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Re: Site Plan Application – Design Brief
16 Edgewater Redevelopment, Ottawa

Application Submission

This summary provides supplemental information for the Design Brief as part of a Site Plan Control and Major Rezoning application for a proposed development at 16 Edgewater Street in Ottawa. The proposed development has changed considerably since the previous pre-consultation meeting in November 2020. The two tall buildings have been replaced by a single tower and the configuration of the townhouses has been changed. Other revisions made in response to the input received at the meeting. The intention is still to provide modestly sized and affordable apartment units.

Legal Description:

Part Lot 30, Concession 12 Part 1, 4R31503 in the City of Ottawa

Municipal Address:

16 Edgewater Street

Purpose of the Application:

The proposal seeks to rezone the Subject Site to a GM2[xxxx] zone to permit a planned unit development consisting of a high-rise apartment dwelling with commercial space at the base, low rise buildings with commercial on the Ground Floor and residential on the upper floors and townhomes. The site-specific exceptions permit a increased height, and the projection of a washroom, lounge, and laundry room.

Overall Vision Statement:

The overall plan for the development is to provide a range of affordable housing unit types to serve a part of the City with limited currently available affordability, in an attractive development on-site amenity spaces and an internal network of pedestrian connections.

The proposed development is for 4 buildings over a common underground parking structure.

An 10 storey mixed-use building¹ with apartments, and with ground floor commercial and common amenity space is proposed for the Edgewater frontage of the property. Three stacked townhouse buildings are located along the north and eastern sides that provide residential and live-work opportunities. The location of these buildings achieves several objectives:

- As the largest and tallest building on the site, the 10-storey building is situated furthest from the existing neighbouring residential community.
- The apartment building provides a natural buffer and noise barrier along Edgewater, as well as an edge for both the existing and proposed residential community
- Ground floor commercial space is oriented towards the Edgewater frontage with the residential building above set back from the ground floor front wall.
- Shadows cast by the apartment building will have minimal impact on neighbours to the east and north. This is supported by the shadow studies provided. The adjacent uses to the south are industrial and commercial, and will not be adversely affected by the proposal.
- Impact on neighbours from shadowing is more favourable than that which would be permitted by the existing zoning.
- Vehicle access to the buildings, and to the underground parking is limited to a part of the site to minimize impact on pedestrian movement. It is arranged to optimize the distance from Edgewater, with distance from the street to the parking access providing plenty of stacking of cars waiting for access both into and out of the site, and while limiting the heaviest traffic flow to a small part of the site.
- The pedestrian “mall” and network of walkways provides for safe internal movement throughout the site as well as a pedestrian connection to Edgewater and the surrounding area. Over time, as future sidewalks are developed along Edgewater safe pedestrian access to neighboring amenities.
- Generous interior amenity space is provided for apartment building residents with adjacent exterior amenity space and access to the pedestrian “mall”. These spaces include recreation and co-working facilities, and community laundry is provided.
- Waste handling is provided off of the main entrance driveway and a recessed loading provided for trucks to do a 3-point turn on the site and leave in a forward direction.
- Fire-fighting access will be provided with an internal fire route that complies with the Building Code for distance from hydrants and length of dead-end route.
- Visitor parking is provided in a well-landscaped court space.
- An 11m landscaped buffer with trees is provided along the eastern property line adjacent to existing residential development.
- The landscaped buffer includes a garden benefiting all residents with access off of the pedestrian “mall”.
- The stacked townhouse buildings provide both a buffer for the neighbouring from the taller building, as well as enclosing and defining an interior network of pedestrian and greens spaces that enhance the urban quality for residents of the development.

¹ The building is classified as a 10 storey building for Zoning purposes only.

