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522 Lower Byron Avenue

Planning Rationale and Design Brief



**522 Lower Byron Avenue
Ottawa, Ontario**

**Planning Rationale and Design Brief
in support of
Minor Zoning By-law Amendment
and Site Plan Control Applications**

Prepared By:

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July / 7 / 2022

Novatech File: 121097
Ref: R-2022-113

July 7, 2022

City of Ottawa
Planning Infrastructure and Economic Development Department
110 Laurier Avenue West, 4th Floor
Ottawa, ON, K1P 1J1

Attention: Steve Gauthier, Planner II, Development Review (Central Area)

Dear Mr. Gauthier:

**Reference: Minor Zoning By-law Amendment and Site Plan Control Applications
522 Lower Byron Avenue
Our File No.: 121097**

The following Planning Rationale and Design Brief have been prepared in support of Minor Zoning By-law Amendment and Site Plan Control applications for the lands located at 522 Lower Byron Avenue (the "Subject Property").

The Subject Property is designated General Urban Area on Schedule B of the in-force City of Ottawa Official Plan. The Subject Property is designated Neighbourhood and Evolving Neighbourhood Overlay in the Inner Urban Transect on Schedule B2 of the adopted City of Ottawa Official Plan (November 2021). The property is zoned Residential Fourth Density, Subzone UC, Urban Exception 2685 (R4UC [2685]) in the City of Ottawa's Zoning By-law 2008-250. The Subject Property is located within the boundary of Schedule 430 of the Zoning By-law and is subject to provisions of the Westboro Development Overlay.

It is proposed to develop a three-storey residential apartment building with 18 dwelling units on the Subject Property. A minor rezoning application is required to provide relief from zoning provisions on the Subject Property for a decrease to the minimum required rear yard setback, a decrease to the minimum required rear yard area, an increase to the maximum permitted balcony projection above the first floor into the rear yard, a decrease to the minimum required resident parking spaces and a decrease to the minimum required visitor parking spaces. No off-street motor vehicle parking is provided on the Subject Property. The proposed development will require demolition of existing buildings, modifications to site grading, drainage and landscaping on the property. No significant impacts to traffic or existing land uses surrounding the Subject Property are anticipated as a result of the proposed development. A concurrent site plan control application is being filed as part of the submission package to facilitate development on the Subject Property.

This Planning Rationale examines the location and context of the Subject Property, the planning policy and regulatory framework applicable to the site and makes recommendations on the Minor Zoning By-law Amendment and Site Plan Control applications required to facilitate the proposed development on the Subject Property.

Should you have any questions regarding any aspect of this application please do not hesitate to contact me at your earliest convenience.

Yours truly,

NOVATECH



Jeffrey Kelly, MCIP RPP
Project Planner

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1.0 INTRODUCTION

Novatech has prepared this Planning Rationale in support of Minor Zoning By-law Amendment and Site Plan Control applications to permit development of a low-rise apartment building on the property municipally known as 522 Lower Byron Avenue (the “Subject Property”). The proposed development comprises a three-storey residential apartment building with 18 dwelling units on the Subject Property. A walkway from Lower Byron Avenue is proposed along the front and east property lines to facilitate pedestrian and bicycle access off Lower Byron Avenue to building entrances and bicycle parking located within the building. No off-street motor vehicle parking is provided on the Subject Property. The proposed development will require demolition of existing buildings, modifications to site grading, drainage and landscaping on the property.

The Subject Property is designated General Urban Area on Schedule B of the in-force City of Ottawa Official Plan. The Subject Property is designated Neighbourhood and Evolving Neighbourhood Overlay in the Inner Urban Transect on Schedule B2 of the adopted City of Ottawa Official Plan (November 2021). The property is zoned Residential Fourth Density, Subzone UC, Urban Exception 2685 (R4UC [2685]) in the City of Ottawa’s Zoning By-law 2008-250. The Subject Property is located within the boundary of Schedule 430 of the Zoning By-law and is subject to provisions of the Westboro Development Overlay.

This Planning Rationale will demonstrate that the Minor Zoning By-law Amendment and Site Plan Control applications will:

- Be consistent with the Provincial Policy Statement (2020);
- Conform to the policies of the in-force City of Ottawa Official Plan (up to and including Official Plan Amendment 243);
- Conform to the policies of the adopted City of Ottawa Official Plan (November 2021);
- Establish appropriate Zoning standards for the Subject Property; and
- Maintain compatibility with surrounding development and community characteristics

1.1 Description of Subject Property

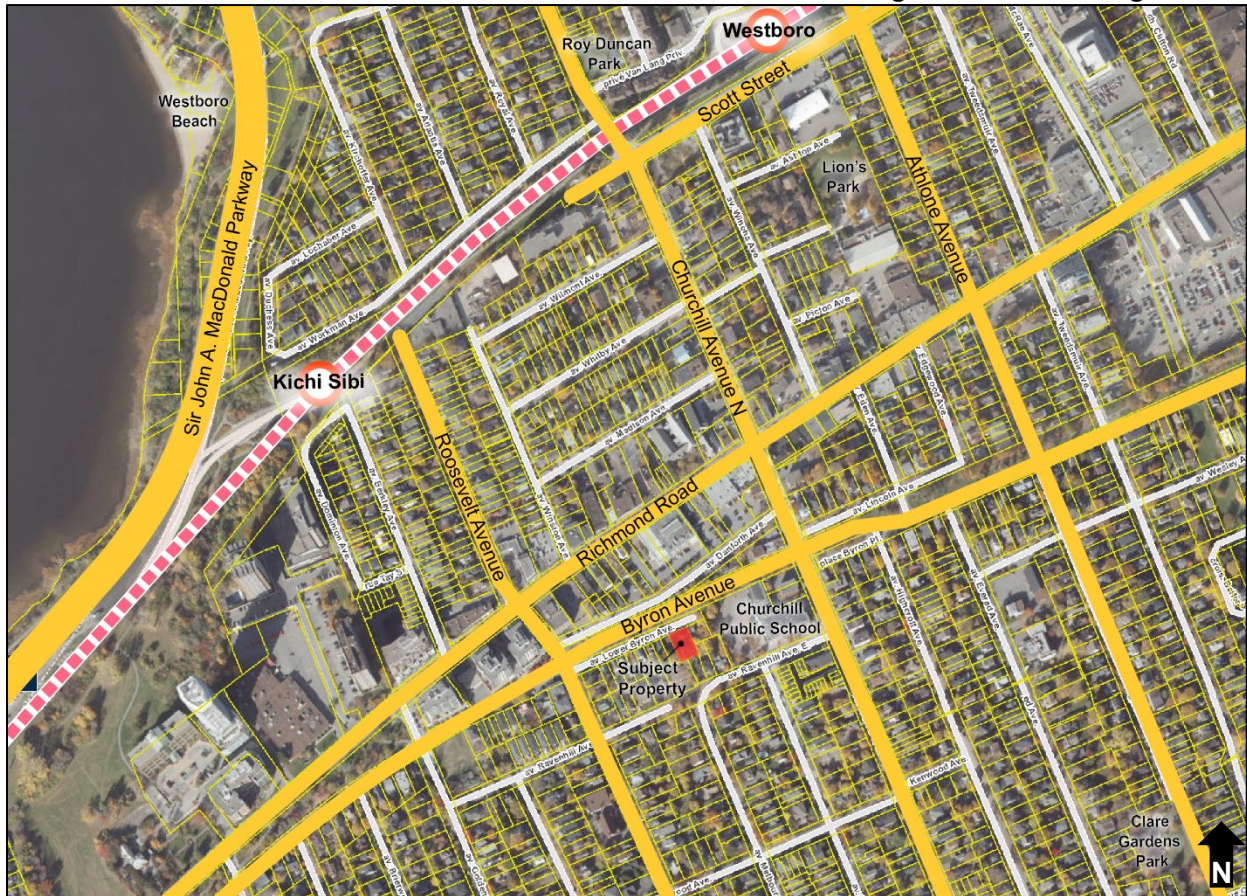
The Subject Property is located in Ward 15 (Kitchissippi) of the City of Ottawa. The Subject Property is located in an area bounded by Byron Avenue to the north, Ravenhill Avenue to the south, Roosevelt Avenue to the west, and Churchill Avenue North to the east (See Figure 1). Lower Byron Avenue does not have a direct street connection to the east of the Subject Property. The Subject Property is currently occupied by a low-rise residential building. The Subject Property has approximately 20 metres of frontage along Lower Byron Avenue and an approximate area of 600 square metres.

Figure 1. Aerial Photo of Subject Property



1.2 Site Location and Community Context

Figure 2. Surrounding Context



The surrounding context of the Subject Property to the east, west and south side along Lower Byron Avenue, Roosevelt Avenue and Ravenhill Avenue is predominantly characterized by a mix of low-rise residential uses. Further to the east of the Subject Property is Churchill Public School. To the north across Byron Avenue are a mix of low-rise retail and commercial uses, restaurants and mid-rise residential uses along the Richmond Road corridor. Further to the north of the Subject Property across Richmond Road are low-rise residential uses and the planned Westboro O-Train station. Further to the west of the Subject Property across Richmond Road are low to high-rise residential uses, the planned Kichi Sibi O-Train station and the Ottawa River (see Figure 2).

Additional details are provided in Figures 3 to 6 and in the descriptions below

Figure 3. Land uses north of Subject Property



North: Immediately north of the Subject Property is Byron Avenue. Further to the north is a mix of retail, commercial and residential uses along Richmond Road.

Figure 4. Land uses south of Subject Property



South: Immediately south of the Subject Property are low rise residential uses situated at the top of a slope along Ravenhill Avenue.

Figure 5. Land uses east of Subject Property



East: Immediately east of the Subject Property is a low-rise residential building, the end of Lower Byron Avenue and a retaining wall at the base of a slope.

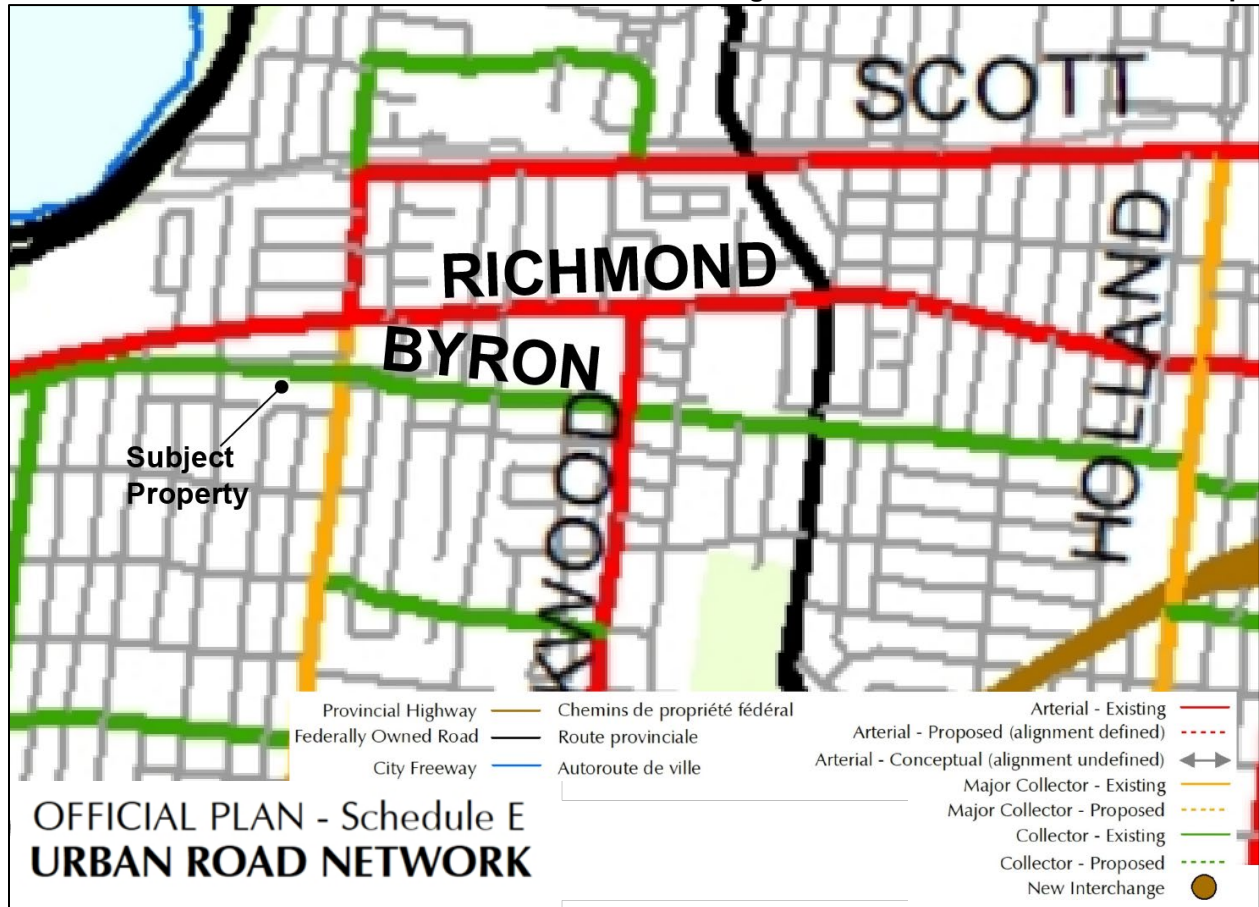
Figure 6. Land uses west of Subject Property



West: Immediately west of the Subject Property are low-rise residential uses.

