

# Planning Rationale

## Tartan Carp Subdivision



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Submitted by:

The Estate of Douglas Gordon Rivington on behalf  
of Tartan Homes.

October 18, 2021

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# 1 Introduction

## 1.1 Background

Tartan Land Consultants Inc. is the planning consultant for the Estate of Douglas Gordon Rivington ('owner') who own the lands legally described as Part of Lot 17, Concession 2, Geographic Township of Huntley, City of Ottawa. The property is 25.37 hectares in size and for the purposes of this Planning Rationale is divided into two sections. The north section of the property is 17.36 hectares and are lands that will be retained as Natural Environment Area. The south section of the property is 8.01 hectares and are development lands ('subject site').

The subject site is designated *Village* in the City of Ottawa Official Plan and is zoned *Development Reserve 3* in the City of Ottawa zoning bylaw 2008-250. The owner is proposing to develop the subject site as a residential subdivision. An application for a Plan of Subdivision and a Zoning Amendment is required to facilitate the proposed development.

The purpose of this Planning Rationale is to evaluate the proposed development in the context of relevant policy and regulatory frameworks and to provide justification for the proposed development being in accordance with good planning principles and is compatible with the surrounding area.

## 1.2 Pre-Consultation

A pre consultation meeting was held December 16<sup>th</sup>, 2020 with City of Ottawa staff. Meeting minutes dated February 1, 2021 were prepared by Adam Brown of the City of Ottawa and is provided as Appendix A. The meeting minutes identify the plan and study requirements for the planning applications. The following plan and studies are submitted in support of the applications:

- Draft Plan of Subdivision;
- Assessment of Adequacy of Public Services;
- Transportation Impact Assessment;
- Noise Control Feasibility Study;
- Geotechnical Investigation Study;
- Phase 1 Environmental Site Assessment;
- Environmental Impact Study and Tree Conservation Report;
- Headwater Drainage Feature Assessment;
- Archeological Study; and a
- Combined Planning Rationale, Design Brief and Integrated Environmental Review.

A summary of the above noted studies are provided in Section 5 of this Planning Rationale.



## 2 Site Context

### 2.1 Location and Site Description

Located within the Village of Carp, the subject site is legally described as Part of Lot 17, Concession 2, Geographic Township of Huntley, City of Ottawa. The civic address is 232 Donald B. Munro Drive. For the purposes of the report, we assume Donald B. Munro Drive is an east-west direction.

The subject site has a total land area of 8.01 hectares. The current use of the subject site is vacant lands. Existing site conditions include stripped topsoil and soil stockpiles from the residential development to the south. Along the north portion of the site are deciduous forests. There is a deciduous hedgerow abutting existing residential on the western boundary of the site and in the south central boundary along Donald B. Munro Drive. Overall, the terrain is relatively flat along the southern portion of the site with an upward slope towards the treed area to the north. There is a central water channel that runs north – south. Flows in the channel stem from the base of the bedrock and flows south where it enters a storm grate located on the north side of Donald B. Munro Drive.

### 2.2 Surrounding Area

The surrounding area can be described as having a mix of land uses. The following are key existing and planned land uses in the vicinity of the subject site:

**North:** The subject site is bounded to the north by a Natural Environment Area (NEA) which is a forested area with rocky outcrops and which forms part of the Carp Hills Natural Area.

**South:** The subject site is bounded by Donald B. Munro Drive to the south. The Green Meadows subdivision is located on the south side of Donald B. Munro Drive and is mostly developed with single detached, semi-detached and townhouse units and Doug Rivington park. The Renfrew Rail Corridor is an active railway owned by the City of Ottawa and is approximately 250 metres south of the subject site. The Carp River Corridor is approximately 400 metres south of the subject site.



*Photo 1: View looking north from Donald B. Munro Drive*



*Photo 2: View looking west on Meadowridge Circle, Green Meadows Subdivision*

**East:** To the east of the subject site is a single residential dwelling unit and treed lands that are designated as General Rural Area.

**West:** To the west of the subject site are existing single detached residential homes.



*Photo 3: View looking east from Donald B. Munro Drive*



*Photo 4: View looking north on Robertlee Drive, existing homes to the west of the subject site*

Located within the Village of Carp are a number of community amenities including schools, parks, environmental areas and community centers. Key community amenities in proximity to the subject site are shown on Figure 1.



*Figure 1: Surrounding Community Amenities*

The surrounding transportation network is provided as Figure 2. Donald B. Munro Drive is an undivided collector road that runs east to west and has a posted speed limit of 60 km/h along the subject site which is then



reduced to 40 km/h at the western boundary of the site. The closest arterial roads are March Road located approximately 1km to the east and Carp Road located approximately 1km to the south. There are no cycling facilities on the roadways abutting the subject site. There is an existing path located south of the subject site which terminates at Doug Rivington Park. Existing pedestrian facilities include a concrete sidewalk on the south side of Donald B. Munro Drive.



Figure 2: Road Classification and Existing Paths

OC Transpo Route #303 is the only transit service route in the Village of Carp. OC Transpo Route #303 operates on Wednesdays only and is a 'Shopper Route'. A single trip morning service and a single trip afternoon service is provided between Dunrobin / Carp and shopping areas located at Kanata Centrum, Bayshore, Lincoln Fields and Carlingwood. The closest bus stop is located approximately 200m west of the subject site at the Donald B. Munro Drive and Robertlee Drive intersection.

Overall, transit services, pedestrian and cycling linkages are limited in the Village of Carp.

### 3 Proposed Development

The owner is proposing a residential subdivision containing single detached, semi-detached and townhouse units. The Draft Plan of Subdivision is provided as Figure 3 (full version as Appendix B) and the Schedule of Land Use is provided in Table 1. The development objectives include: enhancing the sites strategic entry location into the Village of Carp with an appropriate built form; suitable architectural expressions that compliments the rural character of Carp; that is pedestrian friendly; and that incorporates sustainable design features.



Figure 3: Draft Plan of Subdivision

Table 1: Schedule of Land Use

Land Use	Lot/Block numbers	Area (ha)	% of total land	Number of units
Single detached units	Lots 1 to 57	4.306	54%	57
Semi-detached units	Blocks 61, 64 and 73	0.264	3%	6
Townhouse units	Blocks 58, 59, 60, 62, 63, 65 to 72, 74 and 75	1.590	20%	54
Walkways	Blocks 76 and 77	0.338	4%	n/a
Streets/Road Widening	Streets 1, 2 and 3	1.512	19%	n/a
<b>Total</b>		<b>8.01</b>		<b>117</b>



## **Residential Uses**

There are a total of 117 residential units which include 57 single detached units (49% of total units), 6 semi-detached units (5% of total units) and 54 townhouse units (46% of total units). The total area of residential lots and blocks is 6.16 hectares. Site density based on the aforementioned unit count is 18.99 units per net hectare. The proposed residential units will be traditional ground and street-oriented homes. Typical block depths are a minimum 32.50 metres for semi-detached and townhouse units and a minimum of 33.00 metres for singles detached units. Lots backing onto the NEA lands have lot depths between 49.03 metres and 69.87 metres which will accommodate a 15-metre naturalized buffer within the development lots. Typical lot frontages for single detached units are 10.95 metres, 11.93 metres and 14.65 metres. Semi-detached units have a minimum lot frontage of 6.675 metres and townhouse units a minimum lot frontage of 6.0 metres.

The streetscape will have a varied built form and façade options, various native tree species in the front yards, and varying setbacks and porch protrusions along the street. Garages will not project beyond the front face of the building. Corner units will have enhanced brick treatments.

There is a transition from low to high density with the lowest density units being located in the north and west portions of the subject site adjacent to existing residential and abutting NEA lands. Lots directly backing onto existing residential have a minimum lot depth of 39.12 metres and frontage of 14.65 metres ensuring compatibility in lot sizes. The higher density semi-detached and townhouse units are located in the south and east section of the subject site, nearest to the collector road.

All residential units have a minimum of two parking spots per unit. All single detached units have a double garage and driveway sized for two parking spots. All semi-detached units have a parking spot in an oversized single garage and up to two tandem parking spots in the driveway. All townhouse units have a parking spot in the garage and up to two tandem parking spots in the driveway. The typical length of driveways are 10.75 metres to the curb or 8.95 metres to the sidewalk where one is present.

The topography of the subject site may pose some challenges. Retaining walls and terracing will likely be required in some areas. There may also be opportunities for walk out residential units. The impacts of the topography will be reviewed in greater detail at the detailed design stage.

## **Parks and Greenspace**

Doug Rivington Park is located approximately 100 metres south of the subject site in the Green Meadows subdivision, and is intended to service the proposed development. Doug Rivington Park can be accessed via a walkway block on the south side of Donald B. Munro Drive which is opposite of walkway block 76 on the Draft Plan of Subdivision. Blocks 76 and 77 on the Draft Plan of Subdivision are a 10.0 metre pathway block which provides pedestrian connectivity to Doug Rivington Park to the south and the NEA lands to the north. A 2 to 3 metre wide stone dust pathway is proposed to be built in blocks 76 and 77. The stone dust pathway will be designed in a manner that maximizes tree retention and that provides nature trail characteristics thereby providing an appropriate transition to the informal trails in the NEA lands to the north. Block 76 will additionally serve as a buffer between the new and existing community.

As part of the subdivision agreement for the Green Meadows subdivision, an over-dedication of parkland was provided. The relevant condition in the Green Meadows subdivision states:

"In recognition of the over-dedication of parkland by the Owner, the City agrees to transfer the parkland dedication in excess of 5% to the future development of other lands owned by the Owner described as



Part of Lot 17, Concession 2, Geographic Township of Huntley, City of Ottawa being Part 1 on Plan 4R-7027".

As agreed, between the City of Ottawa and the owner of the Green Meadows subdivision, the over-dedication of parkland would transfer to the parkland dedication requirements of the subject site. As a result, parkland dedication nor Cash-In-Lieu of Parkland are required for the proposed development.

## Transportation

The subject site has 339.27 metres of street frontage on Donald B. Munro Drive. There are two proposed new intersection on Donald B. Munro Drive at Streets No. 1 and No. 2. The proposed internal road network is comprised of local roads that have an 18 metre right-of-way. A typical 18 metre right-of-way cross section is provided as Figure 4.

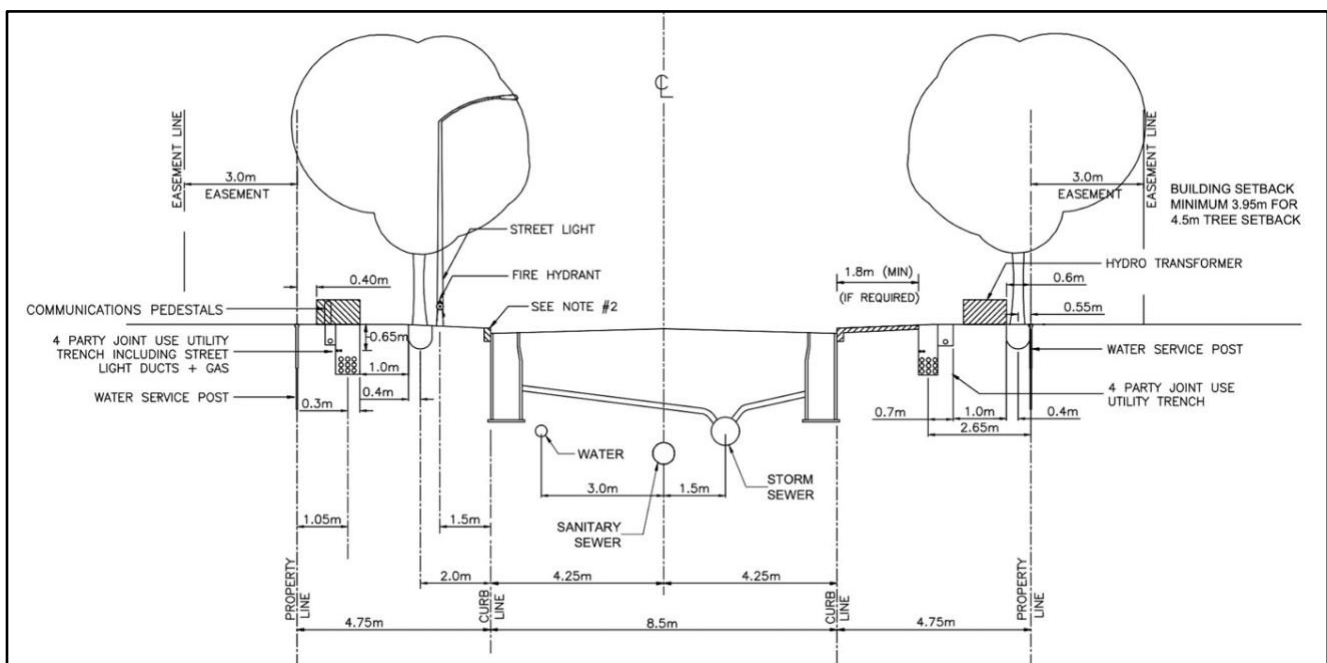


Figure 4: Typical 18 metre right-of-way cross section

The proposed development is planned to be designed with quality pedestrian facilities that provide access to greenspace and amenities in the surrounding community (see Figure 5). A sidewalk is proposed on the north side of Donald B. Munro Drive, on the south and west side of Street No. 1, and the east side of Street No. 2. A 2 to 3 metre wide stone dust pathway is proposed within blocks 76 and 77 as it is expected these pathway blocks will not be winter maintained.

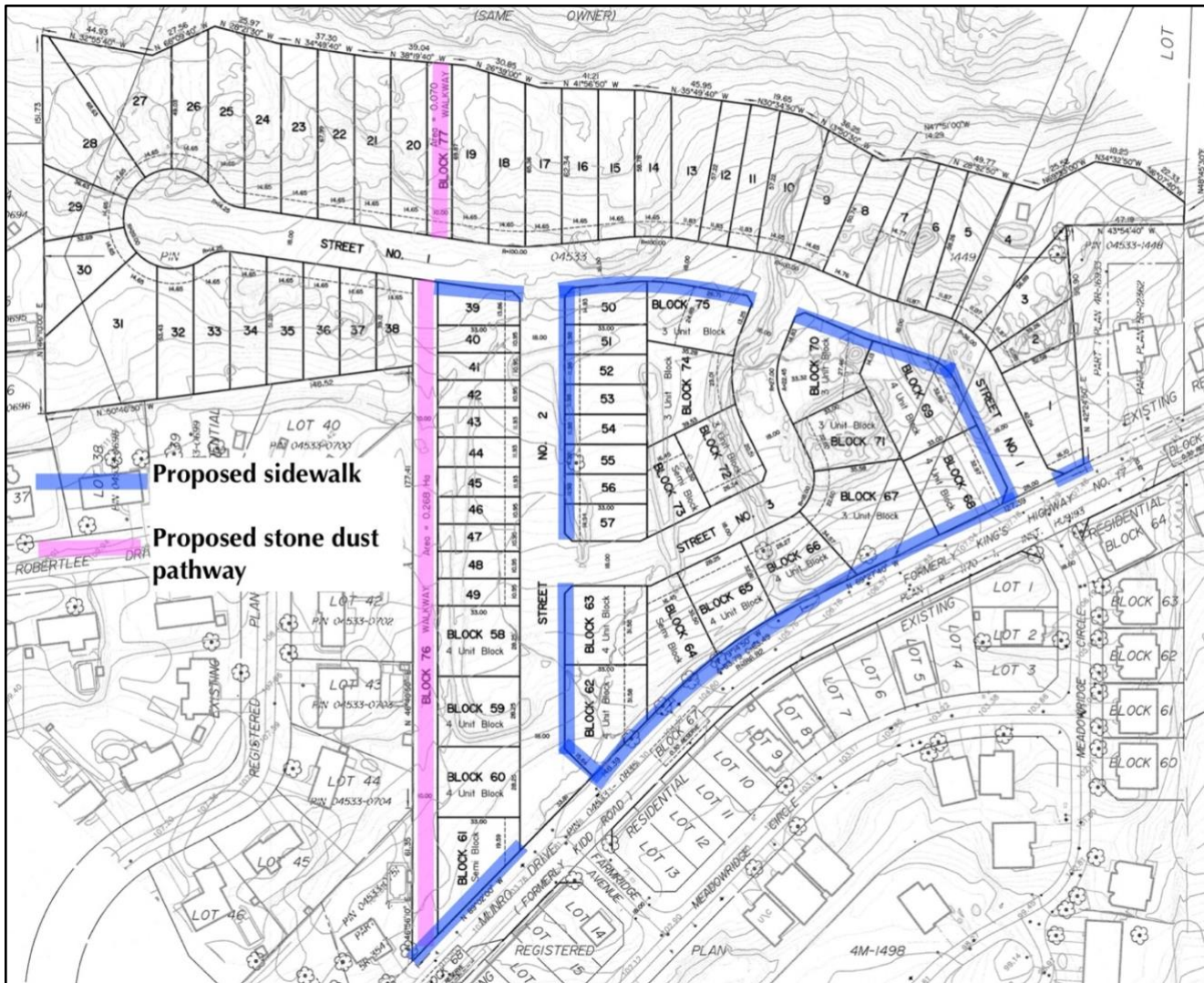


Figure 5: Proposed Pedestrian Facilities

## Site Servicing

Conventional site servicing and storm water management are proposed for the subject site and is discussed in further detail in Section 5.4 of this Planning Rationale.