Response to City Documents

Official Plan and Policies:

The Subject Site is designated General Urban Area in the City of Ottawa Official Plan (OP). The General Urban Area is defined in Section 3.6.1 of the Official Plan as:

The General Urban Area designation permits the development of a full range and choice of housing types to meet the needs of all ages, incomes and life circumstances, in combination with conveniently located employment, retail, service, cultural, leisure, entertainment and institutional uses. This will facilitate the development of complete and sustainable communities. A broad scale of uses is found within this designation, from ground-oriented single-purpose buildings to mid-rise buildings with a mix of uses along Mainstreets or Transit Priority Corridors; from a dwelling or corner store to a shopping centre or office.

The proposed building is a high-rise apartment dwelling. The proposed high-rise apartment dwelling contributes to a range and choice of housing types to meet the needs of all ages, incomes, and life circumstances. The proposed commercial use at the base of the high-rise apartment dwelling provides convenient employment, retail, and services uses. The proposed development contributes to the supply of affordable housing.

The relevant policies of the Official Plan are further discussed in the Planning Rationale.

Context Plan

Contextual Analysis:

North

Immediately to the north of the Subject Site is a low-rise commercial building.

West

To the west of the Subject Site is a heavy equipment storage yard, with frontage on Hazeldean Road. This heavy equipment storage yard is designated Arterial Mainstreet in the City of Ottawa Zoning By-law, and has a planned maximum height of 30 metres. It is likely that this property will be re-developed in the near future considering the Official Plan designation, and the existing Zoning.

South

To the south of the Subject Site is low-rise commercial building with frontage on Hazeldean Road. This commercial building is designated Arterial Mainstreet in the City of Ottawa Zoning By-law, and has a planned maximum height of 20 metres. It is likely that this property will be re-developed in the near future considering the Official Plan designation, and the existing Zoning.

East

To the east of the Subject Site is a set of low-rise semi-detached buildings.

Photographs:



Context Map.



Image 01. South Corner of Site



Image 02. West Corner of Site



Image 03. South West side of Site



Image 04. East Corner of Site



Image 05. Neighbouring Property (North East Side)



Image 06. Neighbouring Properties (North West side) on Foulis Crescent



Image 07. Neighbouring Properties (North West side) on Foulis Crescent



Image 08. Neighbouring Property on Hazeldean Road.



Image 09. Adjacent Property (South West side)



Image 10. Intersection at Hazeldean and Edgewater.



Image 11. Adjacent Property (South West side)



Image 12. Property Across the Street



Image 13. Entrance to Property Across the Street.

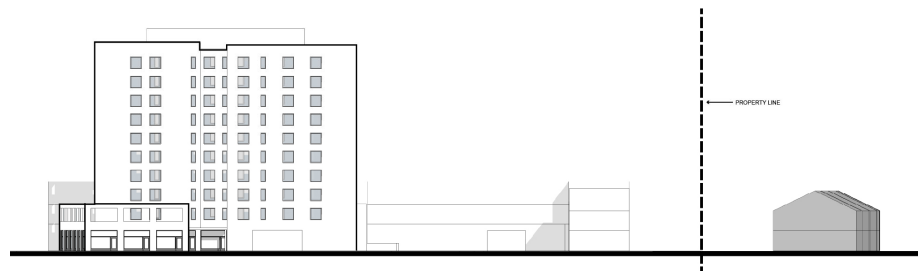


Image 14. Adjacent Property (North East Side)

Design Proposal

Building Massing

The section below shows the scale of the proposed development. The taller building has been placed closer to the West side of the site away from the residential properties. The community garden has been placed along the West side to create a buffer between the existing small scale residential properties and our proposed development. The smaller town house units have been placed after the community garden as this scale is more in keeping with the adjacent properties.