1.3 Linkages and Transportation Framework

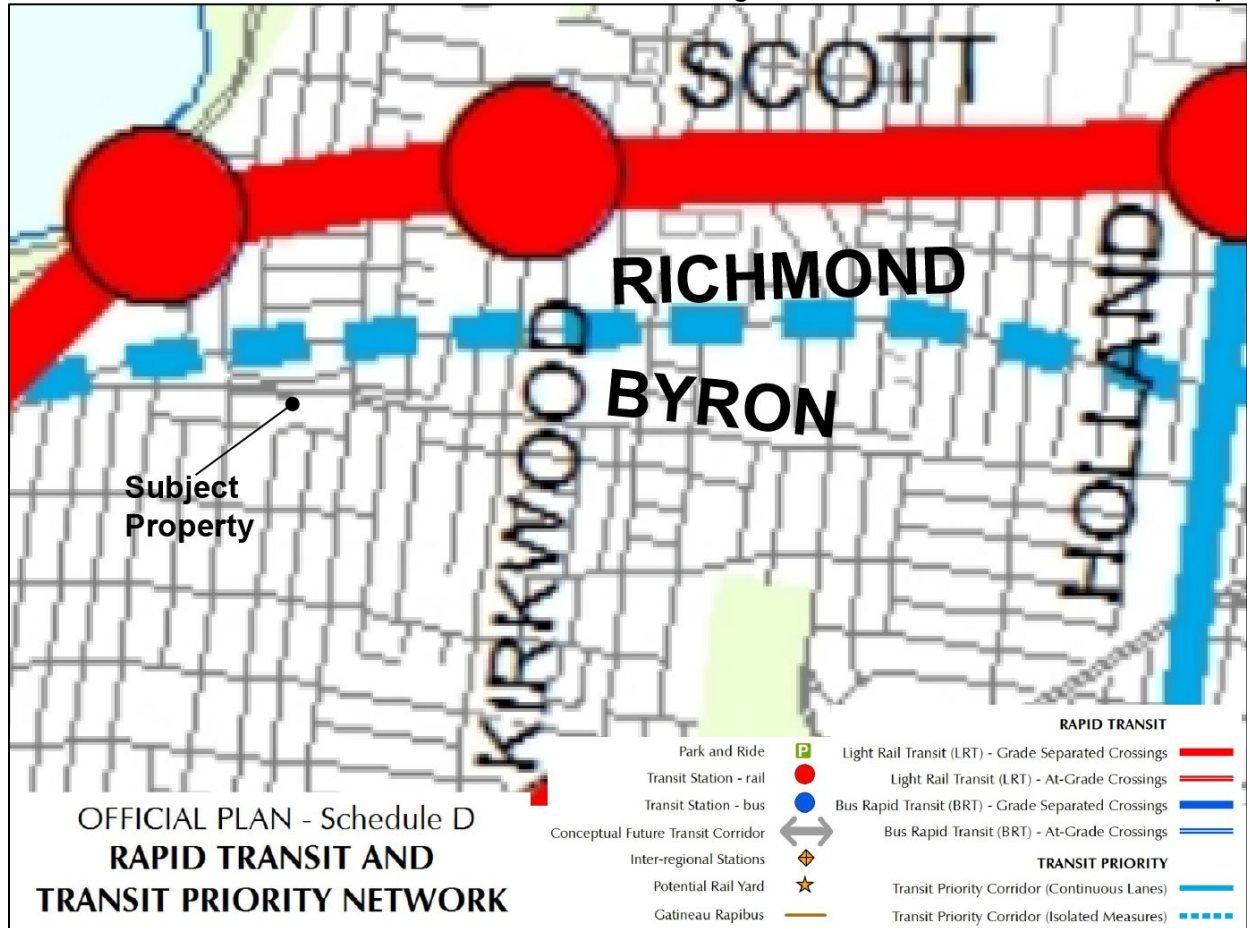
Figure 7. Official Plan Schedule E Excerpt



The Subject Property has frontage on Lower Byron Avenue. The Subject Property is located east of the intersection of Lower Byron Avenue and Roosevelt Avenue and south of Byron Avenue and Richmond Road. Byron Avenue is designated as an existing collector and Richmond Road is designated as an existing arterial road on Schedule E of the in-force Official Plan (see Figure 7).

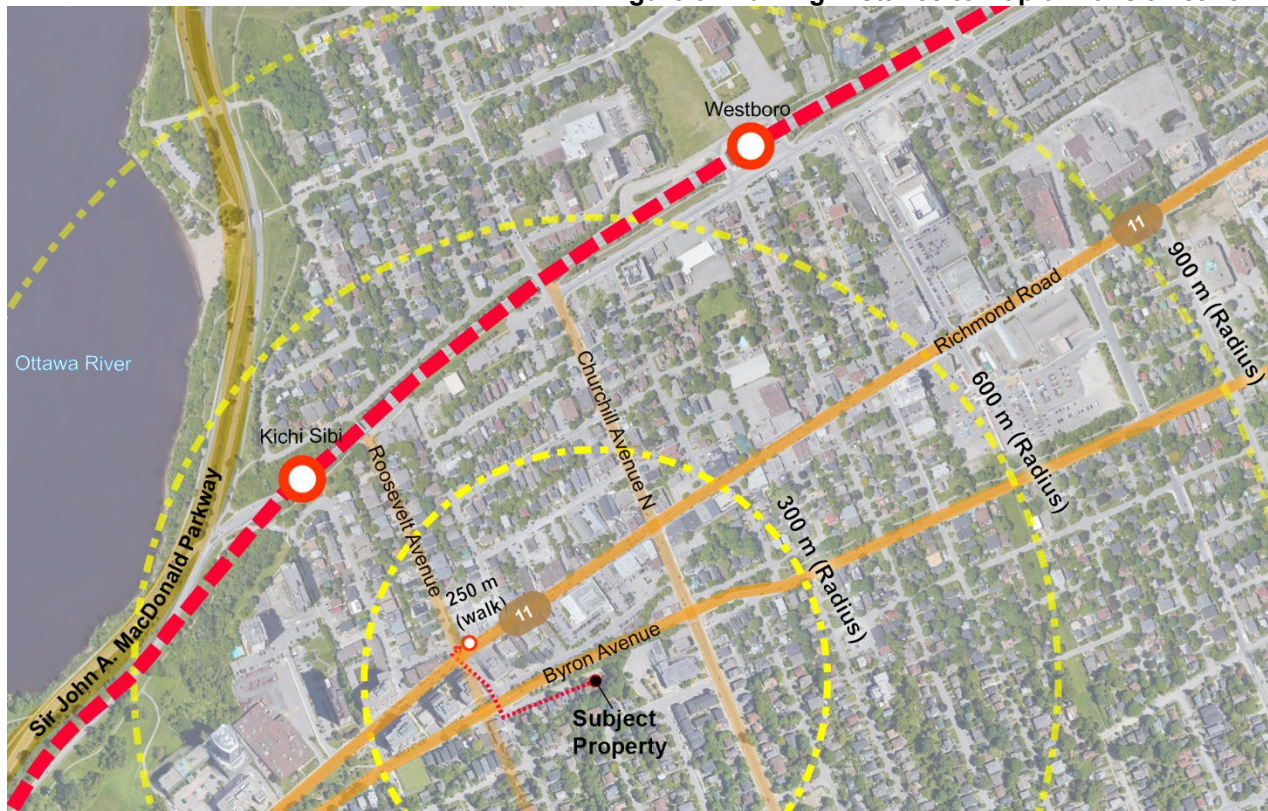
No off-street motor vehicle parking spaces are proposed and no vehicular access is provided to the Subject Property. There is no public sidewalk along Lower Byron Avenue. A walkway from Lower Byron Avenue is provided along the building frontage and the east property line to facilitate pedestrian and bicycle access off Lower Byron Avenue to internal bicycle parking and the rear yard.

Figure 8. Official Plan Schedule D Excerpt



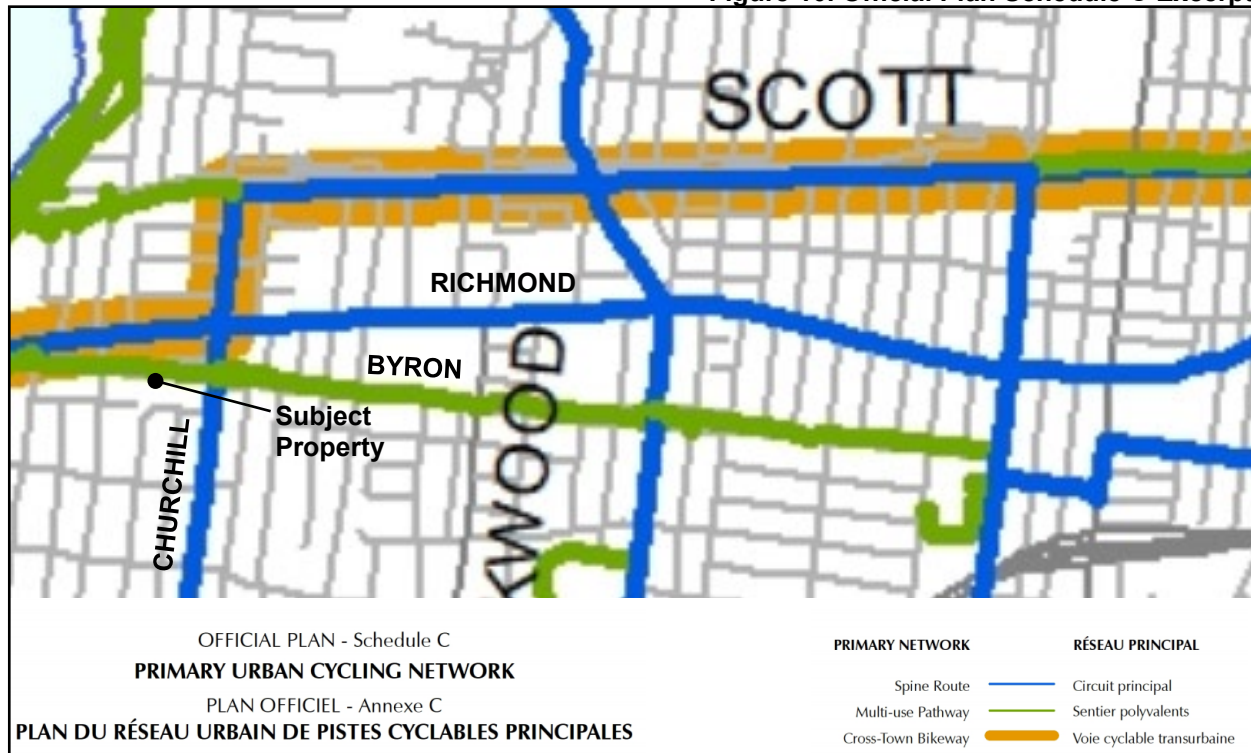
Richmond Road is located to the north of the Subject Property and is designated Transit Priority Corridor (Isolated Measures) on Schedule D of the in-force Official Plan (see Figure 8).

Figure 9. Walking Distance to Rapid Transit Network



The Subject Property is located within 600 metres radius of the planned Kichi Sibi O-Train station and within 900 metres radius of the planned Westboro O-Train station. Several bus transit route options are available in the immediate area. Bus route 50 services the Subject Property with a bus stop provided on the east and west side of Churchill Avenue North between Ravenhill Avenue and Byron Avenue. Bus route 11 services the Subject Property with bus stops provided on the north and south sides of Richmond Road. Bus route 11 is designated as a frequent route offering high frequency bus service along major roads on the OC Transpo network (see Figure 9).

Figure 10. Official Plan Schedule C Excerpt



Churchill Avenue North is designated as a spine route and Cross-Town Bikeway on Schedule C of the in-force Official Plan. Richmond Road is designated as a spine route and a Cross-Town Bikeway west of Churchill Avenue North on Schedule C of the in-force Official Plan. Byron Avenue is designated as a Multi-Use Pathway on Schedule C of the in-force Official Plan. These routes provide access to the City of Ottawa’s active transportation network in proximity to the Subject Property (see Figure 10).