The proposed water servicing plan indicates the site will be serviced internally by 200mm watermain and will connect at Street No. 1 to the existing 200mm diameter watermain in Donald B. Munro Drive and Street No. 2 at the existing watermain stub.

The proposed wastewater plan indicates the site will be serviced internally by 200 mm diameter sanitary sewers. There are two proposed connections to the existing 250 mm diameter sewer in Donald B. Munro Drive. One connection at Street No. 2 will service the western portion of the site. The second connection at Street No. 1 will service the east portion of the site and will require a new 200 mm diameter sewer running westward along Donald B. Munro Drive.



The proposed minor storm plan indicates the site will be serviced internally by storm sewers. The majority of the site would drain from Street No. 2 into the existing 1050 mm diameter sewer in Donald B. Munro Drive. The eastern portion of the site would drain from Street No. 2 into Donald B. Munro Drive where the existing 250 mm diameter storm sewer will need to be replaced with a new 450 mm and 525 mm diameter sewer which would connect to the existing 1050 mm diameter sewer. The minor storm sewers would capture flows from the NEA lands to the north by constructing a new drainage swale along the edge of the NEA lands. Minor runoff from the subject site will discharge into the Green Meadows minor storm sewer and will pass through an oil and grit separator prior to being released to the Carp River.

It is acknowledged that water and sewer capacity constraints external to the site will require remediation / upgrades prior to the commencement of development. The City of Ottawa are currently assessing options for interim and permanent solutions.

It is expected the subject site will be serviced in one phase.

## 4 Policy and Regulatory Framework

The following section sets out the planning policy framework, regulations and guidelines applicable to the proposed development. The proposed development is assessed in the context of relevant policies and regulations.

### 4.1 Provincial Policy Statement, 2020

The Provincial Policy Statement, 2020 (PPS), issued under Section 3 of the *Planning Act*, came into effect on May 1, 2020. The PPS provides policy direction on matters of provincial interest related to land use planning and development in the Province of Ontario. Under the *Planning Act*, decisions that affect planning matters “shall be consistent with” policy statements issued under the Act. The proposed development was reviewed against the PPS in its entirety with relevant policies discussed in detail below.

Under Section 1.0 - *Building Strong Healthy Communities* of the PPS, policies are provided for managing change and direction to achieve efficient land use and development patterns to support community, environment and economic sustainability.

*Policy 1.1.1* calls for healthy, liveable and safe communities by promoting efficient development and land use patterns that are economically cost efficient and sustainable; that provide affordable and market-based range and mix of housing and other uses such as employment and recreation; that provide environmentally sound and safe development which also considers a changing climate; and through development that provides necessary infrastructure and facilities that is accessible and which meets both current and projected needs.

Section 1.1.3 of the PPS provides policies related to settlement areas that promote the appropriate and efficient use of land and resources.

*Policy 1.1.3.1* states that settlement areas shall be the focus of growth and development.

*Policy 1.1.3.2* states that land use patterns within settlement areas shall be based on densities and mix of land uses that support the efficient use of land, infrastructure and public service facilities, active transportation, transit, energy efficiency and that minimize negative impacts to climate change.

*Policy 1.1.3.6* states that new development should take place adjacent to existing built up areas and should be compact in form and contain a mix of uses and densities to allow for the efficient use of land, infrastructure and public service facilities.

Section 1.1.4 of the PPS provides policies related to rural areas in municipalities. The most relevant policies are provided below.

*Policy 1.1.4.1 Healthy, integrated and viable rural areas should be supported by:*

- a) building upon rural character, and leveraging rural amenities and assets;*
- c) accommodating an appropriate range and mix of housing in rural settlement areas;*
- e) using rural infrastructure and public service facilities efficiently;*



*h) conserving biodiversity and considering the ecological benefits provided by nature; ...*

*Policy 1.1.4.2 In rural areas, rural settlement areas shall be the focus of growth and development and their vitality and regeneration shall be promoted.*

Section 1.4 of the PPS provides policies related to Housing. The primary policy direction is for the provision of a range and mix of housing options and density that are required to meet the projected market-based and affordable housing needs of current and future residents of the regional market area. Planning authorities shall permit and facilitate new housing that promote densities that efficiently use land, resources and infrastructure, is compact in form and which minimize the cost of new housing.

*Policy 1.4.3* requires an appropriate range and mix of housing options and densities to meet projected housing needs that are market-based and affordable for both current and future residents of the regional market area. Planning authorities shall permit and facilitate new housing that promote densities that efficiently use land, resources and infrastructure, is compact in form and which minimize the cost of new housing.

Response: The subject site is located in a designated *Village* settlement area on *Schedule A Rural Policy Plan* of the City of Ottawa Official Plan. The proposed development is located adjacent to a developing residential subdivision to the south and an existing residential development to the west, making it a logical and efficient expansion of development. The proposed development provides for a range of housing types and mix of densities that supports efficient and sustainable development and makes the best use of a vacant land parcel. The variety of ground oriented units will help meet the current and projected housing needs of residents by contributing to a greater range of housing price points that accommodate a greater range of incomes and demography. Development of the subject site can be supported by existing and planned infrastructure upgrades in the Village of Carp. The subject site provides key linkages to meet the recreational needs of residents. The NEA lands to the north are being conserved and protected.

*Policy 1.5.1 Healthy, active communities should be promoted by:*

- a) planning public streets, spaces and facilities to be safe, meet the needs of pedestrians, foster social interaction and facilitate active transportation and community connectivity; ...*

Response: Active transportation will be supported within the subject site with the installation of sidewalks in key locations and by providing pathway connections to NEA lands to the north and to the pathway leading to Doug Rivington Park in the Green Meadows subdivision to the south.

*Section 1.6.6 Sewage, Water and Stormwater* provide policies that identify the preferred means and methods of providing services in a manner that is efficient and optimizes existing and planned services.

Response: The City of Ottawa has planned water and sewer infrastructure upgrades to accommodate the expansion of urban development in the Village of Carp. Once the infrastructure upgrades have been completed, the subject site can be serviced in accordance with policies in Section 1.6.6 of the PPS, as demonstrated in the Conceptual Servicing and Stormwater Management Report which is summarized in Section 5.4 of this Planning Rationale.

Under *Section 2.0 Wise Use and Management of Resources*, policies encourage the protection of natural heritage, water, agricultural, minerals and petroleum, mineral aggregate resources and cultural heritage and archaeological resources for their economic, environment and social benefits.

Response: The subject site is consistent with the policies under Section 2.0 *Wise Use and Management of Resources* of the PPS. The subject site does not contain any natural heritage features, with the exception of butternut trees for which compensation will be provided in accordance with provincial and federal requirements. The subject site does not contain any significant water features. The subject site is not a prime agricultural area and is not subject to policies relating to Agriculture. The subject site is not located in an area with known Mineral and Petroleum resources nor mineral aggregate resources. The subject site is located in an area of archaeological potential. An archaeological assessment completed in support of the proposed development found that there was nothing of archaeological concern on the subject site.

Under *Section 3.0 Protecting Public Health and Safety*, policies encourage reduction of the potential for public cost or risk from natural or human-made hazards.

Response: The subject site is consistent with the policies under *Section 3.0 Protecting Public Health and Safety* of the PPS as it is not located near natural or human-made hazards.

## **4.2 City of Ottawa Official Plan**

The City of Ottawa Official Plan (“Official Plan”) was adopted November 10, 2003 and provides a vision and policy framework to guide the future growth of the City of Ottawa. There have been a number of modifications and amendments to the Official Plan. In 2019, the City of Ottawa began the process of developing a new Official Plan, which is currently in its final stages of approval and is expected to be adopted by Council on October 27<sup>th</sup>, 2021. Transition policies for the new Official Plan have been released which states if an application for a Zoning By-law Amendment and Plan of Subdivision is received prior to the new Official Plan being adopted, it will be processed on the basis of the existing Official Plan, provided it is consistent with the 2020 Provincial Policy Statement. The applications for the proposed development will be submitted prior to the adoption of the new Official Plan and as a result will be assessed against the existing Official Plan.

The subject site is located in the Village of Carp which is subject to *Section 4.1 Site-Specific Policies and Secondary Policy Plans* as it is located within the Carp Community Design Plan (“CCDP”) area. Relevant Official Plan policies that are further detailed in the CCDP will be discussed in section 4.3 of this Planning Rationale.

*Schedule A Rural Policy Plan* in the Official Plan designates the subject site as *Village* (Figure 6). *Section 3.7.1 Villages* of the Official Plan provide policies to guide growth in the villages located throughout the City’s rural area and aims to preserve the villages and their traditional functions. Relevant policies include:

*Policy 3.7.1.10 A wide range of housing forms to meet the needs of the Village’s population will be permitted in Villages. The form and scale of development will be limited by the available servicing methods and subject to the policies of Section 4.4 on water and wastewater servicing...*

*Policy 3.7.1.11 The City will encourage the achievement of affordable housing targets in villages, as defined in Section 2.5.2, to the extent that servicing methods allow for a variety of housing forms.*



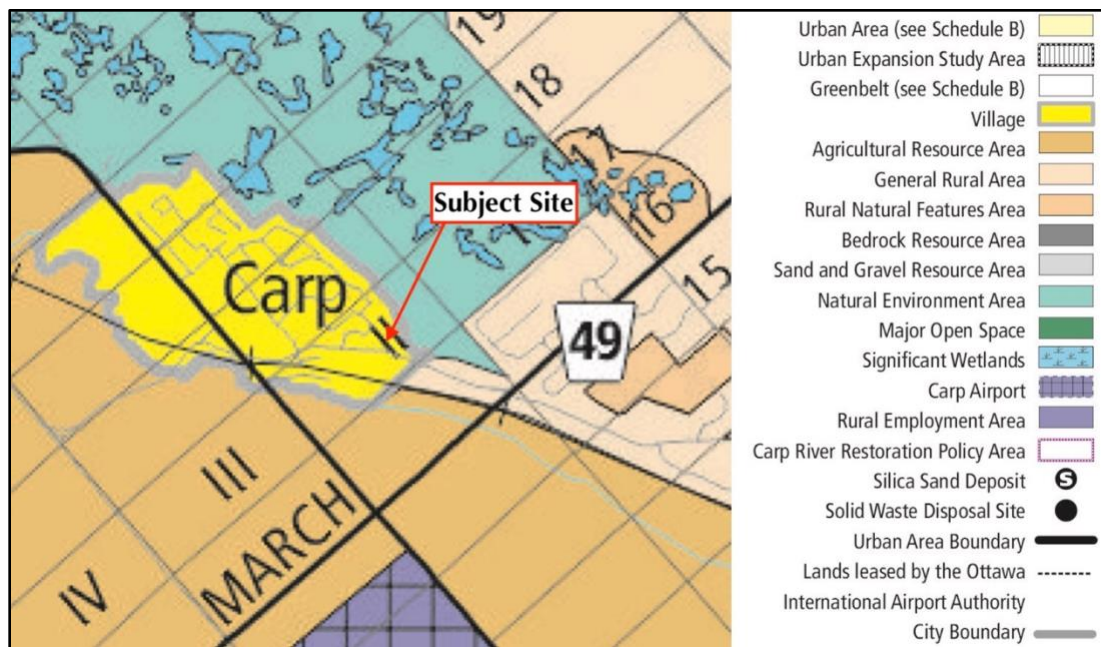


Figure 6: City of Ottawa Official Plan, Schedule "A", Rural Policy Plan

Response: The proposed development meets the intent of the *Villages* designation by providing a range of dwelling unit types and densities which include single detached, semi-detached and townhouse units. The inclusion of townhouse units will provide a lower housing price point and will contribute to housing choice in low density residential areas.

Section 3.2 – *Natural Environment* of the Official Plan are environmental designations on Schedules A and B and include lands identified as having high environmental value such as Significant Wetlands, Natural Environment Areas (NEA), Rural Natural Features, and Urban Natural Features. Protection of these environmental systems are addressed through policies in the Official Plan. Section 4.7.4 – *Protection of Endangered and Threatened Species* provide policies for the protection of these species and their habitat. Section 4.7.8 – *Environmental Impact Statement* is required adjacent to Natural Environment Areas where site alterations within 120 metres of the boundary are required. Site alterations are only permitted when an Environmental Impact Statement can demonstrate that there will be no negative impact.

Response: The Carp Hills are significant woodlands which abut the subject site to the north and are designated as a *Natural Environment Area* on Schedule B of the Official Plan. An Environmental Impact Statement was prepared for the subject site and is summarized in section 5.3 of this Planning Rationale. The only species at risk found to be present on the subject site are Butternut trees. Compensation for the removal and harm of Butternut trees will be required in accordance with provincial and federal regulations. The Environmental Impact Statement demonstrates that there is no negative impact on the ecological functions and natural features within the area provided the recommended mitigation measures and setbacks to the NEA lands are properly implemented.

Additional policy consideration as it relates to the NEA lands include:

*Policy 3.2.2.9 The boundaries of land designated Natural Environment Area are based on a variety of more detailed mapping sources. When more up-to-date information becomes available through such means as detailed environmental studies, an adjustment to the definition of features and functions and an adjustment*

*to the interpretation of the boundary may be warranted. The City's zoning by-law will reflect the most up-to-date and accurate information and therefore any changes, whether minor or major, will require an amendment to the zoning by-law. However, minor changes to the boundaries will not require an amendment to the Official Plan. Where boundary interpretation impacts provincially significant Areas of Natural and Scientific Interest, identified on maps prepared by the Ministry of Natural Resources, the agreement of the Ministry will be required. [Amendment #76, OMB File # PL100206, July 21, 2011.]*

Response: The boundary of the NEA lands were defined as a result of detailed mapping, environmental studies, and a 'boundary flagging' exercise that took place on May 28<sup>th</sup>, 2021 by City of Ottawa biologists and Bernie Muncaster, a biologist who is the author of the Tree Conservation Report and Environmental Impact Statement submitted in support of the proposed development. As a result of the review, minor changes to the boundary of the NEA lands is required and is depicted as the north boundary line for the proposed development. The adjustment to the NEA line will not require an Official Plan amendment and will be reflected as the zoning line between development and NEA lands.

In accordance with Section 4.8.2 *Source Water Protection*, a Protection of Aquifer study is required in areas identified as being within a Wellhead Protection Area on Schedule K of the Official Plan. Certain land uses within a Wellhead Protection Area may be restricted if they are determined to be a threat to municipal drinking water. Lands within the Village of Carp that are located within the Wellhead Protection Area are subject to the Mississippi-Rideau Source Protection Plan.

Response: The subject site is located within a Wellhead Protection Area and has a vulnerability score of 4 and 6 (Wellhead Protection Area – 6 or WHPA-6). In WHPA-6 areas, the Mississippi-Rideau Source Protection Plan prohibits the use of some types of chemicals by new businesses, requires Risk Management Plans for some types of chemical use by existing businesses, and identifies voluntary good stewardship measures such as how residents, businesses and governments can protect groundwater. The proposed development does not contain land uses that are a threat to municipal drinking water and are not subject to the WHPA-6 restrictions in place as provided in the Mississippi-Rideau Source Protection Plan. An aquifer study was not required by the City of Ottawa as part of the submission package.

Section 4.9 *Energy Conservation Through Design* provide policies that encourage consideration of energy conservation through landscaping, road layout and general site design. Relevant policies for energy conservation include road layouts that maximize solar exposure through street and building orientation, consideration of alternative energy systems, landscaping using trees that are native to the area, are low water demand and that provide shade, and the use of permeable surfaces where possible to reduce heat retention and which maximizes natural infiltration of stormwater.