Views

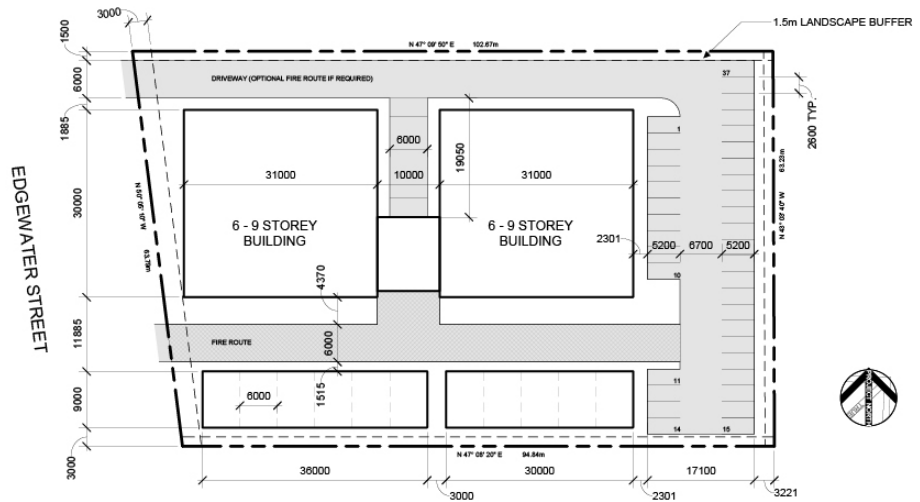
The two perspectives below show the proposed development looking in either direction along Edgewater. The two adjacent properties both have small scale buildings and are not highly developed.