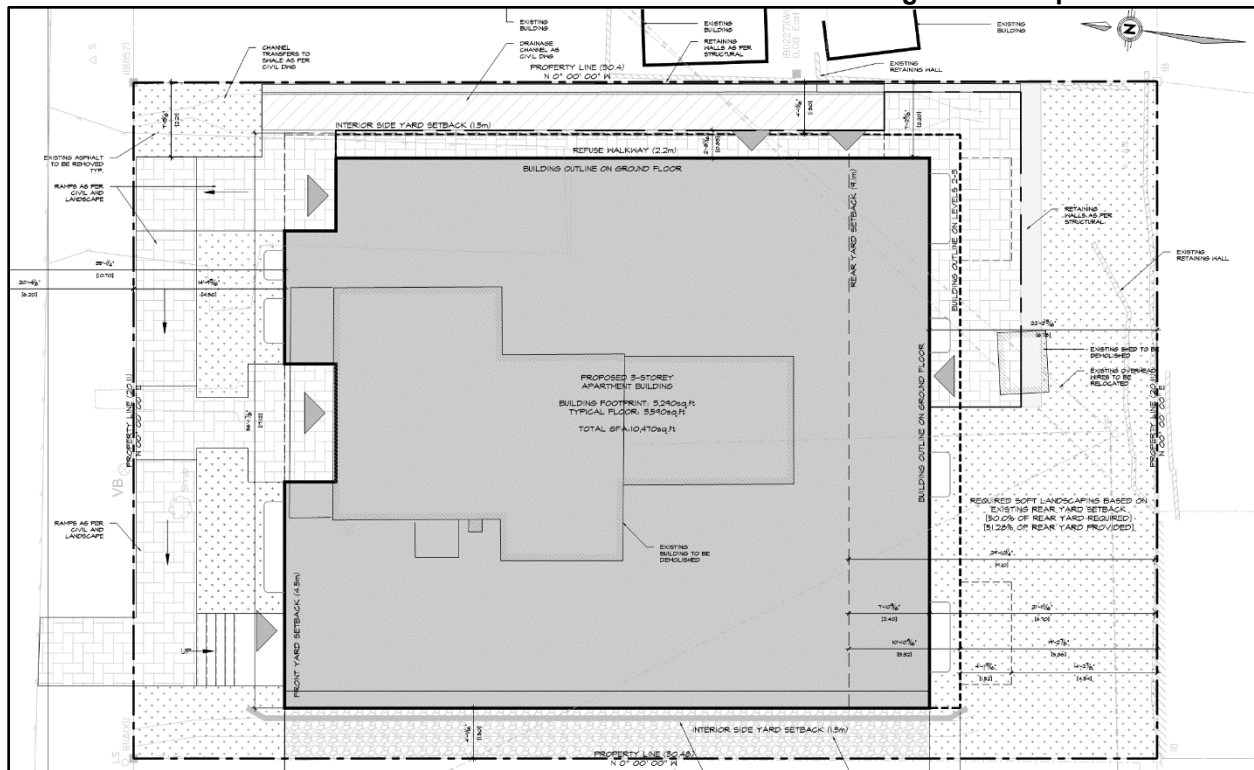
2.0 PROPOSED DEVELOPMENT

It is proposed to develop a three-storey residential apartment building containing 18 dwelling units on the Subject Property. No off-street motor vehicle parking spaces are proposed.

A Minor Zoning By-law Amendment application is required to facilitate the proposed development on the Subject Property. Site-specific amendments for the Subject Property are required to provide relief from zoning provisions for a decrease to the minimum required rear yard setback, a decrease to the minimum required rear yard area, an increase to the maximum permitted balcony projection above the first floor into the rear yard, a decrease to the minimum required resident parking spaces and a decrease to the minimum required visitor parking spaces.

A detailed Site Plan Control application is required to facilitate development on the Subject Property and is being filed as part of this submission. The proposed development will require demolition of the existing building, modifications to site grading, drainage and landscaping. The proposed site plan is attached as Appendix A (see Figure 11).

Figure 11. Proposed Site Plan



3.0 PLANNING POLICY AND REGULATORY FRAMEWORK

3.1 Provincial Policy Statement

The 2020 Provincial Policy Statement (PPS) provides policy direction on land use planning and development matters of provincial interest. The PPS was issued under the authority of Section 3 of the Planning Act and came into effect on May 1, 2020. All decisions affecting planning matters “shall be consistent with” policies issued under Section 3 of the Planning Act.

Section 1.3 of the PPS sets out policies for settlement areas. Policy 1.1.3.2 states:

“Land use patterns within settlement areas shall be based on densities and a mix of land uses which:

- a) efficiently use land and resources;*
- b) are appropriate for, and efficiently use, the infrastructure and public service facilities which are planned or available, and avoid the need for their unjustified and/or uneconomical expansion;*
- c) minimize negative impacts to air quality and climate change, and promote energy efficiency;*
- d) prepare for the impacts of a changing climate;*
- e) support active transportation;*
- f) are transit-supportive, where transit is planned, exists or may be developed;*
and
- g) are freight-supportive.”*

The Minor Zoning By-law Amendment and Site Plan Control applications will facilitate the development of a low-rise apartment building on the Subject Property. The proposed development is consistent with Policy 1.1.3.2. as it represents an efficient use of an existing fully serviced property and existing municipal infrastructure. The property is within proximity of a designated spine route along Churchill Avenue North and Richmond Road and a Multi-Use Pathway along Byron Avenue and promotes active transportation opportunities for residents. The Subject Property is located within 600 metres walking distance of the planned Kichi Sibi O-Train station to the north and within 300 metres walking distance of transit stops located along route 11 of the frequent transit network. The proposed development is transit supportive.

Policy 1.1.3.3 states:

“Planning authorities shall identify appropriate locations and promote opportunities for transit-supportive development, accommodating a significant supply and range of housing options through intensification and redevelopment where this can be accommodated taking into account existing building stock or areas, including brownfield sites, and the availability of suitable existing or planned infrastructure and public service facilities required to accommodate projected needs.”

The Subject Property is underutilized and can accommodate intensification and redevelopment. The proposed development is situated in an appropriate location to establish transit-supportive development. The proposed low-rise apartment building on the Subject Property will increase the City’s housing supply and provide a range of housing options within proximity to the planned Kichi Sibi O-Train station, existing active transportation routes and transit stops located along the frequent transit network.

Section 1.4 of the PPS sets out policies for housing. Policy 1.4.3 states:

“Planning authorities shall provide for an appropriate range and mix of housing options and densities to meet projected market-based and affordable housing needs of current and future residents of the regional market area by:

b) permitting and facilitating:

- 1. all housing options required to meet the social, health, economic and well-being requirements of current and future residents, including special needs requirements and needs arising from demographic changes and employment opportunities; and*
- 2. all types of residential intensification, including additional residential units, and redevelopment in accordance with policy 1.1.3.3;*

c) directing the development of new housing towards locations where appropriate levels of infrastructure and public service facilities are or will be available to support current and projected needs;

d) promoting densities for new housing which efficiently use land, resources, infrastructure and public service facilities, and support the use of active transportation and transit in areas where it exists or is to be developed;

e) requiring transit-supportive development and prioritizing intensification, including potential air rights development, in proximity to transit, including corridors and stations;

The proposed low-rise apartment building on the Subject Property is an example of residential intensification directed to an area where appropriate levels of infrastructure and public service facilities exist to support the needs of the community. The proposed development will make efficient use of underutilized land and be supportive of active transportation and transit in an area where suitable infrastructure is in place. The Subject Property is located in an area where low-rise intensification is appropriate and supports the use of transit in proximity to the planned Kichi Sibi O-Train station, existing active transportation corridors and within walking distance of bus stops along the frequent transit network.

Section 1.6.6 of the PPS sets out policies for Sewer, Water and Stormwater. Policy 1.6.6.2 of the PPS states: “Within settlement areas with existing municipal sewage services and municipal water services, intensification and redevelopment shall be promoted wherever feasible to optimize the use of the services.” The proposed development on the Subject Property represents intensification from the current use. The proposed development of a low-rise apartment building has been designed to align with and optimize existing municipal infrastructure capacity and avoid the requirement for servicing upgrades on the Subject Property.

Section 1.6.7 of the PPS sets out policies for transportation systems. Policy 1.6.7.4 states: “A land use pattern, density and mix of uses should be promoted that minimize the length and number of vehicle trips and support current and future use of transit and active transportation.” The proposed development of a low-rise apartment building with no off-street motor vehicle parking on the Subject Property will introduce a level of density that promotes the viability of active transportation and transit use and may contribute to minimizing the required length and number of vehicle trips for residents.

Section 1.6.8 of the PPS sets out policies for transportation and infrastructure corridors. Policy 1.6.8.3 states: “New development proposed on adjacent lands to existing or planned corridors and transportation facilities should be compatible with, and supportive of, the long-term purposes of the corridor and should be designed to avoid, mitigate or minimize negative impacts on and from the corridor and transportation facilities.” The Subject Property is located south of Richmond Road and west of Churchill Avenue North. Churchill Avenue North is designated as a spine route and Cross-Town Bikeway on Schedule C of the in-force Official Plan. Richmond Road is designated as a spine route and a Cross-Town Bikeway west of Churchill Avenue North on Schedule C of the in-force Official Plan. Byron Avenue is designated as a Multi-Use Pathway on Schedule C of the in-force Official Plan. Transit stops along route 11 of the frequent transit network are within walking distance of the Subject Property. The proposed development of a low-rise apartment building on the Subject Property will be compatible with the existing use of the Richmond Road corridor, will be transit-supportive and designed to create no negative impacts on the existing or planned function of transportation corridors in the area.

The Minor Zoning By-law Amendment and Site Plan Control applications are consistent with the policies of the Provincial Policy Statement.

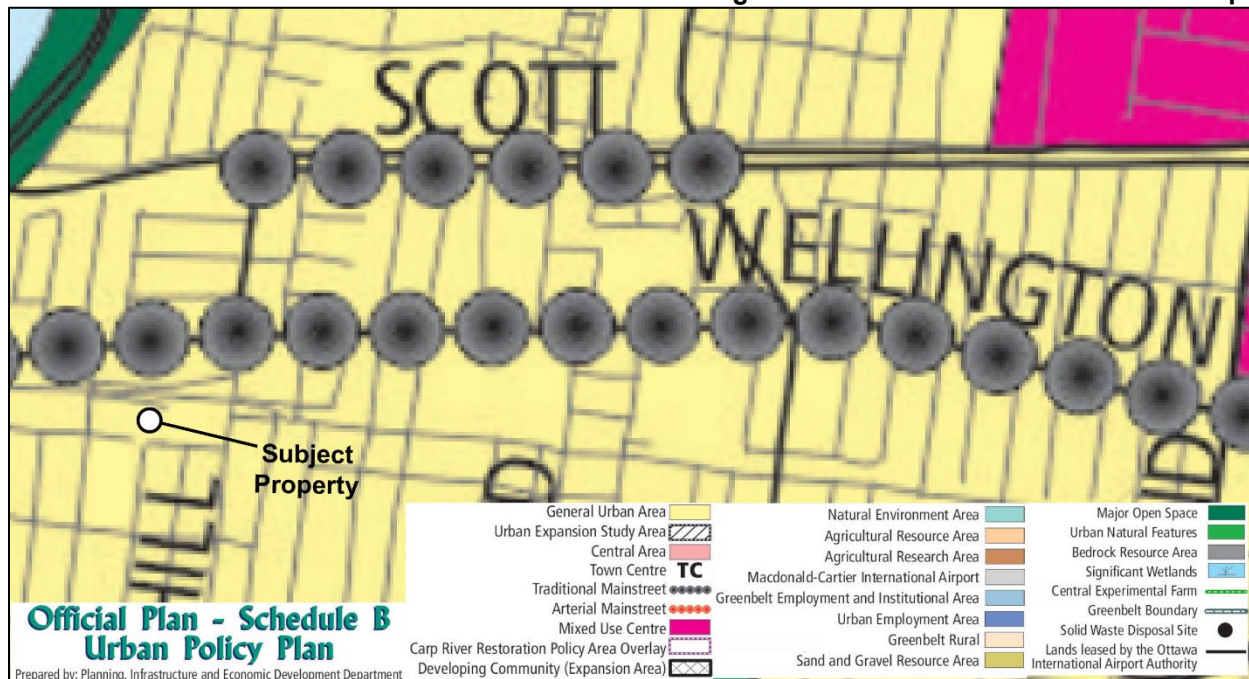
3.2 City of Ottawa Official Plan

The new City of Ottawa Official Plan (“the adopted Official Plan”) was adopted by Ottawa City Council on November 24, 2021. It is noted that at the time this Planning Rationale was prepared, the adopted Official Plan has not received approval from the Province of Ontario’s Ministry of Municipal Affairs and Housing. This Planning Rationale will address the existing in-force Official Plan and the adopted Official Plan regarding the proposed development per *Official Plan Document 6 – Transition of In-Stream Applications*.

3.3 In-force City of Ottawa Official Plan

The in-force City of Ottawa Official Plan was adopted by City Council on May 14, 2003 and modified by the Minister of Municipal Affairs on November 10, 2003. There have been numerous modifications and amendments approved by City Council and former Ontario Municipal Board. For the purposes of this Planning Rationale, the Official Plan Consolidation up to and including Official Plan Amendment No. 243 was used for reference.

Figure 12. Official Plan Schedule B Excerpt



3.3.1 Land Use Designation

The Subject Property is designated General Urban Area on Schedule B of the in-force City of Ottawa Official Plan (see Figure 12).

Section 3.6.1, Policy 1 of the in-force Official Plan states that the *“General Urban Area designation permits many types and densities of housing, as well as employment, retail uses, service, industrial, cultural, leisure, greenspace, entertainment and institutional uses.”*

Policies of the in-force Official Plan are supportive of increased densities through intensification and redevelopment. The proposed development of a low-rise apartment building is a permitted use on the Subject Property.