Response: The proposed development maximizes east-west roads to allow for many homes to have south facing windows. Native trees will be planted along the street boulevards which will provide a street canopy and shading along the street. Boulevards will be grassed. A mix of building materials including stone, brick, wood and siding will be used which are suitable for the climate and are low maintenance. The builder provides upgrade options to prepare the home for easy installation of solar panels by the homeowner.

Section 4.10- *Greenspace requirements* provide policies related to parkland dedication requirements.

*Policy 4.10.7 The City may require payment-in-lieu of the parkland dedication: where there is insufficient land within the development; where the lands to be dedicated are not appropriate for park development; or where open space and parkland targets have already been met...*

Response: The proposed development is located in an area where open space and parkland targets have been determined through the CCDP or have already been met. As part of the subdivision agreement for the Green Meadows subdivision, an over-dedication of parkland was provided with the over-dedication amount being transferred to the parkland dedication requirements of the subject site. In accordance with the CCDP, a park is not required to be provided on the subject site.

Section 4.3 *Walking, Cycling, Transit, Roads and Parking Lots* provide policies that address the interrelationship between transportation and land use to ensure the transportation network is able to support the new development. Additionally, *Schedule H - Road Network - Select Villages* identifies Donald B. Munro Drive as an existing collector Road; *Schedule J –Cycling, Multi-use Pathways and Scenic Entry Routes (Rural)* identifies a Multi-Use Pathway system 250 metres south of the subject site and Donald B. Munro Drive as being a Scenic Entry Route; and *Annex 1- Road Classifications and Right-of-Width (ROW)* identifies a ROW protection of 26 metres for Donald B. Munro Drive between Farmridge Avenue and March Road and 23 metres between Langstaff Drive and Farmridge Avenue.

Response: The subject site is located along Donald B. Munro Drive which is an existing collector street with a protected Right-of-Way (ROW) of 23 metres west of Farmridge Avenue and 26 metres east of Farmridge Avenue. A road widening block will be added to the registered Plan of Subdivision to ensure the required ROW protection is secured for and provided to the City of Ottawa. Policies related to Scenic-Entry Routes will be discussed in detail in Section 4.3 of this Planning Rationale report. To facilitate active transportation and provide linkages to existing and planned pedestrian and cycling networks, two pathway blocks are provided along with sidewalks proposed to be located along the north side of Donald B. Munro Drive, the east side of street No. 2 and the south and west side of Street No. 1.

Section 2.5.1 *Designing Ottawa* discusses means to achieve compatible development and provides design objectives in development. Compatible development is identified as development that enhances established communities and does not cause adverse impacts on the surrounding area while urban design focuses on building quality spaces for people. This Planning Rationale gives consideration to the following design objectives:

1. *To enhance the sense of community by creating and maintaining places with their own distinct identity;*
2. *To define quality public and private spaces through development;*
3. *To create places that are safe, accessible and are easy to get to, and move through;*
4. *To ensure that new development respects the character of existing areas;*
5. *To consider adaptability and diversity by creating places that can adapt and evolve easily over time and that are characterized by variety and choice;*
6. *To understand and respect natural processes and features in development design;*
7. *To maximize energy-efficiency and promote sustainable design to reduce the resource consumption, energy use, and carbon footprint of the built environment.*

Response: The design and built form of the proposed development is consistent with the CCDP thereby helping to create a distinct community. The dwelling units are street oriented and will frame the street and help define private spaces. A mix of building materials including stone, brick, wood and siding along with trees planted along the ROW will contribute to an appealing streetscape. Along with the proposed pathways, many of the local streets will contain sidewalks that provide areas that are easy to move through and which connect to the



adjacent development area to the south and the NEA lands to the north. The street orientation of the dwelling units will provide eyes on the street making it a safer community. In addition to providing similar street layouts of adjacent developments, the proposed development compliments the existing area by developing at densities similar to those of the surrounding area. The varying housing types will help meet the needs of residents with different income levels, lifestyle and household types over time. The street pattern is efficient which reduces land consumption and provides for sustainable development.

Section 4.11 – *Urban Design and Compatibility* policies are provided to ensure consideration of compatible development between new and existing development. The policies in Section 4.11 of the Official Plan establish the requirement of high quality urban design in all parts of the city. The policies relevant to the proposed development are addressed below.

**Building Design:** Good building design will be achieved through high quality architectural and urban design elements for all dwelling units. Similar to the surrounding community, the dwelling units will include elements such as large glazed windows fronting the street, upgraded brick façades on corner units, varied roof lines, and front entrance porches which will enhance the interaction between public and private space and create a pleasant pedestrian environment. Pedestrian connections via sidewalks and walkway blocks will be provided to connect to the amenities within the Village of Carp and will promote active transportation.

**Massing and Scale:** The proposed development is similar in both massing, scale and building setbacks with the developing community to the south. The community to the west of the of the subject site are single detached dwelling units on large lots. To ensure compatibility with the existing community to the west, extra lot depth is provided for lots backing directly onto existing properties and a 10-metre wide pathway block is provided to create a buffer between the existing homes and the proposed development. To ensure a proper transition in density, the low density single detached units are located in the western section of the plan while the higher density semi-detached and townhouse units are located in the east and south section of the site. The proposed dwelling units will vary between one and two storeys in height.

**Outdoor Amenity Areas:** In addition to vast amount of outdoor green space abutting the subject site to the north, each residential unit will have a minimum rear yard setback of 7.5 metres providing quality private outdoor amenity space. The lots backing onto the NEA lands have extra lot depth where it incorporates a 15-metre buffer for tree retention where deciduous forests are present.

The proposed development is in conformity with all other relevant policies in the City of Ottawa Official Plan.

### **4.3 The Carp Community Design Plan**

The Carp Community Design Plan (“CCDP”) provides policies and guidelines for the long-term growth and development of the Village of Carp. The policies and guidelines reflect the vision held by its residents which include a village that is rich in heritage and unique rural character, is a great place to live and visit, that is safe and healthy, and that is committed to sustainable development.

The CCDP Land Use Plan provides a conceptual distribution of land uses, roadway locations, and of community facilities and amenities locations. The subject site is designated as *Residential – One and Two Unit Dwellings* and *Open Space* along the western boundary, as shown on Figure 7.

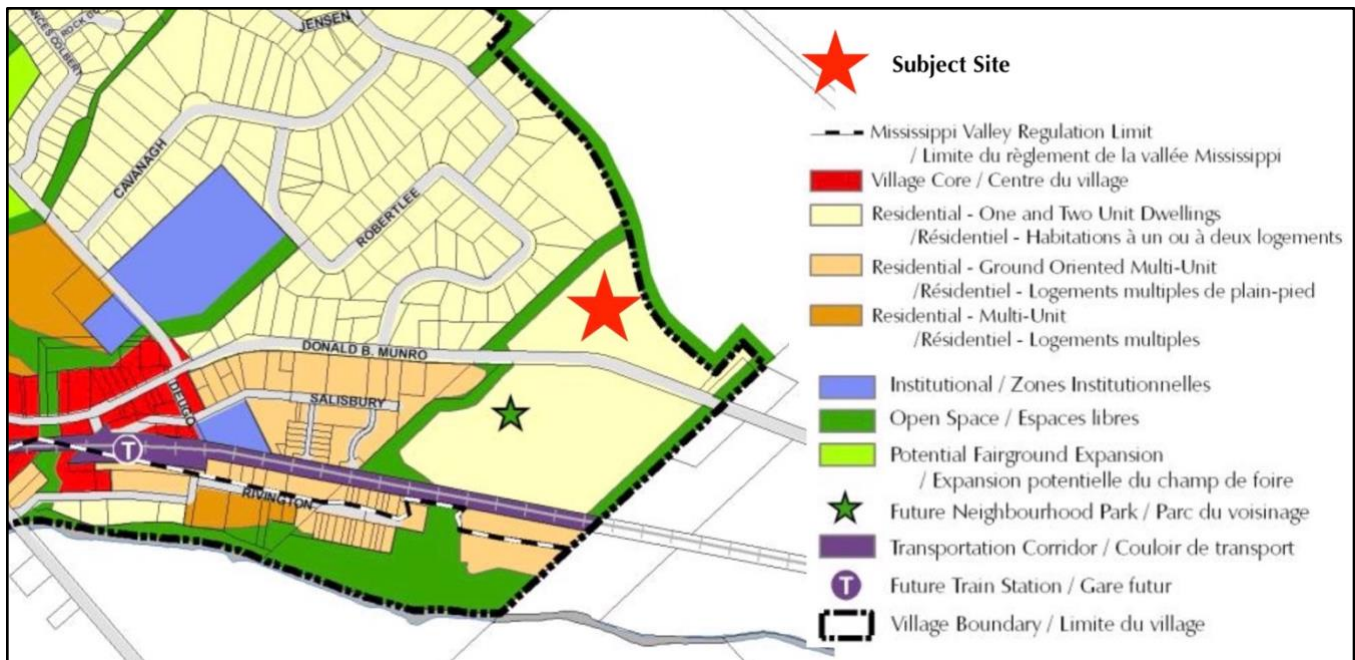


Figure 7: Schedule A - Land Use, Village of Carp Community Design Plan

Section 2.1 *Future Growth and Servicing* identifies municipal water and wastewater system capacity constraints that would limit the ability to service all potential dwelling units within the village boundary.

*Policy 2.1.1 All new development and redevelopment in Carp will only be approved on the basis of available public water and wastewater services;*

*Policy 2.1.2 The city will establish a plan to upgrade the water and wastewater services to provide for the full development of the Village of Carp.*

**Response:** The City of Ottawa has planned infrastructure upgrades to accommodate the expansion of urban development in the Village of Carp. Development in the Village of Carp can proceed when capacity has been increased to both the Village of Carp water supply system and the Carp Sanitary Pump Station. The City has indicated they are currently reviewing the issue. Once the infrastructure upgrades have been completed, the subject site can be serviced as demonstrated in the Conceptual Servicing and Stormwater Management Report submitted in support of the proposed development.

Section 2.2 *Village Boundary* identifies the Carp Hills as a natural boundary for the village. The Area of Natural and Scientific Interest (ANSI) boundary defines the Natural Environment Area with the exact boundary of the Carp Hills ANSI being further defined through detailed environmental studies.

**Response:** In accordance with the City of Ottawa Official Plan, the boundary of the NEA lands were defined through detailed mapping, environmental studies, and a 'boundary flagging' exercise that took place on May 28<sup>th</sup>, 2021 with biologists representing both the City of Ottawa and the applicant. The agreed upon NEA line is represented as the north boundary line for the proposed development.

Section 2.3 *Environmental Protection* provide policies to protect the village's environmental features, Natural Heritage System and Source Water Protection. The subject site is located within the Source Water Protection

area identified on Schedule E of the CCDP and abuts the Carp Hills which is identified as significant woodlands on Schedule F of the CCDP. As a result, an Environmental Impact Statement (EIS) and Integrated Environmental Review Statement (IER) is required.

Response: The Tree Conservation Report (TCR) and EIS submitted in support of the proposed development is summarized in section 5.3 of this Planning Rationale. The EIS and TCR concludes that with the proper implantation of recommended mitigation measures, the proposed development of the site will not negatively impact the natural heritage system and features of the Carp Hills Natural Area, including the significant woodlands. Additionally, the proposed development does not contain any land uses that may threaten the municipal water supply. IER is provided in section 5 of this Planning Rationale.

*Section 3.3 Residential Areas* highlights an objective of the plan is to strengthen residential neighbourhoods and maintain village character. Furthermore, a range of housing opportunities are an important part of the provision of a complete community. Schedule A of the CCDP identifies a variety of densities throughout the plan with development standards reflecting the rural character of the village.

*Section 3.3.5 Residential – One and Two Unit Dwellings* are located on the land use plan in areas farther away from the village core than denser unit types. The intent of this designation is to help meet the objective of strengthening residential neighbourhoods and maintaining the village character. Detached, semi-detached and duplex dwelling units are permitted uses in this designation. Townhouse units are permitted in areas designated *Residential – Ground Oriented Multi-Unit*.

Response: The proposed development includes single detached, semi-detached and townhouse units. The single detached and semi-detached units are consistent with the Residential-One and Two Unit Dwellings designation. Permitting townhouse units in this area will not significantly deviate from the overall intent of the CCDP as the net residential density of the site remains low at 18.99 units per net hectare. The higher density townhouse units are appropriately located in the areas nearest to the collector road which effectively mitigates impacts to existing residential homes. The CCDP requires an amendment to permit the inclusion of townhouse units, similar to what was provided for in the adjacent Green Meadows subdivision. The amendment can be provided for by virtue of approval of the subdivision and zoning applications. The City has confirmed that as the applications will be filed prior to Council approval of the new Official Plan that they will be assessed under the current CDP protocols, as opposed to requiring an Official Plan Amendment once they are entrenched as secondary plans under the new Official Plan.

*Section 3.3.2 Design Guidelines for New Residential Development* provides guidelines that reflect the characteristics of a traditional village. Key design guideline policies are as follows:

*Policy 3.3.2.1 To maintain the character of traditional village streets, and ensure the buildings define the streetscape, the building face to building face distance should be in the range of 24 to 25 metres for smaller singles, semi, duplexes, town houses, and not greater than 30 metres for larger singles, or low rise apartments.*

*Policy 3.3.2.2 Zoning and subdivision plans will address the following aspects:*

- *Residential streets will be 18.0m wide*
- *The length of the driveway to accommodate cars can be measured from the curb, or back of sidewalk rather than from the ROW, provided pedestrian access is not blocked. The result will be parking within the public ROW*



- *Building setbacks may be reduced to as low as 3.0 metres from the ROW or 6.0 metres from the sidewalk if it is provided for.*
- *The front of garages should not extend beyond the front façade of the house, either as attached buildings or separate structures.*
- *The tree lined village streets will be created through the provision of one tree per lot and two on corner lots as part of subdivision development agreements.*

Response: The proposed development is consistent with the design guidelines for new residential developments. The proposed local streets are 18.0 metres wide. Assuming the building is setback 6 metres from the front property line, driveway lengths would be 10.75 metres in length to the curb and 8.95 metres in length to the sidewalk where one is present. The front of garages will not extend beyond the front façade of the house. Building face to building face distances will be 30 metres with a 6 metre front yard setback which would help maintain the character of traditional village streets. One tree per interior lot and two trees per corner lot will be provided.

Section 3.7 *Open Space* provide policies for lands with this designation on Schedule A of the CCDP. The Open Space designation is intended for parks, pathways, drainage control areas, preservation of significant vegetation and access to other open space areas. Schedule A of the CCDP shows an Open Space corridor along the western boundary of the subject site which is further defined as a pathway on Schedule C – Pedestrian Pathway System of the CCDP (Figure 8). Additional policies for pedestrian pathways are provided in Section 7.4. Relevant policies to the subject site are provided below.

*Policy 3.7.1 The lands identified Open Space may be used for parks, open space, recreational pathways, drainage control and conservation uses.*

*Policy 3.7.3 The Open Space corridors are intended to be sufficiently wide so that proposed uses can be comfortably accommodated, taking into account the natural terrain and abutting uses they should not be less than 10 metres in width. Pathways may be either publicly or privately owned. Where they remain in private ownership the city will use a variety of mechanisms such as leasing, right-of-way or land trusts to ensure a reasonable degree of public access.*

*Policy 7.4.2 The City will ensure that new developments are linked to the existing or planned network of public sidewalks, recreational pathways and on-road cycle routes, which connect parks and other open spaces, and community services and facilities.*

*Policy 7.4.4 Laneways, public ROWs, and public land that provide access to the Carp River, the fairgrounds, or to municipal woodlots in the Carp Hills will be protected and made accessible.*

*Policy 7.4.5 The City will explore opportunities to provide public access to City owned land in the Carp Hills.*

Response: Blocks 76 and 77 on the proposed draft plan of subdivision are 10 metre wide walkway blocks that are consistent with the CCDP Pedestrian Pathway System. The walkway blocks will provide public access to the Carp Hills to the north and will link to the walkway path located south of Donald B. Munro Drive. A 2 to 3 metre wide stone dust pathway is proposed to be built which will provide nature trail characteristics as a transition into the NEA lands.

