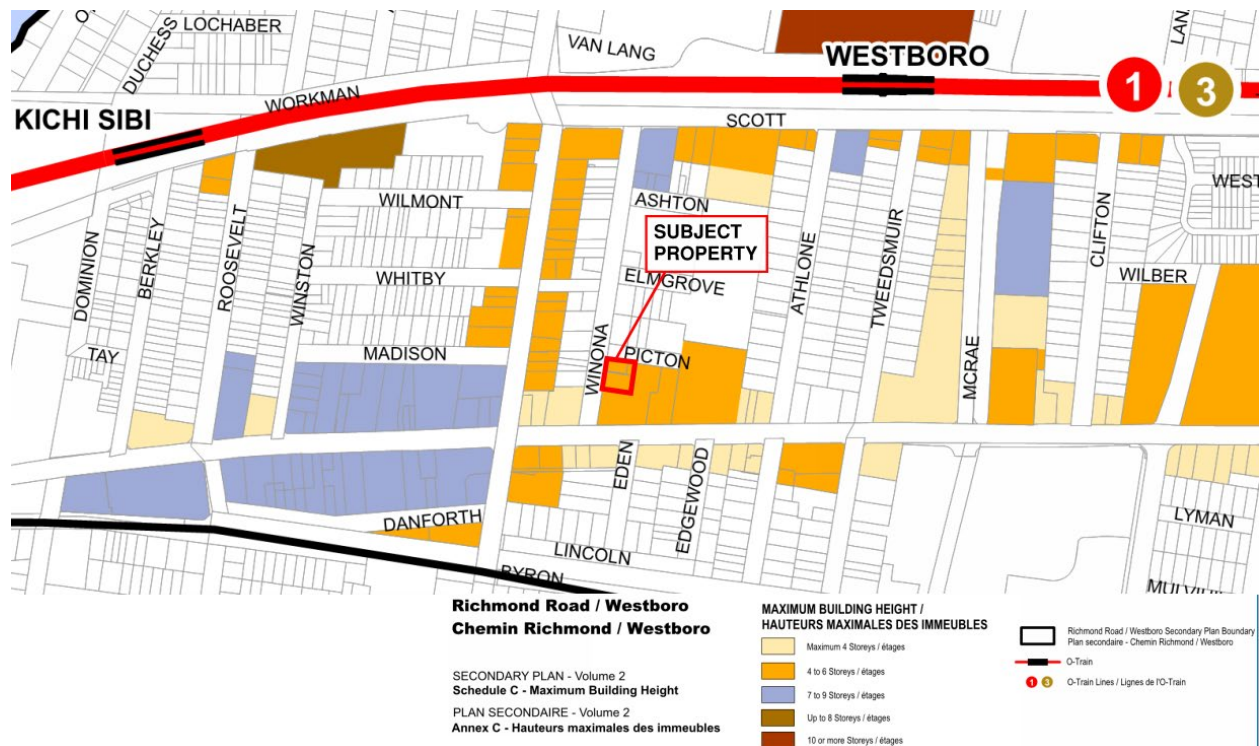
**3.3 Richmond Road/Westboro Secondary Plan**



The Subject Property is located within the boundary of the Richmond Road / Westboro Secondary Plan. The Richmond Road / Westboro Secondary Plan was adopted by City Council along with the new Official Plan on November 24, 2021.

Detailed policy direction regarding development within distinct sectors apply where provided by the adopted Secondary Plan. The Subject Site is located within the boundary of “Sector 3 – Westboro Village” (see Figure 16).

Figure 17: Richmond Road/Westboro Secondary Plan Schedule C Excerpt



Section 5.3 of the adopted Secondary Plan sets out policies related to land use and maximum building heights for each sector that:

“5) Maintain a pedestrian scale in Westboro Village by providing for mixed-use buildings of an appropriate height that ensures street proportionality and is consistent with transition and other built form policies of the Official Plan, generally within the range of four to six storeys, with a minimum height of two storeys. Buildings should be located close to the street...”

7) Encourage a mix of uses, including a continuity of ground floor retail / restaurant uses with residential and office uses on the upper floors.”

The proposed six-storey, mixed-use building is located close to Winona Avenue and Picton Avenue and is designed with a defined base and middle portion that is proportionate in scale and utilizes materials that are compatible with the character of surrounding low-rise built forms. A maximum building height of four to six storeys is permitted on the Subject Property on Schedule C of the Richmond Road/Westboro Secondary Plan (see Figure 17). The proposed development is designed to be compatible with higher density development anticipated along the Richmond Road corridor. Ground floor commercial and retail uses are proposed with residential uses located on upper floors consistent with policies of the 2021 Official Plan and the Richmond Road/Westboro Secondary Plan.

**The proposed Minor Zoning By-law Amendment and Site Plan Control applications conform to the policies of the 2021 City of Ottawa Official Plan.**

## 4.0 URBAN DESIGN GUIDELINES

The City of Ottawa's Urban Design Guidelines provide guidance in order to promote and achieve appropriate development within key growth areas throughout the City. Where the Urban Design guidelines apply, not all of the direction provided will apply to the proposed development. The Urban Design Guidelines are non-statutory documents and are intended to supplement the policies and regulations of the Official Plan and Zoning By-law. Two sets of Urban Design Guidelines apply to the Subject Property. These are the Urban Design Guidelines for Development along Traditional Mainstreets and the Transit-Oriented Development Guidelines.

### 4.1 Urban Design Guidelines for Development along Traditional Mainstreets

The Urban Design Guidelines for Development along Traditional Mainstreets were completed and approved on May 24, 2006. These guidelines apply across the City to all streets identified as a Traditional Mainstreet in the Official Plan (2003) and provide urban design guidance for new developments. The primary objectives of the guidelines are as follows:

- To promote development that will enhance and reinforce the recognized or planned scale and character of the street
- To promote development that is compatible with, and complements its surroundings
- To achieve high-quality built form and strengthen building continuity along Traditional Mainstreets
- To foster compact, pedestrian-oriented development linked to street level amenities
- To accommodate a broad range of uses including retail, services, commercial uses, offices, residential and institutional uses where one can live, shop and access amenities.

The proposed development adheres to the objectives of the Urban Design Guidelines for Development along Traditional Mainstreets. The proposed six-storey, mixed-use development includes a stepback above the fourth storey along Winona Avenue and Picton Avenue which contributes to maintaining the existing scale and character of the streetscape and improving pedestrian comfort. The proposed building design provides a transition between the street and adjacent low-rise built forms. Ground floor commercial uses are intended to extend the existing retail character of the Richmond Road Corridor along Winona Avenue. A significant proportion of glazed surfaces and landscape treatments at grade create active frontages designed to provide an open and transparent pedestrian experience and provide direct access to the building from Winona Avenue. The proposed residential density provided on the site will support the surrounding commercial uses, allowing residents to access their daily needs within walking distance of home. All parking on the site is separated from Winona Avenue with access off Picton Avenue. Off-street parking is located below grade to minimize any potential impact on the street.



## 4.2 Transit-Oriented Development Guidelines

The Transit-Oriented Development Guidelines were completed and approved by City Council on September 26, 2007. The Subject Property is located within 600 meters walking distance of the planned Westboro O-Train station. The guidelines provide direction for all development located within a 600-meter walking distance of a rapid transit stop or station and apply to the proposed development.

The primary objectives of the guidelines are related to the following themes:

- Land Use – Locating the right type and combination of uses for supporting transit ridership
- Layout – to foster development that is convenient and accessible for transit use
- Built Form – to create more interesting and attractive public realms around transit stations through “place-making”
- Pedestrians & Cyclists – to enhance the experience of getting to and from a transit station
- Vehicles & Parking – to minimize conflicts between vehicles and pedestrians using transit
- Streetscape & Environment – quality of design in spaces of the public realm leading to and from transit stations or stops

The proposed development adheres to the objectives of the Transit-Oriented Development Guidelines. The Subject Property is located within 600 meters radius and is within a 10 minute walking distance from the planned Westboro O-Train station. The proposed development provides an increased residential density on the site and supports increased transit ridership. A distinct building base and middle including building stepbacks and material transitions above the fourth storey create a human scaled streetscape condition along Winona Avenue and Picton Avenue. Providing principal entrances to commercial uses and the residential lobby at grade level establishes direct access to the sidewalk for pedestrians and supports a pedestrian-oriented environment. Ground-floor commercial uses with significant glazing enhance the streetscape condition along Winona Avenue and establish an enjoyable and comfortable pedestrian experience. Vehicle parking is located away from the intersection of Winona Avenue and Picton Avenue, minimizing potential conflict with pedestrians moving around the Subject Property.

## 5.0 CITY OF OTTAWA ZONING BY-LAW 2008-250

The Subject Property is zoned Traditional Mainstreet (TM) in the City of Ottawa Zoning By-law 2008-250. The property is also subject to the Mature Neighbourhoods Overlay.

The purpose of the TM zone is to:

1. *accommodate a broad range of uses including retail, service commercial, office, residential and institutional uses, including mixed-use buildings but excluding auto-related uses, in areas designated **Traditional Mainstreet** in the Official Plan;*
2. *foster and promote compact, mixed-use, pedestrian-oriented development that provide for access by foot, cycle, transit and automobile;*
3. *recognize the function of Business Improvement Areas as primary business or shopping areas; and*

4. *impose development standards that will ensure that street continuity, scale and character is maintained, and that the uses are compatible and complement surrounding land uses.*

A six-storey, mixed-use building is proposed for the Subject Property. The proposed development will include two ground-floor commercial units and 60 residential dwelling units. The incorporation of ground-floor commercial uses, proximity to the Richmond Road corridor and the planned Kichi Sibi and Westboro O-Train stations will promote a pedestrian-oriented public realm where residents can access daily needs by walking, cycling, or taking transit. The built form of the building is designed to maintain the scale and character of the area by providing setbacks above the fourth storey facing Winona Avenue and Picton Avenue to reduce the scale and visual impact of the building from the street. The proposed development is consistent with the purpose of the TM zone.

Section 197 (1) of the Zoning By-law lists a number of permitted non-residential uses for the TM zone including retail store, personal service business, restaurant and office. The proposed commercial units are permitted to be occupied by a variety of retail and employment uses. No specific employment use has been identified for the proposed development. Section 197(2) of the Zoning By-law lists “*apartment dwelling, mid rise*” as a permitted use in the TM zone. The proposed mixed-use building is permitted in the TM zone.

Table 1 below summarizes the applicable zoning provisions for the Subject Property.

**Table 1: Zoning Review Table**

Zoning Provision	Required	Provided
Minimum Lot Area (m <sup>2</sup> )	No minimum	961 m <sup>2</sup>
Minimum Lot Width (m)	No minimum	29.9 m
Max Front Yard Setback (m)	2 m	0 m
Max Interior Yard Setback (m)	3 m between a non-residential use building or a mixed-use building and another non-residential use building or mixed-use building	n/a  Interior yard abuts a surface parking lot
Min Interior Yard Setback (m)	No minimum	4.2 m
Minimum Corner Yard Setback (m)	3 m, except for any part of a building above 15 metres for which an additional 2 metre setback must be provided	<b>0 m</b>  Additional 2 m setback provided on 4 <sup>th</sup> storey
Minimum Rear Yard Setback (m)	No minimum	0 m
Minimum Building Height (m)	6.7 metres for a distance of 20 metres from the front lot line as set out under subsection 197(5)	19.00 m

Maximum Building Height (m)	20 m / 6 storeys  Additional setback of 2 meters where building greater than 4 storeys	19.00 m  Additional 2 m setback provided above 4 <sup>th</sup> storey
Minimum Driveway Width (m)	3 m for parking lots with less than 20 parking spaces	3.88m
Maximum Driveway Width (m)	3.6 m	<b>3.88m</b>
Required Total Amenity Area	360 m <sup>2</sup> (6 m <sup>2</sup> per dwelling unit)	360 m <sup>2</sup>  Ground Floor = <b>40m<sup>2</sup></b> Lvl 5 terrace = <b>80m<sup>2</sup></b> Roof amenity = <b>240m<sup>2</sup></b> Total = <b>360m<sup>2</sup></b>
Required Communal Amenity Area	180 m <sup>2</sup> (50% of required total amenity area)	280 m <sup>2</sup>  Ground Floor = <b>40m<sup>2</sup></b> Roof amenity = <b>240m<sup>2</sup></b> Total = <b>280m<sup>2</sup></b>
<b>Projections</b>		
Minimum Setback for Rooftop Terrace	1.5 m from exterior wall	1.5 m
Maximum Projection (Balcony)	2 m	n/a
<b>Parking Requirements - Residential</b>		
Minimum Parking Spaces	24 spaces (0.5 spaces per dwelling unit after the first 12 units)	<b>13 spaces</b> <b>(0.28 spaces per dwelling unit after the first 12 units)</b>
Minimum Visitor Parking Spaces	5 spaces (0.1 spaces per dwelling unit after the first 12 units)	5 visitor parking spaces
Maximum Visitor Parking Spaces	30 spaces	5 visitor parking spaces
Minimum Barrier Free Parking Spaces (as per Section 3.1.2 Table 3 of Accessibility Design Standards)	1 Type A space 1 Type B space	1 Type A space 1 Type B space
<b>Parking Requirements – Non-Residential</b>		
Area Y – Ground Floor/Basement	GFA= 500m <sup>2</sup> or less, no off-street parking required	n/a
<b>Bicycle Parking Requirements</b>		
Minimum Bicycle Parking Spaces – Residential	30 (0.5 spaces per dwelling unit)	36 bicycle spaces
Minimum Bicycle Parking Spaces – Retail / Commercial	2 (1 space per 250 m <sup>2</sup> gross floor area)	26 interior bicycle parking 10 exterior bicycle parking

The Subject Property is located in Area Y on Schedule 1A of the Zoning By-law. Section 101 of the Zoning By-law states that ground floor commercial uses located in Area Y which do not exceed a total gross floor area of 500 square metres do not require motor vehicle parking. Each of the proposed ground floor commercial units are less than 500 square metres in gross floor area. No off-street parking is required for commercial units as part of the proposed development. Section 64 of the Zoning Bylaw regulates permitted projections above the height limit. The provision states that maximum building limits do not apply to the following structures.

- *“Mechanical and service equipment penthouse, elevator or stairway penthouses*
- *Landscaped areas, roof-top gardens and terraces and associated safety guards and access structures; pursuant to Table 55, Row (8)”*

The proposed development includes a communal and a private rooftop terrace, as well as an elevator and a common stairwell to access the rooftop. A mechanical room is also located on the roof of the building. Each of the proposed uses are permitted to be located above the height limit.

Table 55, Row (8) provides provisions for rooftop landscaped areas, gardens and terraces. For an apartment dwelling, mid-rise where the terrace is located on the roof of the uppermost storey, the terrace is required to be set back a minimum of 1.5 meters from any exterior wall of the building. The proposed development fully conforms with provisions of the Zoning By-law for permitted projections (see Table 1).

The proposed development fully conforms with the required front yard, rear yard and interior side yard setbacks, maximum permitted building height, minimum required visitor parking, minimum required bicycle parking, amenity area and permitted projections. A minor rezoning application is required to provide relief from zoning provisions on the Subject Property to decrease the minimum required corner side yard setback along Winona Avenue, to decrease the minimum required off-street parking spaces for the proposed residential units and to increase the maximum permitted driveway width providing access to the underground parking garage.

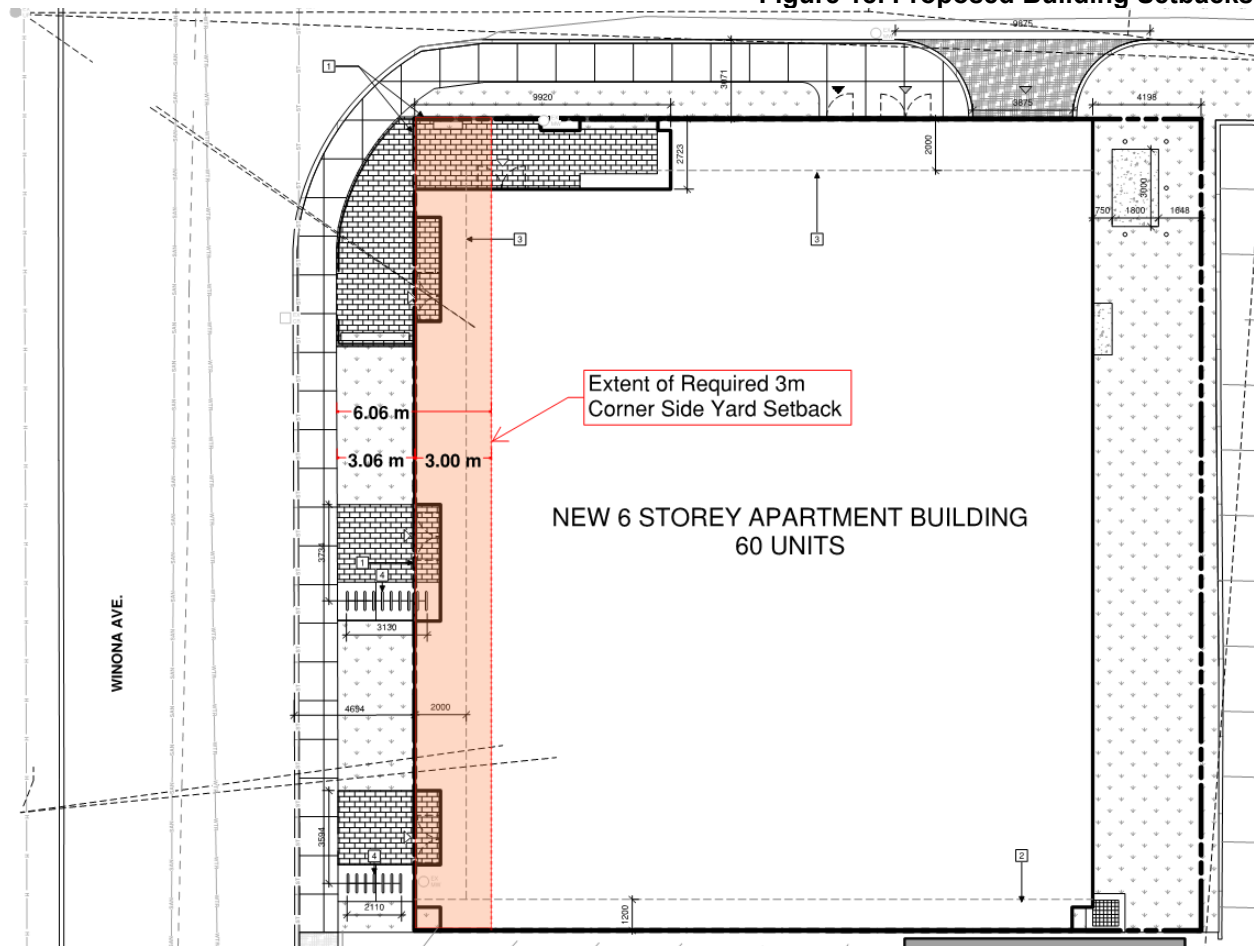
**The proposed Minor Zoning By-law Amendment and Site Plan Control application are consistent with the purpose of the Traditional Mainstreet zone and are consistent with the relevant provisions of the City of Ottawa Zoning By-law.**

## **6.0 PROPOSED ZONING BY-LAW AMENDMENT**

The provisions of the TM zone require a minimum corner side yard setback of three metres for a mid-rise apartment dwelling on the Subject Property. Relief from the zoning provision is required to decrease the minimum required corner side yard setback by three metres to zero metres to facilitate the proposed development. Policies of the 2021 Official Plan and the Richmond Road/Westboro Secondary Plan and relevant Urban Design Guidelines direct new development in areas designated for intensification in proximity to rapid transit to be located close to the street. Providing a three metre corner side yard setback along Winona Avenue would result in the front wall of the proposed building being located approximately six metres from the sidewalk (see Figure 18). A proposed zero metre corner side yard setback will provide approximately three metres of landscaped area between the proposed building and the sidewalk along Winona Avenue and will bring the building entrance and commercial units closer to the street. The

proposed relief will not result in issues providing appropriate building area or building setbacks from surrounding lots and will improve pedestrian access to the proposed building. The proposed zoning amendment will permit a reduction to the required corner side yard setback for the proposed building that reflects a more urban condition and the importance of providing direct pedestrian access to the sidewalk in proximity to future higher density, transit and pedestrian-oriented developments anticipated surrounding the Subject Property. A reduced corner side yard setback for the proposed building is appropriate for the proposed development on the Subject Property.

Figure 18. Proposed Building Setbacks

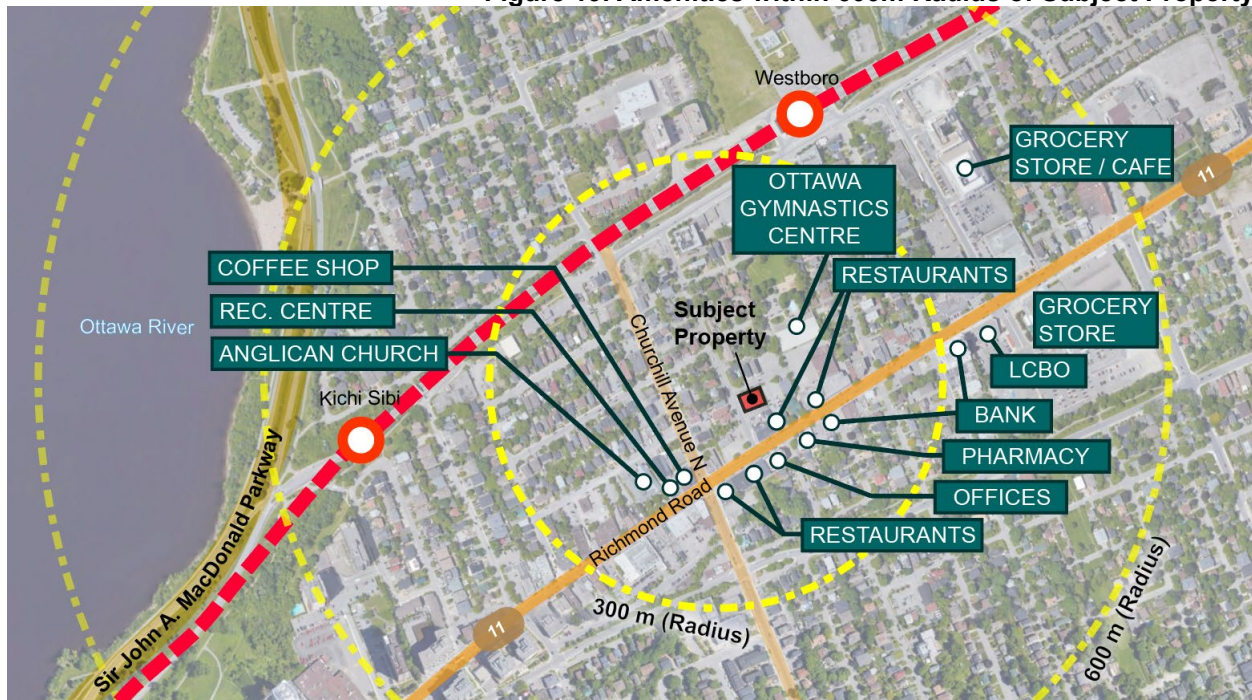


Section 101, Table 101, Row R15 of the Zoning By-law (Area “Y” – Schedule 1A) sets a minimum parking space rate of 0.5 spaces per dwelling unit for dwelling units in a mixed-use building after the first 12 units. 24 parking spaces are required for the proposed residential uses. Section 102, Table 102, Column II of the Zoning By-law (Area “Y” – Schedule 1A) sets a minimum visitor parking space rate of 0.1 spaces per dwelling unit after the first 12 units. Five parking spaces are required for visitor parking.

The combined minimum number of parking spaces required for the proposed development is 29 spaces. A total of five visitor parking spaces are provided. A total of 13 off-street parking spaces are provided within an underground garage. No parking spaces are required for the proposed ground floor retail and commercial uses.

The decision to provide a reduced number of resident parking spaces is in response to the proximity of the Subject Property to planned O-Train stations at Westboro and Kichi Sibi and bus stops along frequent transit routes at the intersection of Winona Avenue and Richmond Road. The location of the Subject Property is within 600 metres radius of the planned Westboro and Kichi Sibi O-Train stations, representing a walking distance of 10 minutes or less and within 150 metres walking distance of transit stops on bus route 11. The Subject Property is within a prime location to encourage the use of active transportation and transit options over use of private automobiles.

**Figure 19. Amenities within 600m Radius of Subject Property**



The Subject Property is within 600 metres radius of a broad range of land uses and amenities that contribute to development of 15-minute neighbourhoods and reduce the need for residents to use private automobiles to access daily needs. Specifically, the Subject Property is located within walking distance of retail stores, coffee shops, restaurants, fitness and recreational uses, banks, pharmacies, places of worship and grocery stores along Richmond Road (see Figure 19). A reduction to the total required residential parking spaces is appropriate for the proposed development on the Subject Property.

A total of five visitor spaces will be provided. Relief from the zoning provisions is required to reduce the number of resident parking spaces from 24 to 13, a reduction of 11 spaces. The proposed parking layout fully conforms with all other provisions of the zoning by-law.

Section 111, Table 111A, Row (b) of the Zoning By-law sets minimum bicycle parking space rates for the residential component of the proposed development. A rate of 0.5 bicycle parking spaces per dwelling unit is required for residential uses. The minimum number of bicycle parking spaces required for the proposed residential use is 30 spaces.

Section 111, Table 111A, Row (e) of the Zoning By-law sets minimum bicycle parking space rates for the commercial component of the proposed development. A rate of 1 bicycle parking space

per 250 square metres of gross floor area is required for retail and commercial uses. A total of two commercial units are proposed. The north commercial unit is approximately 120 square metres in gross floor area. The south commercial unit is approximately 100 square metres in gross floor area. The minimum number of bicycle parking spaces required for the proposed retail and commercial use is two spaces.

26 bicycle parking spaces are provided within the proposed building and 10 bicycle parking spaces are provided at the exterior of the building fronting along Winona Avenue. A total of 36 bicycle parking spaces are provided on the Subject Property as required by the Zoning By-law.

The provision of Section 107(1)(aa)(i) permits a maximum driveway width of 3.6 metres for a double traffic lane leading to less than 20 parking spaces for an apartment, mid-rise on the Subject Property. Relief from the zoning provision is required to increase the maximum permitted driveway width by 0.28 metres to 3.88 metres to facilitate the proposed development and provide access to the underground parking garage. A proposed increase of the driveway width by 0.28 metres is minor in nature and will not result in issues providing appropriate pedestrian access to the proposed building. The proposed zoning amendment will permit a minor increase to the maximum permitted width of the driveway leading to the underground parking garage. An increase to the maximum permitted width of the driveway is appropriate for the proposed development on the Subject Property.

To facilitate the proposed development of a mid-rise, mixed-use building on the Subject Property, this application requests a site-specific amendment to incorporate the required relief from the zoning provisions for a decrease the minimum required corner side yard setback along Winona Avenue, a decrease the minimum required parking spaces for the proposed development and to increase the maximum permitted driveway width to access the underground parking garage as highlighted in orange on the Zoning Review Table (see Table 1). The site-specific amendment will result in development of a six-storey, mixed-use building located with direct access and opportunities for residents to increase use of transit and active transportation infrastructure in the Westboro community. The proposed development is within proximity to restaurants, shops and outdoor amenity areas for residents. The requested relief from provisions of the TM zone is appropriate to facilitate the proposed development of a six-storey, mixed-use building on the Subject Property.

## **7.0 PUBLIC CONSULTATION STRATEGY**

### **Prior to Submission**

A formal pre-application consultation meeting was held with City staff and members of the Westboro Community Association in April 2022.

### **Upon Submission**

The public will be consulted with regarding the proposed development through the legislated public consultation requirements. This includes a signed posted on the site and the posting of the application on the City's 'DevApps' website. At this time, neighbours will have the opportunity to comment on the proposal.

Immediately following the filing of the application, an information meeting will be coordinated with the Councillor's office. If necessary, a second public meeting will take place to discuss this development application with the community.

### **Virtual Open House**

<b>Who:</b>	Residents of the community
<b>Where:</b>	The Open House may be held electronically via Zoom, subject to the necessary COVID-19 protocols.
<b>When:</b>	Soon after the City's circulation. This is to ensure that members of the public are aware of the project well in advance of any public meeting of Planning Committee.
<b>City rep.:</b>	The File Lead may wish to attend, depending on the level of interest from the public. This meeting will be coordinated with the Ward Councillor.
<b>Follow up:</b>	Attendees wishing to receive follow-up information may email Novatech's file lead or the City's file lead. The Project Team will do their best to keep interested citizens informed of significant changes and/or the final submission that will be heard at Planning Committee.
<b>Notes:</b>	At the time of the public meeting, COVID-19 health restrictions may allow for an in-person meeting. In this case, the meeting will be held during the week in the early evening. The location of the meeting will be shared as early as possible to ensure all interested members of the public can attend. Interested parties can request further information through the Project Team or their Community Association Primary Contact.

## **8.0 CONCLUSION**

This Planning Rationale has been prepared in support of Minor Zoning By-law Amendment and Site Plan Control applications to facilitate the development of a six-storey, mixed-use building on the Subject Property. The Subject Property is within the Inner Urban Transect and is designated Mainstreet Corridor on Schedule B2 of the 2021 Official Plan adopted by Council on November 24<sup>th</sup>, 2021. The Subject Property is within the Evolving Neighbourhood Overlay of the 2021 Official Plan. The Subject Property is located in the Westboro Village Planning Area Sector of the Richmond Road / Westboro Secondary Plan. The property is zoned Traditional Mainstreet (TM) and is located within the Mature Neighbourhoods Overlay in the City of Ottawa's Zoning By-law 2008-250.

The purpose of these applications is to facilitate the proposed development of a six-storey, mixed-use building through establishing site-specific zoning provisions for the Subject Property to decrease the minimum required corner side yard setback along Winona Avenue, decrease the minimum required off-street parking spaces for the proposed development and to increase the maximum permitted driveway width providing to access the underground parking garage. Approval of the proposed development will permit a total of 60 residential dwelling units and two commercial units on the Subject Property. A total of 13 off-street resident parking spaces and five visitor parking spaces are provided in an underground garage. A total of 36 bicycle parking spaces are proposed on the Subject Property.



The proposed mid-rise development is appropriate to support growth objectives and residential intensification targets within the urban area of the City of Ottawa. The proposed Minor Zoning By-law Amendment and Site Plan Control applications will have no negative impacts to natural heritage and features, natural resources, or cultural heritage resources. The requested Minor Zoning By-law Amendment and Site Plan Control applications are consistent with the policies of the Provincial Policy Statement.

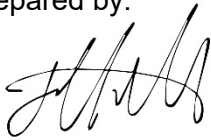
The Minor Zoning By-law Amendment and proposed development are consistent with the policies of the City of Ottawa Official Plan (2021) and the Richmond Road/Westboro Secondary Plan. The proposal is consistent with the Corridor designation and meets the City's intensification goals while maintaining a form that is context-sensitive and adds to the characteristics of the existing Westboro community. The proposed ground-floor commercial uses are consistent with the retail context of the Richmond Road corridor and contribute to the City's objectives to establish 15-minute neighbourhoods. The requested Minor Zoning By-law Amendment establishes appropriate zoning provisions for the proposed mixed-use building and permits development that is compatible with surrounding land uses.

The Minor Zoning By-law Amendment and Site Plan Control applications are appropriate for the development of the Subject Property and represent good land use planning.

Yours Truly,

**NOVATECH**

Prepared by:



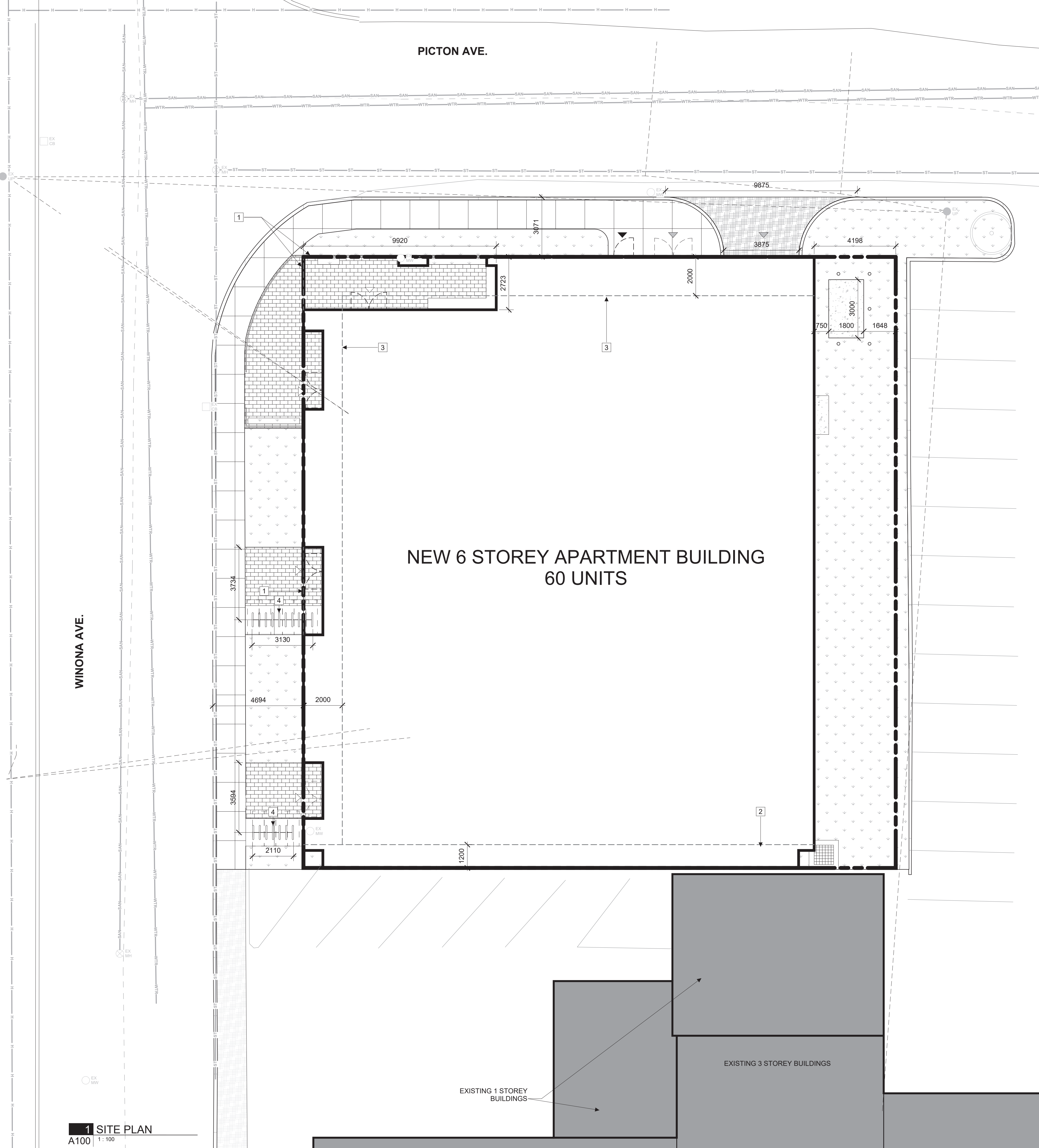
Jeffrey Kelly, MCIP, RPP  
Project Planner

Reviewed by:



Murray Chown, MCIP, RPP  
Director | Planning & Development

Appendix A:  
Site Plan



1 SITE PLAN  
A100 1:100

**LEGAL DESCRIPTION:**

PART 1 PLAN OF LOT 2 AND PART OF LOTS 1 AND 3 REGISTERED PLAN 183 CITY OF OTTAWA

**REFERENCE SURVEY:**

BASED ON INFORMATION FROM A SURVEY PREPARED BY ANNIS, O'SULLIVAN, VOLLEBEKK LTD. DATED AUGUST 25, 2022.

**MUNICIPAL ADDRESS:**

377 - 381 WINONA AVE. OTTAWA, ON

**DEVELOPMENT INFORMATION:**

SITE AREA	960 m <sup>2</sup>
BUILDING AREA	787 m <sup>2</sup>
GROSS FLOOR AREA	4,666 m <sup>2</sup>
BUILDING HEIGHT	19m

**ZONE**

SCHEDULE 1:	AREA B INNER URBAN
SCHEDULE 1A:	AREA Y INNER URBAN MAINSTREET
SCHEDULE 2:	FULLY WITHIN 600 m RADIUS

**UNIT MIX:**

TWO BEDROOM	18
ONE BEDROOM	18
STUDIO	24
<b>TOTAL</b>	<b>60</b>

**ZONING PROVISION**

	REQUIRED	PROVIDED
MIN. LOT WIDTH	No Minimum	29.9 m <sup>2</sup>
MIN. LOT AREA	No Minimum	961 m <sup>2</sup>
MAX. FRONT YARD SETBACK	2 m	0 m
MIN. REAR YARD SETBACK	No Minimum	0 m
MIN. INTERIOR YARD SETBACK	No Minimum	0 m

**MAX. HEIGHT**

20 m / 6 storeys	19.00 m
Additional setback of 2 meters where building greater than 4 storeys	Additional 2 m setback provided above 4 <sup>th</sup> storey

**AMENITY AREA**

360 m <sup>2</sup> (6 m <sup>2</sup> per dwelling unit)	360 m <sup>2</sup>
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**PARKING QUEING + LOADING**

	REQUIRED	PROVIDED
RESIDENTIAL SPACES	13	13
VISITOR SPACES	5	5
ACCESSIBLE PARKING	1 - Type A & B	1 - Type A & B
BICYCLE PARKING	32	36

**SITE PLAN GENERAL NOTES:**

- ALL GENERAL SITE INFORMATION AND CONDITIONS COMPILED FROM EXISTING PLANS AND SURVEYS
- DO NOT SCALE THIS DRAWING
- REPORT ANY DISCREPANCIES PRIOR TO COMMENCING WORK. NO RESPONSIBILITY IS BORN BY THE CONSULTANT FOR UNKNOWN SUBSURFACE CONDITIONS
- CONTRACTOR TO CHECK AND VERIFY ALL DIMENSIONS ON SITE AND REPORT ANY ERRORS AND/OR OMISSIONS TO THE CONSULTANT
- REINSTATE ALL AREAS AND ITEMS DAMAGED AS A RESULT OF CONSTRUCTION ACTIVITIES TO THE SATISFACTION OF THE CONSULTANT
- CONTRACTOR TO LAYOUT PLANTING BEDS, PATHWAYS ETC. TO APPROVAL OF CONSULTANT PRIOR TO ANY JOB EXCAVATION
- THE ACCURACY OF THE POSITION OF UTILITIES IS NOT GUARANTEED - CONTRACTOR TO VERIFY PRIOR TO EXCAVATION
- INDIVIDUAL UTILITY COMPANY MUST BE CONTACTED FOR CONFIRMATION OF UTILITY EXISTENCE AND LOCATION PRIOR TO DIGGING
- ALL DISTURBED AREAS TO BE RESTORED TO ORIGINAL CONDITION OR BETTER UNLESS OTHERWISE NOTED

**SITE PLAN KEYNOTES:**

- BUILDING OVERHANG ABOVE
- BUILDING SETBACK FLOOR 2 - 6
- BUILDING SETBACK FLOOR 5 + 6
- BIKE RACKS

**SITE PLAN LEGEND:**

- EXISTING BUILDING
- ASPHALT PAVING
- NEW LANDSCAPING, REFER TO LANDSCAPING PLAN
- CONCRETE SIDEWALK
- PAVERS
- CONCRETE PAD
- VENTILATION GRILL
- SERVICE DOORS
- BUILDING MAIN ENTRANCE
- EMERGENCY EXIT
- PROPERTY LINE
- OVERHEAD WIRES
- EDGE OF ASPHALT
- EXISTING EDGE OF ASPHALT TO BE REVISED
- ROAD CENTERLINE
- EXISTING DOMESTIC WATER
- EXISTING SANITARY
- EXISTING STORM
- EXISTING GAS
- EXISTING CATCH BASIN
- EXISTING FIRE HYDRANT
- EXISTING MANHOLE
- EXISTING UTILITY POLE
- EXISTING MONITORING WELL
- NEW TREE
- EXISTING TREE
- PLANTER
- BICYCLE PARKING

**CSV ARCHITECTS**

sustainable design - conception écologique  
613.564.8118 190 O'Connor Street, Suite 100  
www.csv.ca Ottawa, Ontario  
K1P 2P3

**PLANNER & LANDSCAPE**  
Novatech  
240 Michael Cowpland Drive, Suite 200  
Ottawa, Ontario  
613-254-9643  
novatech-eng.com

**CIVIL ENGINEER**  
McIntosh Perry  
115 Walgreen Road R.R. 3  
Carp, Ontario  
613-714-4621  
www.mcintoshperry.com



**STAMP**

1	2022.10.13	ISSUED FOR SPC
REV DATE		ISSUE

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- ALL WORK SHALL BE IN ACCORDANCE WITH THE ONTARIO BUILDING CODE AND ALL SUPPLEMENTS AND APPLICABLE MUNICIPAL REGULATIONS.

**CLIENT**

**AZURE**

OTTAWA  
ONTARIO, CANADA

**PROJECT**

**WINONA PH DEVELOPMENT**

377 -381 WINONA AVE. OTTAWA,  
ON

**TITLE**

**SITE PLAN**

PROJECT NO: 2022-1290  
DRAWN: DF  
APPROVED: DH  
SCALE: As indicated  
DATE PRINTED: 2022-10-26 3:19:19 PM

REV DRAWING NO.

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