Section 3.6.1, Policy 3 of the in-force Official Plan states that: *“Building height in the General Urban Area will continue to be predominantly Low-Rise”* and *“changes in building form, height and density will be evaluated based on compatibility with the existing context and the planned function of the area”*. “Low-Rise” is classified in Figure 2.4 of the in-force Official Plan as buildings having a maximum height of four storeys or less. The proposed development of a low-rise apartment building is permitted on the Subject Property.

Section 3.6.1, Policy 5 of the in-force Official Plan states that:

“The City supports intensification in the General Urban Area where it will complement the existing pattern and scale of development and planned function of the area. The predominant form of development and intensification will be semi-detached and other ground-oriented multiple unit housing. When considering a proposal for residential intensification through infill or redevelopment in the General Urban Area, the City will:

- a. Assess the compatibility of new development as it relates to existing community character so that it enhances and builds upon desirable established patterns of built form and open spaces;*
- b. Consider its contribution to the maintenance and achievement of a balance of housing types and tenures to provide a full range of housing for a variety of demographic profiles throughout the General Urban Area;”*

Policies of the in-force Official Plan are supportive of intensification and redevelopment where it complements the existing context and planned function of the area. The proposed development of a low-rise apartment building represents appropriate intensification of the Subject Property with a built form that is compatible with the surrounding site context. The proposed low-rise apartment building will fit within the low-rise, residential character of the immediate area, will contribute to expanding the range of available housing types and will create opportunities for a broader variety of residents to locate within the community.

3.3.2 Managing Growth

Section 2.2 of the in-force Official Plan sets out policies to direct growth to target areas for intensification. The majority of growth is directed within the urban boundary where services are available or can be easily provided for new development to accommodate the creation of jobs, housing and increased transit use.

Section 2.2.2, Policy 23 of the in-force Official Plan states: *“The City supports intensification in the General Urban Area where it will enhance and complement its desirable characteristics and long-term renewal. Generally, new development, including redevelopment, proposed within the interior of established neighbourhoods will be designed to complement the area’s desirable character reflected in the pattern of built form and open spaces.”*

The Subject Property is located within an area designated as General Urban Area on Schedule B of the in-force Official Plan. Policies of the in-force Official Plan are supportive of low-rise

intensification of properties within the interior of established residential neighbourhoods provided that it is complementary to the scale and characteristics of the existing site context. The proposed development of a three-storey apartment building is compatible with the existing low-rise context along Lower Byron Avenue and is consistent with the growth objectives of the in-force Official Plan.

3.3.3 Urban Design and Compatibility

Section 2.5.1 of the in-force Official Plan states: *“compatible development means development that, although it is not necessarily the same as or similar to existing buildings in the vicinity, can enhance an established community through good design and innovation and coexists with existing development without causing undue adverse impact on surrounding properties. It ‘fits well’ within its physical context and ‘works well’ with the existing and planned function. Generally speaking, the more a new development can incorporate the common characteristics of its setting in the design, the more compatible it will be. Nevertheless, a development can be designed to fit and work well in a certain existing context without being ‘the same as’ the existing development.”*

The proposed development is designed to be compatible with the surrounding Westboro community, the planned function of development along Lower Byron Avenue and the Richmond Road corridor to the north. The proposed development is designed to incorporate a variety of high-quality materials, balconies and glazing along the building façade fronting on Lower Byron Avenue that are well-articulated and appropriate for the existing neighbourhood context. Private entrances to the building along Lower Byron Avenue are intended to soften the interface between semi-private space and the public realm while establishing a welcoming and active street frontage for building residents and members of the public (see Figure 13).

Figure 13. Proposed Elevation along Lower Byron Avenue



The Subject Property is located along a major street within the recently approved Westboro Development Overlay and is zoned to permit a low-rise apartment dwelling. The proposed development represents appropriate intensification planned along Lower Byron Avenue and the surrounding area. The proposed development will contribute to residential intensification within the urban area and establish transit-supportive densities within walking distance of the Kichi Sibi and Westboro O-Train stations, and to bus transit stops on the frequent transit network to the north of the Subject Property along Richmond Road. Development of a low-rise apartment building on the Subject Property is appropriate for the existing site and surrounding context and will not cause undue adverse impacts on abutting properties.

3.3.4 *Designing Ottawa*

The design objectives are broadly applied to all plans and development in the City of Ottawa and relevant objectives are addressed below.

<p>To enhance the sense of community by creating and maintaining places with their own distinct identity.</p>	<p>The proposed development is designed to complement the existing context by presenting a high-quality, architectural design with appropriate built form, materials and glazing that frames the public realm along Lower Byron Avenue and enhances the existing character of the surrounding community.</p>
<p>To define quality public and private spaces through development.</p>	<p>The proposed development is designed to establish high-quality public and semi-private spaces that fit well with the interface along the Lower Byron Avenue frontage, are compatible with the surrounding community and will provide a thoughtful transition to the existing scale of development surrounding the Subject Property. The front entrance is designed to be visually dominant on the front façade in order to orient and welcome residents and visitors. This design also informs the public feel of the steps and walkway toward the front entrance. The entrance feature and interior entrance lobby are designed with appropriate, well-proportioned glazing that is recessed from the façade to provide shelter. Dedicated entrances at grade provide semi-private spaces for dwelling units along the Lower Byron Avenue frontage to create an animated street edge that transitions between exterior public space and interior private space.</p>
<p>To create places that are safe, accessible and are easy to get to, and move through.</p>	<p>Pedestrian access to the Subject Property is provided via a walkway connecting the primary entrance to Lower Byron Avenue. Amenity space is provided in the rear yard and on the roof for building residents. Designated spine routes are located further to the north of the Subject Property west of Churchill Avenue North along Richmond Road providing access to the larger active transportation network across the City. Access to the planned Kichi Sibi O-train station is within 600 metres walking distance of the Subject Property. No off-street motor vehicle parking is provided on the Subject Property. Secure bicycle parking is provided within the building and accessed from the walkway along the east property line. Limiting vehicular access to the property reduces potential conflicts and improves the safety, accessibility, and movement of pedestrians and cyclists to and from the Subject Property.</p>

<p>To ensure that new development respects the character of existing areas.</p>	<p>The proposed development is designed to establish a more transit-supportive character on the Subject Property and maintain compatibility with the existing residential community to the east, west, and south and with residential, office, retail and commercial uses located further to the north along Richmond Road. Proposed landscape treatments along Lower Byron Avenue maximize soft landscaping along the pedestrian walkway. The proposed landscape design will establish a high-quality treatment to interface private spaces with the public realm at grade level.</p>
<p>To consider adaptability and diversity by creating places that can adapt and evolve easily over time and that are characterized by variety and choice.</p>	<p>The proposed development is designed in a compact built form that contributes to the existing neighbourhood context, increases the range of housing choices for a variety of potential residents and is located in proximity to several transit and active transportation options for residents in the area. No off-street parking spaces are provided for motor vehicles. All residential units will not be provided with dedicated parking spaces and will emphasize the use of active transportation and transit for residents and visitors.</p>
<p>To maximize energy-efficiency and promote sustainable design to reduce the resource consumption, energy use, and carbon footprint of the built environment.</p>	<p>The proposed development is an example of transit-supportive residential intensification in an existing serviced area that contributes to efficient and sustainable building design. As part of the concurrent site plan application process, consideration will be given to incorporating a range of sustainability initiatives including high performance glazing and building materials and the use of white reflective roofing membranes to minimize heat island effect created from sun absorption at the horizontal roof surface.</p>

3.3.5 Review of Development Applications

Section 4 of the City of Ottawa’s in-force Official Plan sets out the policies for review of development applications. These policies ensure that development applications meet the objectives contained in the in-force Official Plan. The appropriate policies, related studies and plans were identified through a pre-application consultation meeting with the City at the beginning of the application review process.

Required studies and plans identified as relevant have been prepared in support of the proposed development. Detailed and technical information can be obtained by reviewing the respective documents.

Relating to Section 4.7 – Environmental Protection

Policy 1 of Section 4.7.2 of the in-force Official Plan requires that applications for Site Plan Approval will be supported by a Landscape Plan. A Tree Conservation Report and Landscape Plan has been prepared by James B. Lennox and Associates Inc., dated June 2022 in support of the Site Plan Control application (see Figure 16). The Landscape Plan provides details on possible soft and hard landscaping treatments between the public and private realm on Lower Byron Avenue and to abutting lands to the east, west and south of the Subject Property.

Section 4.7.6 of the in-force Official Plan states the City will require that stormwater and site servicing plans be submitted, in accordance with the guidance set out in the environmental

management, subwatershed and watershed plans. A Development Servicing and Stormwater Management Report has been prepared by Novatech, dated July 7, 2022 for the Subject Property. The Development Servicing and Stormwater Management Report includes a series of conclusions on water and sanitary servicing, storm servicing, stormwater management, site grading, erosion and sediment control. The recommendations of the Development Servicing and Stormwater Management Report are consistent with the relevant policies in Section 4.7.6 of the in-force Official Plan.

Relating to Section 4.8 – Protection of Public Health and Safety:

Policy 1 in Section 4.8.3 of the in-force Official Plan states that Site Plan Control applications shall be supported by a geotechnical study to demonstrate that the soils on site are suitable for development. In support of the Site Plan Control application, a Geotechnical Investigation has been prepared by Paterson Group Inc., dated October 1, 2021.

Section 5.1 of the Geotechnical Investigation states: *“From a geotechnical perspective, the subject site is suitable for the proposed development. The proposed building is recommended to be founded on conventional spread footings placed on clean, surface sounded bedrock.”*

Recommendations provided in the Geotechnical Investigation regarding site preparation and grading, foundation design, drainage, excavation, groundwater control and construction are consistent with the relevant policies in Section 4.8.3 of the in-force Official Plan.

City staff requested a Phase I Environmental Site Assessment (ESA) in support of the Site Plan Control application. Policy 3 in Section 4.8.4 of the in-force Official Plan states:

“Where a Phase 1 ESA indicates that the property or properties that are subject of a development application under the Planning Act may be contaminated, the City will require the application to be supported by an affidavit from a qualified person as defined by provincial legislation and regulations, confirming that a Phase 2 ESA has been completed in accordance with Ontario Regulation 153/04, as amended from time to time.”

A Phase I ESA study was prepared by Paterson Group Inc, dated January 25, 2022 in support of the Site Plan Control application. The recommendations of the Phase I ESA study state that:

“Based on our findings of the assessment, it is our opinion that a Phase II- Environmental Site Assessment is required for the subject property.”

Based on recommendations of the Phase I ESA study, a subsequent Phase II ESA study was prepared by Paterson Group Inc, dated February 23, 2022 in support of the Site Plan Control application. The recommendations of the Phase II ESA study state that:

“The purpose of the Phase II ESA was to address an off-site potentially contaminating activity (PCA) that was identified during the Phase I ESA and considered to result in an area of potential environmental concern (APEC) on the northeastern portion of the Phase II ESA Property. Groundwater was identified as the potentially impacted media.” and that, “Based on the analytical results of the Phase II ESA, no further environmental investigation is recommended at this time.”

The recommendations of the Phase II ESA report are consistent with the relevant policies in Section 4.8.4 of the in-force Official Plan.

Relating to Section 4.11 – Urban Design and Compatibility

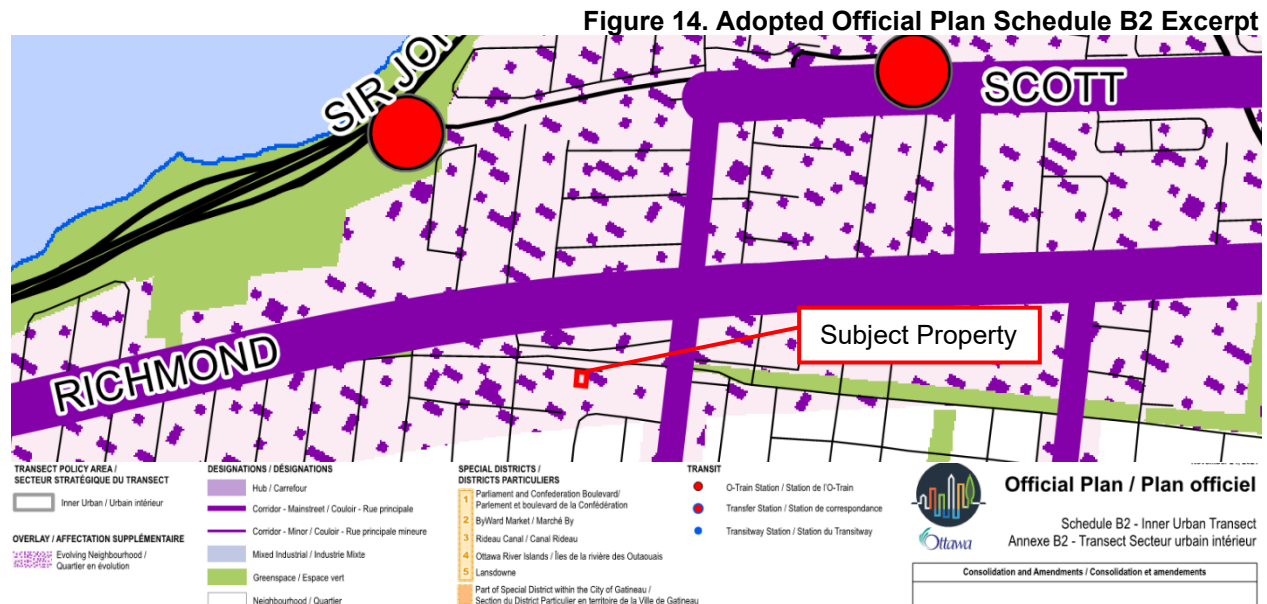
Section 4.11 of the in-force Official Plan sets out policies for requiring high quality urban design for development proposals within the City of Ottawa. Urban Design and compatibility of proposed developments are considered in the context of the policies of Section 4.11 of the in-force Official Plan. Policies in Section 4.11 of the in-force Official Plan are organized under the following subheadings: Views, Building Design, Massing and Scale, High-rise buildings, Outdoor Amenity Areas, Public Art, Design Priority Areas, and First Nations Peoples Design Interests.