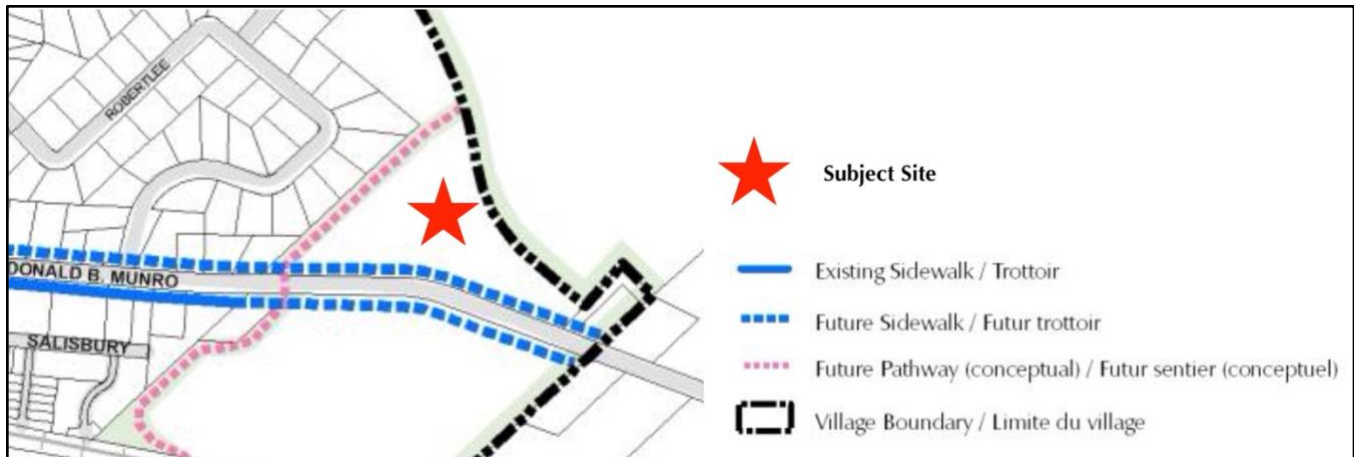


Figure 8: Schedule C Pedestrian Pathway System

Donald B. Monroe is identified as a Scenic Entry Route. Section 4.10 *Create Prominent Approaches to the Village* provide key initiatives to achieve this objective. Relevant key initiatives include:

1. *At the four approaches to the Village identified on Figure 2:*
  - a. *Reconfigure the road from a rural cross-section to a village cross-section (by providing sidewalks, landscaping etc.); and*
  - b. *Add specific design elements as visual accents that give the impression that travelers are entering a unique village with character.*

**Response:** The proposed development will contribute to creating a prominent approach into the village by building a sidewalk on the north side of Donald B. Munro Drive and planting native trees within the boulevard. Complementary design elements to the design features provided on the south side of Donald B. Munro Drive will be considered at the design stage of development.

Section 8.1 *Amendments to the Community Design Plan* identify the process required for an amendment to the CDP.

*Policy 8.1.1 Significant changes to the land use policies set out in the Community Design Plan will require approval by Council. An example of a significant change would be changing the land use category of a whole area from one land use category to another (e.g. Residential – Multi-Unit to Residential – One and Two Unit Dwellings). Minor revisions/interpretations of the land uses identified on Schedule A will not require Council approval. Precise boundaries for land uses will set out in the zoning by-law;*

*Policy 8.1.2 Policies within the City of Ottawa Official Plan will guide when an Official Plan Amendment will be required. Two examples are changes to the village boundary and the Right-of-Way protection.*

**Response:** The proposed development requires an amendment to the CCDP in order to permit the inclusion of townhouse units, similar to what was provided for in the adjacent Green Meadows subdivision. An amendment to the CCDP will be required to designate certain portions of the subject site from Residential – *One and Two Unit Dwellings* to Residential to *Ground Oriented Multi-Unit*. The amendment can be provided for by virtue of approval of the subdivision and zoning applications. The City has confirmed that as the applications will be filed prior to Council approval of the new Official Plan that they will be assessed under the current CDP protocols, as

opposed to requiring an Official Plan Amendment once they are entrenched as secondary plans under the new Official Plan.

#### **4.4 Design Guidelines for Rural Villages**

Ottawa City Council approved the *Design Guidelines for Rural Villages* which provide guidelines intended to assess and guide appropriate development in Villages. The guidelines are to be applied in conjunction with Council Approved documents, such as Community Design Plans. These design guidelines include direction on:

- Community Layout and Design;
- Heritage and Architecture;
- Built Form;
- Streetscape; and
- Open Space.

The proposed development was assessed against the relevant guidelines and is consistent with the direction provided in the Design Guidelines for Rural Villages.

#### **4.5 City of Ottawa Zoning By-Law No. 2008-250**

Under the Comprehensive Zoning By-law 2008-250, the subject site is zoned *Development Reserve 3 (DR3)*.

In accordance with an approved Draft Plan of Subdivision, the requested Zoning By-law Amendment is illustrated in Figure 9 and would zone the subject site:

- Village Residential First Density Zone, Subzone N (V1N), for single detached dwellings;
- Village Residential Third Density Zone, Subzone B (V3B) for semi-detached and townhouse dwellings; and
- Parks and Open Space Zone (OS) for the pathway block.





Figure 9: Proposed Zoning Map

Site specific exceptions for the single detached, semi-detached and townhouse dwelling units are required in both the VIN and V3B zone. The requested modifications to the performance standards are:

- Village Residential First Density Zone:
  - Minimum Lot Width of 10.8 meters whereas By-law 2008-250 requires 18.0 metres;
  - Minimum Lot Area of 350m<sup>2</sup> whereas By-law 2008-250 requires 540m<sup>2</sup>;
  - Maximum Lot Coverage of 45% whereas By-law 2008-250 requires 40%; and
  - Minimum Front Yard Setback of 4.0 metres whereas By-law 2008-250 requires 6.0 metres.
- Village Residential Third Density Zone for Semi-detached dwelling and Townhouse dwelling:

- Minimum Corner Side Yard of 4.0 metres whereas By-law 2008-250 requires 6.0 metres;
- Minimum Interior Side Yard Setback of 1.5 metres whereas By-law 2008-250 requires 3.0 metres; and
- Minimum Front Yard setback of 4.0 metres whereas By-law 2008-250 requires 6.0 metres.

The requested site specific exceptions would permit the subject site to be developed in a manner that is tailored to the full range of unit models offered by Tartan Homes, while maintaining compatibility with existing residential dwelling units in the Village of Carp.

The adjustment to the NEA line will be reflected as the zoning line between the development lands and NEA lands.

## 5 Integrated Environmental Review

In the Official Plan, Section 4.7.1 *Integrated Environmental Review to Assess Development Applications* is the means to ensure each development considers significant findings from each support study; complies with environmental policies outlined in Section 4; and applies design with nature principles. In keeping with the requirements outlined in Section 4.7.1 of the Official Plan, the following technical studies submitted in support of the proposed development are summarized in sub sections below.

### 5.1 Stage 1 and 2 Archaeological Assessment

A combined Stage 1 and 2 Archaeological Assessment was completed by Matrix Heritage Inc., September 2021. A Stage 1 background assessment was completed which identified portions of the study area as having archaeological potential. The survey completed as part of the stage 2 assessment did not find anything of archaeological concern on the subject site. The study required no further archaeological assessment.

### 5.2 Phase 1 Environmental Site Assessment

A Phase One Environmental Site Assessment (ESA) was completed by Paterson Group, dated September 8, 2021. The Phase 1 ESA was conducted for the south portion of the property located at 232 Donald B. Munro Drive.

The study does not identify any environmental concerns based on the current and past use of the subject site which includes former agriculture use or treed land. Adjacent land uses have been agriculture use, treed land and more recently residential land. Railway tracks located south of the subject site were identified as historical Potentially Contaminating Activities (PCAs) but does not result in the phase 1 property being identified as Areas of Potential Environmental Concern (APECs) based on the distance from the subject site and downgradient orientation. No other PCAs were identified with the current use of the Phase 1 Property or lands within the Study Area.

The study concludes that based on the findings of the report, a Phase Two ESA would not be required for the subject site.

### 5.3 Tree Conservation Report and Environmental Impact Statement

An Environmental Impact Statement (EIS) and Tree Conservation Report (TCR) was prepared by Muncaster Environmental Planning Inc., dated October 9, 2021, in support of the proposed development. For discussion purposes in the report, the lands proposed for development are south of the recommended zoning line and the Natural Environment Area (NEA) are north of the recommended zoning line.

Map 1 in the EIS and TCR report is provided as Figure 10 in this Planning Rationale. Map 1 identifies existing site conditions which include the north-south channel, cultural meadows, cultural thicket, cultural woodlands, upland maple deciduous forest and deciduous hedgerows. The proposed NEA line, Natural Heritage System line (Schedule L3 City of Ottawa Official Plan) and the Carp Hills Natural Area are also identified on Map 1.



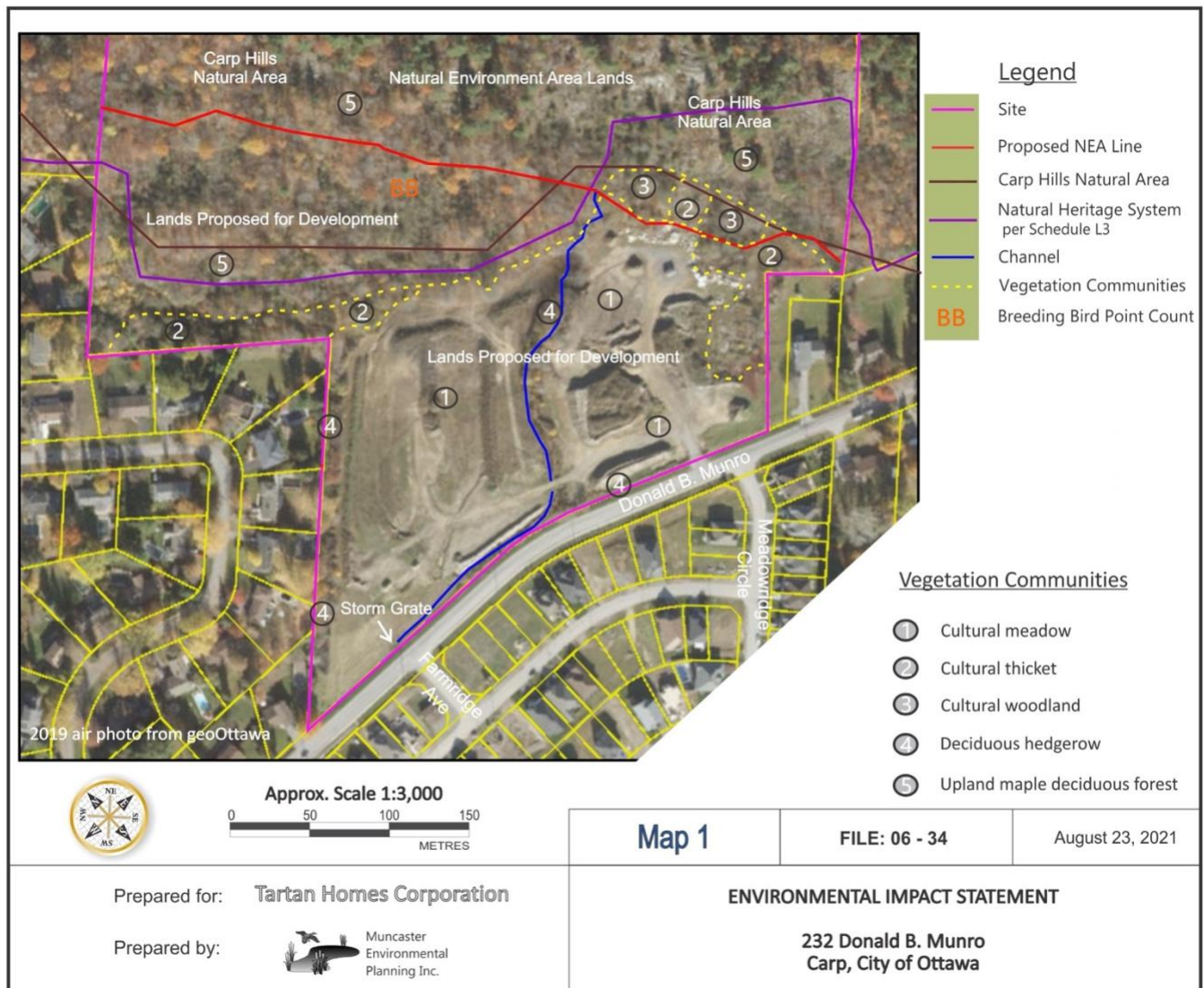


Figure 10: Map 1 Existing Environmental Features

For the lands identified for development, the report notes there are no areas of rare vegetation, wetlands, or woodlands greater than 50 years of age.

The north-south channel is a tributary of the Carp River. The channel flows south with discharge originating from the base of the bedrock outcrops located approximately 190 metres north of Donald B. Munro Drive. In the southwest corner of the site on the north side of Donald B. Munro Drive, flow from the channel enters a storm grate which is entombed for approximately 170 metres south of Donald B. Munro Drive. The channel is not considered to provide direct fish habitat on the site due to limited water volume, steep gradient and lack of connection to potential downstream habitat as a result of the channel entering into a storm grate. The channel is not considered a significant feature and is not proposed for retention. Recommendations to accommodate the flow include retaining an open feature such as through the use of vegetated swale where detailed grading and engineering assessments permit.

Potential Species at Risk and species of special concern were identified using various online resources and databases along with field surveys to determine potential habitat and sightings. The results indicate the following:

- Blanding's turtle, snapping turtle and northern map turtle were recorded for the overall 10km square. The north-south channel is not considered suitable turtle habitat due to the lack of adjacent wetland habitat and minimal flows and wetted widths of the channel. Targeted surveys did not observe any turtles on the site and no evidence of nesting were noted. No suitable wetland habitat for turtles were observed on the proposed development lands. A loss of approximately 5.2 hectares of Category 3 habitat is identified, however the development lands do not lend itself to the migrating function of Category 3 habitat.
- No potential structures are located on the site for Chimney swift or barn swallow nesting;
- Bobolink and eastern meadowlark habitat that is present on the subject lands is heavily disturbed and does represent potential nesting habitat for these species. No bobolink and eastern meadowlark were observed during the field surveys;
- Butternut are identified as being present on and adjacent to the subject lands. There are approximately 13 healthy butternut trees (12 category 2 trees and 1 category 3 tree) that will need to be removed and approximately 8 healthy butternut trees that will be harmed. Compensation for the removal and harm of healthy butternut trees will be provided in accordance with provincial and federal requirements.
- Eastern whip-poor-will were not heard during the targeted surveys.
- No good quality potential summer bat colony habitat are present on the site.
- No aquatic Species at Risk are reported in this portion of the Carp River Watershed

Significant woodlands are present in the NEA lands to the north. In the north portion of the land proposed for development, the upland maple deciduous forest identified on Map 1 in between the Natural Heritage System line and the proposed NEA line would also be considered significant woodlands. The lots backing onto the NEA lands have extra lot depth and will provide a minimum 15-metre of tree retention along the NEA line. The 15-metre buffer, the installation of permanent fencing along the NEA boundary, the upward slope towards the NEA, and greater than 15-metre setback to the foundation of the units, is determined to be sufficient to protect the features and functions of the NEA. The buffer is reduced in the northeast portion of the lands where the upland maple deciduous forest is no longer present. Approximately 0.9 hectares of forest interior habitat is proposed for removal, however there is no impacts on the overall forest as approximately 273 hectares of forest interior habitat is retained in the NEA lands.

Significant wildlife habitat and wildlife linkage functions are present in the overall Carp Hills Natural Area. Significant wildlife habitat does not appear to be present on the lands proposed for development as detailed in the field observations and with the on-site meadow habitat being too disturbed. Significant wildlife linkage functions on the proposed development lands are expected to be minimal due to the disturbed meadow habitat, adjacent residential areas to the west and south and Donald B. Munro Drive.

The NEA boundary was determined during a "boundary flagging" exercise that was completed on May 28<sup>th</sup>, 2021 by City of Ottawa biologist and Bernie Muncaster a representative of Tartan Homes. The precise boundary was agreed upon using factors such as vegetation, topography, condition etc. Two main observations used to determine the boundary were noted by the City of Ottawa and include where there is a logical eastward extension of the NEA boundary and where the rock outcrops were more disturbed, with signs of past grazing.