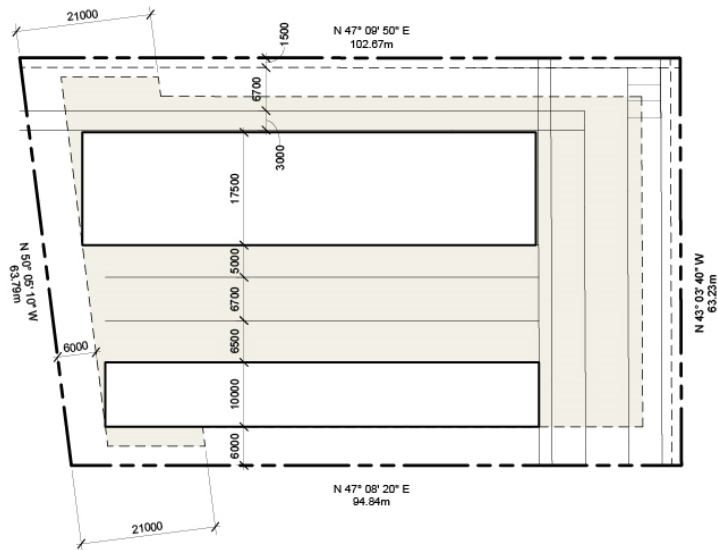
Grading

Alternative Building Massing

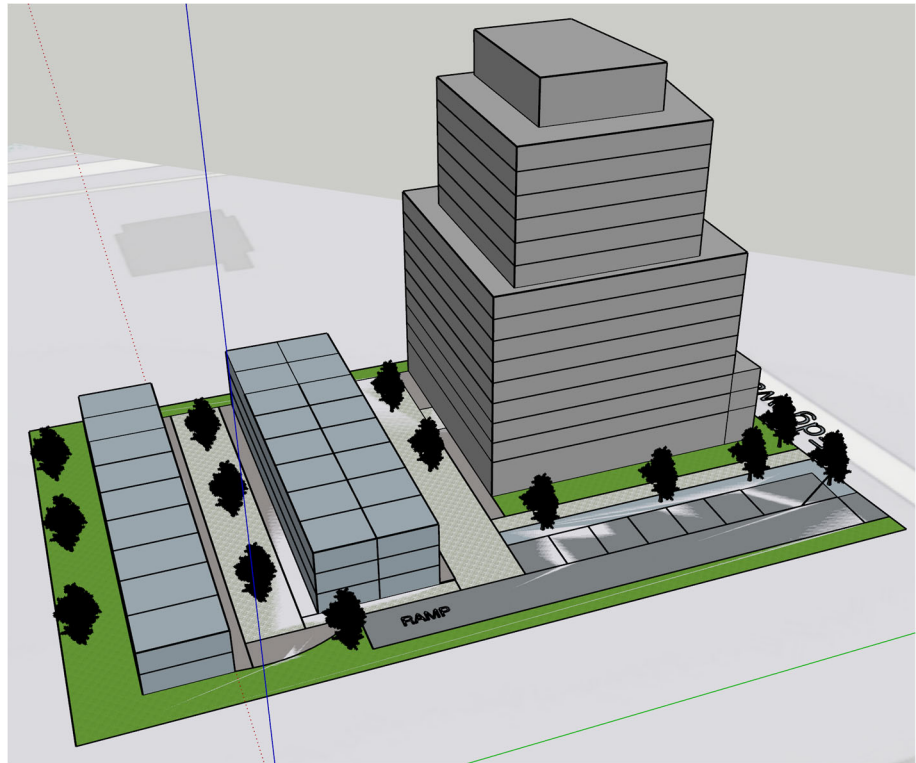
Below are some of the initial massing and site layout options explored by the design team. We began with the initial layout presented at the pre-consultation meeting that showed two tall buildings with smaller town house blocks. The designs evolved to the current proposal of one large tower situated at the corner of the site closest to Hazeldean. The smaller town house blocks wrap the site acting as a buffer to the adjacent properties. The buildings were situated to maximize outdoor space on the site to create pedestrian friendly areas and common spaces.



Option 1. This option didn't leave enough space between the two high buildings. Placing the parking next to the residential properties to the West was also not desirable. This option also required a lot of driveway to access different parts of the site, limiting the amount of potential green space. Having the townhouses next to the parking lot to the South was also not ideal.



Option 2. This was the first look at a single large building. The mass of one long building was not right for the site and created long corridors within the site and limited connections beyond. It also did not create a proper buffer to the properties to the West.



Option 3. Explored a single tall building and placed the smaller townhouse units backing onto the adjacent residential properties. A single driveway access was developed which freed up more potential for public space on the site.



Option 4. Is the closest to the design included in the submission. Minor changes included extending the Community Garden along the whole East property line. The buildings in the corner were combined and the parking lot stair access was integrated into this building to free up more space in the site.

Relationship to the Public Realm

The design seeks to create a presence along Edgewater and at the same time create community spaces within the site. The neighbouring properties are currently under-developed and do not have a strong street presence.

- Ground floor commercial space is oriented towards the Edgewater frontage with the residential building above set back from the ground floor front wall.
- Vehicle access to the buildings, and to the underground parking is limited to a part of the site to minimize impact on pedestrian movement. It is arranged to optimize the distance from Edgewater, with distance from the street to the parking access providing plenty of stacking of cars waiting for access both into and out of the site, and while limiting the heaviest traffic flow to a small part of the site.
- We are proposing adding a sidewalk along Edgewater to connect the site to Hazeldean and a large pedestrian network.

Sustainability

Following are some of the sustainable measures provided in this design:

- An exterior building form that limits windows to approximately 25% of the envelope area will reduce seasonal envelope heat losses and gains by approximately 1/3 compared to buildings that use glazed window wall systems.
- A compact building form will reduce envelope heat losses
- Small compact unit design and shared amenity space will reduce overall building area per person with associated reduction in embodied carbon and operating costs
- High level of air tightness and additional insulation where possible
- LED lighting and low flow plumbing fixtures

Accessibility (AODA)

The site design will meet all requirements of the AODA as well as site design provisions of the CSA B651 Standard for the Accessible Design of the Built Environment, including:

- Barrier-free access to building entrances, pedestrian routes onto the site, and between vehicle drop off and parking to building entrances, and access from parking areas.
- Safety measures (such as tactile warning surfaces) at points where pedestrian routes meet or cross vehicle routes
- Adequate site lighting
- Required barrier-free parking spaces and associated access aisles

Building Design

Building drawings including, Site Plan, Floor Plans and Elevations have been included in the drawing package.