A Design Brief providing a description and design justification related to each of the relevant policies within Section 4.11 of the in-force Official Plan has been included under section 4.0 of this Planning Rationale.

The proposed Minor Zoning By-law Amendment and Site Plan Control applications conform to the policies of the in-force City of Ottawa Official Plan.

3.4 Adopted City of Ottawa Official Plan

The new City of Ottawa Official Plan (“the adopted Official Plan”) was approved by Ottawa City Council on November 24, 2021. For the purposes of this Planning Rationale, the adopted Official Plan dated November 24, 2021 was used for reference.



3.4.1 Land Use Designation

The Subject Property is designated “Neighbourhood” and “Evolving Neighbourhood Overlay” in the Inner Urban Transect on Schedule B2 of the adopted Official Plan (see Figure 14).

Section 5.2 of the adopted Official Plan sets out general policies and guidance for proposed development within the Inner Urban Transect.

Section 5.2.4, Policy 1 of the adopted Official Plan states that:

“1) Neighbourhoods located in the Inner Urban area and within a short walking distance of Hubs and Corridors shall accommodate residential growth to meet the Growth Management Framework as outlined in Subsection 3.2, Table 3b. The Zoning By-law shall implement the density thresholds in a manner which adheres to the built form requirements as described in Subsection 5.6.1, as applicable and that:

- a) Allows and supports a wide variety of housing types with a focus on missing-middle housing, which may include new housing types that are currently not contemplated in this Plan;*
- b) The application of Zoning By-law development standards to be applied as one lot for zoning purposes to support missing middle housing;*
- c) Provides for a low-rise built form, by requiring in Zoning a minimum built height of 2 storeys, generally permitting 3 storeys, and where appropriate, will allow a built height of up to 4 storeys to permit higher-density low-rise residential development;*
- d) Provides an emphasis on regulating the maximum built form envelope that frames the public right of way rather than unit count or lot configuration; and*
- e) In appropriate locations, to support the production of missing middle housing, lower density typologies may be prohibited.”*

Policies of the adopted Official Plan direct areas designated “Neighbourhoods” to accommodate residential growth and are supportive of a wide variety of housing types including “missing-middle” housing. Missing-middle housing is generally defined in the adopted Official Plan as “low-rise, multiple unit residential development of between three and sixteen units, or more in the case of unusually large lots”. The Neighbourhoods designation allows higher-density low-rise residential development and would generally permit building heights up to three storeys on the Subject Property, as of right. The proposed development of a three storey low-rise apartment building on the Subject Property is consistent with policies of the adopted Official Plan for Neighbourhoods within the Inner Urban Transect.

Section 5.6.1.1, Policy 1 of the adopted Official Plan states that:

“1) The Evolving Overlay will apply to areas that are in a location or at stage of evolution that create the opportunity to achieve an urban form in terms of use, density, built form and site design. These areas are proximate to the boundaries of Hubs and Corridors as shown in the B-series of schedules of this Plan. The Evolving Overlay will be applied generally to the properties that have a lot line along a Minor Corridor; lands 150 meters from the boundary of a Hub or Mainstreet designation; and to lands within a 400-metre radius of a rapid transit station. The Overlay is intended to provide opportunities that allow the City to reach the goals of its Growth Management Framework for intensification through the Zoning By-law, by providing:

- a) Guidance for a gradual change in character based on proximity to Hubs and Corridors,*
- b) Allowance for new building forms and typologies, such as missing middle housing;*

- c) *Direction to built form and site design that support an evolution towards more urban built form patterns and applicable transportation mode share goals; and*
- d) *Direction to govern the evaluation of development.”*

The Subject Property is suitable for intensification being located within 150 metres from the boundary of the Richmond Road Mainstreet Corridor. The proposed development of a low-rise apartment dwelling, containing 18 dwelling units on the Subject Property is consistent with City objectives to establish a more urban community character and to create opportunities for new built forms such as missing middle housing within walking distance of planned rapid transit options in the Inner Urban Transect.

Section 5.6.1.1, Policy 3 of the adopted Official Plan states that:

“3) In the Evolving Overlay, the City:

- a) *Where the Zoning By-law for an area has not been updated either before adoption of this Plan in anticipation of this Plan’s policy direction, or post adoption of this Plan, to be consistent with the policy intent of this Plan, the City will generally be supportive of applications for low-rise intensification that seek to amend the development standards of the underlying zone where the proposal demonstrates that the development achieves objectives of the applicable transect with regards to density, built form and site design in keeping with the intent of Sections 3 and 5 of this Plan;”*

The proposed low-rise development represents intensification of the currently underutilized site in proximity to existing and planned rapid transit options. The proposed development will require relief from performance standards of the Zoning By-law for a decrease to minimum required rear yard setback, a decrease to the minimum required rear yard area, an increase to the maximum permitted balcony projection above the first floor in the rear yard, a decrease to the minimum required resident parking spaces and a decrease to the minimum required visitor parking spaces. The proposed development of a low-rise apartment building on the Subject Property is consistent with density objectives of the Inner Urban Transect and is appropriately designed within the context of the existing community.

Section 6.3 of the adopted Official Plan sets out functional policies related to Neighbourhood designations that are to be applied to each Transect on a site-specific basis.

Section 6.3.1, Policy 2 of the adopted Official Plan states that:

“2) Permitted building heights in Neighbourhoods shall be Low-rise, except:

- a) *Where existing zoning or secondary plans allow for greater building heights; or*
- b) *In areas already characterized by taller buildings.”*

Low-rise is defined in the adopted Official Plan as building heights “up to and including four full storeys”. The proposed development of a three-storey, low-rise apartment building on the Subject Property is consistent with permitted heights in the Neighbourhood designation.

Section 6.3.1, Policy 4 of the adopted Official Plan states that:

- “4) The Zoning By-law and approvals under the Planning Act shall allow a range of residential and non-residential built forms within the Neighbourhood designation, including:

- a) Generally, a full range of Low-rise housing options sufficient to meet or exceed the goals of Table 2 and Table 3b;
b) Housing options with the predominant new building form being missing middle housing, which meet the intent of Subsection 6.3.2, Policy 1):”

Policies of the adopted Official Plan permit a range of low-rise residential built forms in areas designated “Neighbourhood” to accommodate residential growth to meet City growth objectives. The predominant housing form anticipated for future development within the Neighbourhood designation is “missing-middle” housing. Missing-middle housing is generally defined in the adopted Official Plan as “low-rise, multiple unit residential development of between three and sixteen units, or more in the case of unusually large lots”. The proposed development of a three-storey, low-rise apartment building containing approximately 18 dwelling units on the Subject Property is consistent with policies of the adopted Official Plan for Neighbourhoods within the Inner Urban Transect.

Section 6.3.2, Policy 1 of the adopted Official Plan states that: “The Zoning By-law and approvals under the Planning Act will allow innovative buildings forms, including in the missing middle housing category, in order to strengthen, guide towards or seed conditions for 15-minute neighbourhoods.” The proposed development of a low-rise apartment building on the Subject Property is consistent with policies of the adopted Official Plan to encourage the creation of missing middle housing and to establish conditions that promote development of 15-minute neighbourhoods.

Section 6.3.2, Policy 2 of the adopted Official Plan states that:

- “2) The City will establish form-based regulation through the Zoning By-law, Site Plan Control and other regulatory tools as appropriate, consistent with Transect direction. Such form-based regulation may include requirements for articulation, height, setbacks, massing, floor area, roofline, materiality and landscaped areas having regard for:
- a) Local context and character of existing development;
 - b) Appropriate interfaces with the public realm, including features that occupy both public and private land such as trees;
 - c) Appropriate interfaces between residential buildings, including provision of reasonable and appropriate soft landscaping and screening to support livability;
 - d) Proximity to Hubs, Corridors and rapid-transit stations;
 - e) Transition in building form to and from abutting designations;
 - f) The intended density to be accommodated within the permitted building envelope;”

The proposed development is located within the Westboro Development Overlay and is zoned to permit a low-rise apartment dwelling to accommodate planned intensification along Lower Byron Avenue and the surrounding area. The proposed development is designed to define and enhance the public realm along Lower Byron Avenue through material breaks, including well-proportioned glazed surfaces, recesses and balconies that articulate the façade and provide visual interest. Landscape treatments have been designed to establish an appropriate interface between the public and private realm and between adjacent properties to the east, west and south. The Subject Property is located within a 500 metre radius and 600 metre walking distance of the planned Kichi Sibi O-Train station and within 200 metres walking distance of the Richmond Road Mainstreet corridor. The main entrance to the proposed building will provide simple and safe connections on

foot or bicycle to the planned Kichi Sibi O-Train station and bus transit stops on frequent routes along Richmond Road to the north of the Subject Property. The proposed development is of an appropriate form and scale that will present no negative impacts on existing or future built form on abutting properties. The proposed development is consistent with growth objectives of the adopted Official Plan to accommodate densities that align with provision of missing middle housing within the Neighbourhood designation.

3.4.2 Growth Management Framework

Section 3 of the adopted Official Plan sets out policies to direct growth toward target areas for intensification. The majority of projected growth between 2018 and 2046 is directed within the urban boundary, representing 93% of all new development. 47% of the growth allocation is directed to take place within existing built-up areas inside the urban boundary where services are available or can be easily provided for new development to accommodate the creation of jobs, housing and increased transit use.

Section 3.2, Policy 3 of the adopted Official Plan identifies “Neighbourhoods” as one of the target areas designated for residential intensification in the City of Ottawa.

The Subject Property is designated as a Neighbourhood and is located within the Inner Urban Transect on Schedule B2 of the adopted Official Plan. The Subject Property is located within a land use designation targeted for intensification. The proposed development of a three storey low-rise residential apartment building on the Subject Property is consistent with the growth objectives of the adopted Official Plan.

Section 3.2, Policy 10 of the adopted Official Plan sets out residential density targets that apply to Hubs and states: “The residential density and proportion of large household dwelling targets as shown on Schedules B1 through B8 are established in Table 3a for Hubs and Mainstreet Corridors and Table 3b for Neighbourhoods and Minor Corridors.”

The Subject Property is designated as Neighbourhood in the adopted Official Plan. Policies of the adopted Official Plan establish a minimum residential density requirement of between 60 and 80 dwellings per net hectare for proposed developments in the Inner Urban Transect. The proposed development on the Subject Property achieves a density of approximately 294 dwelling units per net hectare and is consistent with residential density targets of the adopted Official Plan.

3.4.3 City-wide Policies

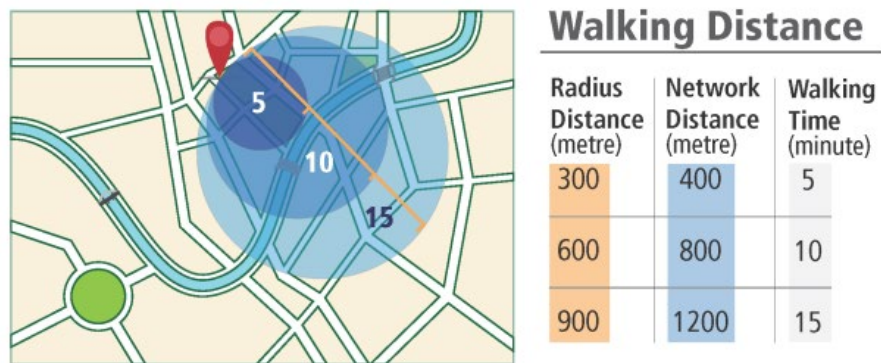
Section 4 of the adopted Official Plan sets out city-wide policies to be considered where all new development is proposed.

Section 4.1.2, Policy 1 of the adopted Official Plan sets out the framework for establishing 15-minute neighbourhoods across the City and states that:

- “1) In general, this Plan equates a walking time of:
- a) 5 minutes to be equivalent to a radius of 300 metres, or 400 metres on the pedestrian network;
 - b) 10 minutes to be equivalent to a radius of 600 metres, or 800 metres on the pedestrian network; and

c) 15 minutes to be equivalent to a radius of 900 metres or 1,200 metres on the pedestrian network.”

Figure 15 – Adopted Official Plan Figure 11



The Subject Property is located fully within 600 metres radius and within 800 metres network distance of the planned Kichi Sibi O-Train station. Transit stops along frequent bus route 11 are located north of the Subject Property along Richmond Road. The proximity of the proposed development to a range of transit options in the immediate area represents a walking distance of 10 minutes or less for residents to access rapid transit and frequent street transit from their home (see Figure 15).