Proposed tree retention is provided as Map 2 in the EIS and TCR and is provided as Figure 11 in this Planning Rationale report. As noted, the lots backing onto the NEA lands have extra lot depth which will accommodate a 15 meter naturalized buffer within the development lots. The naturalized buffer will facilitate tree retention and protect the critical root zones for the trees in the NEA that abut the development lands. Along the western boundary, the existing hedgerow trees will be retained in the 10-metre wide walkway block, subject to detailed grading and servicing assessments. A stone dust pathway is recommended in the walkway block to minimize disturbance and impacts to the critical root zones of these hedgerow trees. Potential tree retention in the north east corner and along Donald B. Munro Drive will protect any co-owned trees or the critical root zone of adjacent trees.

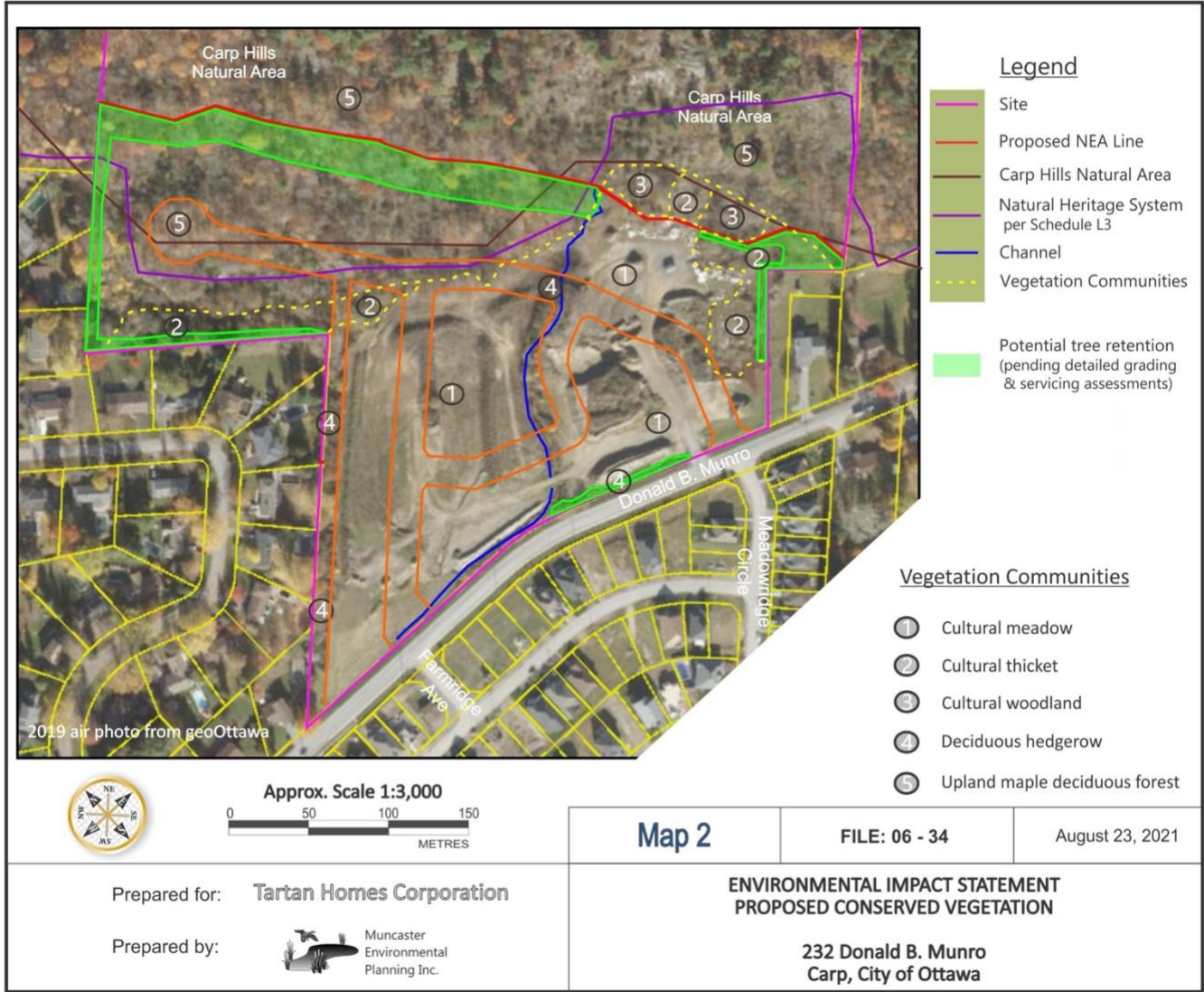


Figure 11: Map 2 Proposed Conserved Vegetation

Tree planting restrictions due to clay soils being present on the site are identified. Trees with a high water demand are not recommended. Planting of native trees and shrubs are recommended.



The study concludes that with the proper implantation of recommended mitigation measures, the proposed development of the site will not negatively impact the natural heritage system / features of the Carp Hills Natural Area, including the significant woodlands.

## 5.4 Assessment of Adequacy of Public Services

An Assessment of Adequacy of Public Services report was completed by IBI Group, October 2021. The purpose of the report was to provide a review of the adequacy of public services needed to support the proposed development.

**Water Supply:** The proposed water servicing plan indicates the site will be serviced internally by 200mm watermain and will connect to the existing 200mm diameter watermain in Donald B. Munro Drive at Street No. 1 and at the existing watermain stub at Street No. 2. A hydraulic analysis will be required once the City is able to provide updated boundary conditions for the existing watermain in Donald B. Munro Drive, at which point fire flow rates can be determined. A water age analysis indicates an automatic flushing unit may be required on the cul-de-sac to ensure sufficient water turnover until there are enough occupancies on the street.

The City of Ottawa has advised that the Carp water distribution network is operating at capacity and is currently reviewing the situation. Increasing capacity to the Village of Carp water supply system is required in order to service any future developments in Carp including the proposed development.

**Wastewater Disposal:** The Carp Sanitary Pump Station is the wastewater outlet for all lands within the Village of Carp, including the subject site. Flows from the pump station are carried via a forcemain to Kanata where flows are ultimately directed to the Robert O. Pickard Wastewater Treatment Facility. Currently, a 250 mm diameter sanitary sewer in Donald B. Munro Drive is terminated just east of street one and a 300 mm diameter forcemain is located in Donald B. Munro Drive along the full boundary of the site. In order to service the subject site, the existing sanitary sewer along Donald B. Munro Drive will need to be extended and capacity to the Carp Sanitary Pump Station will be required as it is currently operating at capacity. The City has advised they are reviewing the situation so that urban expansion can continue in the Village of Carp.

The proposed wastewater plan indicates the site will be serviced internally by 200 mm diameter sanitary sewers. To service the western portion of the site, one connection to the existing 250 mm diameter sewer in Donald B. Munro Drive is proposed at Street No. 2. To service the eastern portion of the site, a second connection at Street No. 1 is proposed which will require a new 200 mm diameter sewer running westward along Donald B. Munro Drive.

**Storm Sewer Design:** The Green Meadows subdivision was constructed with a minor storm sewer system that was designed to provide capacity for the subject site. Stormwater runoff from the subject site will enter into the minor storm sewer system of the Green Meadows subdivision via an existing 1050 mm diameter storm sewer, which will ultimately pass through an oil and grit separator prior to the runoff being released to the Carp River.

The proposed minor storm plan includes the installation of storm sewers within the subject site. The majority of the site would drain from Street No. 2 into the existing 1050 mm diameter sewer. It is recommended that drainage from the NEA lands be directed to this sewer section by constructing a new drainage swale along the edge of the NEA lands where it would enter into the minor storm sewer system. Flows from the eastern portion of the site would drain from Street No. 2 into Donald B. Munro Drive where the existing 250 mm diameter storm



sewer will need to be replaced with a new 450 mm and 525 mm diameter sewer which would connect to the existing 1050 mm diameter sewer and will then discharge into the Green Meadows minor storm sewers.

A dual drainage system is planned to accommodate both minor and major storm runoff. Minor storm sewers will accommodate runoff from frequent storm events through catchbasin inlets. The major system will accommodate runoff from less frequent storm events through a system of rear yard swales and street segments. Inlet control devices will be used throughout the site. Major flow from the subject site is proposed to be captured in the minor system however the opportunity for on-site storage in the form of street sags is limited due to the topography of the site. It is anticipated that a combination of a minor system capture and to cascade major flow across Donald B. Munro Drive to the Green Meadows subdivision may be required and will be determined at the detailed design stage.

**Macro Grading Plan:** The topography of the subject site is generally steep and will pose some grading challenges. Along the Environmental Protection lands, terracing will likely be required. Retaining walls will also be required in other locations throughout the site. There will be opportunities for walk out units. At the detailed design stage, drainage designs more suitable to the existing topography will be explored. It is possible that flows from the NEA lands may need to be accommodated in several ditch inlets in which case flows may be incorporated in the rear yard drainage design of lots backing onto the NEA lands. An excerpt of the conceptual macro grading plan is provided as Figure 12.

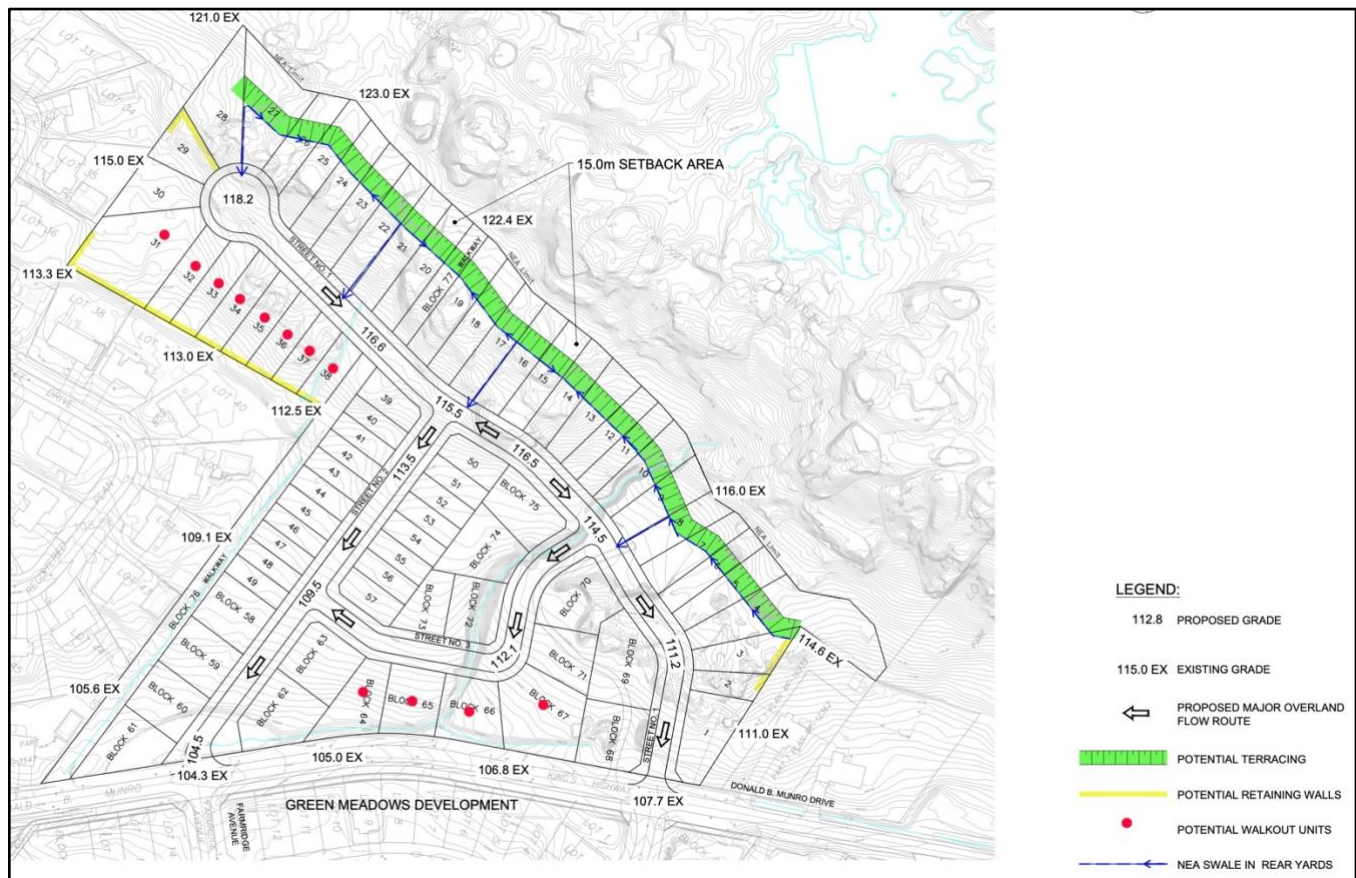


Figure 12: Conceptual Macro Grading Plan

**Sediment and Erosion Control:** In order to reduce unnecessary construction sediment loading, a number of mitigation techniques are proposed:

- Groundwater in trenches to be pumped into a filter mechanism prior to being released to the environment until the local storm sewers are constructed;
- Bulkhead barriers to be installed at the nearest downstream manhole in each sewer which connects to an existing downstream sewer;
- Seepage barriers to be installed on any surface water courses;
- Filter cloths to remain on open surface structures until those structures are commissioned and put into use; and
- Silt fence along the site perimeter.

**Approvals and Permit Requirements:** The report indicates the City of Ottawa will issue approval for construction which include MECP Section 53 Application for Sewers, Form 1 for Watermains and a Commence Work Notification. The Ministry of Environment, Conservation and Parks will approve the local sewers under section 53 of the Ontario Water Resources Act and issue Environmental Compliance Approvals and a Permit To Take Water (if required). The Mississippi Valley Conservation will confirm permit requirements during the detailed design stage. There are no anticipated permit, authorization or approval requirements from the Federal Government.

The report concludes that development on the subject site can proceed when the following is completed:

- The capacity of the Village of Carp water supply system has been increased;
- The capacity of the Carp Sanitary Pump Station has been increased;
- The sanitary sewer along Donald B. Munro Drive has been extended;
- The existing storm sewer in Donald B. Munro Drive has been upgraded.

It is recommended that draft conditions be provided for the above requirements prior to proceeding to final development.

## 5.5 Geotech Report

A preliminary Geotechnical Investigation was completed by Paterson Group Inc., dated October 4, 2021, in support of the proposed development. The objective of the investigation was to determine subsurface soil and groundwater conditions and provide preliminary geotechnical recommendations for the geotechnical design aspects of the proposed development.

The subsurface conditions observed on the subject site generally consist of topsoil and/or fill material. The fill material consists of dark brown silty clay to silty sand, crushed stone and gravel, traces of roots overlying very stiff to stiff brown silty clay and/or glacial till. Geological mapping information indicate there is bedrock with overburden thickness that varies approximately 3 to 25 metres in depth. Ground water levels that were recorded range from 0.8 to 4.4 metres below existing ground surface. It is expected that long-term groundwater levels will be found at approximately 3.0 to 4.0 metres below existing grade but it is noted that this may fluctuate throughout the year and could be encountered during construction.

Geotechnical considerations and precautions are provided in detail in the report. Notably for design purposes key information provided in the report is detailed in this summary. A geotechnical evaluation is recommended where existing fill is encountered below the footing of settlement-sensitive structures to determine if it is suitable to remain in place, or the use of an engineered granular fill pad over a proof-rolled fill layer can be implemented; Grade raise restrictions may be required due to the presence of a sensitive silty clay layer with preloading potentially being required if higher than permissible grade raises are required; Bedrock removal in the southeastern portion of the site may be required depending on the depth of services and building foundations; A Permit-To-Take-Water (PTTW) from the Ministry of the Environment of Ontario may be required depending on the quantity of ground and/or surface water to be pumped during construction.

A soils review of the site was completed in accordance with the City of Ottawa Tree Planting in Sensitive Marine Clay Soils guidelines (2017). Two tree planting setback areas within the subject site are indicated as shown in Figure 13. Area 1 will have no tree planting restrictions as the subsoil indicates silty sand will be encountered. Area 2 will have tree planting restrictions and setback requirements due to low to medium sensitivity clay soil being present in this area. The tree planting restrictions in Area 2 are only required where the design finished grades and top of the silty clay layer is less than 3.5 metres. The Geotechnical design consultant will review the grading plan and will confirm tree planting restrictions in this area on a lot by lot basis. Tree planting restrictions in area 2 are as follows:

- Large trees (mature height over 14m) can be planted where the tree to foundation setback equals the full mature height of the tree;
- Small trees (mature height up to 7.5m) and medium trees (mature height 7.5m to 14m) may have reduced foundation to tree setback to 4.5m provided the following conditions are met:
  - For footings within 10m from centerline of tree, the underside of footing is 1.9m or greater below the lowest finished grade;
  - Soil volumes must be a minimum 25 m<sup>3</sup> for a small tree and 30 m<sup>3</sup> for a medium tree;
  - The foundation walls are reinforced nominally (minimum of two upper and two lower 15M bars in foundation wall); and
  - The areas around the tree is graded to promote drainage to the tree root zone.