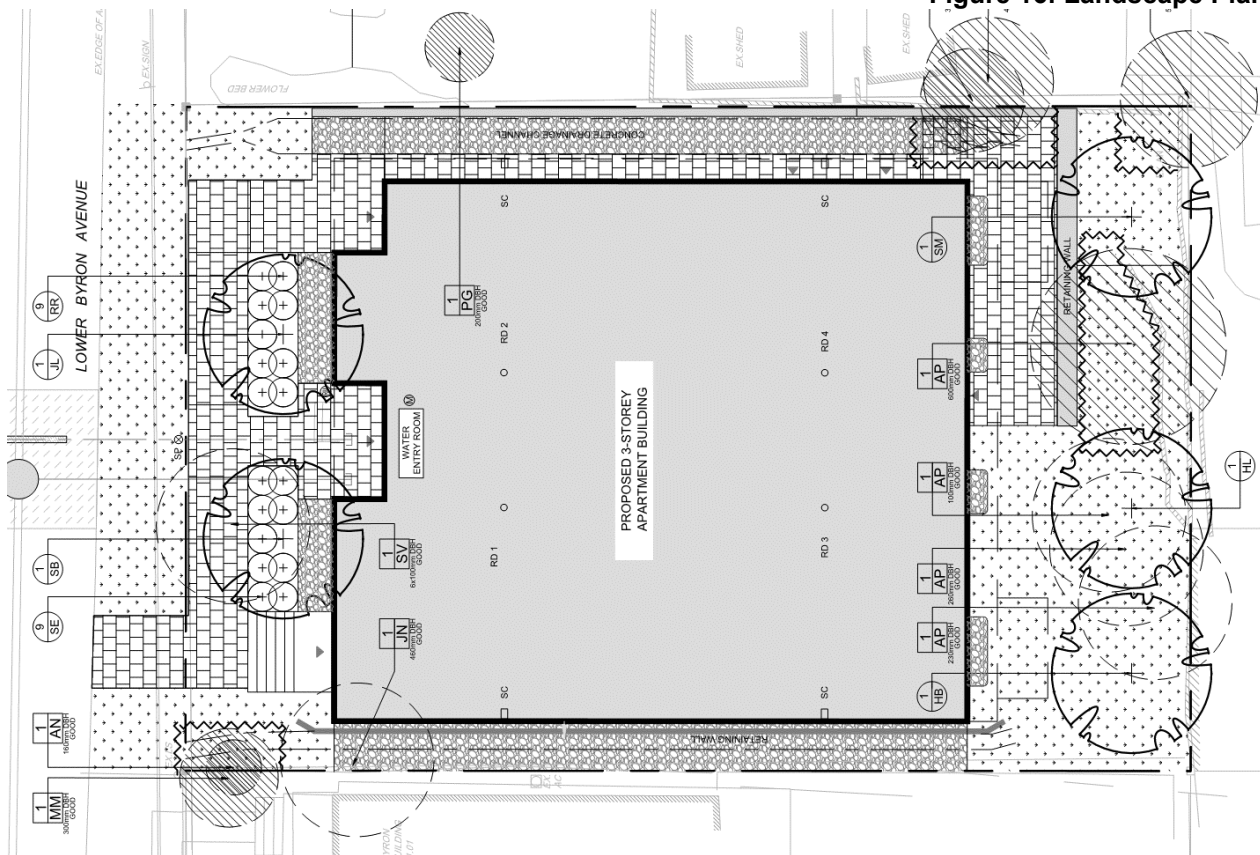
Section 4.6 of the adopted Official Plan sets out policies related to Urban Design and is intended to promote design excellence in Design Priority areas, encourage innovation in site planning and building design, support the objectives of Corridors, Hubs and Neighbourhoods and enable integration of new development with existing communities.

Section 4.6.5, Policy 3 of the adopted Official Plan states that:

“Development shall minimize conflict between vehicles and pedestrians and improve the attractiveness of the public realm by internalizing all servicing, loading areas, mechanical equipment and utilities into the design of the building, and by accommodating space on the site for trees, where possible. Shared service areas, and accesses should be used to limit interruptions along sidewalks. Where underground parking is not viable, surface parking must be visually screened from the public realm.”

Direct pedestrian access to the Subject Property is provided from the principal building entrance in the front yard to Lower Byron Avenue. Grade level amenity space, including soft landscaping and low plantings are provided along Lower Byron Avenue and in the rear yard for building residents (see Figure 16). No motor vehicle access is provided for the proposed development. The proposed development has been designed with all mechanical equipment internal to the building and on the rooftop. Providing no motor vehicle access to the Subject Property reduces the potential for vehicle conflicts and improves the safety, accessibility, and movement of pedestrians and bicycles to and from the Subject Property and is consistent with policies of the adopted Official Plan.

Figure 16: Landscape Plan



Section 4.6.6, Policy 6 of the adopted Official Plan states that: “Low-rise buildings shall be designed to respond to context, and transect area policies, and shall include areas for soft landscaping, main entrances at-grade, front porches or balconies, where appropriate. Buildings shall integrate architecturally to complement the surrounding context.

The proposed development has been designed to compliment the existing low-rise scale of the surrounding community in accordance with policies of the adopted Official Plan for development within Neighbourhoods in the Inner Urban Transect. Soft landscaping is provided within the building setbacks in the front yard and rear yard. The principal barrier-free entrance to the proposed building is provided at the centre of front façade facing Lower Byron Avenue. Semi-private terrace entrances to residential units are provided along Lower Byron Avenue to improve the interface with the public realm for the proposed building. The proposed development integrates building materials that blend within the surrounding urban fabric and contribute to the overall quality and longevity. Proposed materials include masonry (brick), concrete panels, and wood siding features to soften the façade. The design incorporates traditional architectural features that are common to the existing context while providing subtle contemporary components such as large glass windows, asymmetry, and contrasting materials that ensure future compatibility as the surrounding community matures.

Please note that the policies of the adopted Official Plan are subject to modification by the Province of Ontario’s Ministry of Municipal Affairs and Housing.

The proposed Minor Zoning By-law Amendment and Site Plan Control applications conform to the policies of the adopted City of Ottawa Official Plan.

4.0 DESIGN BRIEF

Section 4.11 of the in-force Official Plan sets out directions for high quality urban design and compatibility at the city-wide scale. Several policies within Section 4.11 are not relevant to the Subject Property. This Design Brief has been scoped to reflect the relevant sections accordingly. A brief explanation of the sections not reviewed as part of this brief are provided below:

Discussion of Views relates to high-rise buildings and protecting the views of the Parliament Buildings. Policies on Views do not apply to the proposed development. The in-force Official Plan policies on High-rise Buildings and Public Art do not apply to the proposed development. The policy on First Nations Peoples Design Interests applies to public lands and does not apply to the Subject Property.

4.1.1 Building Design

Section 4.11 of the Official Plan under “Building Design” states: *“Good building design contributes to successful neighbourhood integration and the compatibility of new development with the existing or planned character of its surroundings. The façades of buildings influence the feel and function of public spaces and define the edges of the pedestrian environment. Good building design is required throughout the city.”*

The following section provides a review of each relevant policy regarding Building Design and provides design justification in support of the development of a low-rise apartment building on the Subject Property.

5. *Compatibility of new buildings with their surroundings will be achieved in part through the design of the portions of the structure adjacent to existing buildings and/or facing the public realm. Proponents of new development will demonstrate, at the time of application, how the design of their development fits with the existing desirable character and planned function of the surrounding area in the context of:*
 - a. *Setbacks, heights and transition;*
 - b. *Façade and roofline articulation;*
 - c. *Colours and materials;*
 - d. *Architectural elements, including windows, doors and projections;*
 - e. *Pre- and post-construction grades on site; and*
Incorporating elements and details of common characteristics of the area.

Policies of the adopted Official Plan and zoning provisions of the Westboro development Overlay anticipate Lower Byron Avenue to experience a transition to higher density built form including the surrounding context to the north of the Subject Property along the Richmond Road corridor and toward the planned Kichi Sibi and Westboro O-Train stations. The proposed three-storey apartment building is designed to complement the existing character of the neighbourhood to provide an appropriate scale of development while also facilitating intensification of the Subject Property. The proposed development is designed to complement the existing buildings abutting the Subject Property by matching their widths and overall scale.

Figure 17. View from Lower Byron Avenue looking south



The 28-unit apartment building includes ample amenity space located at grade, private balconies, and a rooftop terrace. By proposing a two-storey datum line the proposed building ties into the surrounding properties seamlessly while introducing some additional height. The building articulation includes a series of setbacks, protrusions, and balconies which help break up the built-form and animate the facade. The corner balconies minimize the building edges that bleed into the adjacent properties. The north elevation (front) introduces strong vertical elements using indentation and materiality as well as a strong masonry base to ground the project.

The proposed development responds to the existing and planned scale envisioned for the surrounding context through an appropriate building setback and façade design along the Lower Byron Avenue frontage that frames the street, improves pedestrian access and interfaces well with the public realm (see Figure 17). The proposed three-storey building provides an appropriate scale of development that is compatible with existing low-rise buildings to the east, west and south of the Subject Property.

The proposed building materials facing Lower Byron Avenue consist of grey brick base, concrete panelling, and wood siding which are appropriate for the existing neighbourhood context. The materials closer to grade are all masonry which is quite durable and contributes to the overall quality and longevity of the building. We introduced a secondary building material to lighten the upper portion of the building, putting an emphasis on the lower portion which aligns with the adjacent low-rise residential properties. Wood siding is used as our accent feature on the north elevation (front) which helps soften the façade and relate to the greenery at the rear of the property. The façade articulation is comprised of vertical elements including fenestrations and panelling that are broken up by center alcoves to put an emphasis on the principal entrance. The

covered entrance along Lower Byron Ave helps relate to a human scale while create a welcoming atmosphere to the public.

The proposed development establishes a transit-supportive level of density within walking distance of the planned Kichi Sibi O-Train station and transit stops along the frequent transit network on Richmond Road. The proposed development of a low-rise apartment on the Subject Property is appropriate for the existing site and surrounding context and will not cause undue adverse impacts on abutting properties.

6. *The City will require that all applications for new development:*
 - a. *Orient the principal façade and entrance(s) of main building(s) to the street.*
 - b. *Include windows on the building elevations that are adjacent to public spaces;*
 - c. *Use architectural elements, massing, and landscaping to accentuate main building entrances.*

The proposed development is designed with the principal building façade and main building entrance oriented to Lower Byron Avenue. The principal façade comprises a high-quality material treatment where the building interfaces with abutting properties and toward the public realm. The proposed building provides a well-articulated combination of traditional materials such as masonry (brick) and patterned fenestrations along with contemporary features that compliment the established residential character of the area and fit within the existing built form surrounding the Subject Property. Strategically located alcoves containing more contemporary materials such as concrete panel cladding in a darker palette is used to accent building entrances and grade level amenity spaces, creating visual breaks in the façade along Lower Byron Avenue and achieving a design aesthetic that is compatible with existing characteristics of the surrounding community. The recessed entrance creates a welcoming atmosphere while relating to a human scale.

8. *To maintain a high quality, obstacle free pedestrian environment, all servicing, loading areas, and other required mechanical equipment and utilities should be internalized and integrated into the design of the base of the building where possible. If they cannot be internalized these services are to be screened from public view (i.e. trees, landscaping, decorative walls and fences etc.) and are to be acoustically dampened where possible. The location and operation these areas and equipment should be designed to maintain a pedestrian friendly environment and not impede public use of the sidewalk.*

Direct pedestrian connections are provided from the principal building entrance to Lower Byron Avenue. There are no existing sidewalks along Lower Byron Avenue. No motor vehicle entrance is provided off Lower Byron Avenue. Access to all service entrances, garbage facilities and loading space for tenant move-in purposes is provided off Lower Byron Avenue. Providing limited vehicle access directly to the Subject Property reduces potential conflicts and improves the safety, accessibility, and movement of pedestrians and cyclists to and from the Subject Property.

4.1.2 Massing and Scale

Section 4.11 of the Official Plan under “Massing and Scale” states: *“Complementary to building design, the massing and scale of new development also contributes to successful neighbourhood integration and the compatibility of new development with the character of the surrounding*

community. Massing and scale describe the form of the building, how tall it is, how much of the lot it occupies and how it is positioned in relation to the street and surrounding buildings.”

The following section provides a review of each relevant policy related to Massing and Scale and provides design justification in support of the proposed development of a low-rise apartment building on the Subject Property.

10. “Where a secondary planning process establishes criteria for compatibility of new development or redevelopment in terms of the character of the surrounding area, the City will assess the appropriateness of the development using the criteria for massing and scale established in that Plan. Where there are no established criteria provided in an approved Plan, the City will assess the appropriateness of the proposal relying upon its approved Design Guidelines, as applicable, and the following criteria:

- a. *Building height, massing and scale permitted by the planned function of adjacent properties as well as the character established by the prevailing pattern of abutting development and development that is across the street;*
- b. *Prevailing patterns of rear and side yard setbacks, building separation and landscaped open spaces and outdoor amenity areas as established by existing zoning where that pattern is different from the existing pattern of development;*
- c. *The need to provide a transition between areas of different development intensity and scale as set out in policy 12 of this section.”*

12. “Transition refers to the integration of buildings that have greater height or massing than their surroundings. Transition is an important building design element to minimize conflicts when development that is higher or has greater massing is proposed abutting established or planned areas of Low-Rise development. Proponents for developments that are taller in height than the existing or planned context or are adjacent to a public open space or street shall demonstrate that an effective transition in height and massing, that respects the surrounding planned context, such as a stepping down or variation in building form has been incorporated into the design.”

The proposed development is designed to provide an appropriate transition in building scale and height along the Lower Byron Avenue frontage and the surrounding community to the north, east and south. The Subject Property and lands located immediately to the east and west along Lower Byron Avenue are zoned to permit low-rise intensification within the Westboro Development Overlay. To the south of the Subject Property are low-rise residential uses at the top of a large slope. To the north between the Subject Property and Richmond Road are existing mid to high-rise, mixed-use buildings. The proposed development of a three-storey apartment building has regard for the scale, massing and building materials that are desirable characteristics of buildings in the existing and planned community context and represents appropriate intensification of the Subject Property. The design presents a two-storey datum line relating to the low-rise residential properties on the west and east. This datum is achieved with the transition of materials from masonry (brick) which grounds to project to light concrete panels above. Where the datum line is broken with vertical elements, we introduced a series of balconies to soften the building edge.

4.1.3 Outdoor Amenity Areas

Section 4.11 of the in-force Official Plan under “Outdoor Amenity Areas” states: “*Outdoor amenity areas are the private and communal areas of a property that are designed to accommodate a variety of leisure activities.*”

Policy 20 of Section 4.11 of the in-force Official Plan states that: “*Applications to develop residential or mixed-use buildings incorporating residences will include well-designed, usable amenity areas for the residents that meet the requirements of the Zoning By-law, and are appropriate to the size, location and type of development. These areas may include private amenity areas and communal amenity spaces such as: balconies or terraces, rooftop patios, and communal outdoor at-grade spaces (e.g. plazas, courtyards, squares, yards). The specific requirements for the private amenity areas and the communal amenity spaces shall be determined by the City and implemented through the Zoning By-law and site plan agreement.*”

The proposed development includes both private and communal outdoor amenity spaces (see Figure 16). Grade level residential units along the Lower Byron Avenue frontage have access to semi-private front terraces accessed from street level and upper units have access to private balconies. Rear facing dwelling units also have access to private terraces and balconies. Communal amenity spaces are provided for residents at grade level along Lower Byron Avenue, at the rear of the proposed building and on the building roof. The proposed development is consistent with urban design policies of the in-force Official Plan for outdoor amenity areas on the Subject Property.

The proposed Minor Zoning By-law Amendment and Site Plan Control applications conform to the policies of the in-force City of Ottawa Official Plan.

5.0 CITY OF OTTAWA ZONING BY-LAW 2008-250

The Subject Property is zoned Residential Fourth Density, Subzone UC, Urban Exception 2685 (R4UC [2685]) in the City of Ottawa Zoning By-law 2008-250. The Subject Property is located within the boundary of Schedule 430 of the Zoning By-law and is subject to provisions of the Westboro Development Overlay.

The purpose of the R4 – Residential Fourth Density Zone is to:

- (1) allow a wide mix of residential building forms ranging from detached to low rise apartment dwellings, in some cases limited to four units, and in no case more than four storeys, in areas designated as General Urban Area in the Official Plan;*
- (2) allow a number of other residential uses to provide additional housing choices within the fourth density residential areas;*
- (3) permit ancillary uses to the principal residential use to allow residents to work at home;*
- (4) regulate development in a manner that is compatible with existing land use patterns so that the mixed building form, residential character of a neighbourhood is maintained or enhanced*

Under the provisions of the Westboro Development Overlay, Lower Byron Avenue is characterized as an appropriate location to accommodate a higher level of density. The provisions of the R4UC zone permit development of a low-rise residential apartment dwelling with 9 or more dwelling units on the Subject Property. Section 161, Section 162, Table 162A and Table 162B set out the performance standards for the Residential Fourth Density, Subzone UC zone. Site-specific performance standards for the Subject Property are set out by Urban Exception 2685. The Subject Property is located within the Greenbelt. Provisions of Section 139 and Section 144 of the Zoning By-law apply to all low-rise residential developments located within the Greenbelt (see Figure 18).

A low-rise apartment building containing 18 residential units is proposed on the Subject Property.

The proposed zoning will incorporate the required relief from the zoning provisions for a decrease to minimum required rear yard setback, a decrease to the minimum required rear yard area, an increase to the maximum permitted balcony projection above the first floor in the rear yard, a decrease to the minimum required resident parking spaces and a decrease to the minimum required visitor parking spaces. A summary of the relief requested is highlighted in orange on the Zoning Review Table (see Figure 18).

The proposed low-rise apartment building conforms with all other zoning provisions of Section 139, Section 144, Section 162, Table 162A and Table 162B including lot width, lot area, minimum front yard setback, minimum interior side yard setback, maximum building height, and minimum landscape area. The proposed development is designed to conform with the Residential Infill performance standards of Section 161 including the required minimum number of principal entrances, minimum required percentage of fenestration, minimum required two-bedroom dwelling units and minimum required percentage of recessed façade area to improve the building interface with the public realm along Lower Byron Avenue.

Figure 18: Zoning Review Table

**ZONING PROVISIONS: R4-UC, Urban Exception 2685, Low Rise Apartment (9 or more units)
City of Ottawa By-Law 2008 -250**

	Required	Provided
Minimum Lot Width (m) Part 15, Urban Exception 2685, Column V	10.0 m	20.11 m
Maximum Lot Width (m) Part 6, Table 162B, Endnote 11	38.0 m	20.11 m
Minimum Lot Area (m ²) Part 15, Urban Exception 2685, Column V	300 m ²	613.71 m ²
Maximum Lot Area (m ²) Part 6, Table 162B, Endnote 12	1,070 m ²	613.71 m ²
*Minimum Front Yard (m) Part 15, Urban Exception 2685, Column V	4.50 m	4.50 m
*Minimum Rear Yard (m) Part 5, Sec. 144 (3)(a)(i) - Table 144A, Row iii, Columns I,II	30% of lot depth (30.4m x 30%) = 9.12m	5.86 m
	<i>*(min. 25% of total lot area 613.71m² x 25%) = 153.43m²</i>	130.1 m ²
*Minimum Interior Side Yard (m) Part 15, Urban Exception 2685, Column V	1.50 m	1.50 m
Maximum Height (m) Part 6, Table 162A, Subzone R4-UB, Column VI	11.0 m	10.22 m
*Required Landscape Area		
Total Landscape Area: Section 161 (8)	min. 30% of total lot area	(200.07m ² / 613.71m ²) = ±32.6%
Front Yard Soft Landscape Area: Section 161 (15)(d) - Table 161	min. 40% of front yard	(± 76.62m ² / 90.52 m ²) = ±84.6%
Rear Yard Landscape Area: Section 161 (15)(b)(iii),(iv) & (c)	min. 50% of the rear yard	(± 94.78 / ±118.07 m ²) = ±80.0 %
*Balcony Projection (Rear Yard) Part 2, Section 65, Table 65 (6)(b)(ii) - maximum projection - 1.2 m above the first floor	1.20m	1.52m
Parking Requirements		
	Required	Provided
Minimum Parking Spaces Part 4, Sec. 101 (3)(a) - Schedule 1A: Area "X" - no parking required for first twelve dwelling units	3	0
Minimum Visitor Parking Spaces (Table 102, Row I, Column II) - 0.1 spaces / dwelling un Part 4, Sec. 102 (1),(2) and (3) Schedule 1A: Area "X" - no visitor parking required for first twelve dwelling units	1	0
TOTAL	4	0
Minimum Bicycle Parking Spaces Part 4, Sec 111 (1) - Table 111A, Row (b)(i), Columns I,II (0.5 spaces/du) - 18 x 0.5)	9	9
TOTAL	9	9

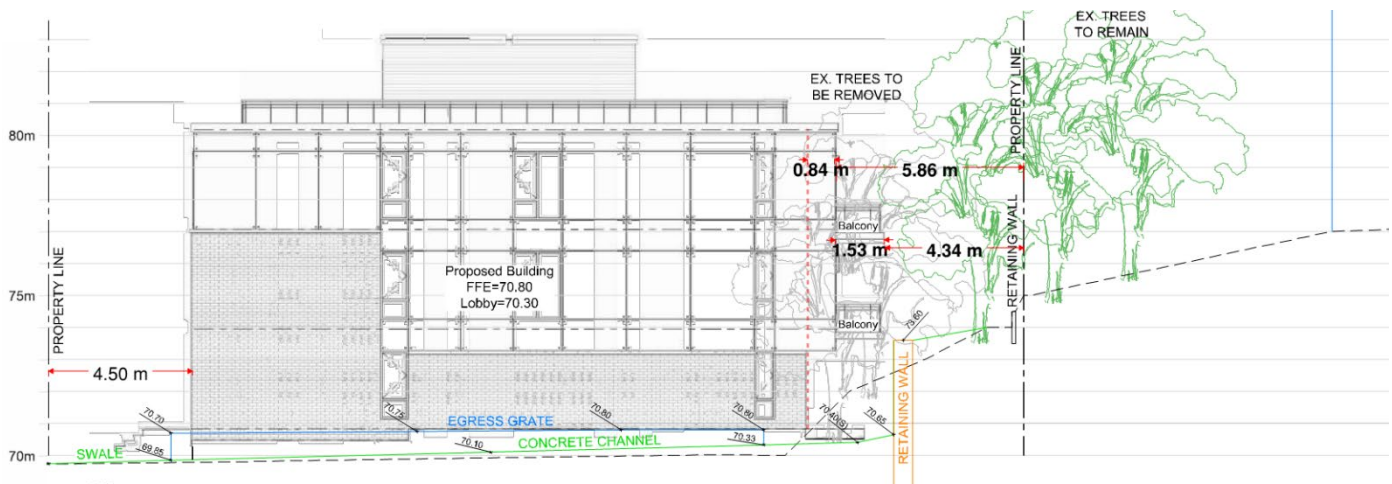
*NOTE: All measurements taken using Bluebeam Revu based on Plans provided by SJL Architect Inc. and should be considered approximate values

The proposed Minor Zoning By-law Amendment will establish appropriate Zoning standards for the Subject Property and will maintain the intent and purpose of the City of Ottawa Zoning By-law 2008-250.

6.0 PROPOSED ZONING BY-LAW AMENDMENT

The provisions of Section 144, Table 144A Row (iii) require a minimum rear yard setback of 30% of the lot depth. The lot depth of the Subject Property is 30.4 metres. A rear yard setback of 9.12 metres would be required by provisions of the Zoning By-law. The proposed rear yard setback for the low-rise apartment building is 5.86 metres to the principal building façade on levels two and three. There is a grade change of approximately 2 metres at the southwest corner sloping up to approximately 4 metres at the southeast corner of the Subject Property (see Figure 19). The existing change in grade provides a vertical separation from the proposed building to existing low-rise buildings to the south of the Subject Property. A proposed rear yard setback of 5.86 metres from the proposed low-rise apartment buildings to property lines abutting to the south will provide appropriate separation from any future development. A reduction to the required rear yard setback is appropriate for the proposed development on the Subject Property.

Figure 19. Section showing proposed building and rear yard



The provisions of Section 65 (6)(b)(ii) permit a maximum projection of 1.2 metres above the first floor into the rear yard. Relief from the zoning provision is required to increase the maximum permitted projection from 1.2 metres to 1.52 metres above the first floor into the rear yard. A 0.32 metre increase to the permitted projection into the rear yard area is a minor increase from the performance standards. There is a grade change of approximately 2 metres at the southwest corner sloping up to approximately 4 metres at the southeast corner of the Subject Property. The existing change in grade provides a vertical separation from the proposed balcony projections above the ground floor to existing low-rise buildings to the south of the Subject Property (see Figure 19). A 4.34 metre separation distance is provided from the furthest extent of the proposed balconies to the rear lot line and will provide appropriate separation from the residential buildings to the south. An increase to the permitted projections into the rear yard area above the ground floor is appropriate for the proposed development on the Subject Property.

The provisions of Section 144 (3)(a) require the rear yard to comprise 25% of the total lot area. Relief from the zoning provision is required to decrease the minimum required rear yard area by 3.8% to 21.2% to facilitate the proposed development. A 3.8% reduction to the required rear yard area is a minor reduction from the zoning provisions. A communal amenity area for building residents is provided on the building roof. Public amenity areas are located within walking

distance of the Subject Property with Westboro Beach and the Ottawa River to the west and Clare Gardens Park, Westboro Kiwanis Park and Dovercourt Recreation Centre further to the south (see Figure 20). A reduced rear yard area is appropriate for the proposed development on the Subject Property.