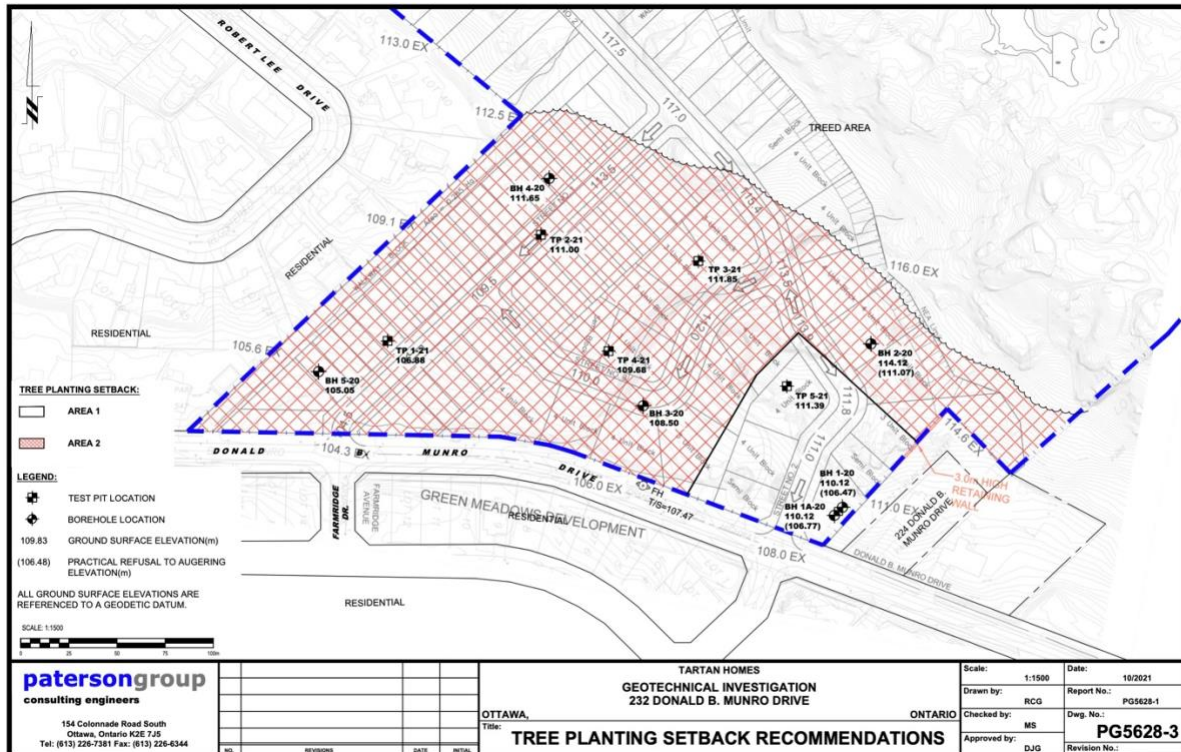


Figure 13: Tree Planting Areas

The report concludes that from a geotechnical perspective, the site conditions are adequate for the proposed residential development. Recommendations and next steps at the detailed design stage include the completion of a geotechnical report confirming site servicing and construction works have been completed in accordance with the geotechnical recommendations.

## 5.6 Noise Control Feasibility Study

A Noise Control Feasibility Study was prepared by IBI Group, October 2021, to determine the impact of transportation-related noise sources on the proposed development.

Roadway noise from Donald B. Munro Drive was assessed. A combination of noise barriers, central air conditioning systems, ventilation requirements, acoustical review/design of building components and warning clauses will be required in certain locations for residential units near Donald B. Munro Drive. The residential units requiring noise mitigation measures will be determined during the detailed design phase of the proposed development.

The subject site is located approximately 2.1km north of the Carp Airport. There are no NEF/NEP noise contours available for the Carp Airport although it is a reasonable assumption that the subject site would be well outside the NEF 25 contour line. Additionally, the flight path is less likely to cross over the subject site due to the east-west runway. Regardless, Transport Canada recommends the following warning clause be provided that advises of the airport's close proximity to the residential units within the subject site:

*"Purchasers/tenants are advised that due to the proximity of the airport, noise from the airport and individual aircraft may at times interfere with outdoor or indoor activities".*



The subject site is located within 250 metres from a single-track rail-line. Due to the distance of the rail line from the subject site and low volume of use, consideration of rail traffic will not be required for the proposed development.

## 5.7 Transportation Impact Assessment

A Transportation Impact Assessment was prepared by IBI Group, October 6, 2021 in support of the proposed development. The study is divided into four major complements which includes screening, scoping, forecasting and analysis.

The proposed development is comprised of an internal road network of local roads with 18-meter right-of-way and is designed using a modified grid pattern with short curvilinear block lengths. The modified grid pattern and short road segments lends itself to being a porous, walkable community and which promotes driver behaviour consistent with the local road classification. A mobility plan is provided which proposes a sidewalk on one side of the Street 1 from Donald B. Munro Drive to the walkway paths and on one side of Street 2. In accordance with the City's 30 km/h Design Guidelines for Local Residential Streets, a proposed traffic calming plan is provided which includes design elements such as bulb-outs, speed humps, chicanes and reduced curb radii. The proposed design elements are to be considered as part of the detailed design work following Draft Approval.

The study area intersections include Donald B. Munro Drive where it intersects with Carp Road, Langstaff Drive / Deugo Street, Robertlee Drive, Farmridge Avenue / Street 2, Meadowridge Circle / Street 1, and the DBM / March Road / Old Carp Road intersection. The study provides the following key conclusions and recommendations regarding the study intersections and roads:

- Traffic generated from the site will access DBM via two access intersections which are proposed to be stop controlled;
- All the study area intersections are stop controlled and none are expected to be signalized in the future;
- All study area intersections are expected to operate at an acceptable level of service during weekday peak hours and the queuing analysis did not identify any operational issues;
- No off-site intersection improvements are required to accommodate the expected traffic generated by the proposed development. As a result, an RMA will not be required;
- The overall impact to traffic on Donald B. Munro Drive is limited with only a small number residences on Donald B. Munro Drive being impacted by traffic generated by the proposed development.
- A collision trend at the March Road & Donald B. Munro Drive / Old Carp Road intersection is identified with no apparent cause or pattern. The City should consider the implementation of an overhead flashing beacon or a single-lane roundabout to address the safety issue if collision patterns persist. The collision trend is an existing condition and not a result of traffic generated by the proposed development.

There is limited and pedestrian and cycling facilities and linkages outside the village and poor Bicycle and Pedestrian Level of Service in proximity to the proposed development. There is also limited transit services in vicinity to the proposed development. To achieve Multi-Modal Level of Service targets while considering the limitations of Donald B. Munro Drive, the following recommendations are provided:

- OC Transpo should consider adding bus stops at the corner of Donald B. Munro Drive & Farmridge Avenue / Street 1 and should consider adding weekday, daily peak direction service if warranted based on demand from the community as a whole;
- A 2% pedestrian mode share target can be achieved through the installation of good pedestrian facilities within the proposed development, as shown on the proposed mobility plan.
- The existing speed transition zone on Donald B. Munro Drive located at Farmridge Avenue should be relocated east of Street 1 / Meadowridge Circle to address poor bicycle and pedestrian Level of Service along the section of Donald B. Munro Drive abutting the subject site;
- A 2.0m wide sidewalk should be provided on the north side of Donald B. Munro Drive;

The study concludes that the proposed development can be accommodated by the adjacent transportation network and a post-development monitoring plan will not be required.

## **5.8 Headwater Drainage Feature Assessment**

A Headwater Drainage Feature Assessment was prepared by Bowfin Environmental Consulting Inc., dated October 2021, in support of the proposed development. The Headwater Drainage Feature Assessment involves the evaluation of the drainage features, classification of features and Management Recommendations.

A single feature was identified in the study area which is 320 metres in length and flows from the northeast to southwest and then veers west along Donald B. Munro Drive where it then enters into an inlet catch basin. The hydrology classification provided for the feature is either 'Valued' or 'Contributing' due to minimal flow and confirmation that the feature was not permanent. The Riparian Classification is 'Valued' and is noted as being surrounded by shrubs and trees but the vegetation does not extend beyond 30 metres from the watercourse and downstream there is little to no riparian vegetation. No fish were captured or observed. The Terrestrial Habitat Classification is 'Limited' with no wetland present and no linkages. The feature is found not to provide amphibian or fish habitat.

Management recommendations include ensuring the same quantity and quality of flow is contributed to the storm water system post-construction and vegetated swales should be used in the development where feasible.

## 6 Conclusions

Based on the review and analysis of the policy and regulatory framework and supporting studies, it is my professional planners' opinion that the proposed development is appropriate for the lands, builds upon and enhances the existing assets of the community, is compatible with its surroundings and is based on good planning.

The proposed development will ensure residential growth in the Village of Carp will be accommodated in a manner that is consistent with the Provincial Policy Statement 2020, The City of Ottawa Official Plan, the Carp Community Design Plan and the City of Ottawa Zoning Bylaw.

Completed By:

A handwritten signature in black ink, appearing to read 'M Côté'.

Melissa Cote, MCIP RPP  
Tartan Land Consultants Inc.

## **Appendix A: Pre-consultation Meeting Minutes**





File Number PC2020-0332

1 February 2021

Tartan Land Consultants Inc.  
Melissa Cote  
237 Somerset St. W  
Ottawa, ON  
K2P 0J3

Dear Ms Cote

**Re: 232 Donald B. Munro Drive, Village of Carp Pre-Consultation Results**

Date of Meeting December 16, 2020

In attendance and/or provided comments:

Ostafichuk, Jeffrey [Jeffrey.Ostafichuk@ottawa.ca](mailto:Jeffrey.Ostafichuk@ottawa.ca)  
Brown, Adam [Adam.Brown@ottawa.ca](mailto:Adam.Brown@ottawa.ca)  
Whittaker, Damien [Damien.Whittaker@ottawa.ca](mailto:Damien.Whittaker@ottawa.ca)  
Melissa Cote [mcote@tartanland.on.ca](mailto:mcote@tartanland.on.ca)  
Pierre Dufresne [pdufresne@tartanland.on.ca](mailto:pdufresne@tartanland.on.ca)  
David Hook [DHook@IBIGroup.com](mailto:DHook@IBIGroup.com)  
[Ben.Pascolo-Neveu@ibigroup.com](mailto:Ben.Pascolo-Neveu@ibigroup.com)  
Stow, Nick [Nick.Stow@ottawa.ca](mailto:Nick.Stow@ottawa.ca)  
Rehman, Sami [Sami.Rehman@ottawa.ca](mailto:Sami.Rehman@ottawa.ca)  
Shepherd, Reid [reid.shepherd@ottawa.ca](mailto:reid.shepherd@ottawa.ca)  
Gervais, Josiane [josiane.gervais@ottawa.ca](mailto:josiane.gervais@ottawa.ca)  
Young, Mark [Mark.Young@ottawa.ca](mailto:Mark.Young@ottawa.ca)  
Erica Ogden [eogden@mvc.on.ca](mailto:eogden@mvc.on.ca)  
Joseph Zagorski [Joseph.Zagorski@ottawa.ca](mailto:Joseph.Zagorski@ottawa.ca)

Please find below the results of our meeting with respect to your proposal to develop a "multi residential" dwellings on a private street.

**Comments**

**Jeff Ostafichuk Planning**

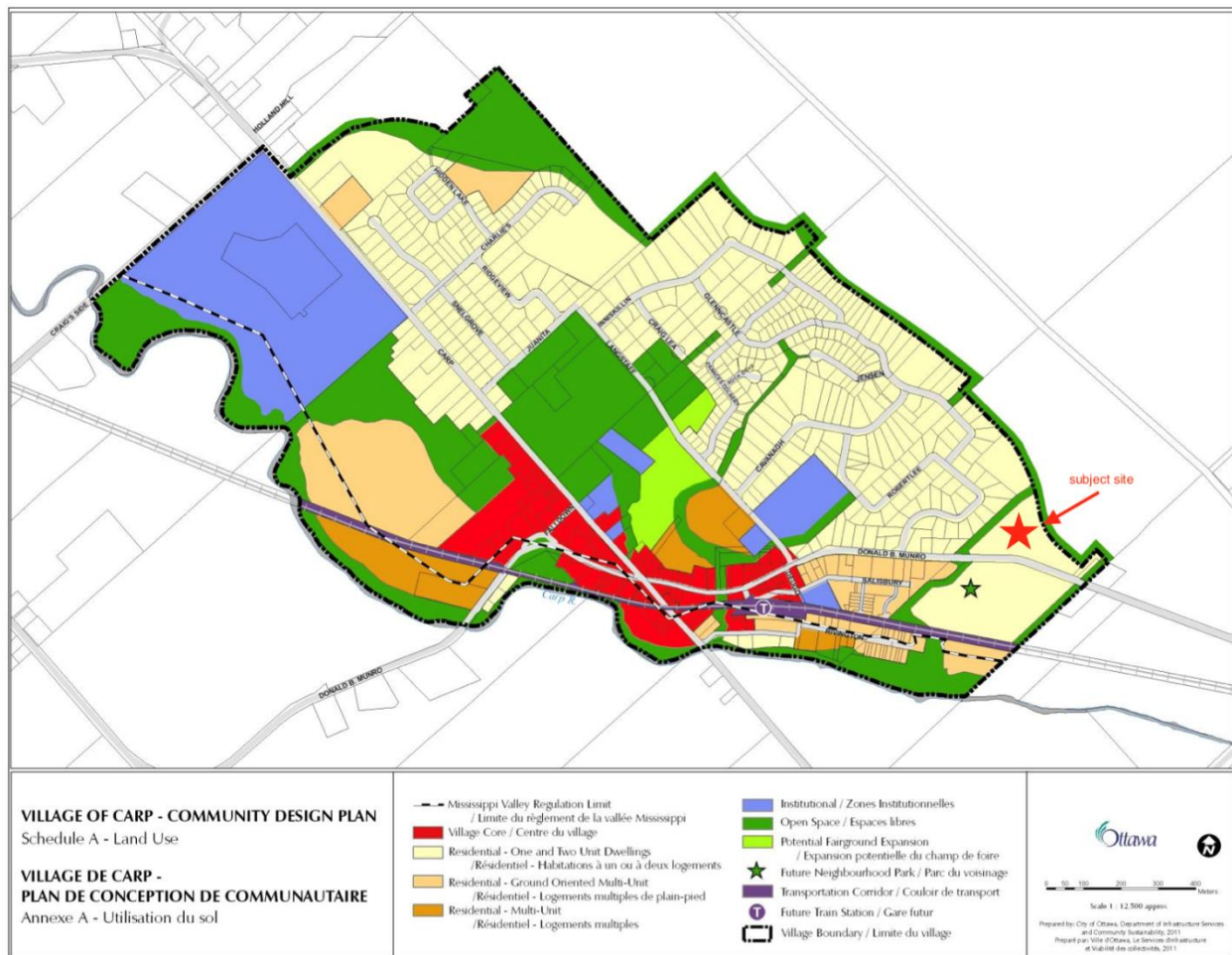
In our discussions you have suggested that you will be filing a plan of subdivision and zoning by-law amendment. Policies that need to be considered as per the Official

Plan and Village of Carp CDP (changing to Secondary Plan through the new Official Plan) are as follows.

## Land Use

The proposed plan of subdivision is located in the Village of Carp. The lands front onto the north side of Donald B. Munro, the main northwest/southwest entrance to the Village. The site, approximately 7.2 ha in size, proposes 64 single family lots and 65 townhouse/semi detached units. Access to the site is provided by two intersections to the north side of Donald B. Munro Drive which service the development via an internal loop road system (18 metre right-of-way). The applicant proposes municipal servicing via an extension to the existing local water and sanitary systems.

The subject lands are within the “Village” designation as identified on Schedule ‘A’, Rural Policy Plan of the City Official Plan. Further land uses within the Village of Carp are determined in the context of the Carp CDP (New OP Secondary Plan). The CDP provides guidelines for land use planning, such as subdivision, zoning applications. The CDP sets aside these lands for residential use.



## **2. Managing Growth**

### **2.3 Environmental Protection**

Policies are addressed by:

Sami Rehman, Environmental Planner, Planning  
Damien Whittaker, Senior Engineer Infrastructure  
Erica Ogden, Environmental Planner, MVCA Mississippi Valley Conservation Authority

#### **2.3.1 The Natural Heritage System**

Policies are addressed by:

Sami Rehman, Environmental Planner, Planning  
Erica Ogden, Environmental Planner, MVCA Mississippi Valley Conservation Authority

#### **2.3.2 Source Water Protection**

Land uses that are determined to constitute a significant threat to municipal drinking water (as defined in the Source Water Protection Act and its regulations) may be restricted. The basis and policy mechanism for restrictions will be in accordance with the Mississippi Rideau Source Water Protection Plan and the Official Plan.

Statement in rationale required.

## **3. Land Use**

### **3.3.2 Design Guidelines for new Residential Development**

#### **Policies**

1. To maintain the character of traditional village streets, and ensure the buildings define the streetscape, the building face to building face distance should be in the range of 24 to 25 metres for smaller singles, semi, duplexes, town houses, and not greater than 30 meters for larger singles, or low rise apartments.

2. Zoning and subdivision plans will address the following aspects:

- Residential streets will be 18.0m wide
- The length of the driveway to accommodate cars can be measured from the curb, or back of sidewalk rather than from the ROW, provided pedestrian access is not blocked. The result will be parking within the public ROW
- Building setbacks may be reduced to as low as 3.0 meters from the ROW or 6.0 metres from the sidewalk if it is provided for.
- The front of garages should not extend beyond the front façade of the house, either as attached buildings or separate structures.
- The tree lined village streets will be created through the provision of one tree per lot and two on corner lots as part of subdivision development agreements.

### **3.3.4 Residential – One and Two Unit Dwellings**

The uses permitted in the area designated Residential - One and Two Unit Dwellings on Schedule A will be detached, semi-detached and duplex dwelling units including secondary dwelling units.

The proposed draft plan of subdivision provides for a full range of ground oriented dwelling types including single family, semi detached and townhouse units. It is the introduction of townhouse units (ground oriented multi-unit) that goes beyond the site objectives. Some rationale needs to be provided to support multiple units; perhaps a discussion is warranted with the Policy team (contact John Lunney) currently updating the OP team because Carp CDP will be amended to become a Secondary Plan.

### **3.7 Open Space**

Policies are addressed by:  
Mark Young, Urban Design Planner  
Reid Shepherd, Parks

### **4.10 Create Prominent Approaches to the Village**

Policies are addressed by:  
Mark Young, Urban Design Planner

#### **Key initiatives**

1. At the four approaches to the Village identified on Figure 2:
  - a) Erect a Carp Village sign using common and well-designed graphics and materials at the four main entrances to the village;
  - b) Reconfigure the road from a rural cross-section to a village cross-section (by providing sidewalks, landscaping etc.); and
  - c) Add specific design elements as visual accents that give the impression that travelers are entering a unique village with character.
2. When undertaking road works or as a special community improvement the following will be considered:
  - Plant an avenue of trees along Donald B. Munro Drive from the southern village limit to the Village Core as part of roadway improvements and development of any subdivisions.

### **5. Road Network and Right-of-Way Protection**



Policies are addressed by:  
Josiane Gervais, P.Eng. Project Manager

## **7. Recreation and Open Space**

Policies are addressed by:  
Mark Young, Urban Design Planner  
Reid Shepherd, Parks

### **7.4 Pedestrian Pathways**

#### **Policies**

1. The pedestrian pathway system is shown on Schedule C.
2. The City will ensure that new developments are linked to the existing or planned network of public sidewalks, recreational pathways and on-road cycle routes, which connect parks and other open spaces, and community services and facilities.

The proposed plan does not provide for pathways as identified on Schedule 'C'- Pedestrian Pathway System ,CDP.

#### **Damien Whittaker, Senior Engineer Infrastructure**

##### **Surveying:**

Survey monument to be shown and annotated, and sufficient information to enable a layperson to locate.

##### **Water pipes:**

There is a municipal water pipe near the application, though presently there is no capacity in the Carp water treatment plant for the application. When capacity is made available, a looped system may be needed. A 203 mm PVC stub exists in the property. A boundary condition request was submitted and the response to that request is as copied herein "It is to our understanding that there is limited/no more capacity in the Village of Carp Water facility to support further developments. With the understanding that any remaining residual capacity has already been allocated we can not provide the Water Boundary Condition for further site applications at this stage."

##### **Sanitary Sewers:**

There is a municipal sanitary sewer adjacent the proposed development, though, presently, there is no capacity in the Carp sanitary pump station for the development. a 200 mm dia sani pipe stub exists in the proposed development. Please check the capacity of the downstream pipes to accept the proposed flows.

The Carp sanitary pump station forcemain is in the ROW and needs to be cautioned against.

**Geotechnical:**

Please note that sensitive marine clays are anticipated in the area of the proposal and, if so, enhanced geotechnical investigation and analysis will be necessary. Investigation of clays should be undertaken with vane shear, Atterberg limits, shrinkage, size, grade raise restriction, consolidation, sensitivity, and liquefaction analysis- amongst others. Further, to maintain the desired result of the trees in clay soils policy all of the conditions of the policy need to be met. Please note that the 2.1 m of cover in the vicinity of the footings is sometimes a challenge as is the necessary comprehensive linkages between geotechnical, grading, parks, utilities, and trees. Organic soils exist in the area and enhanced geotechnical investigation and analysis will be necessary. Thin soils, and possibly bedrock outcrops exist in the area and enhanced geotechnical investigation and analysis will be necessary.

**Hydrogeological:**

A hydrogeological report will be required if a SWM pond, or similar stormwater management infrastructure, is proposed.

**Storm Sewers:**

There is a municipal storm sewer adjacent the proposed development. And a 1050 mm stub in the lands. Please review the downstream system for capacity.

**Groundwater:**

Groundwater is anticipated to be high and the level is to be derived from long-term analysis (12 months, or more). With the high groundwater anticipated, the City advises against basements for the development. An (annual) groundwater elevation, from a long-term study will be required.

**Noise and vibration:**

A noise feasibility study is required showing a number of layouts to minimize noise barriers (if required). In due course a noise report will be required for the traffic from Donald B. Munro Drive, recorded on Official Plan Schedule G as an existing collector, and for the rail corridor located 210 m away (less than the threshold). Rail safety should be reviewed against the document Guidelines for New Development in Proximity to Rail Operations.

**Integrated Environmental Review:**

An integrated environmental review is required being adjacent to an EP3 zoned area.

**Storm Water Management:**

Stormwater management quality criteria shall follow the MVCA's requirements of 80% TSS removal. The quantity criteria for the development is that 100-year

post-development shall match 5-year pre-development. LID is required as per the memo from the former MOECC (now MECP). A water budget will need to be developed for the proposal and resulting in a 15% reduction in the change. Any existing stormwater runoff from adjacent site(s) that crosses the property must be accommodated by the proposed stormwater management design. All stormwater management determinations shall have supporting rationale. The stormwater management shall itemize concurrence with the content of the update Carp River watershed/subwatershed study. In the pre-consultation it was suggested that that quantity control for the lands being applied for currently was provided by the lands already developed to the south of Donald B Munro known as the Rivington lands. Based on a review of the Rivington report, and existing development, quantity control does not appear to be provided.

#### Roads:

Please refer to the City of Ottawa Private Approach By-Law 2003-447 for the entrance design. Some of the driveways might be a challenge at curves. As per the Safer Roads initiative (adopted by Council, late 2019), roads must be designed to limit vehicle speeds to 30 km/h (by design; not merely by signage). Additional ROW will be required if sidewalks and/or sensitive marine clay is found. Please note that additional width is required for SMC and additional width for sidewalks (if required)

Energy conservation is required to be demonstrated throughout design as per section 4.9 of the Official Plan.

#### Permits and Approvals:

Please contact the Mississippi Valley Conservation Authority (MVCA), amongst other federal and provincial departments/agencies, to identify all the necessary permits and approvals required to facilitate the development: responsibility rests with the developer and their consultant for determining which approvals are needed and for obtaining all external agency approvals. The address shall be in good standing with all approval agencies, for example MVCA, prior to approval. Copies of confirmation of correspondence will be required by the City of Ottawa from all approval agencies that a form of assent is given. Please note that a stormwater program for multiple lots is understood to be the expanded type of Environmental Compliance Approval (ECA) application with the MECP; please speak with your engineering consultant to understand the impact this has on the application. An MECP ECA application is not submitted until after City of Ottawa engineering is satisfied that components directly or indirectly aligned with the ECA process concur with standards, directives and guidelines of the MECP. No construction shall commence until after a commence work notification is given by Development Review. Please also note that by the time the ECA is applied for with this application that a different type of process may be underway.

Contact Information:  
Christina Des Rochers  
Water Inspector  
613-521-3450 ext. 231  
[Chstina.Desrochers@ontario.ca](mailto:Chstina.Desrochers@ontario.ca)

Contact Information:  
Erica Ogden  
[eogden@mvc.on.ca](mailto:eogden@mvc.on.ca)

Plan Submission Requirements for engineering:

Site Servicing Plan\*  
Grading and Drainage Area Plan\*  
Erosion and Sediment Control Plan\*

\*All identified required plans are to be submitted on standard A1 size sheets as per [City of Ottawa Servicing and Grading Plan Requirements](#) and shall note the survey monument used to establish datum on the plans with sufficient information to enable a layperson to locate the monument.

Report Submission Requirements:

- Site Servicing Report  
To be prepared as per [requirements](#).
- Storm Water Management Report
- Noise Feasibility Report
- Erosion and Sediment Control Measures
- Geotechnical Investigation Study  
The geotechnical consultant will need to provide full copies of any published and peer reviewed papers relied on to determine results and conclusions  
Earthquake analysis is now required to be provided in the report.

-Phase 1 Environmental Site Assessment (ESA)

The Phase 1 Environmental Site Assessment (ESA) shall be as per O.Reg. 153/04. Phase 1 ESA documents performed to CSA standards are not acceptable. Documents older than 18 months from the time of draft approval will not be accepted

Guide to preparing City of Ottawa Studies and Plans:

<http://ottawa.ca/en/development-application-review-process-0/guide-preparing-studies-and-plans>

To request City of Ottawa plan(s) or report information please contact the ISD Information Centre:

[Information Centre](#)  
(613) 580-2424 ext. 44455

**Joseph Zagorski, P.Eng. Senior Project Manager Asset Management Branch – Infrastructure Planning**

Additional comments provided in lieu of pre-consult meeting on state of water servicing for the Village of Carp provided to applicant's consultant:

- Currently peak wet weather flows to the Carp PS are approaching (exceeded) its rated capacity of 57.7 L/s pumping ability, limiting available capacity for the new residential and commercial development in the village. The station is 25 years old with some mechanical and electrical components quickly reaching the end of their design life. In addition, no overflow is provided to protect the station and houses located close to the Carp River during equipment failure or extreme I/I event. The preferred **long-term** (*to accommodate projected wastewater flows from the Carp build-out development inside village boundary*) solutions to the Carp wastewater system includes emergency overflows at both sewage pumping stations, twinning existing forcemain, upgrading pumps and back-up power. Implementing long term solutions to the Carp water and wastewater systems as proposed in the 2009 Class EA is a time-consuming process, required extensive design and construction work including significant capital budget allocation.
- The City has hired a consultant to investigate the possibility of **short-term** options to increase the Carp PS interim capacity (such as installation of new pumps which would deliver more flow but still be below design operating pressures of the existing forcemain) to provide capacity for the new development. This assignment will also confirm if Carp water facility has presently enough capacity to accommodate additional village and Carp Airport development.

### **Reid Shepherd, Parks**

We understand that during the pre-consultation it was suggested there was a clause in the Green Meadows Subdivision (Former Rivington lands opposite Donald B. Munro Dr.) that spoke to an over-dedication of parkland. More specifically, clause 8b page 31 of the Green Meadows subdivision agreement states:

"In recognition of the over-dedication of parkland by the Owner, the City agrees to transfer the parkland dedication in excess of 5% to the future development of other lands owned by the Owner described as Part of Lot 17, Concession 2, Geographic Township of Huntley, City of Ottawa being Part 1 on Plan 4R-7027". (Agreement attached).

This matter was forwarded to Legal Services for an opinion on the agreement. We understand that the over dedication is applicable to the one who signed the agreement and developed the Rivington subdivision only. Such a clause is not transferable to a new Owner of the lands in question.

As such the following is required with your submission:

Park and Facility Planning Comments:



- The density of this proposal is above 18 units per net hectare and therefore a parkland dedication of 0.43 ha is required based on the current unit numbers.
- Based on the above requirement of 0.43 ha, a parkette located within the development would be feasible. Please revise the concept to include a parkette centrally located within the development. Parkette requirements (location, amenities, etc) and further details can be found within the Park Development Manual, 2<sup>nd</sup> Edition.
- The Carp CDP proposes a north-south pathway connection to link up with the existing pathway across Donald B Munro, and a second east-west pathway along the northern edge of the development. Please revise the concept to show improved pathway connections within the site and to adjacent subdivisions in line with the vision of the CDP.

### **Mark Young, Urban Design Planner**

Please accept the following comments on behalf of PRUD for the proposed plan of subdivision and zoning by-law amendment in the Village of Carp. A Design Brief will be required. The terms of reference is attached.

#### Plan of Subdivision:

1. Please review for compliance with the Village of Carp CDP which is being converted into a Secondary Plan as part of the New Official Plan.
2. The CDP identifies an open space corridor across the subject lands linking the park lands to the west with the Carp Ridge to the east. Open space connection blocks are identified as having a width of 10 m and should include tree retention and a publicly accessible path.
3. The CDP does not identify street townhomes as a permitted use in the subject land use designation.
4. Efforts should be taken to minimize the need for noise walls on Donald B. Munro Drive. Options include a window street, rear lane product or fronting lots and driveways directly onto the existing roadway as-is the case in most of the Village.
5. Connectivity to the Carp Ridge is a significant asset for the site. A minimum of two connection points to the natural area to the east should be provided and should be of an adequate width to allow for some views and vistas of this feature.

#### Zoning By-law Amendment:

1. The Zoning By-law amendment should reflect the need for adequate setbacks and buffering from existing low-density residential uses.
2. The zoning should be reflective of soil conditions, if clay soil tree setbacks are required in front and corner side yards.
3. The zoning should be reflective of the product types proposed. An R1 zone should be utilized abutting the existing dwellings and a minimum lot width and lot coverage should be reflective of the desire to locate the most compatible dwellings adjacent to the existing dwellings.

## **Sami Rehman, Environmental Planner Planning**

The proposed development will require an Integrated Environmental Review (IER) and Environmental Impact Statement (EIS).

- The EIS will review the:
  - NEA boundary,
  - ANSI boundary
  - PSW & wetlands associated with Sign Woodlands
  - SAR, throughout the area
  - Significant Woodlands
  - Sign Wildlife Habitat
  - Results from the RMOC's NESS
  - The surface water feature and the appropriate setbacks from OP 4.7.3.

Plan of Subdivision will require a Tree Conservation Report (TCR), which can be combined with an EIS to avoid duplication.

NEA (and EP3) boundary will need to be verified during the growing season.

### **Site Visit December 18, 2020**

Thank you for inviting me to join your site visit. It was helpful to explore the property, understand the boundaries of your potential purchase and to have the current zoning boundary (as illustrated in GeoOttawa) staked out on the subject property. While it was also useful to explore the geological features when much of the vegetation was in dormancy, it is difficult to identify the boundary of the Natural Environment Area (NEA) until we can examine the vegetation communities during the growing season. As discussed with my colleagues, we would anticipate re-visiting the site after May 2021 to assess the flora and better determine the boundary of the NEA. It is acknowledged that the final NEA boundary will also be the zoning boundary.

## **Josiane Gervais, P.Eng. Project Manager, Infrastructure Approvals**

Follow Traffic Impact Assessment Guidelines:

- Traffic Impact Assessment will be required.
- Screening and Scoping can be submitted together. Start this process asap.
- The application will not be deemed complete until the submission of the draft step 1-4, including the functional draft RMA package (if applicable), draft functional plans (if applicable) and/or monitoring report (if applicable).
- Request base mapping asap if RMA is required. Contact Engineering Services (<https://ottawa.ca/en/city-hall/planning-and-development/engineering-services>)

Local and collector roadways are to be designed for a 30km/hr posted speed, as per the approved Road Safety Action Plan. Further information on design elements to achieve the 30 km/hr design speed can be provided upon request.

If any collector roads are considered, you must follow collector road guidelines for subdivisions, desired 26m ROW for collector Roads.

Geometric Road Design (GRD) drawings will be required with the first submission of underground infrastructure and grading drawings. These drawings should include such items as, but are not limited to:

- Road signage and pavement markings;
- Location of depressed curbs and tactile walking surface indicators (TWSI);
- Traffic calming measures aimed at reducing vehicle speed and enhancing pedestrian safety. Measures may include either vertical or horizontal features, however such measures shall not interfere with stormwater management and overland flow routing. Traffic calming measures shall reference best management practices from the Canadian Guide to Neighbourhood Traffic Calming, published by the Transportation Association of Canada, and/or Ontario Traffic Manual, and/or the City of Ottawa's Traffic Calming Design Guidelines;
- Intersection control measures at new internal intersections; and
- ROW protection on Donald B. Munro between Langstaff and Farm Ridge is 23m even, and between Farm Ridge and March Road is 26m even.
- Requesting to change the speed limit on Donald B. Munro as part of the application is not supported.
- Corner triangles as per OP Annex 1 - Road Classification and Rights-of-Way at the following locations on the final plan will be required:
  - Local Road to Local Road: 3 m x 3 m
  - Local Road to Arterial Road: 5 m x 5 m
- Ensure to pair driveways where possible.
- Noise Impact Studies are required. Both studies must assess:
  - Road, site is within 100m of Donald B. Munro, which is a collector roadway.
  - Rail, site is within the buffer zone Renfrew Rail Corridor, which is an active rail corridor.

It is highly recommended to review noise conditions as soon as possible so that noise effects can be avoided or mitigated as part of the subdivision design. The Noise Feasibility Study is required at the time of application. A detailed Noise Study will be required prior to registration.

### **Erica Ogden. Environmental Planner MVCA**

Please find below a summary of the Conservation Authority's comments.

- The property contains a watercourse and unevaluated wetlands. The Environmental Impact Statement should assess each of these features in regards to their significant under Ontario Regulation 153/06. These features

should be taken into consideration when determining the area for development on the property.

- A headwater feature assessment will be required for the watercourse on the property to provide an understanding of the feature's seasonal functions and develop a mitigation plan. Any hydraulic connection between the wetlands and watercourse should be assessed.
- MVCA will review the stormwater management for the proposed development. The water quality requirement for the Carp River is a normal level of protection which requires 70% total suspended solids removal.
- There is the potential for organic soils on the property, which must be appropriately assessed.

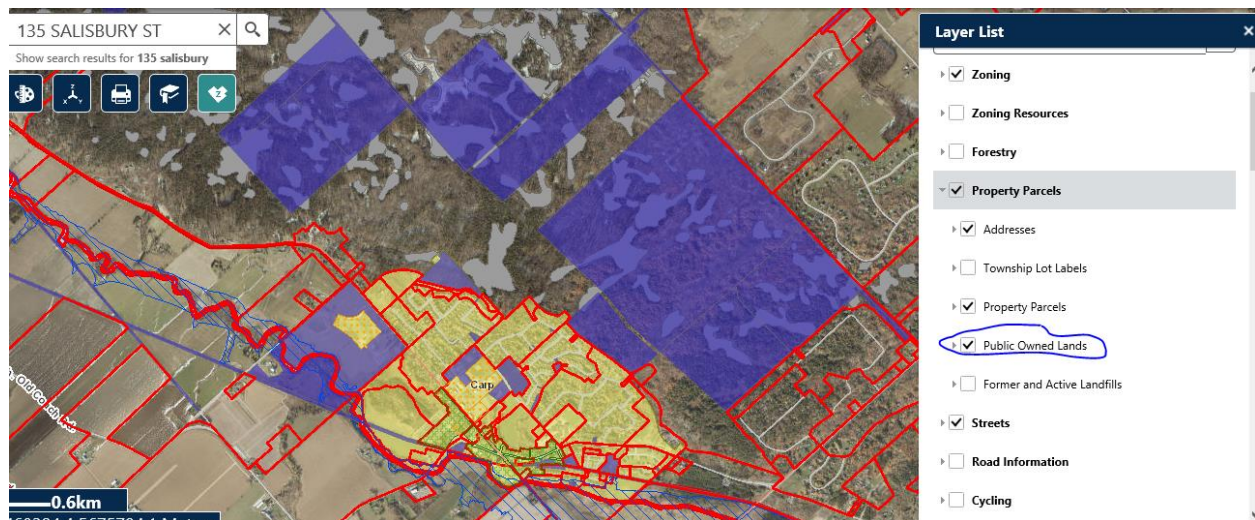
### **Adam Brown, Manager Development Review**

Some information about the Carp Hills.

<https://carphills.com/>

<https://ottawa.ca/en/living-ottawa/environment-conservation-and-climate/conservation-areas#carp-hills>

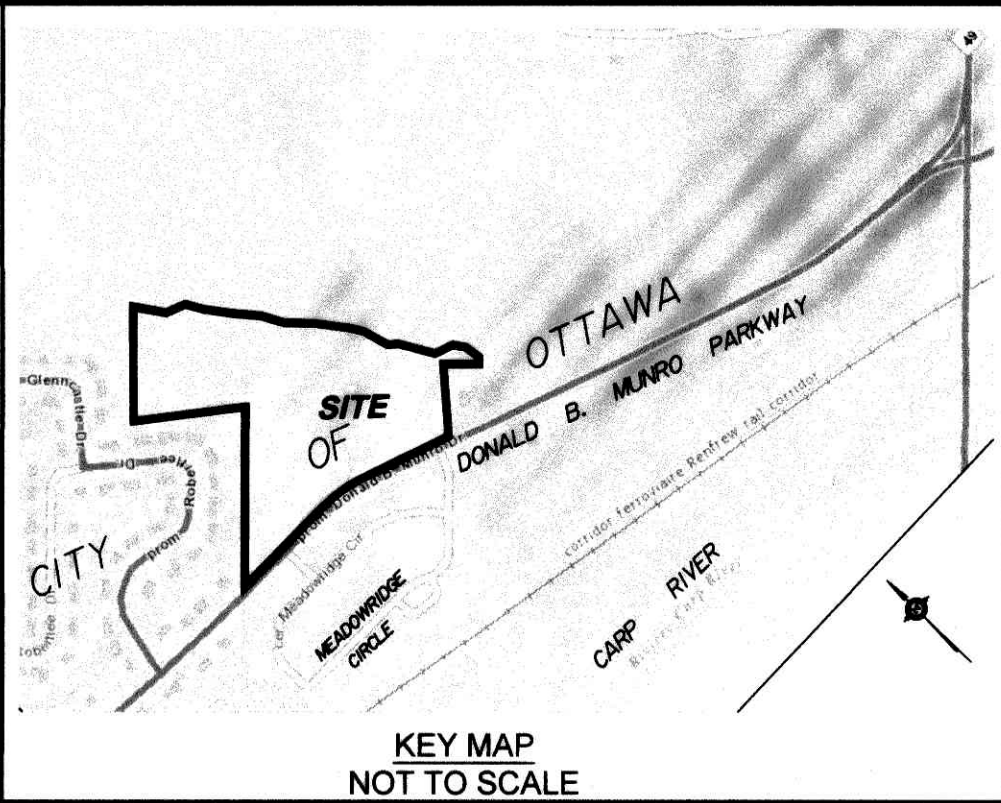
To see what land the City owns in the area, you can go on geoOttawa and turn on the “Property Parcels – Public Owned Lands” box and you will see the City-owned lands in blue.





## **Appendix B: Draft Plan of Subdivision**





**DRAFT PLAN OF SUBDIVISION OF  
PART OF LOT 17  
CONCESSION 2**  
Geographic Township of Huntley  
**CITY OF OTTAWA**  
Prepared by Annis, O'Sullivan, Vollebakk Ltd.

Scale 1:1000  
Metric  
DISTANCES SHOWN ON THIS PLAN ARE IN METRES AND  
CAN BE CONVERTED TO FEET BY DIVIDING BY 0.3048

**SURVEYOR'S CERTIFICATE**

I CERTIFY THAT:  
The boundaries of the lands to be subdivided and their relationship to  
adjoining lands have been accurately and correctly shown.

Date: Oct 5/21  
Signature: [Signature]  
ONTARIO LAND SURVEYOR

**OWNER'S CERTIFICATE**

This is to certify that we are the owners / agents of the lands to be subdivided  
and that this plan was prepared in accordance with our instructions.

Date: Oct 8, 2021  
Signature: [Signature]  
Constable Rivington-Howe (Executor)  
Estate of Douglas Gordon Rivington  
I have authority to bind the corporation

Date: Oct 8, 2021  
Signature: [Signature]  
Susan Rivington (Executor)  
Estate of Douglas Gordon Rivington  
I have authority to bind the corporation

**ADDITIONAL INFORMATION REQUIRED UNDER  
SECTION 51-17 OF THE PLANNING ACT**

- (a) see plan
- (b) see plan
- (c) see plan
- (d) single and multi-family residential housing and walkway
- (e) see plan
- (f) see plan
- (g) see plan
- (h) City of Ottawa
- (i) see soils report
- (j) see plan
- (k) sanitary, storm sewers, municipal water, bell, hydro, cable and gas to be available
- (l) see plan

PROPOSED LAND USE TABLE			
PROPOSED USE	BLOCKS	UNITS	Ha (Ac)
SINGLES	1 - 57	57	4.306 (10.64)
TOWNHOMES	58, 59, 60, 62, 63, 65 - 72, 74 and 75	54	1.590 (3.93)
SEMI-DETACHED	61, 64 and 73	6	0.284 (0.65)
STREETS	1, 2 AND 3	...	1.512 (3.73)
WALKWAY	76 and 77	...	0.338 (0.84)
TOTAL		117	8.01 (19.79)

**ANNIS, O'SULLIVAN, VOLLEBEKK LTD.**  
14 Concourse Gate, Suite 500  
Nepean, Ont. K2E 7S6  
Phone: (613) 727-0850 / Fax: (613) 727-1079  
Email: Nepean@aovltd.com  
Job No. 20703-20-Toronto P1 L1 L2 L3 L4 L5 L6 L7 L8 L9 L10 L11 L12 L13 L14 L15 L16 L17 L18 L19 L20 L21 L22 L23 L24 L25 L26 L27 L28 L29 L30 L31 L32 L33 L34 L35 L36 L37 L38 L39 L40 L41 L42 L43 L44 L45 L46 L47 L48 L49 L50 L51 L52 L53 L54 L55 L56 L57 L58 L59 L60 L61 L62 L63 L64 L65 L66 L67 L68 L69 L70 L71 L72 L73 L74 L75 L76 L77 L78 L79 L80 L81 L82 L83 L84 L85 L86 L87 L88 L89 L90 L91 L92 L93 L94 L95 L96 L97 L98 L99 L100 L101 L102 L103 L104 L105 L106 L107 L108 L109 L110 L111 L112 L113 L114 L115 L116 L117 L118 L119 L120 L121 L122 L123 L124 L125 L126 L127 L128 L129 L130 L131 L132 L133 L134 L135 L136 L137 L138 L139 L140 L141 L142 L143 L144 L145 L146 L147 L148 L149 L150 L151 L152 L153 L154 L155 L156 L157 L158 L159 L160 L161 L162 L163 L164 L165 L166 L167 L168 L169 L170 L171 L172 L173 L174 L175 L176 L177 L178 L179 L180 L181 L182 L183 L184 L185 L186 L187 L188 L189 L190 L191 L192 L193 L194 L195 L196 L197 L198 L199 L200 L201 L202 L203 L204 L205 L206 L207 L208 L209 L210 L211 L212 L213 L214 L215 L216 L217 L218 L219 L220 L221 L222 L223 L224 L225 L226 L227 L228 L229 L230 L231 L232 L233 L234 L235 L236 L237 L238 L239 L240 L241 L242 L243 L244 L245 L246 L247 L248 L249 L250 L251 L252 L253 L254 L255 L256 L257 L258 L259 L260 L261 L262 L263 L264 L265 L266 L267 L268 L269 L270 L271 L272 L273 L274 L275 L276 L277 L278 L279 L280 L281 L282 L283 L284 L285 L286 L287 L288 L289 L290 L291 L292 L293 L294 L295 L296 L297 L298 L299 L300 L301 L302 L303 L304 L305 L306 L307 L308 L309 L310 L311 L312 L313 L314 L315 L316 L317 L318 L319 L320 L321 L322 L323 L324 L325 L326 L327 L328 L329 L330 L331 L332 L333 L334 L335 L336 L337 L338 L339 L340 L341 L342 L343 L344 L345 L346 L347 L348 L349 L350 L351 L352 L353 L354 L355 L356 L357 L358 L359 L360 L361 L362 L363 L364 L365 L366 L367 L368 L369 L370 L371 L372 L373 L374 L375 L376 L377 L378 L379 L380 L381 L382 L383 L384 L385 L386 L387 L388 L389 L390 L391 L392 L393 L394 L395 L396 L397 L398 L399 L400 L401 L402 L403 L404 L405 L406 L407 L408 L409 L410 L411 L412 L413 L414 L415 L416 L417 L418 L419 L420 L421 L422 L423 L424 L425 L426 L427 L428 L429 L430 L431 L432 L433 L434 L435 L436 L437 L438 L439 L440 L441 L442 L443 L444 L445 L446 L447 L448 L449 L450 L451 L452 L453 L454 L455 L456 L457 L458 L459 L460 L461 L462 L463 L464 L465 L466 L467 L468 L469 L470 L471 L472 L473 L474 L475 L476 L477 L478 L479 L480 L481 L482 L483 L484 L485 L486 L487 L488 L489 L490 L491 L492 L493 L494 L495 L496 L497 L498 L499 L500 L501 L502 L503 L504 L505 L506 L507 L508 L509 L510 L511 L512 L513 L514 L515 L516 L517 L518 L519 L520 L521 L522 L523 L524 L525 L526 L527 L528 L529 L530 L531 L532 L533 L534 L535 L536 L537 L538 L539 L540 L541 L542 L543 L544 L545 L546 L547 L548 L549 L550 L551 L552 L553 L554 L555 L556 L557 L558 L559 L560 L561 L562 L563 L564 L565 L566 L567 L568 L569 L570 L571 L572 L573 L574 L575 L576 L577 L578 L579 L580 L581 L582 L583 L584 L585 L586 L587 L588 L589 L590 L591 L592 L593 L594 L595 L596 L597 L598 L599 L600 L601 L602 L603 L604 L605 L606 L607 L608 L609 L610 L611 L612 L613 L614 L615 L616 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L817 L818 L819 L820 L821 L822 L823 L824 L825 L826 L827 L828 L829 L830 L831 L832 L833 L834 L835 L836 L837 L838 L839 L840 L841 L842 L843 L844 L845 L846 L847 L848 L849 L850 L851 L852 L853 L854 L855 L856 L857 L858 L859 L860 L861 L862 L863 L864 L865 L866 L867 L868 L869 L870 L871 L872 L873 L874 L875 L876 L877 L878 L879 L880 L881 L882 L883 L884 L885 L886 L887 L888 L889 L890 L891 L892 L893 L894 L895 L896 L897 L898 L899 L900 L901 L902 L903 L904 L905 L906 L907 L908 L909 L910 L911 L912 L913 L914 L915 L916 L917 L918 L919 L920 L921 L922 L923 L924 L925 L926 L927 L928 L929 L930 L931 L932 L933 L934 L935 L936 L937 L938 L939 L940 L941 L942 L943 L944 L945 L946 L947 L948 L949 L950 L951 L952 L953 L954 L955 L956 L957 L958 L959 L960 L961 L962 L963 L964 L965 L966 L967 L968 L969 L970 L971 L972 L973 L974 L975 L976 L977 L978 L979 L980 L981 L982 L983 L984 L985 L986 L987 L988 L989 L990 L991 L992 L993 L994 L995 L996 L997 L998 L999 L1000 L1001 L1002 L1003 L1004 L1005 L1006 L1007 L1008 L1009 L1010 L1011 L1012 L1013 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