Figure 20. Amenities within 800m Radius of Subject Property



Section 101 (3) of the Zoning By-law (Area “X” – Schedule 1A) does not require off-street motor vehicle parking for the first twelve residential dwelling units and parking requirements only apply to dwelling units in excess of twelve. A total of 18 dwelling units are proposed. Three off-street parking spaces are required for the proposed residential uses. Section 102, Table 102, Column II of the Zoning By-law (Area “X” – Schedule 1A) sets a minimum visitor parking space rate of 0.1 spaces per dwelling unit after the first 12 units. One parking space is required for visitor parking.

The combined minimum number of parking spaces required for the proposed development is four spaces. The decision to provide no off-street parking spaces is in response to the proximity of the Subject Property to the planned Kichi Sibi and Westboro O-Train stations and transit stops along frequent transit routes at the intersection of Roosevelt Avenue and Richmond Road. The

location of the Subject Property is within 600 metres radius of the planned Kichi Sibi O-Train station. The location of the Subject Property is within 300 metres walking distance of transit stops on bus route 11 and is within a prime location to encourage the use of active transportation and transit options over use of private automobiles.

The Subject Property is within 800 metres radius of a broad range of land uses and amenities that contribute to development of 15-minute neighbourhoods and reduce the need for residents to use private automobiles to access daily needs. Specifically, the Subject Property is located within walking distance of retail stores, coffee shops, restaurants, recreational uses, banks, pharmacies and grocery stores along Richmond Road. The Subject Property is also located near several parks, recreation centres, public schools, Westboro Beach and the Ottawa River (see Figure 20).

Relief is required to reduce the number of resident parking spaces from 3 to 0, a reduction of 3 spaces. Relief from the zoning provisions is required to reduce the number of visitor parking spaces from 1 to 0, a reduction of 1 space. A reduction to the total required off-street parking spaces is appropriate for the proposed development on the Subject Property.

Section 111, Table 111A, Row (b) of the Zoning By-law sets out minimum bicycle parking space rates for the proposed development. A rate of 0.5 bicycle parking spaces per dwelling unit is required for residential uses. The minimum number of bicycle parking spaces required for the proposed residential use is 9 spaces.

9 bicycle parking spaces are provided within the proposed building. A total of 9 bicycle parking spaces are provided on the Subject Property.

To facilitate the proposed development of a low-rise apartment building on the Subject Property, this application requests a site-specific amendment to incorporate the required relief from the zoning provisions for a decrease to minimum required rear yard setback, a decrease to the minimum required rear yard area, an increase to the maximum permitted balcony projection above the first floor in the rear yard, a decrease to the minimum required resident parking spaces and a decrease to the minimum required visitor parking spaces as highlighted in orange on the Zoning Review Table (see Figure 18). The site-specific amendment will result in development of a low-rise residential apartment building that is located with direct access and opportunities for residents to increase use of transit and active transportation infrastructure. The proposed development is within proximity to restaurants, shops, parks and outdoor amenity areas for residents. The requested relief from provisions of the R4UC[2685] zone is appropriate to facilitate the proposed development of a low-rise apartment building on the Subject Property.

7.0 PUBLIC CONSULTATION STRATEGY

It is proposed to consult with the public through the legislated public consultation requirements. This includes a sign posted on the site and the posting of the application on the City's 'DevApps' website. Full details are available under 'Community and Neighbourhood Notification' on the City's website. Neighbours will have the opportunity to comment on the proposal via the notification requirements.

An Information Meeting will be held to discuss the proposed development with the community. This meeting will be coordinated with the Councillor's office immediately following filing of the application. A second meeting will be held if deemed necessary.

8.0 CONCLUSION

This Planning Rationale has been prepared in support of Minor Zoning By-law Amendment and Site Plan Control applications to facilitate the development of a low-rise apartment building on the Subject Property. The Subject Property is designated General Urban Area on Schedule B of the in-force City of Ottawa Official Plan. The Subject Property is designated Neighbourhood and Evolving Neighbourhood Overlay in the Inner Urban Transect on Schedule B2 of the adopted City of Ottawa Official Plan (November 2021). The property is zoned Residential Fourth Density, Subzone UC, Urban Exception 2685 (R4UC [2685]) in the City of Ottawa's Zoning By-law 2008-250. The Subject Property is located within the boundary of Schedule 430 of the Zoning By-law and is subject to provisions of the Westboro Development Overlay.

The purpose of this application is to facilitate the proposed development of a low-rise residential apartment building through establishing site-specific zoning provisions for the Subject Property including a decrease to minimum required rear yard setback, a decrease to the minimum required rear yard area, an increase to the maximum permitted balcony projection above the first floor in the rear yard, a decrease to the minimum required resident parking spaces and a decrease to the minimum required visitor parking spaces. Approval of the proposed development will permit a total of 18 residential dwelling units. No off-street parking spaces are provided. 9 bicycle parking spaces are proposed on the Subject Property.

The proposed low-rise development is appropriate to support growth objectives and residential intensification targets within the urban area of the City of Ottawa. The proposed Minor Zoning By-law Amendment and Site Plan Control applications will have no negative impacts to natural heritage and features, natural resources, or cultural heritage resources. The requested Minor Zoning By-law Amendment and Site Plan Control applications are consistent with the policies of the Provincial Policy Statement.

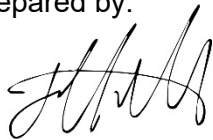
The proposed development is consistent with policies of the City of Ottawa's in-force Official Plan and adopted City of Ottawa Official Plan. This Planning Rationale demonstrates the proposed development is consistent with the intent and policies of the General Urban Area designation of the in-force Official Plan. The proposed development is consistent with the intent and policies of the Neighbourhood and Evolving Neighbourhood Overlay designation within the Inner Urban Transect of the adopted Official Plan and is compatible with surrounding land uses. Site specific zoning is proposed for the Subject Property to bring the proposed development into conformity with the City of Ottawa Zoning By-law.

The proposed Minor Zoning By-law Amendment and Site Plan Control applications are appropriate for the Subject Property and represent good land use planning.

Yours Truly,

NOVATECH

Prepared by:



Jeffrey Kelly, MCIP, RPP
Project Planner

Reviewed by:

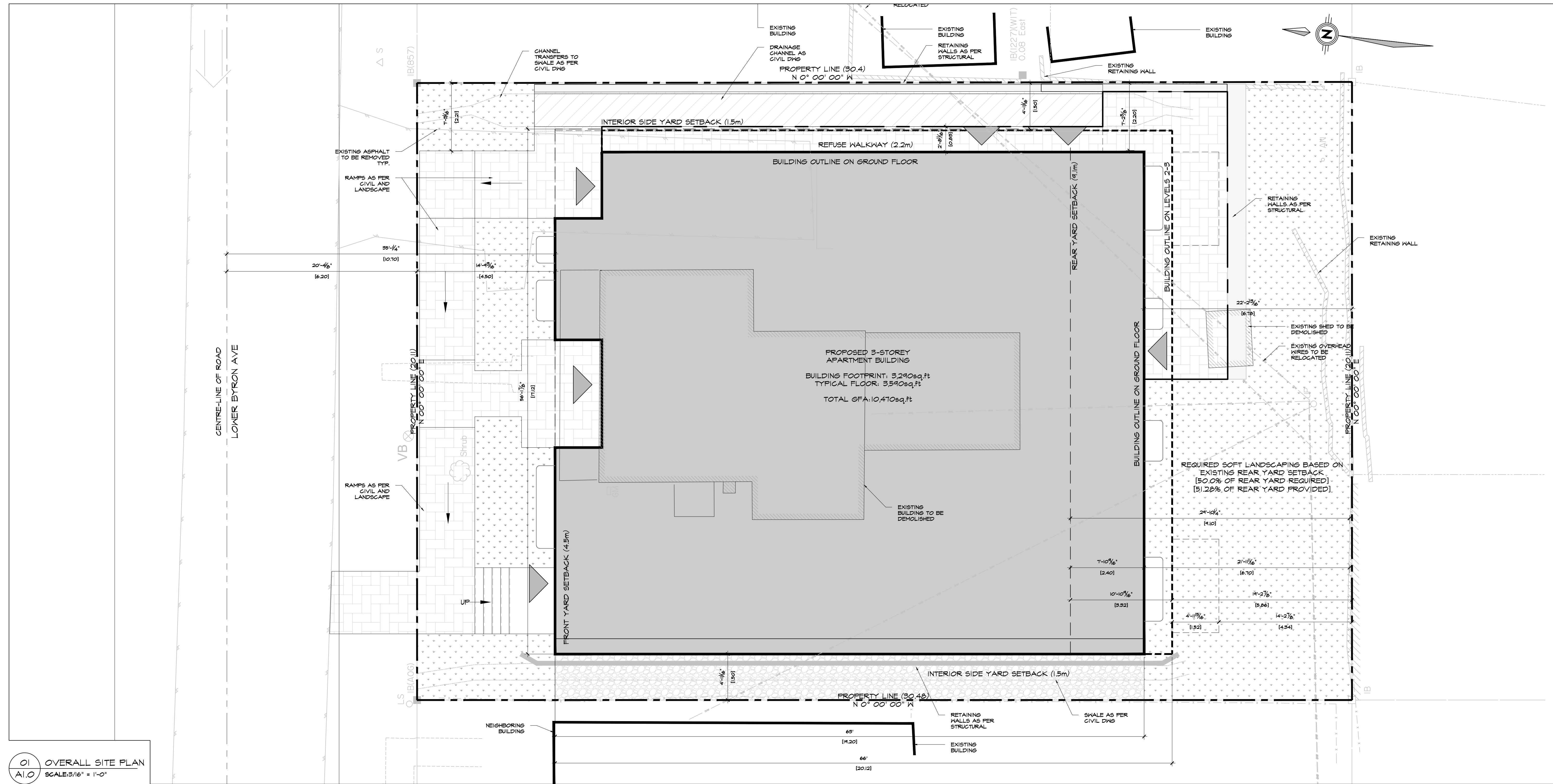


Murray Chown, MCIP, RPP
Director | Planning & Development

Appendix A:

Site Plan

NOTES:
 1) ALL WORK TO BE IN COMPLIANCE WITH LOCAL BUILDING CODES, REGULATIONS AND BY-LAWS.
 2) ADDITIONAL DRAWINGS MAY BE ISSUED FOR CLARIFICATION TO ASSIST PROPER EXECUTION OF WORK. SUCH DRAWINGS WILL HAVE THE SAME REVISION AND INTENT AS IF THEY WERE INCLUDED WITH PLANS IN CONTRACT DOCUMENTS.
 3) DO NOT SCALE DRAWINGS.
 4) ALL SUB-CONTRACTORS TO TAKE THEIR OWN ON-SITE MEASUREMENTS AND BE RESPONSIBLE FOR THEIR ACCURACY.
 5) NOTIFY SHAWN J. LAWRENCE ARCHITECT FOR ANY ERRORS AND/OR OMISSIONS PRIOR TO START OF WORK.



O1 OVERALL SITE PLAN
SCALE: 3/16" = 1'-0"

CITY OF OTTAWA ZONING BY-LAW

ZONING PROVISIONS	REQUIRED	PROPOSED
R4-UG [2685]		
MIN. LOT AREA	900sq.m	619.71sq.m
MAX. LOT AREA	N/A	619.71sq.m
MIN. LOT WIDTH	12m	20.12m
MAX. LOT WIDTH	N/A	20.12m
MIN. FRONT YARD SETBACK	4.5m	4.5m
MIN. REAR YARD SETBACK	9.12m	9.06m
MIN. INTERIOR SIDE YARD SETBACK	1.5m	1.5m
MAX. BUILDING HEIGHT	11m	10.22m
LANDSCAPING REQUIREMENTS 50% OF THE REAR YARD	42.8sq.m	44.85sq.m

BUILDING AREAS:

GROUND FLOOR = 305.6 SQ.M [3,290sq.ft]
 SECOND FLOOR = 333.5 SQ.M [3,590sq.ft]
 THIRD FLOOR = 333.5 SQ.M [3,590sq.ft]

TOTAL = 972.6 SQ.M [10,470sq.ft]

BUILDING FLOOR STATISTICS - RENTAL APARTMENT:

FLOOR	SUITES	STUDIO	1 BED.	2 BED.
LOWER LEVEL	4	2	2	0
GROUND FLOOR	4	1	3	0
SECOND FLOOR	5	1	2	2
THIRD FLOOR	5	1	2	2
TOTAL	18	5	9	4
SUITE MIX	0	28	50	22



No.	DATE	REVISION
08	2022.07.07	ISSUED FOR REZONING AND SITE PLAN CONTROL
07	2022.06.06	ISSUED FOR REVIEW
06	2021.05.05	ISSUED FOR COORDINATION
05	2021.11.02	ISSUED FOR REVIEW
04	2021.09.14	ISSUED FOR COORDINATION
03	2021.03.18	ISSUED FOR REVIEW
02	2021.03.11	ISSUED FOR REVIEW
01	2021.02.19	ISSUED FOR REVIEW

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PROJECT:
 BYRON APARTMENTS
 522 LOWER BYRON AVE, OTTAWA, ON

SHEET TITLE:
 SITE PLAN

DRAWN BY: S.L. **CHECKED BY:** S.L.

PLOT DATE: 2022.06.06

JOB NUMBER: SL-1052-21 **SCALE:** AS NOTED

SHEET NUMBER: