



# 2070 SCOTT STREET OTTAWA

DESIGN BRIEF  
OCTOBER 02, 2019

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# 1.0 WRITTEN DESCRIPTION

- Urban Design and Compatibility

# Urban Design and Compatibility

## 1.1 Views to and from the site:

The Design Brief will demonstrate how the proposed development will fit with the existing skyline. The site is not within the view cones in Annexes 8A through 8D and will not impact views to and from the parliamentary precinct.

Scott Street is currently in transition from low-rise infill dwellings and automobile-oriented uses to mixed-use high-rise developments. These changes are a response to the area's proximity to the Westboro Transit Station, future LRT station, Westboro Village and the commercial and retail hub along Richmond Road. High-rise developments that have been approved, developed or are in the planning process include 1960 Scott Street, 1950 Scott Street, 315 Tweedsmuir Avenue, 320 McRae Avenue and 1976 Scott Street. The proposal at 2070 Scott Street will contribute positively to a neighbourhood in transition and will contribute to the emerging skyline along Scott Street. The proposed building will have a high degree of amenity for its inhabitants with uninterrupted views over established low-rise neighbourhoods and towards the Ottawa river from the amenity area at the top of the building.



Figure 10 – Richmond Road / Westboro Community Design Plan Sector 7 and 9

## 1.2 Building Design

Massing and Scale/Transition/Stepbacks/ Setbacks:  
A 23-storey mixed-use building is proposed with 243 residential units and 434 sm of at grade retail uses. The tower portion of the building is sited on the most northern edge of the site while providing a step-backs to the adjacent neighbourhood as follows: step back in elevation at the top of the ground floor a distance of 7.5m and at the top of the 5 storey podium a distance of 10m to provide transition to the adjacent residential neighbourhood on the East side of the south elevation. The building also sets back 4.8m at the South East corner to align with the existing predominant setbacks present along Winona Avenue.

On the west side where the site abuts a commercial mixed-use zone, the building podium steps and follows Churchill Street creating a lively commercial street wall condition.

Note that building is set back at grade resulting in generous pedestrian clearway dimensions of : between 8.4 and 9.1m on Scott Street, 8.8 and 7.4m on Winona Avenue and 14.7 and 8.9m on Churchill. In addition to conforming to prevailing building heights, the transition into the adjacent residential neighbourhood to the south has been considered carefully; the building provides significant setbacks at the second and sixth storey to appropriately transition

into the neighbourhood. As well, three townhouse units are located along the podium on Winona Avenue, complementing the street's ground-oriented character. The proposed ramp into the underground parking facility has been located in line with the development opposite the site for consistency and minimal impact to traffic. The tower portion of the building has been located as far north towards Scott Street as possible to reduce potential impacts on the adjacent residential neighbourhood.

### Ground Floor Planning:

The Churchill/Scott corner will act as a gateway along Churchill Ave to the retail shopping district along Richmond Rd.

The open space on the ground floor at the corner of Churchill Avenue and Scott Street allows for a plaza related to the retail uses. This corner plaza will provide a strong design statement at grade. Landscaping is proposed along Churchill Avenue, Scott Street and Winona Avenue to provide natural drainage and animate the pedestrian experience.

Active entrances will be located on all three street facing sides of the building. Retail entrances along Churchill Avenue enhance the street's mixed-use character, with one retail entrance off Scott Street. The main residential entrance/lobby is from Scott Street and will have visible signage to indicate the entrance/lobby. The three townhouse units will have individual entrances to each unit from Winona Avenue.

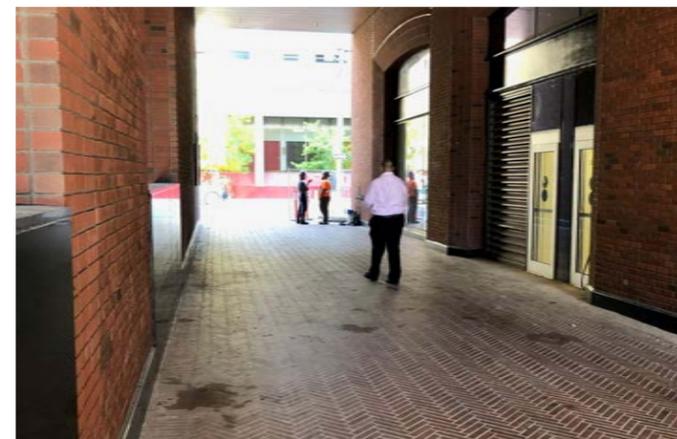
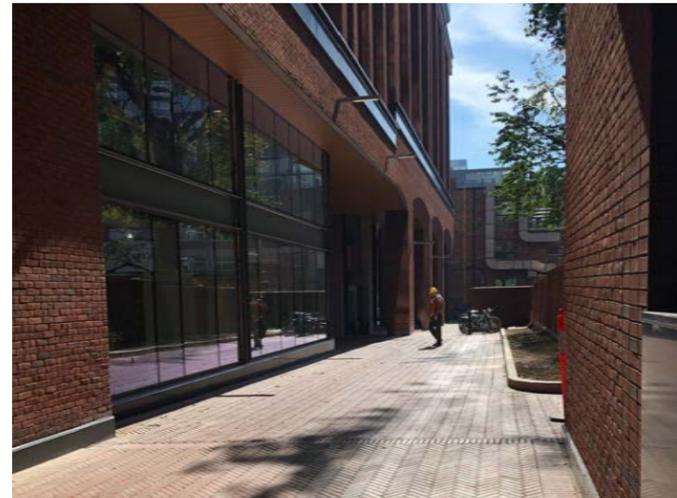
Landscaping:

The proposed site design includes various building step-backs, landscape plantings, interior rooftop amenity area and an exterior amenity area that will clearly define and connect the public and private spaces. New trees and landscape features are proposed, achieving an improved pedestrian-focused public realm.

The design respects the existing trees on adjacent properties through building setbacks along the rear, adjacent to the low-rise neighbourhood to the south.

The landscape design for the development seeks to increase the natural stormwater permeation into the ground through tree planting, planting beds and pavers. The Landscape Plan makes use of native species trees to increase urban forest and mitigate urban heat island effect.

An accessible, pedestrian, through-block connection has been provided along the full length of the southern edge of the site. This access widens at the mid-point with an outdoor greenspace as well as entrance to visitor bike parking. This proposed connectivity responds to and continues the emerging open space proposals continuing along Scott Street to the West.



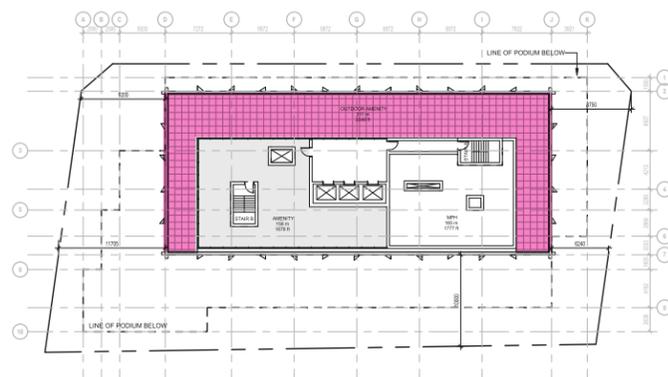
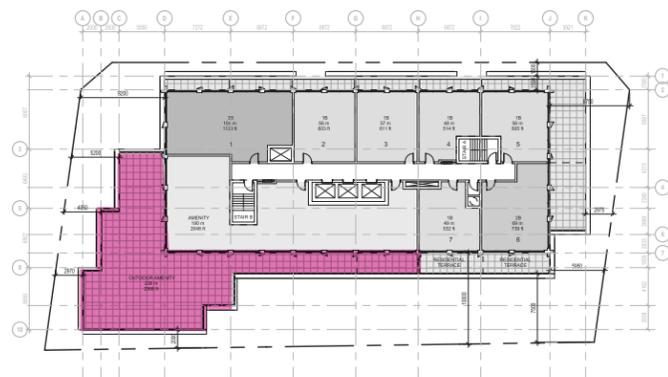
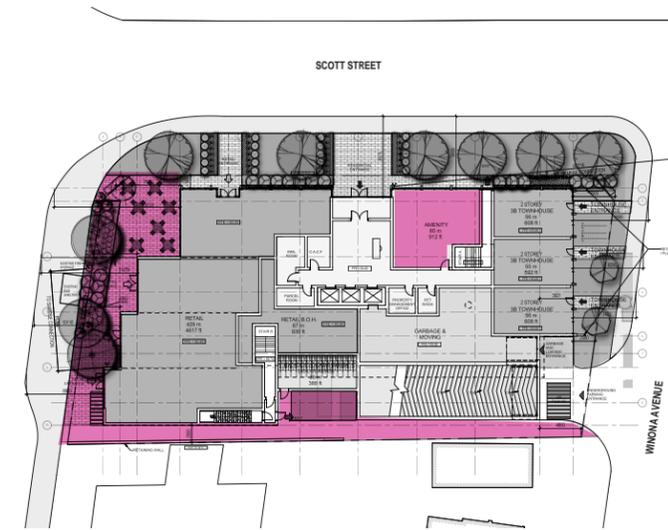
Examples of accessible, pedestrian, through-block connection



## 1.4 Outdoor Amenity Areas:

A proposed mix of outdoor private amenity space and retail amenity space is provided through building at key corners and on the rooftops of building taking advantage of the building step-backs. The design provides for a total of 1,447 sm of amenity space for the development. The open space on the ground floor at the corner of Churchill Avenue and Scott Street allows for a plaza related to the retail uses. 85sm of grade related amenity space is provided at the ground floor.

There is communal exterior 'rooftop' amenity area (238sm) located on the sixth floor with direct access from the interior communal amenity space (190sm). Additional interior (156 sm) and exterior amenity (311 sm) are provided at top floor on the uppermost rooftop.



## Development adjacent to major greenspaces and waterways

Scott Street is identified as an on-road cycling route. There are many cycling and multi-use pathways available in close proximity to the site with a multi-use pathway system on the north side of Scott Street. This pathway connects directly to the Trans Canada Trail (approx. 300m from the site) which winds along the Ottawa River connecting all of the neighbourhoods along the rivers South side. The site is located within a 8-10 min walk to large greenspaces to the West such as Rochester Field and Westboro Beach proper.

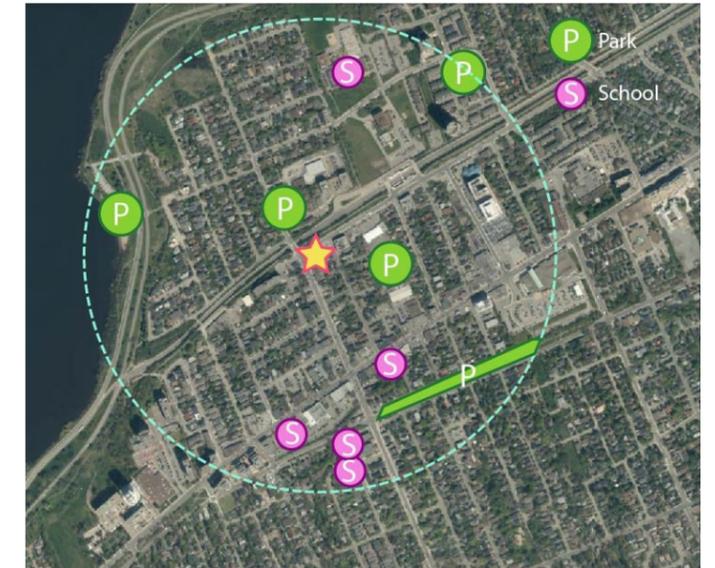


Figure 2 – Community Amenities within 600 metres of the Site.



Figure 4 – Schedule C Primary Urban Cycling Network.

## 1.5 Design Priority Areas:

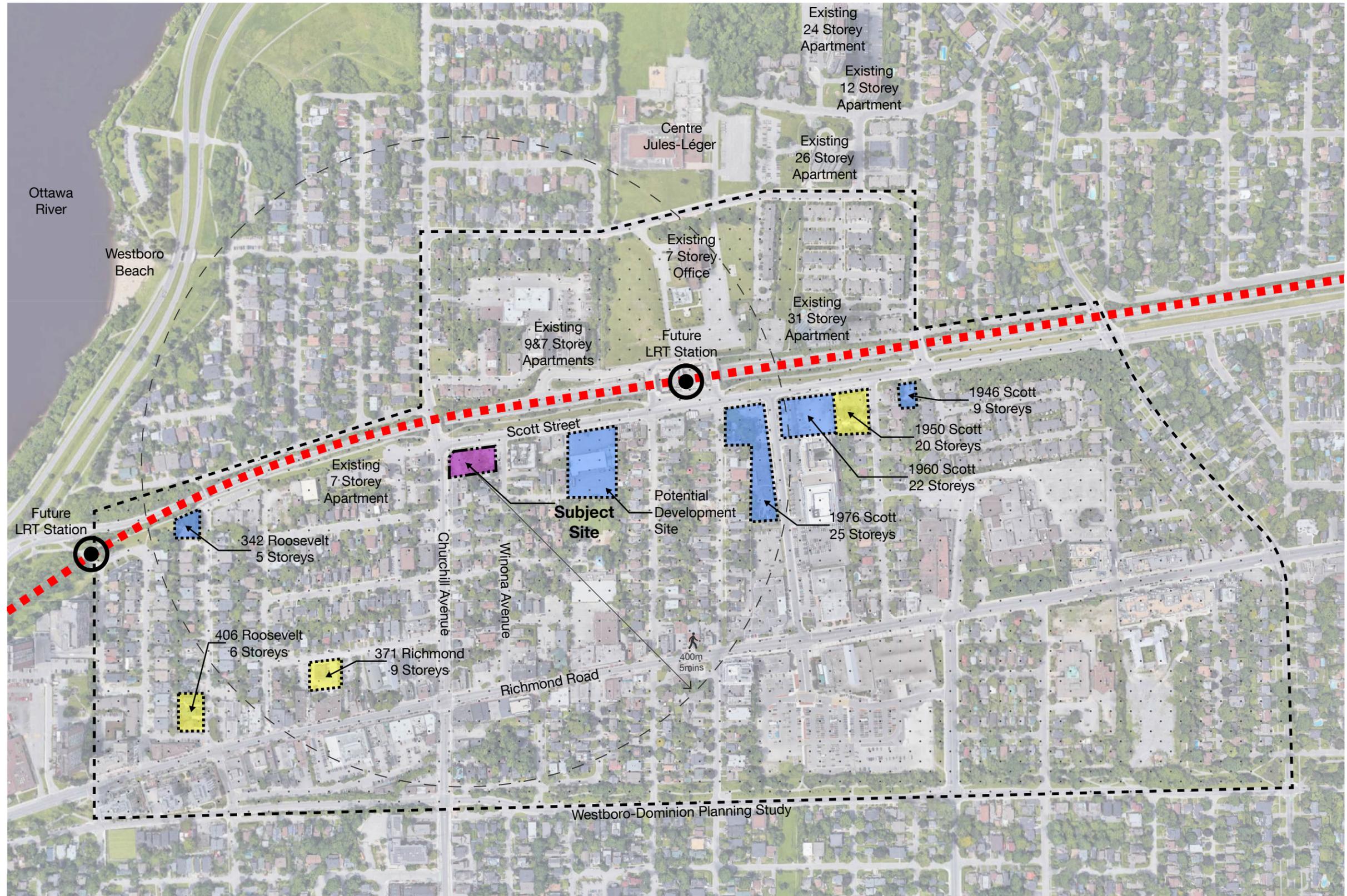
The site is not located within a Design Priority Area but principles of the guidelines for intensification have been used to inform the proposed design.

## 2.0 SITE CONTEXT

- Opportunities and Constraints
- Subject Site Analysis
- Street Views

**Legend**

- Subject Site 
- Future LRT 
- Future LRT Station 
- Contextual Development (Approved | Under Construction) 
- Contextual Development (Proposed) 



**Legend**

Subject Site 

**Approximate Lot Area**

Subject Site:  
+/- 20,037sf (1,861sm)

**Zoning Summary**

Zoning By-law 2010-40  
C9  
Commercial

**Official Plan Summary**

Located in the Richmond Road /  
Westboro Secondary Plan  
- Maximum 6 storey height  
- Mixed Use permissions

**Site Analysis**



1:1,000



**General Disclaimer for Site Analysis:**  
For the purpose of this preliminary study,  
the property lines shown are approximate.  
The information has been obtained from  
online property data maps.



SCOTT STREET LOOKING WEST

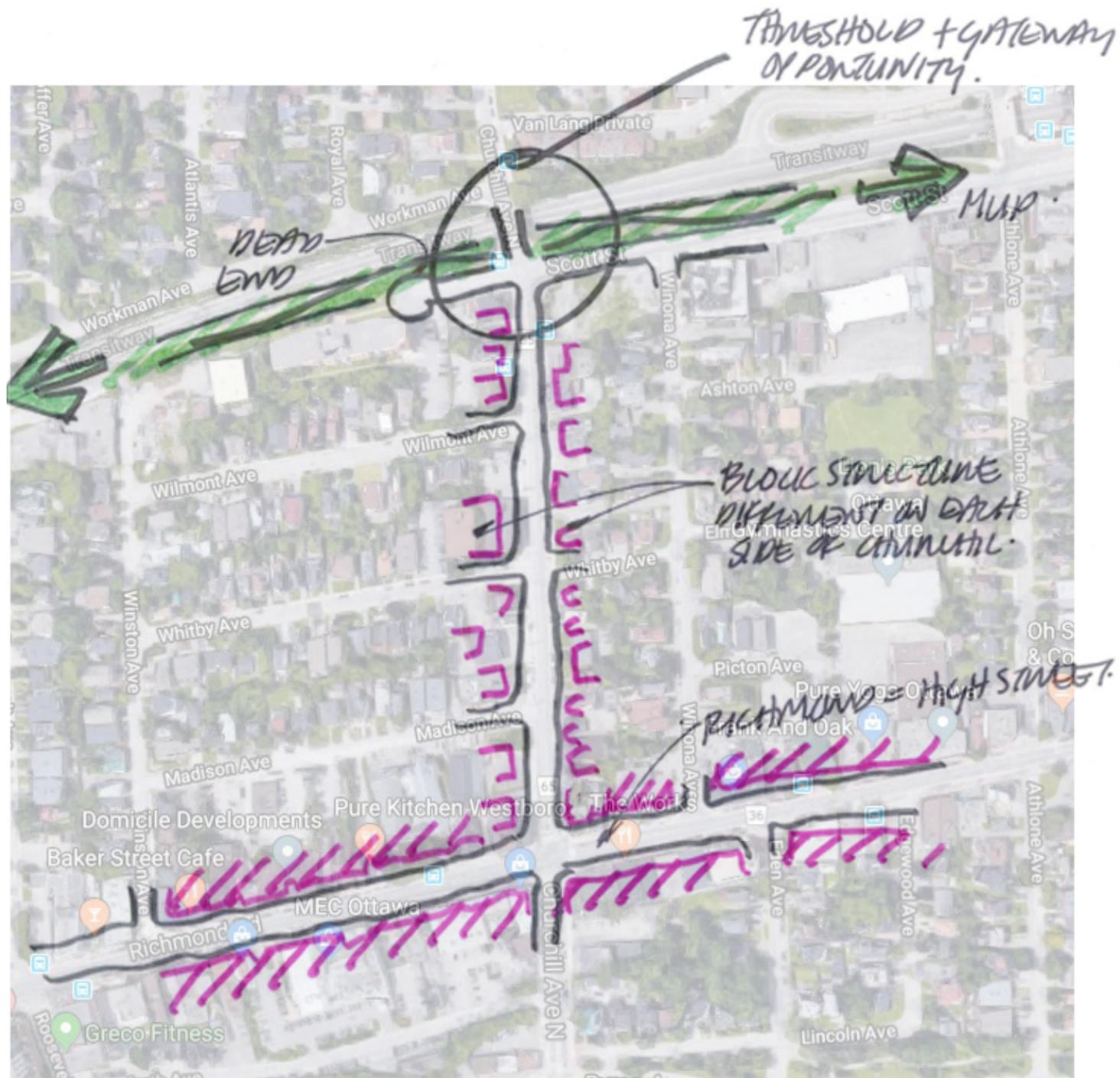


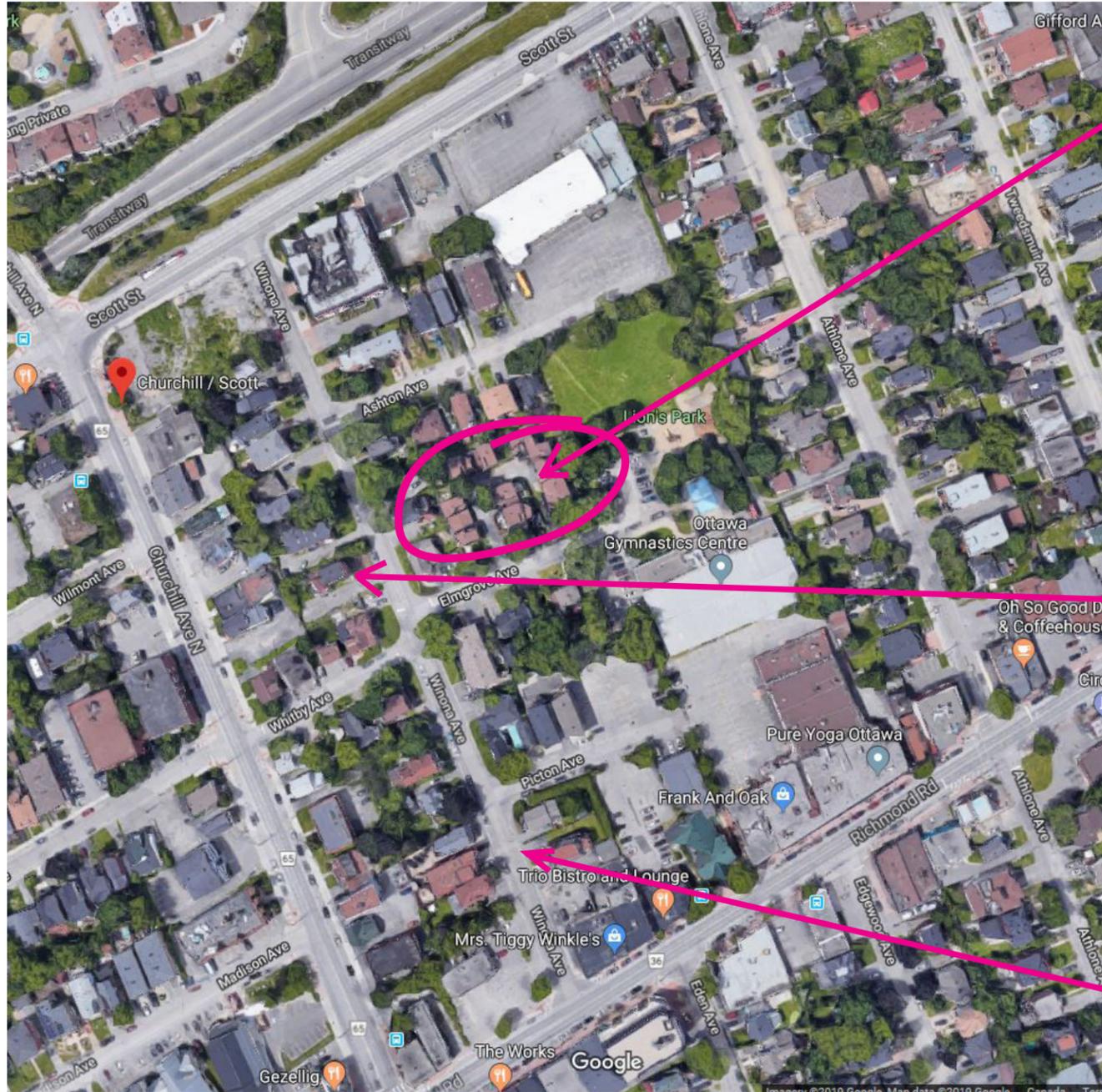
SCOTT STREET LOOKING EAST

## 3.0 SITE ANALYSIS

- T.O.D Scale
- Scott Street and Churchill Avenue Analysis
- Variety of Conditions Along Winona Avenue







Aerial illustrating the variety of conditions along Winona



Dense multi-unit enclave



Semis, tri-plexes and walk-ups make up 38% percent (5 of 13 structures) of street frontage

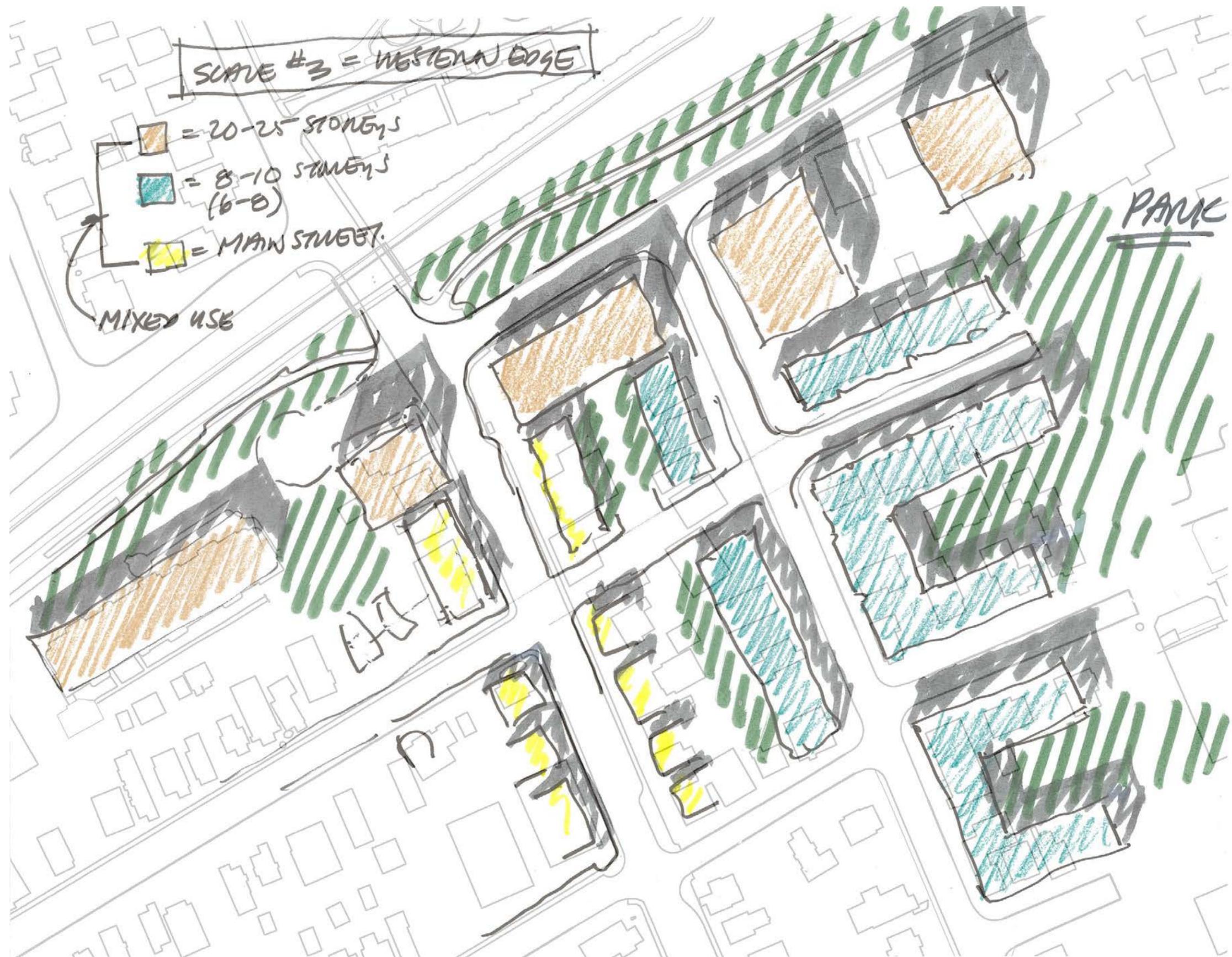


Paved public realm traveling north from Richmond



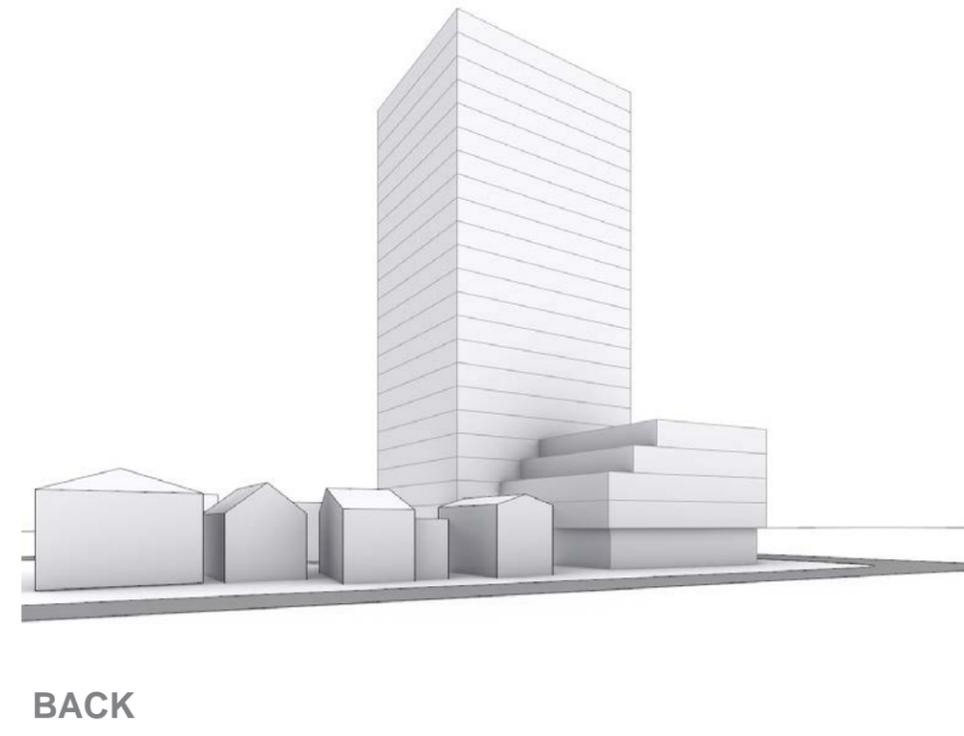
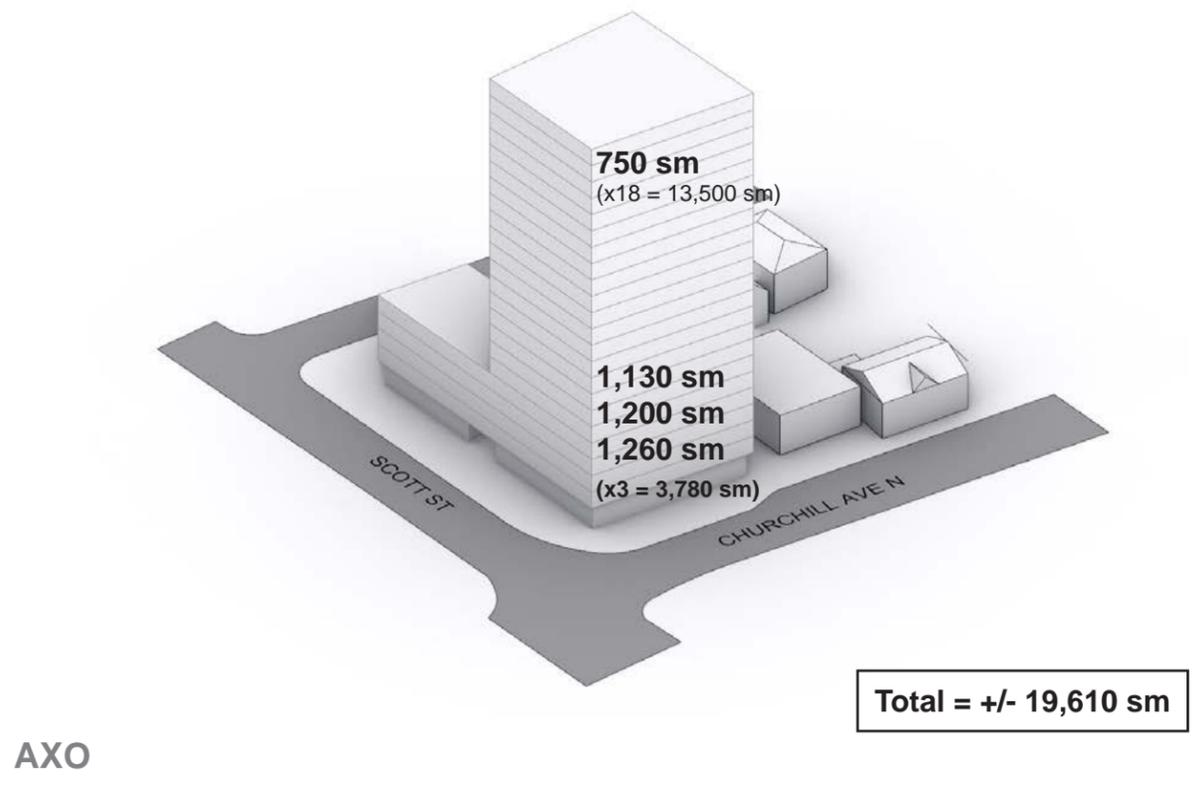
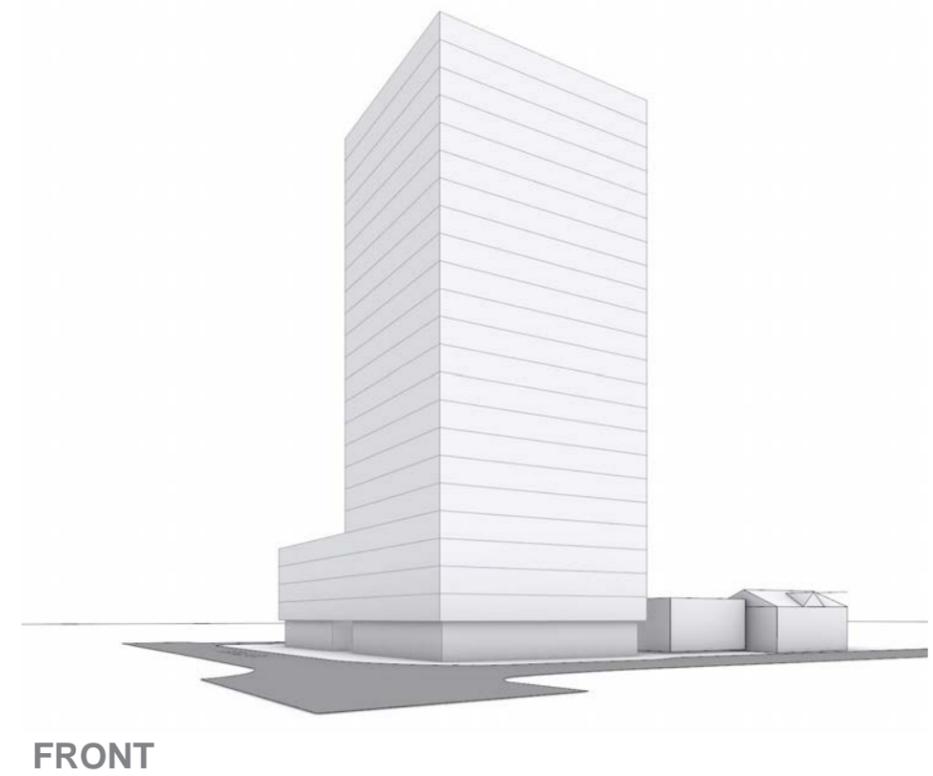
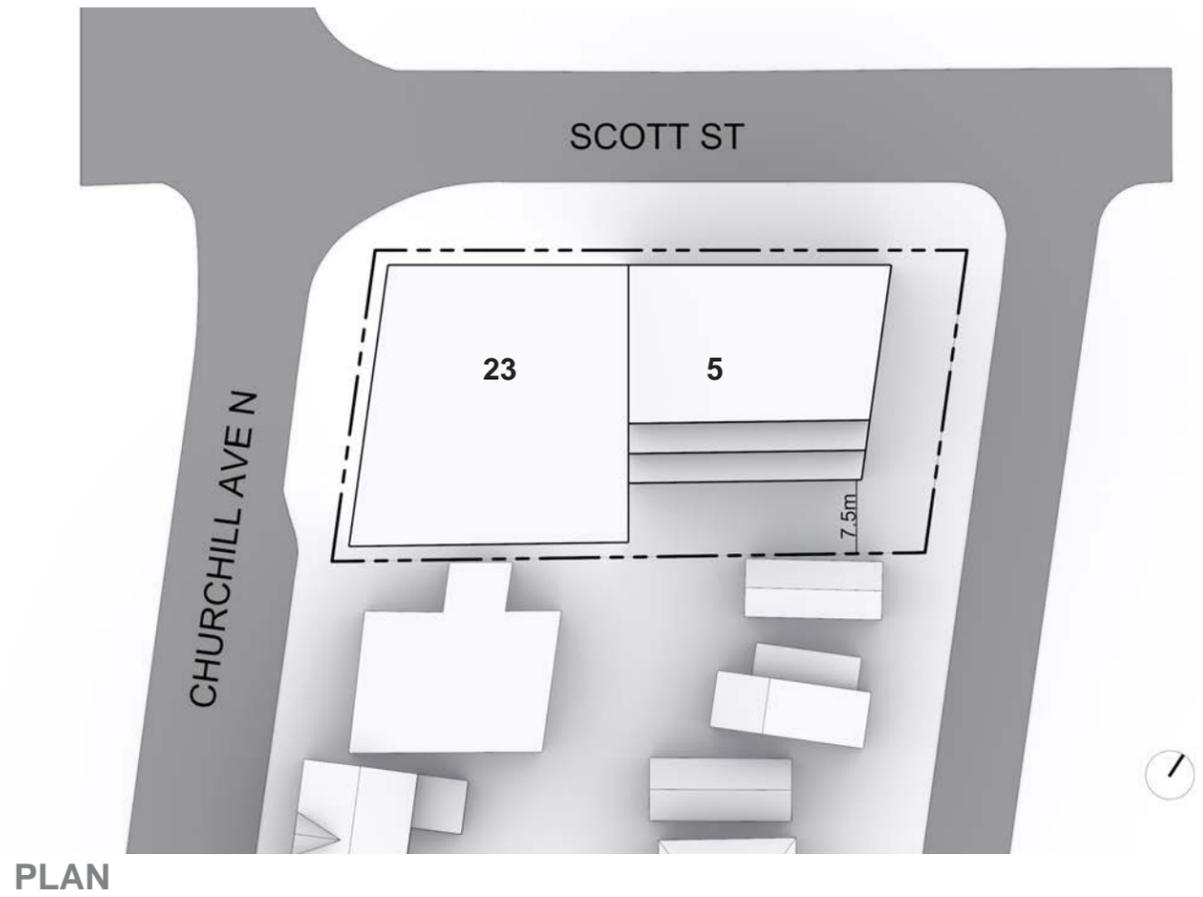


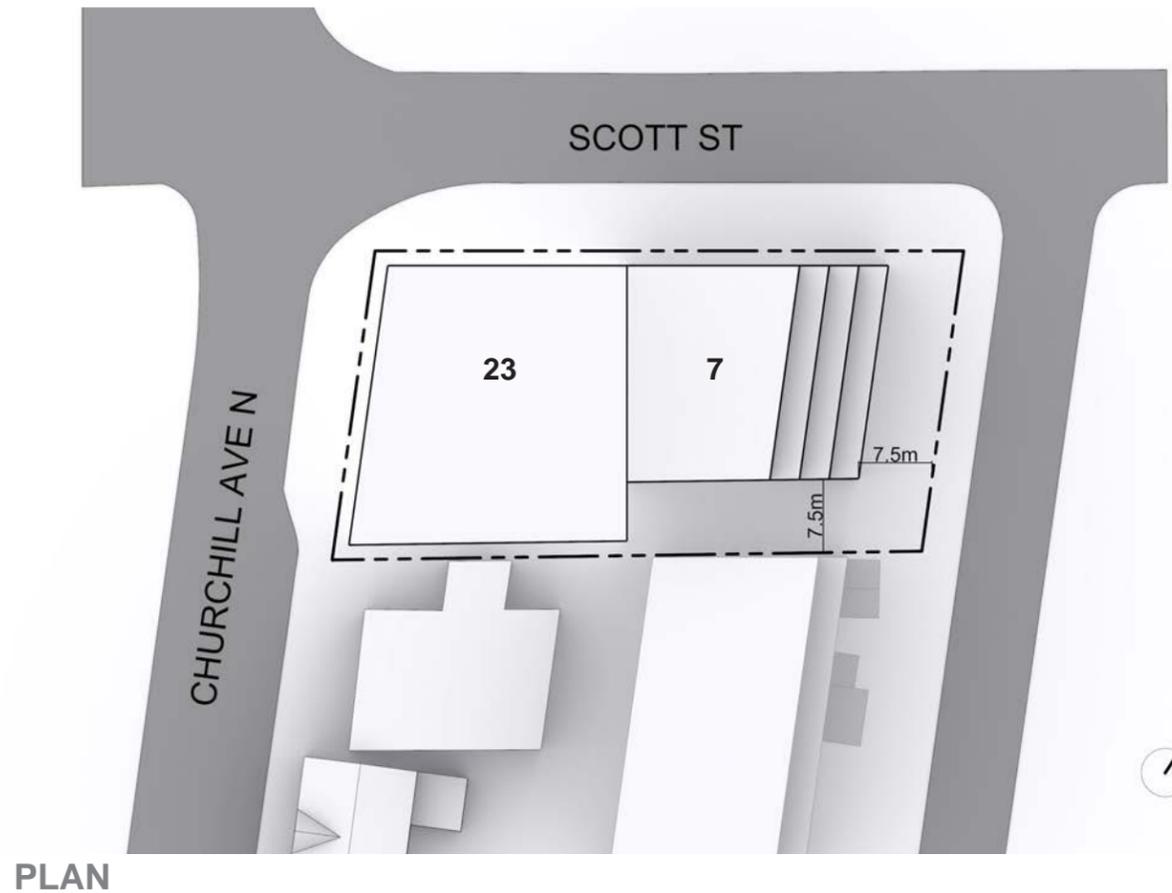




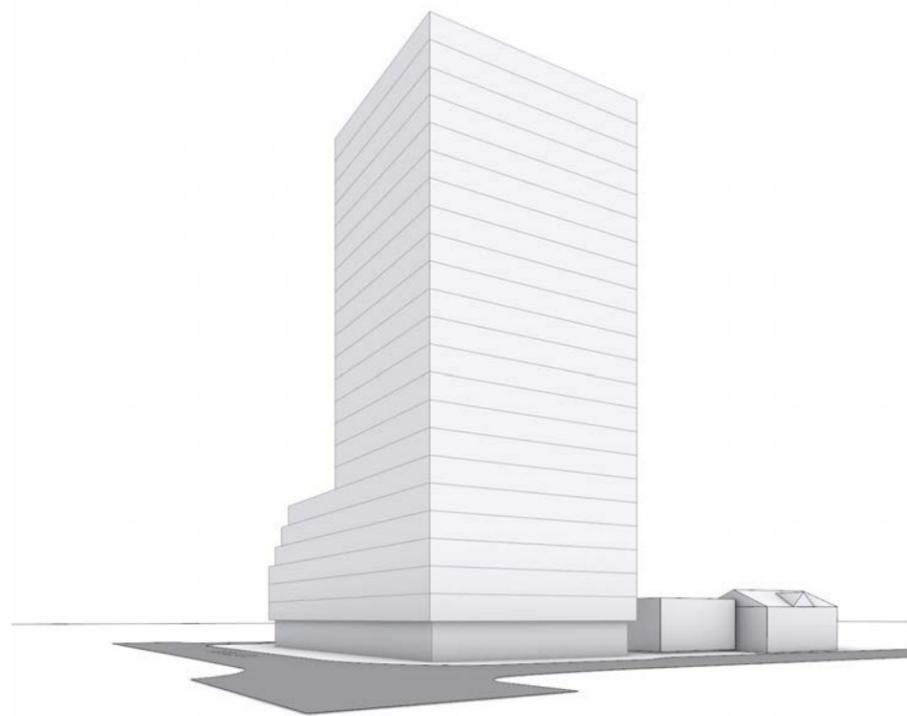
# 4.0 APPROACH TO MASSING

- Studies / Approach to Transition

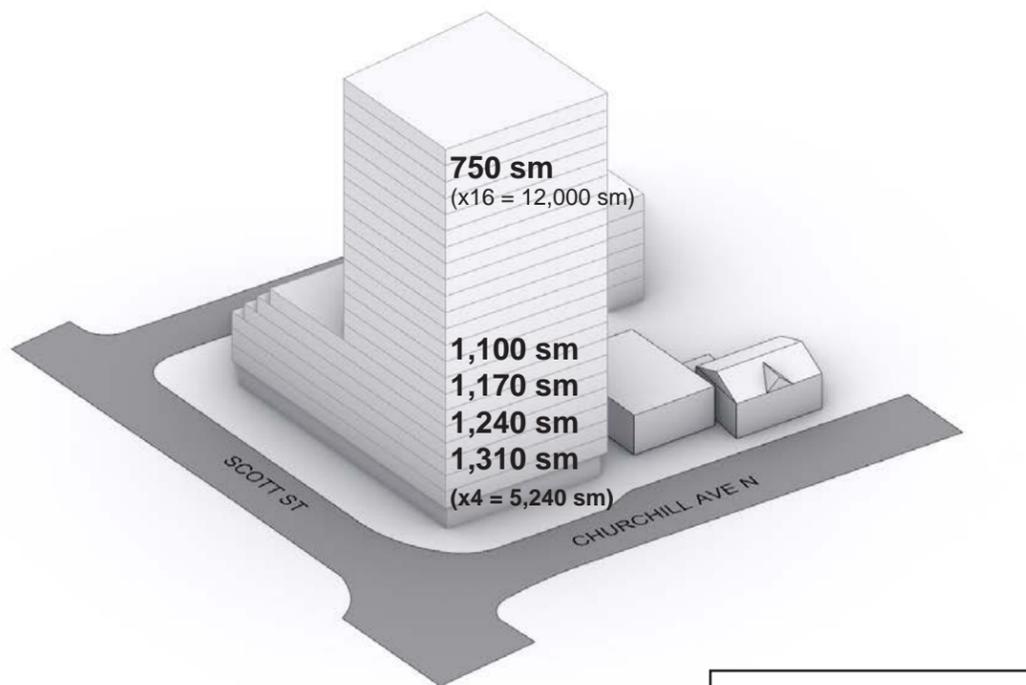




PLAN

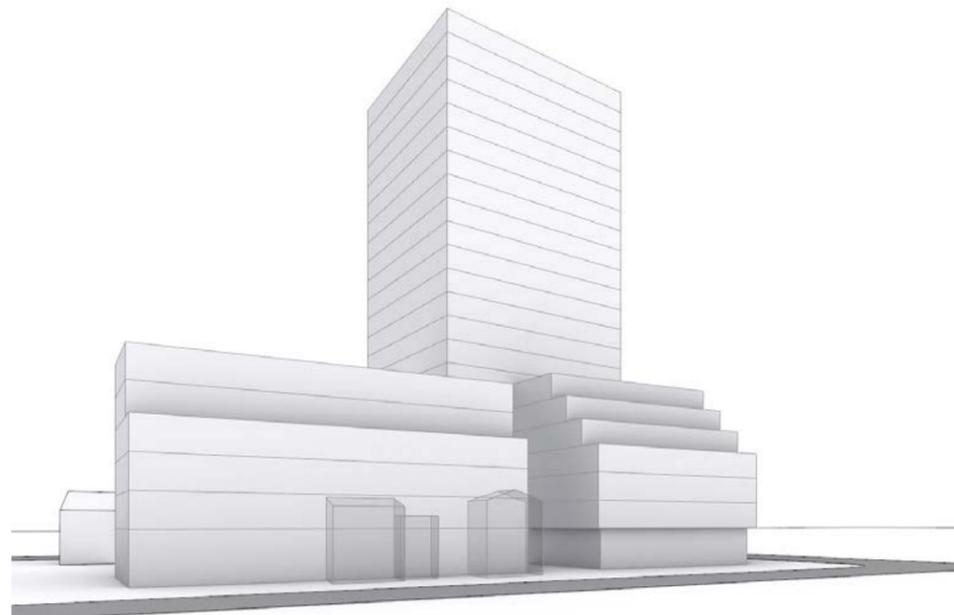


FRONT

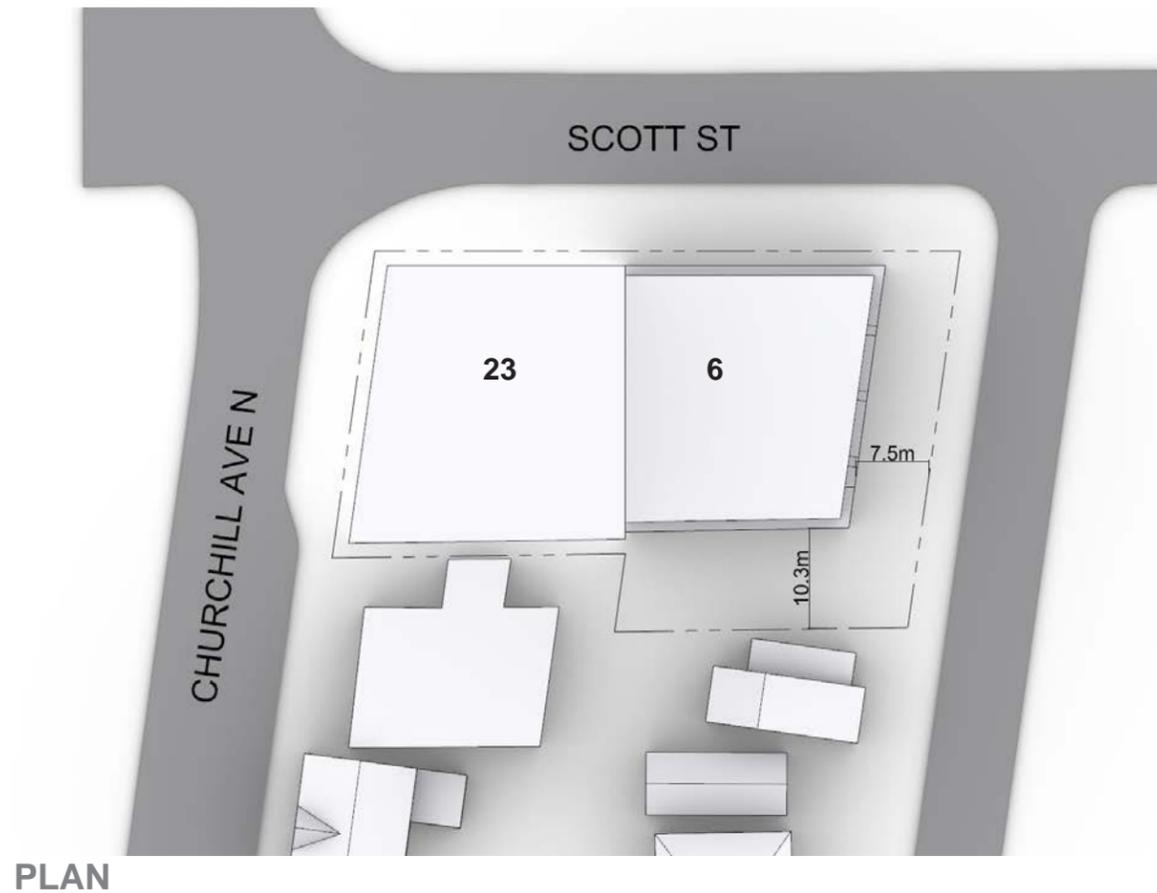


AXO

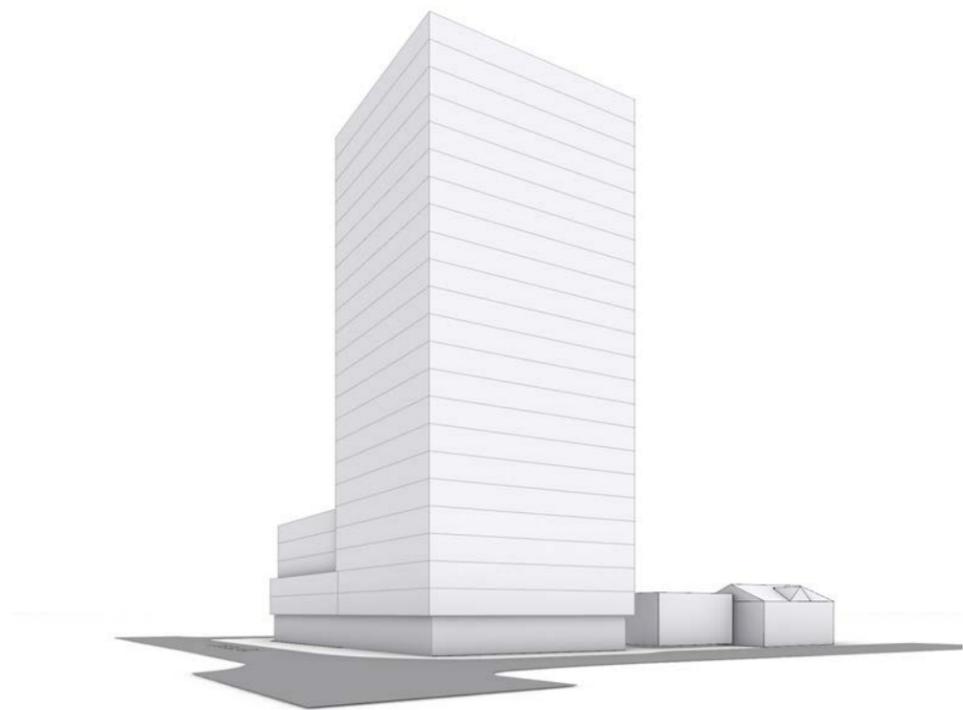
Total = +/- 20,750 sm



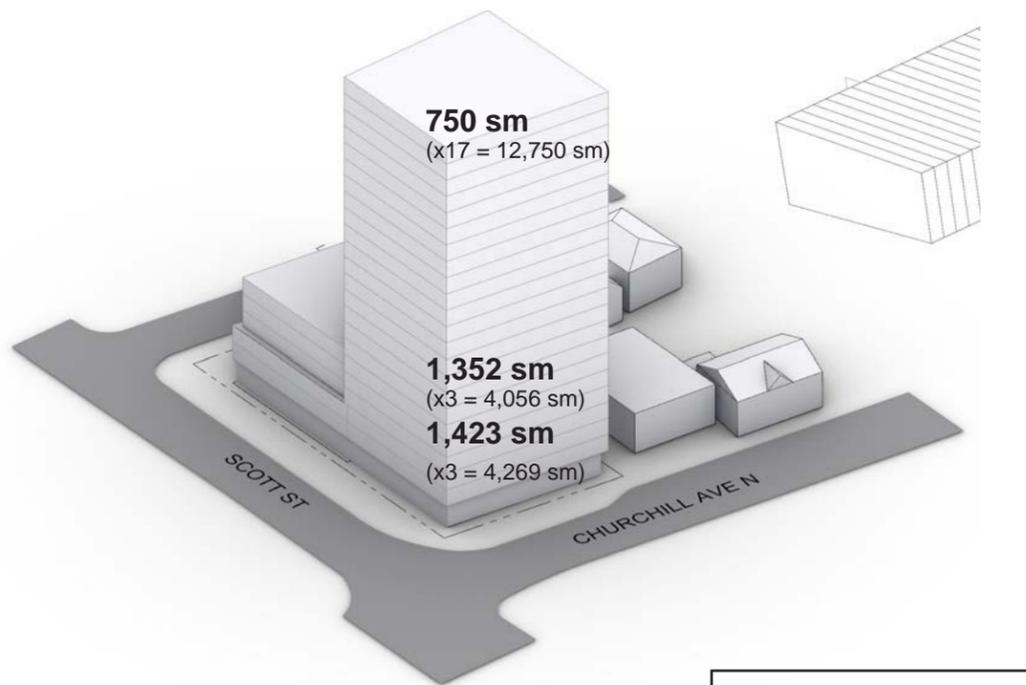
BACK



PLAN

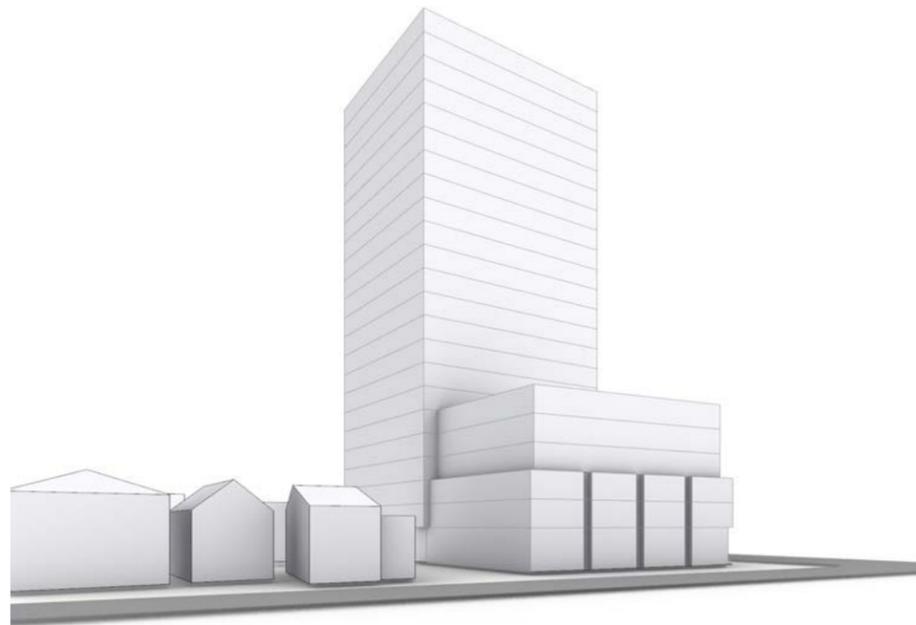


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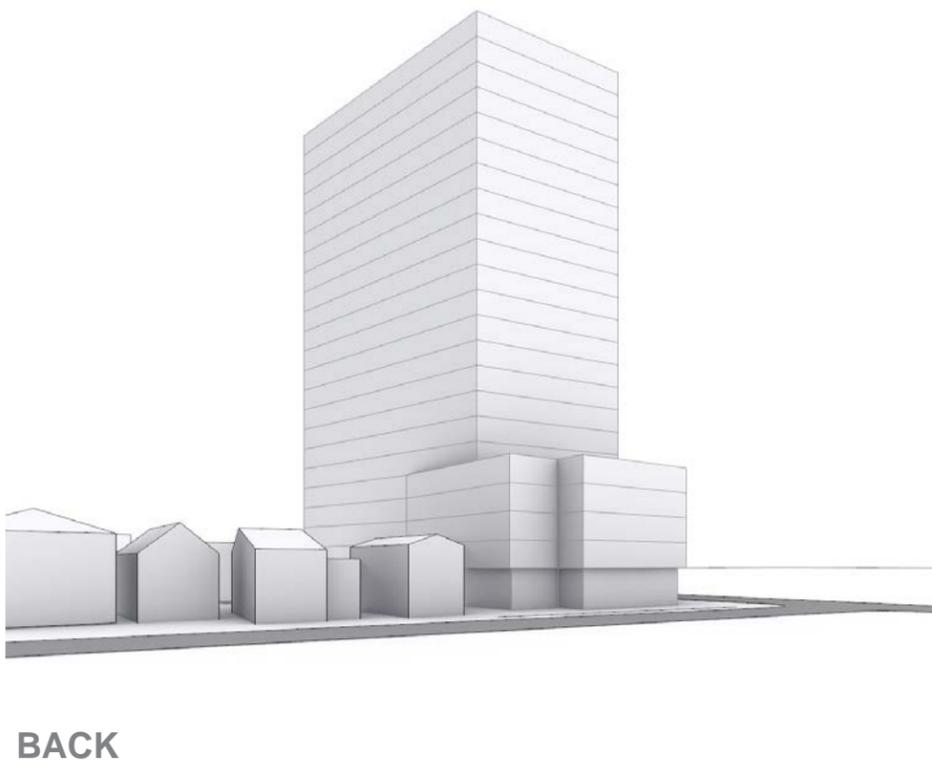
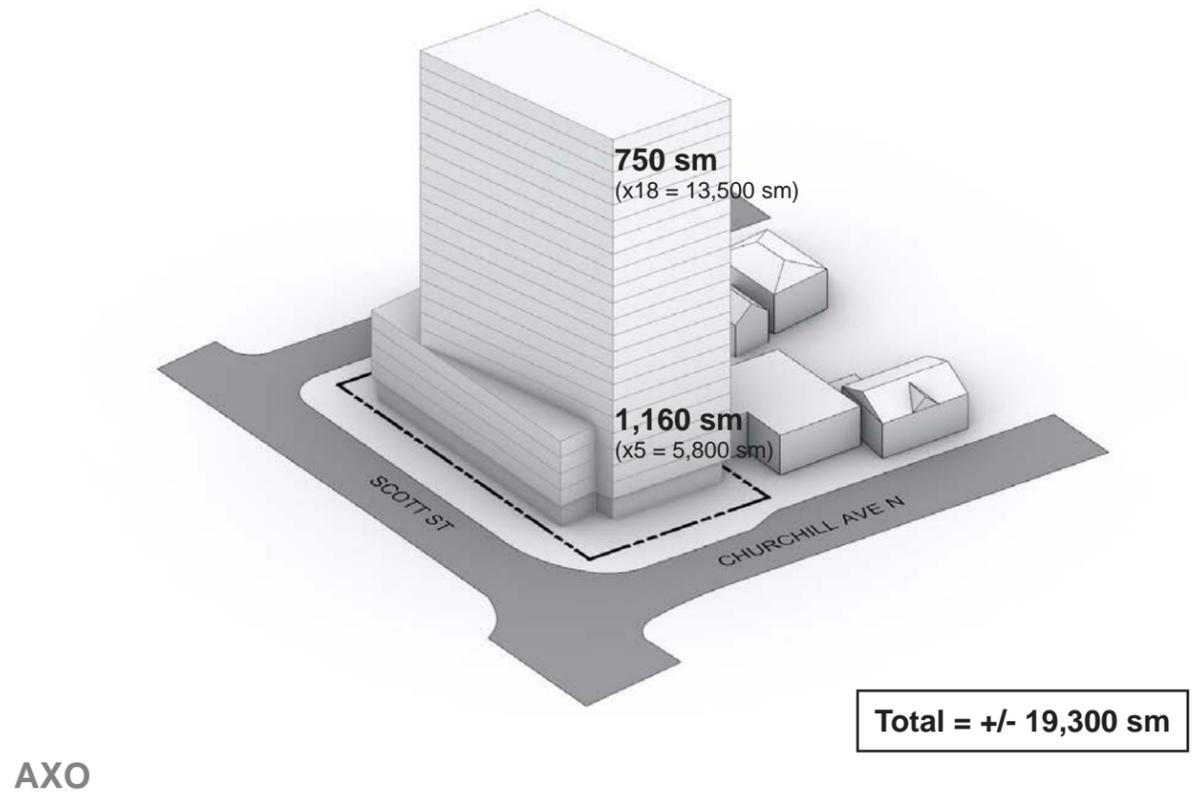
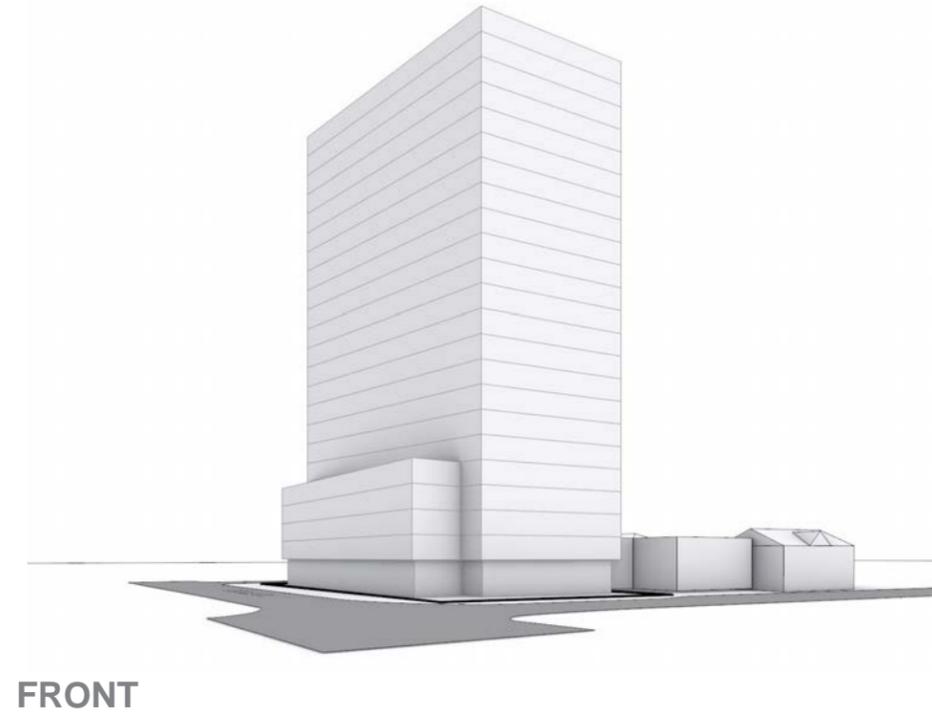
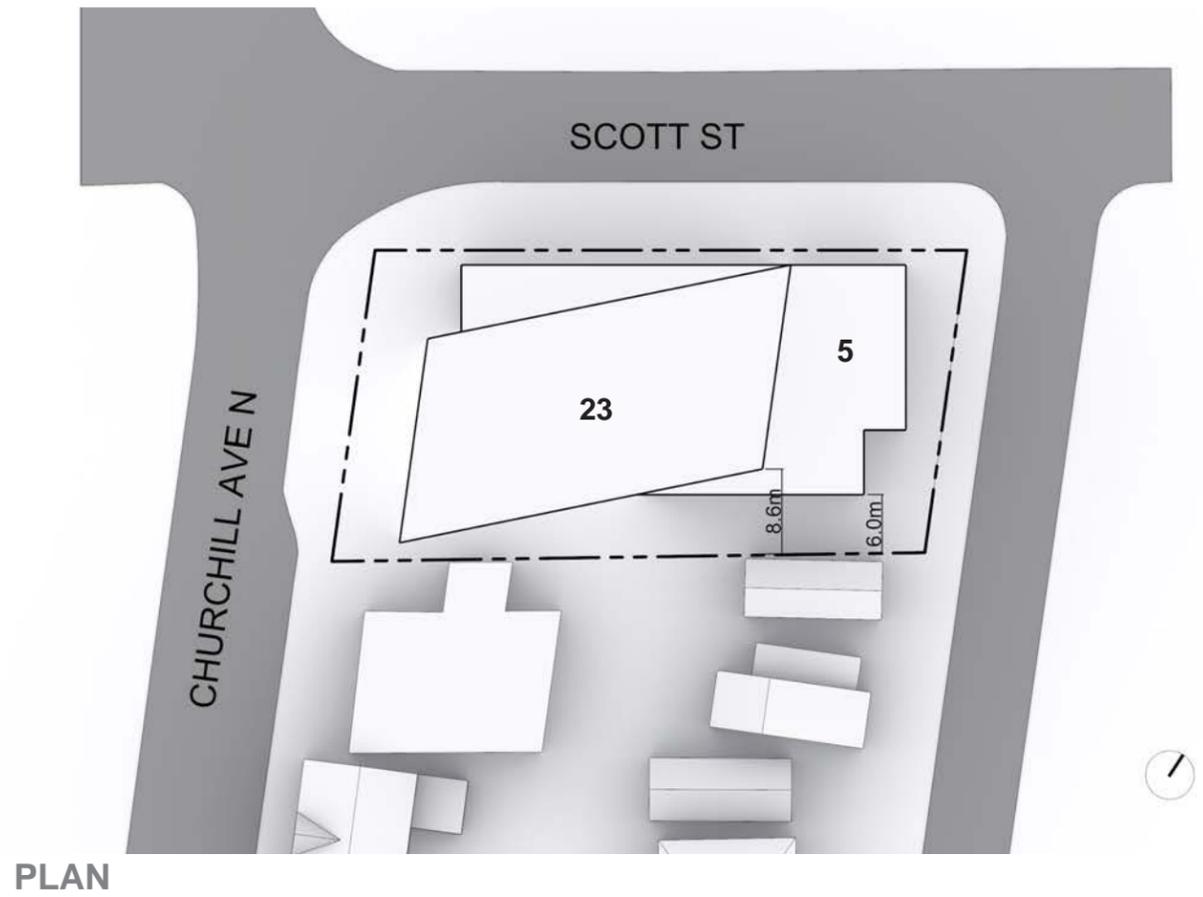


Total = +/- 21,075 sm

AXO



BACK

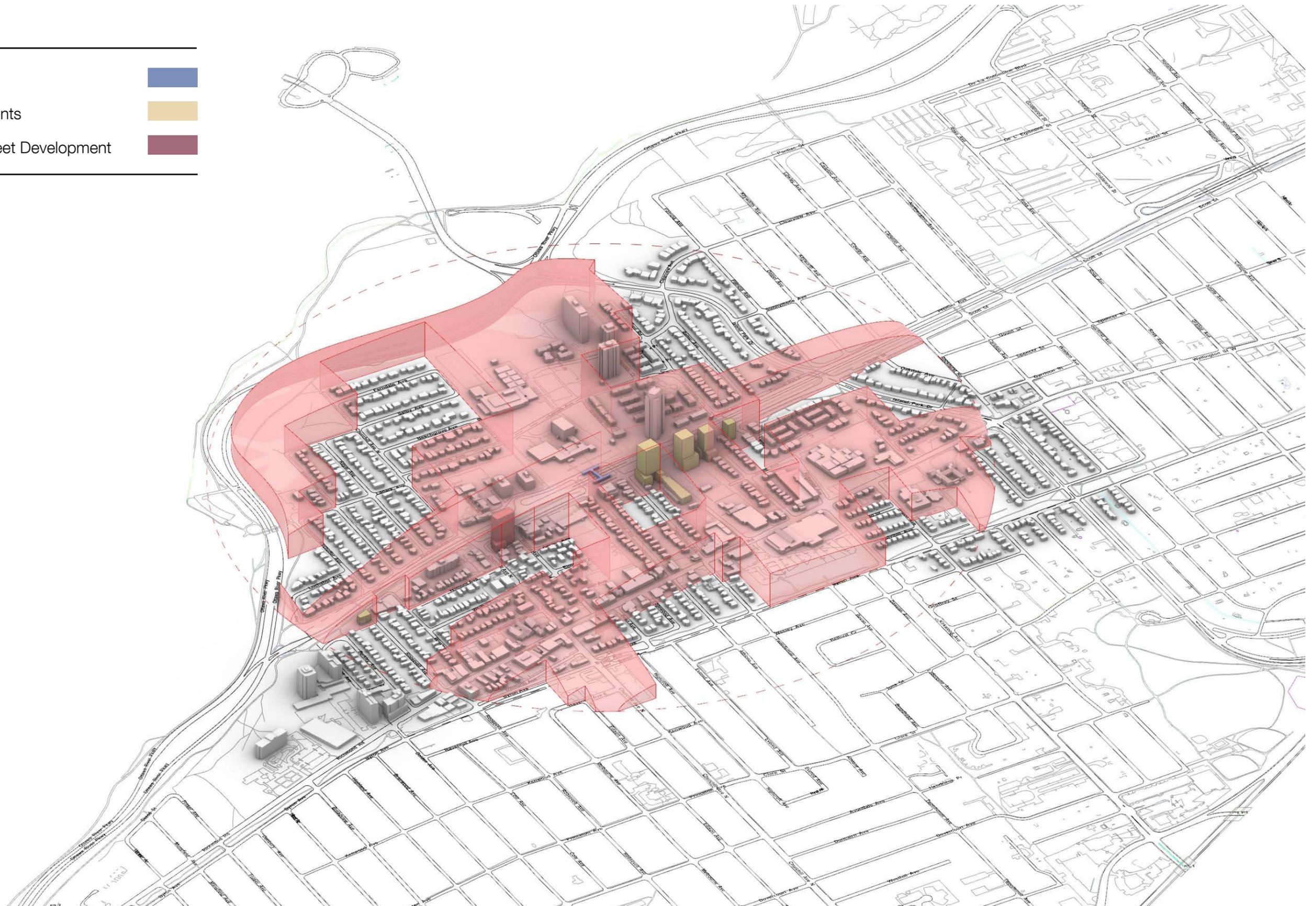


# 5.0 PROJECT STATUS UPDATE

- Meeting with City Staff
- Panels presented at Community Open House

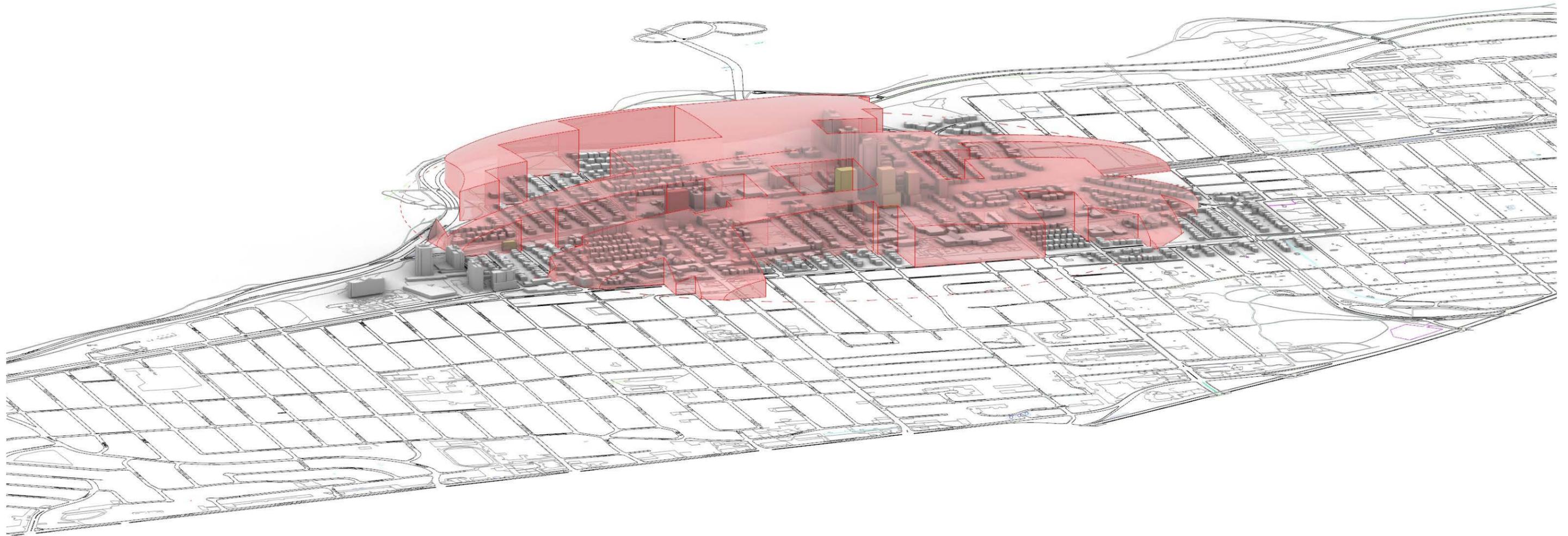
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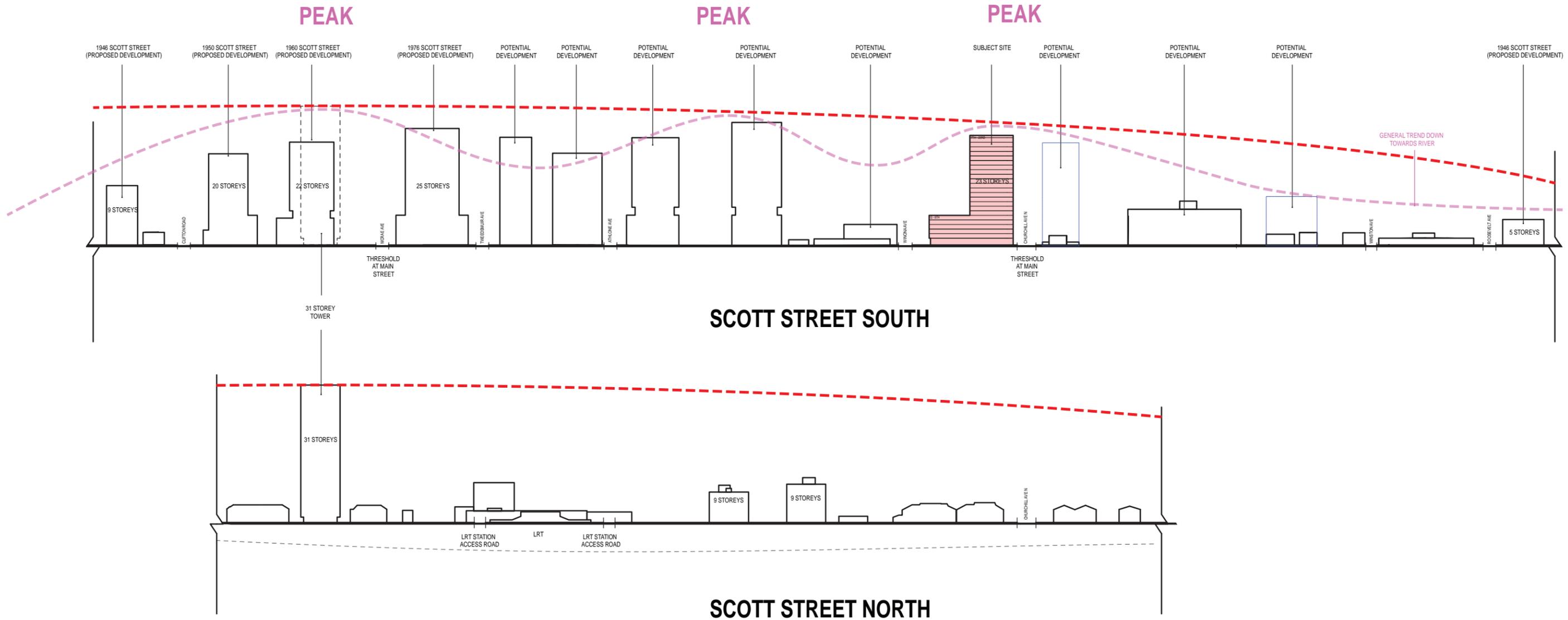
- CRT Station 
- Adjacent New Developments 
- Proposed 2070 Scott Street Development 

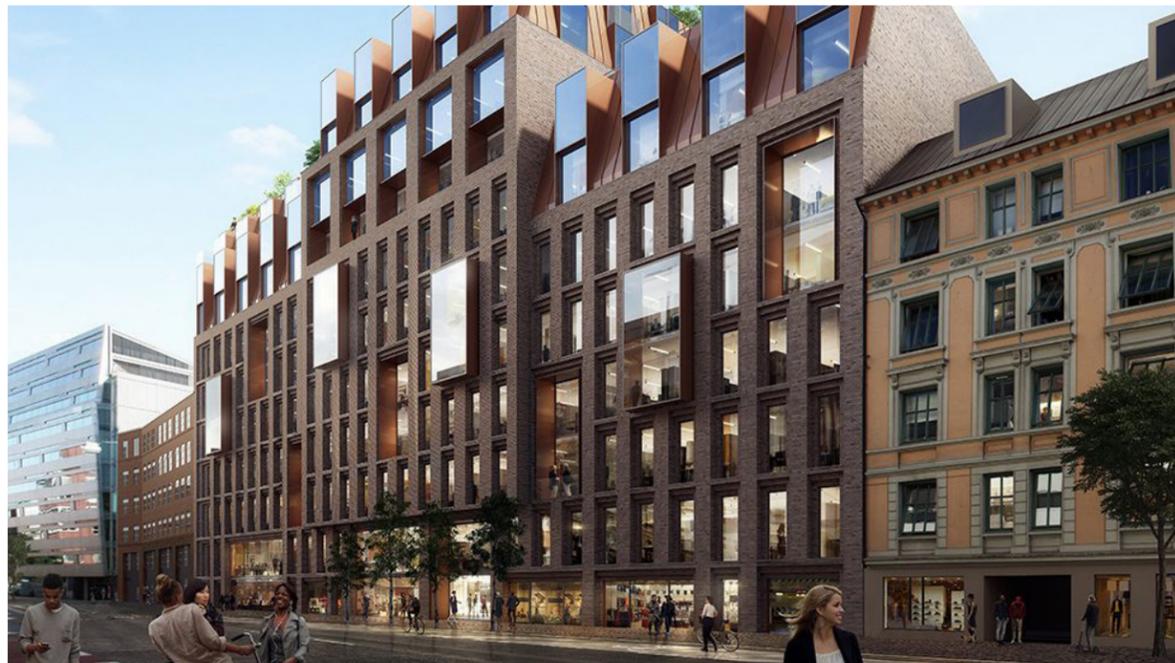


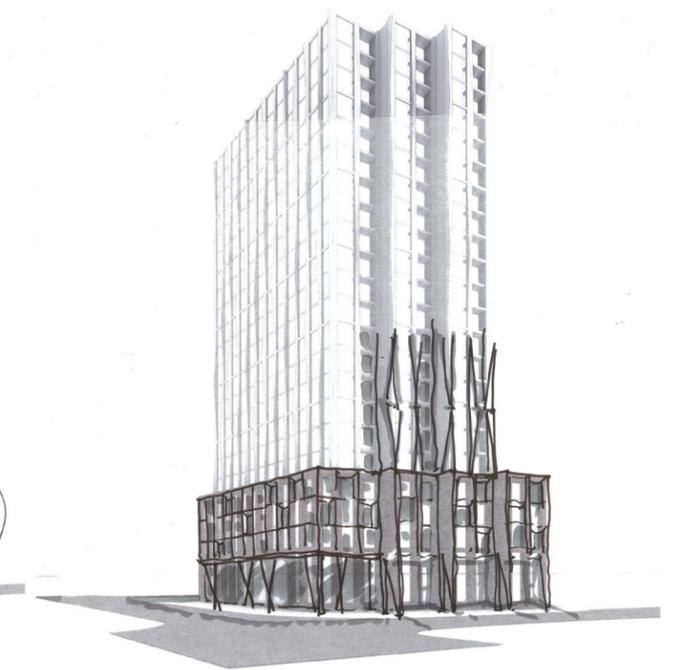
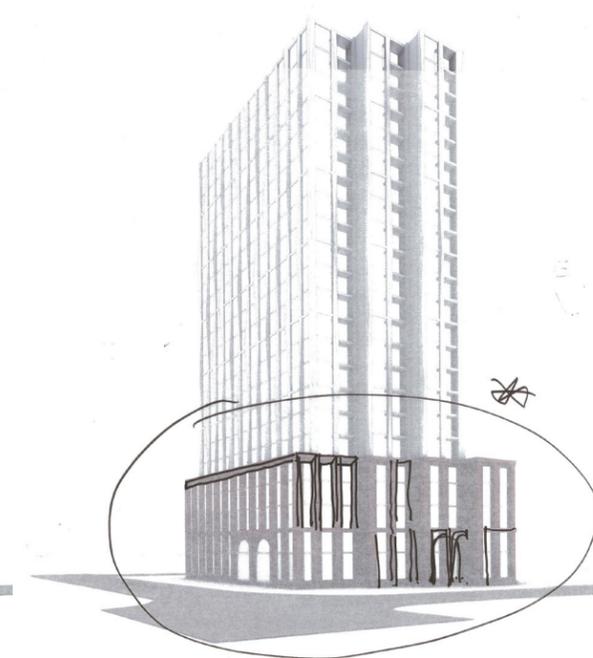
# Legend

- CRT Station 
- Adjacent New Developments 
- Proposed 2070 Scott Street Development 











BIKE REPAIR SHOP ADJACENT TO MULTI-USE PATHWAY

ACTIVATED LOBBY / WORKSPACE

RESTAURANT AND PATIO USES TO ANIMATE CHURCHILL STREET



PROPOSED VISION (CORNER OF SCOTT AND CHURCHILL STREETS, LOOKING SOUTH-EAST)

# 6.0 DEVELOPMENT PROPOSAL (ZBA SUBMISSION)

- Floor Plans
- Shadow Studies

# Ground Floor Statistics

GBA: 1,327 sm / 14,284 sf  
 GFA Res: 168 sm / 1,808 sf

Townhouses: 3

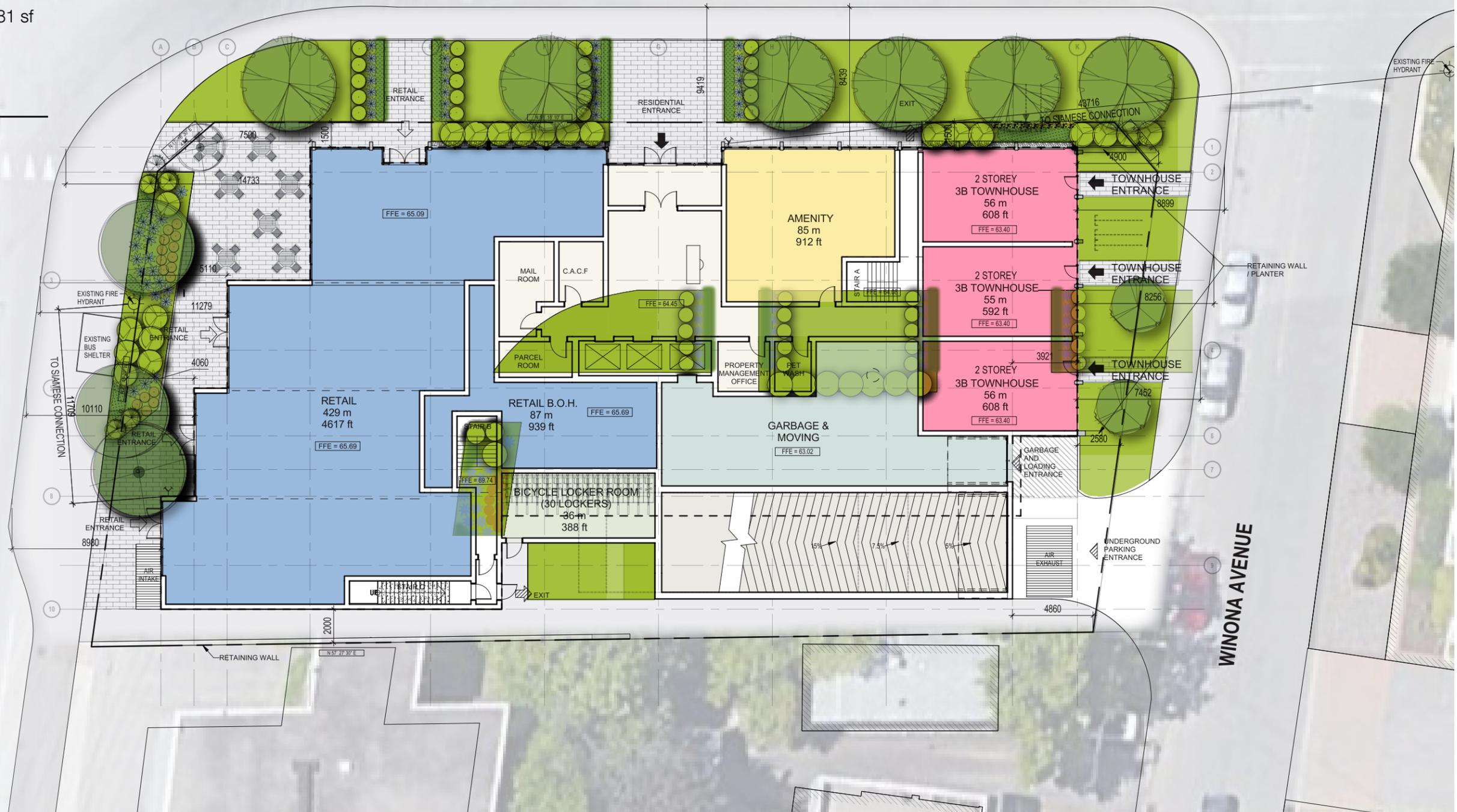
Amenity (Indoor): 85 sm / 915 sf  
 Amenity (Outdoor): 184 sm / 1981 sf

Retail: 516 sm / 5,556 sf  
 Bicycle: 30 spaces

SCOTT STREET

CHURCHILL AVENUE

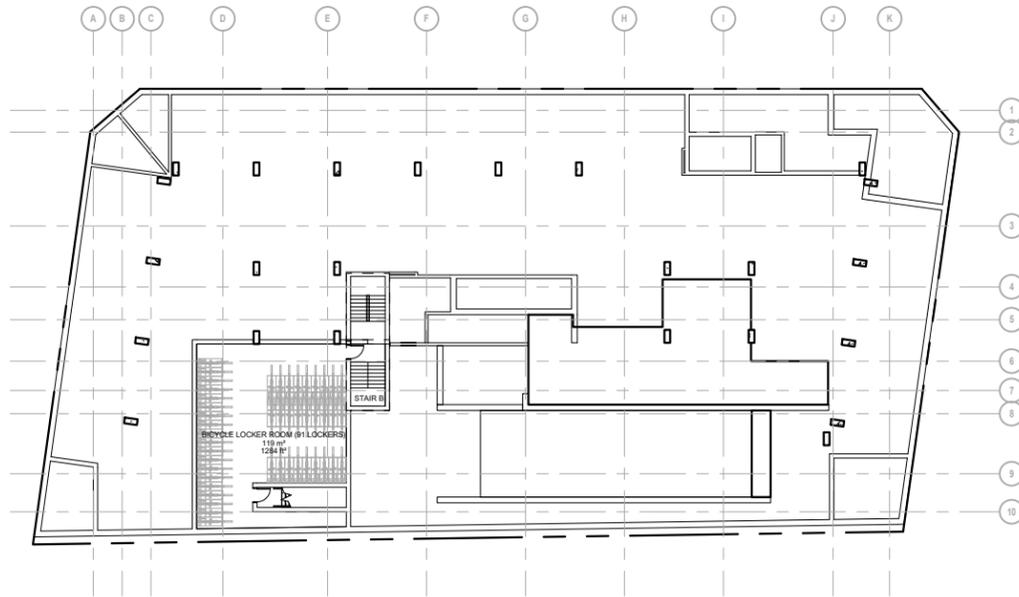
WINONA AVENUE



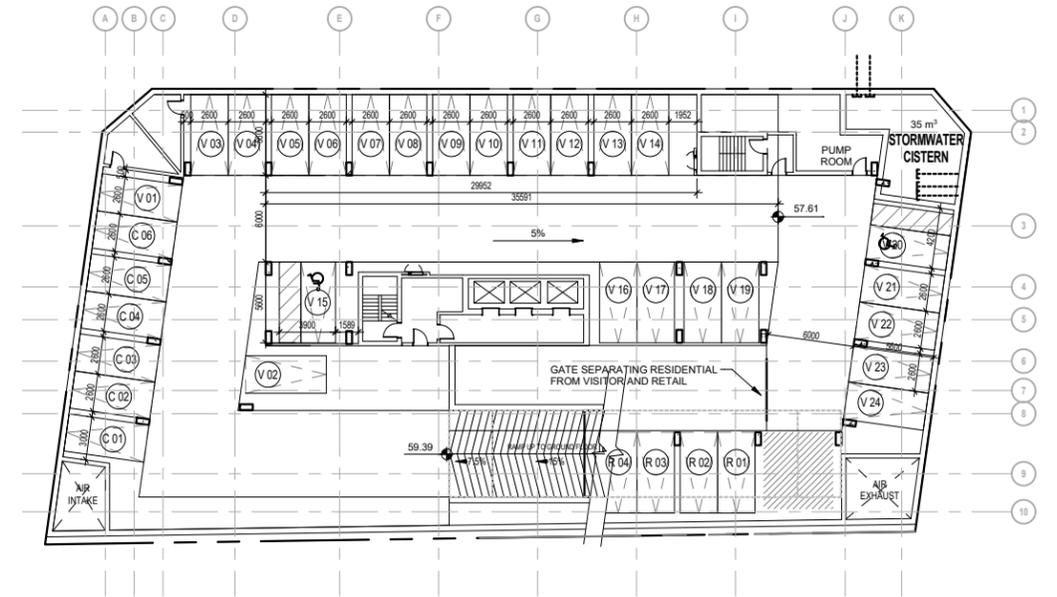
# Underground Parking

Total Parking Spaces: 144

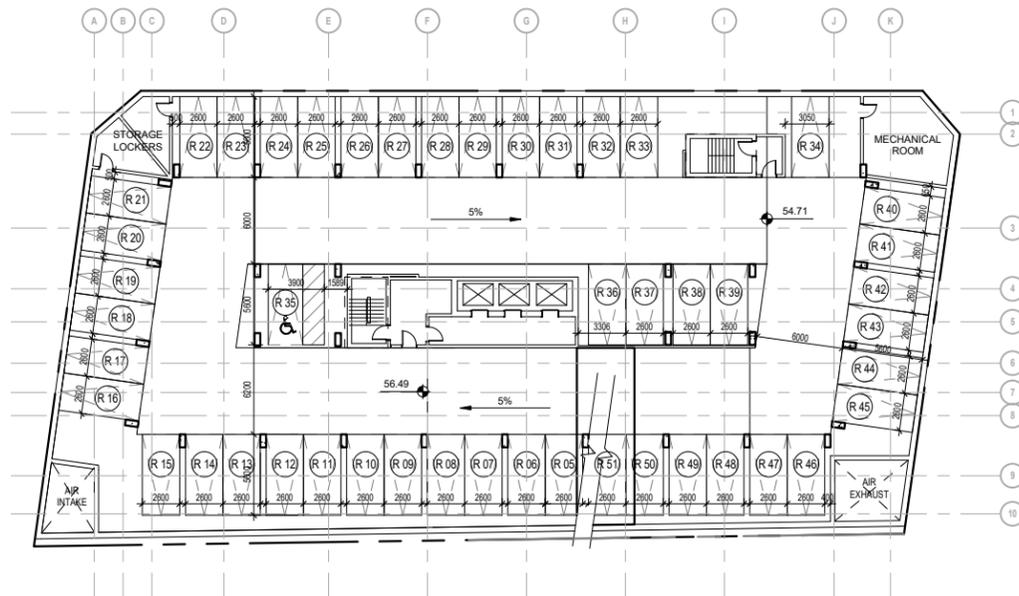
Bicycle: 91 spaces



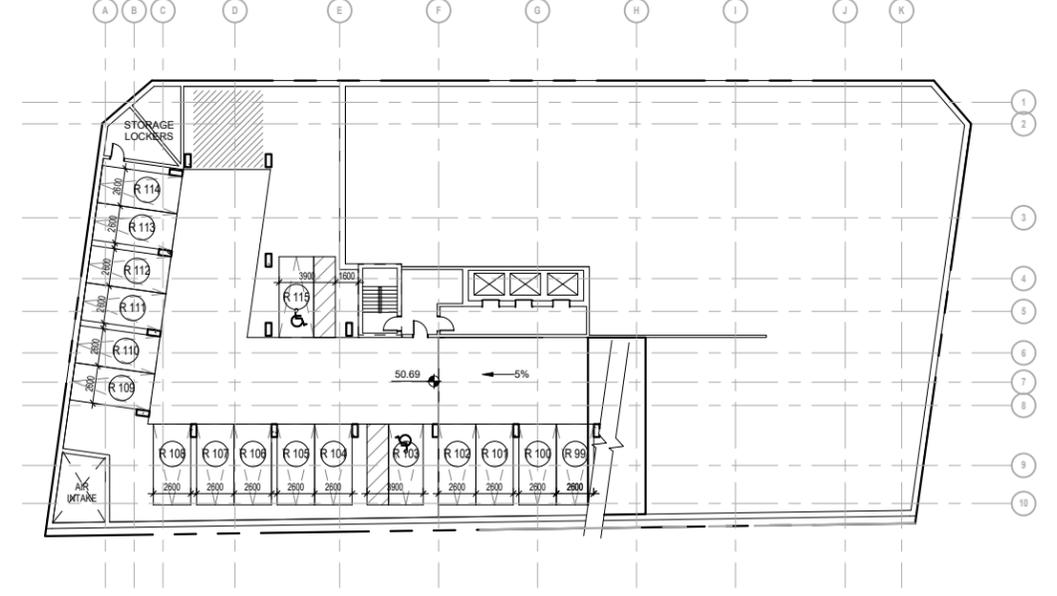
MEZZANINE



P1 PARKING



P2 - P3 PARKING



P4 PARKING



## Level 2 Statistics

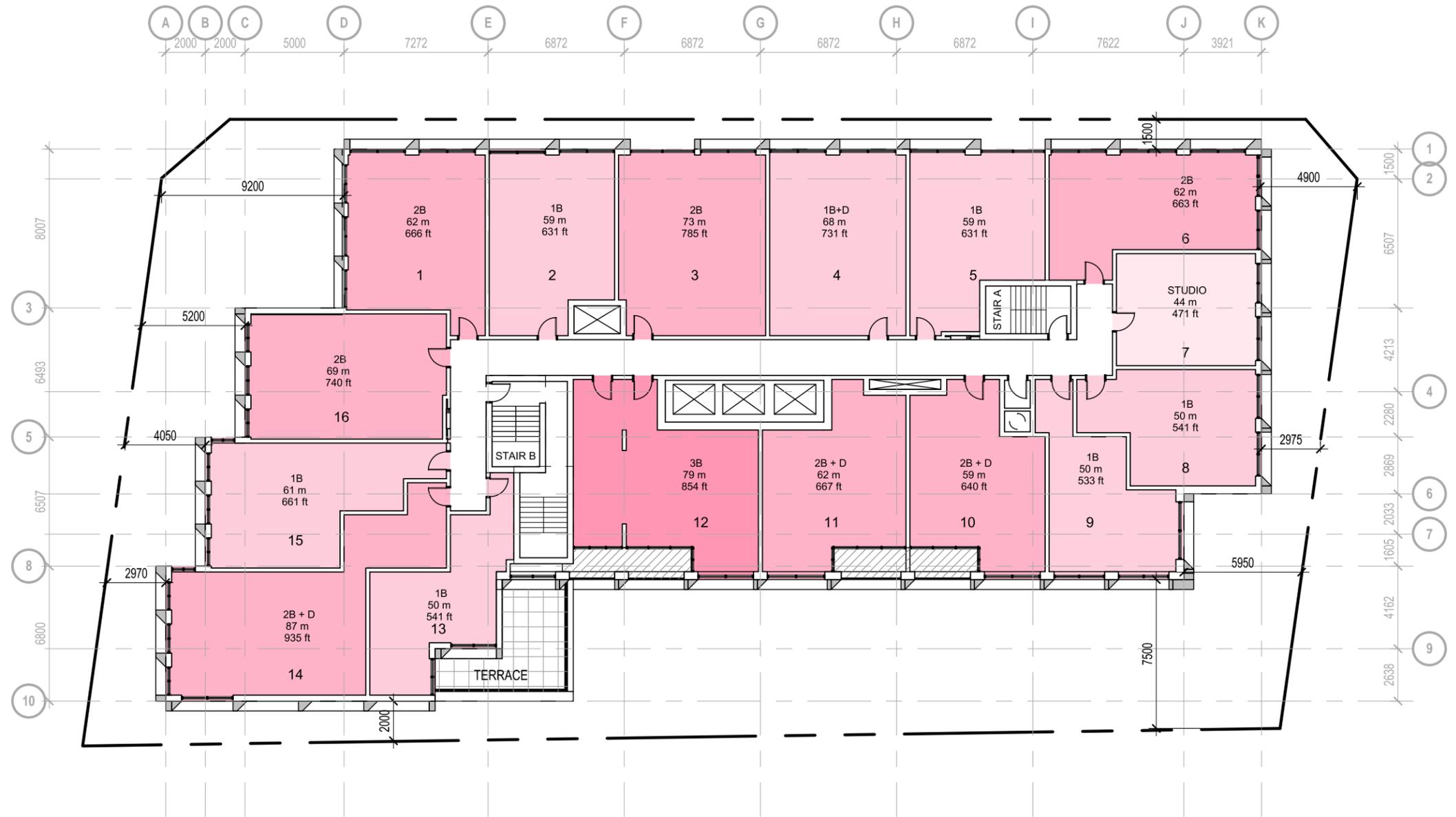
GBA: 1,145 sm / 12,325 sf

GFA Res: 1,015 sm / 10,922 sf

Amenity (Outdoor): 42 sm / 450 sf

Studio: 1  
 1 B (1B+Den): 7  
 2 B (2B+Den): 7  
 3 B: 1

Total Suite Count: 16



• The building is setback at the 2nd floor at the rear to provide transition.



## Typical Podium Statistics (Level 3-5)

GBA/Floor: 1,145 sm / 12,324 sf

Total GBA: 3,435 sm / 36,974 sf

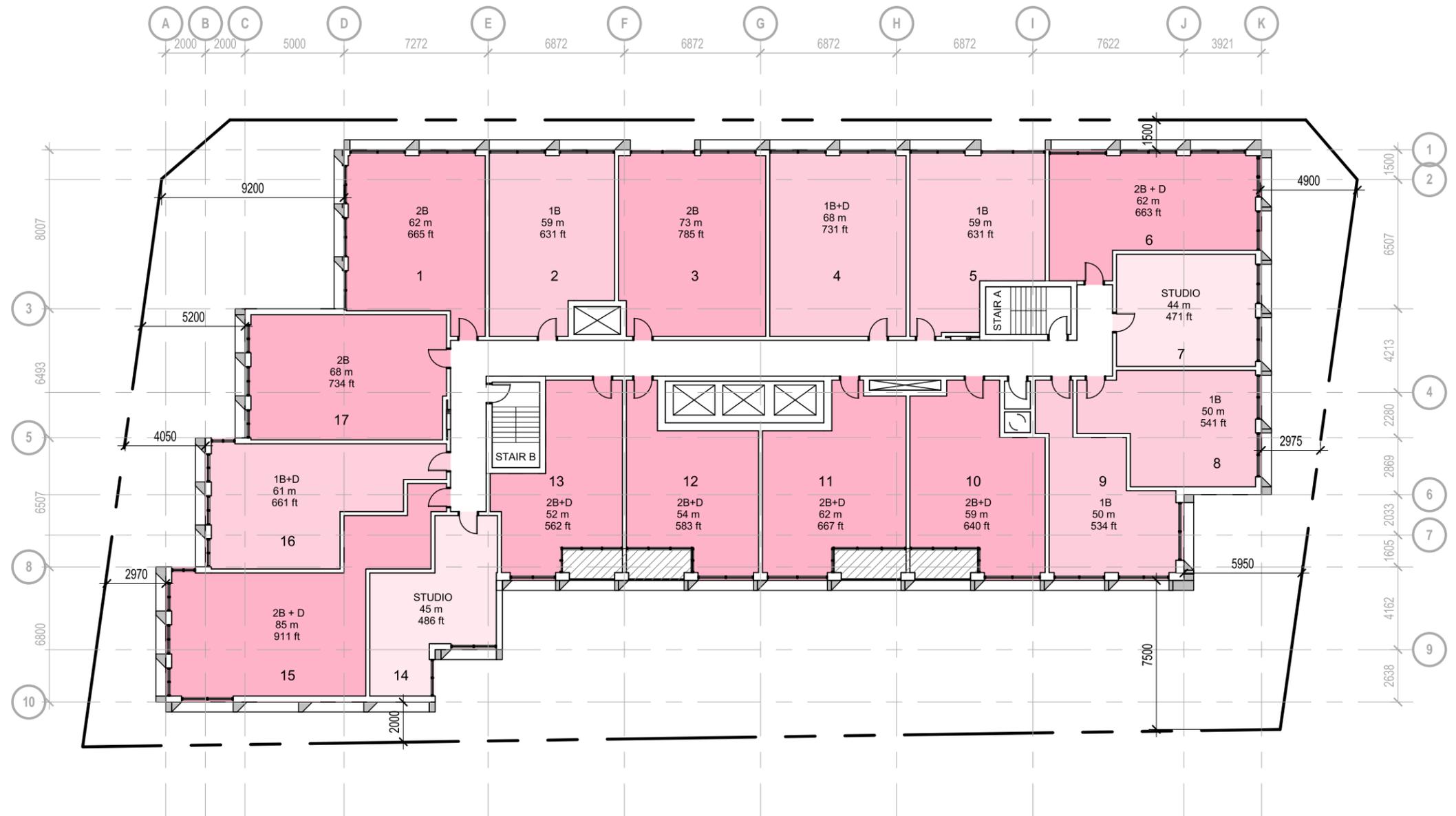
GFA/Floor: 1,025 sm / 11,029 sf

Total GFA Res: 3,074 sm / 33,088 sf

Suite count per floor: 17

|               |   |
|---------------|---|
| Studio:       | 2 |
| 1 B (1B+Den): | 6 |
| 2 B (2B+Den): | 9 |
| 3 B:          | 0 |

Amenity (Outdoor): 77 sm / 827 sf



## Level 6 Statistics

GBA: 757 sm / 8,148 sf

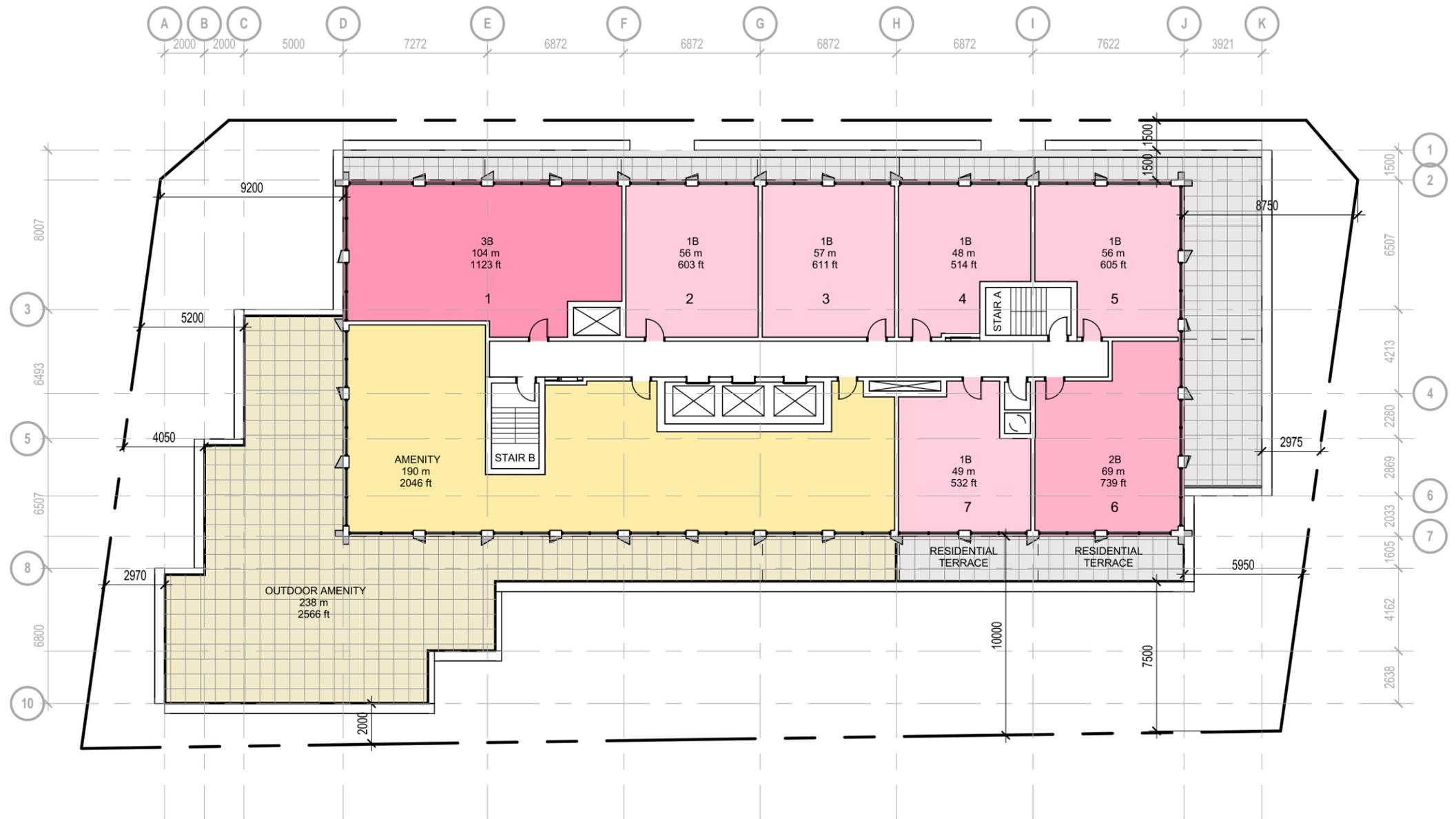
GFA Res: 471 sm / 5,066 sf

Amenity (Indoor): 190 sm / 2,045 sf

Amenity (Outdoor): 401 sm / 4,316 sf

Suite count: 7

|               |   |
|---------------|---|
| Studio:       | 0 |
| 1 B (1B+Den): | 5 |
| 2 B (2B+Den): | 1 |
| 3 B:          | 1 |



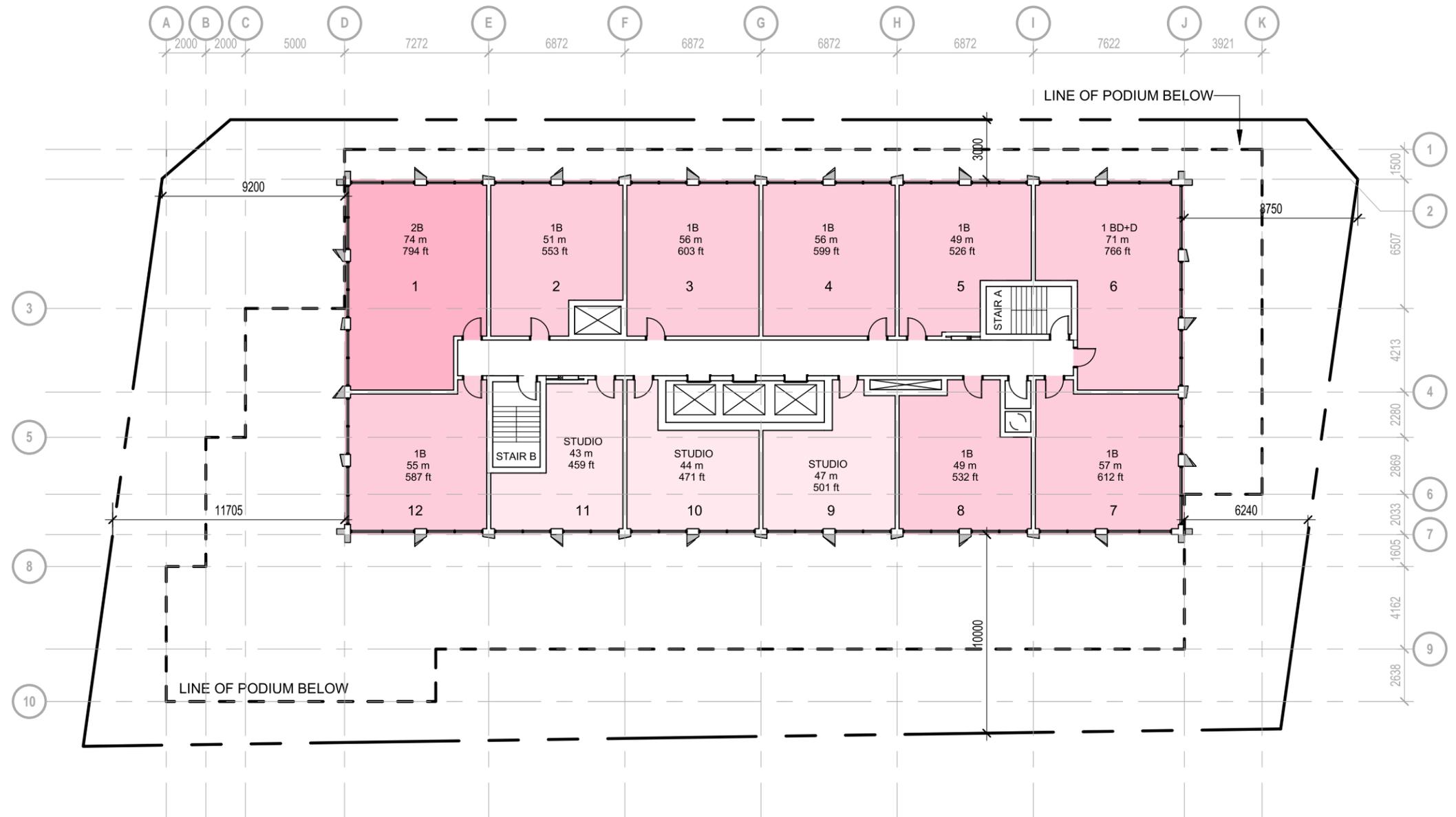
## Lower Tower Typical (Level 7-15) Statistics

GBA/Floor: 757 sm / 8,148 sf  
 Total GBA: 6,813 sm / 73,335 sf

GFA/Floor: 658 sm / 7,079 sf  
 Total GFA Res: 5,919 sm / 63,710 sf

Suite count per floor: 12

|               |   |
|---------------|---|
| Studio:       | 3 |
| 1 B (1B+Den): | 8 |
| 2 B (2B+Den): | 1 |
| 3 B:          | 0 |



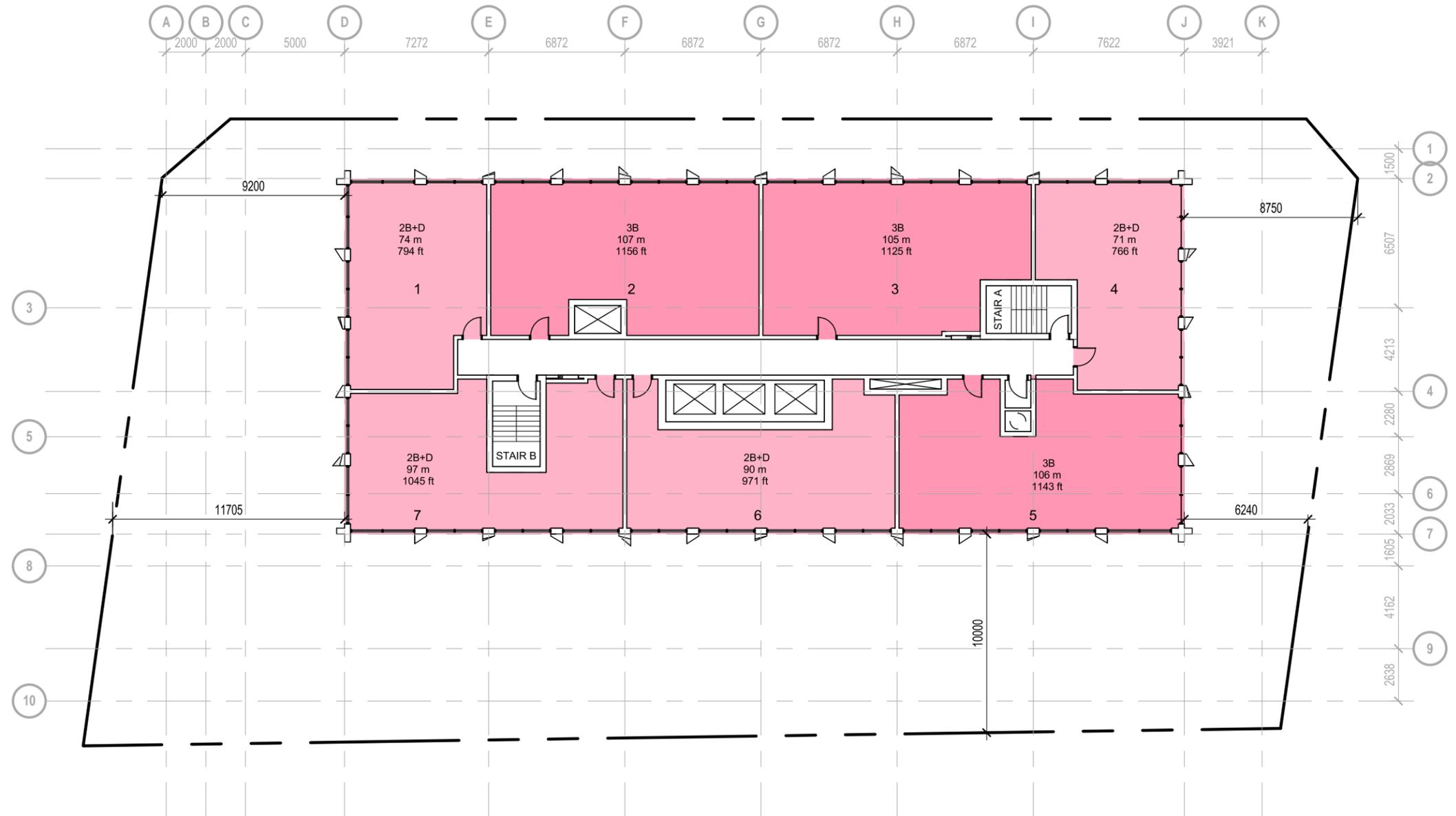
# Upper Tower Typical (Level 16-23) Statistics

GBA/Floor: 757 sm / 8,148 sf  
 Total GBA: 6,056 sm / 65,186 sf

GFA/Floor: 658 sm / 7,079 sf  
 Total GFA Res: 5,261 sm / 56,631 sf

Suite count per floor: 7

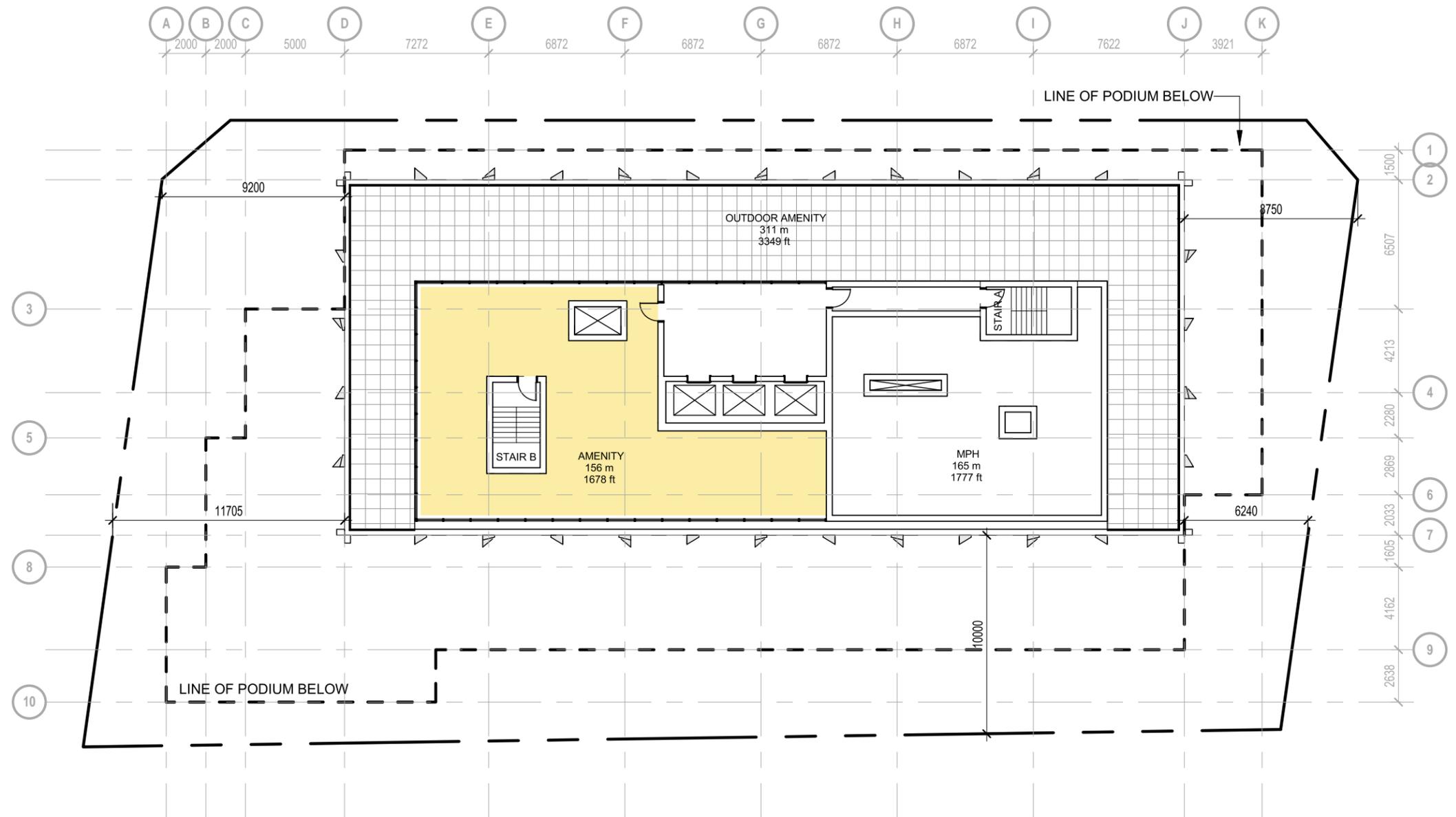
|               |   |
|---------------|---|
| Studio:       | 0 |
| 1 B (1B+Den): | 0 |
| 2 B (2B+Den): | 4 |
| 3 B:          | 3 |



# Amenity and Mech. Penthouse

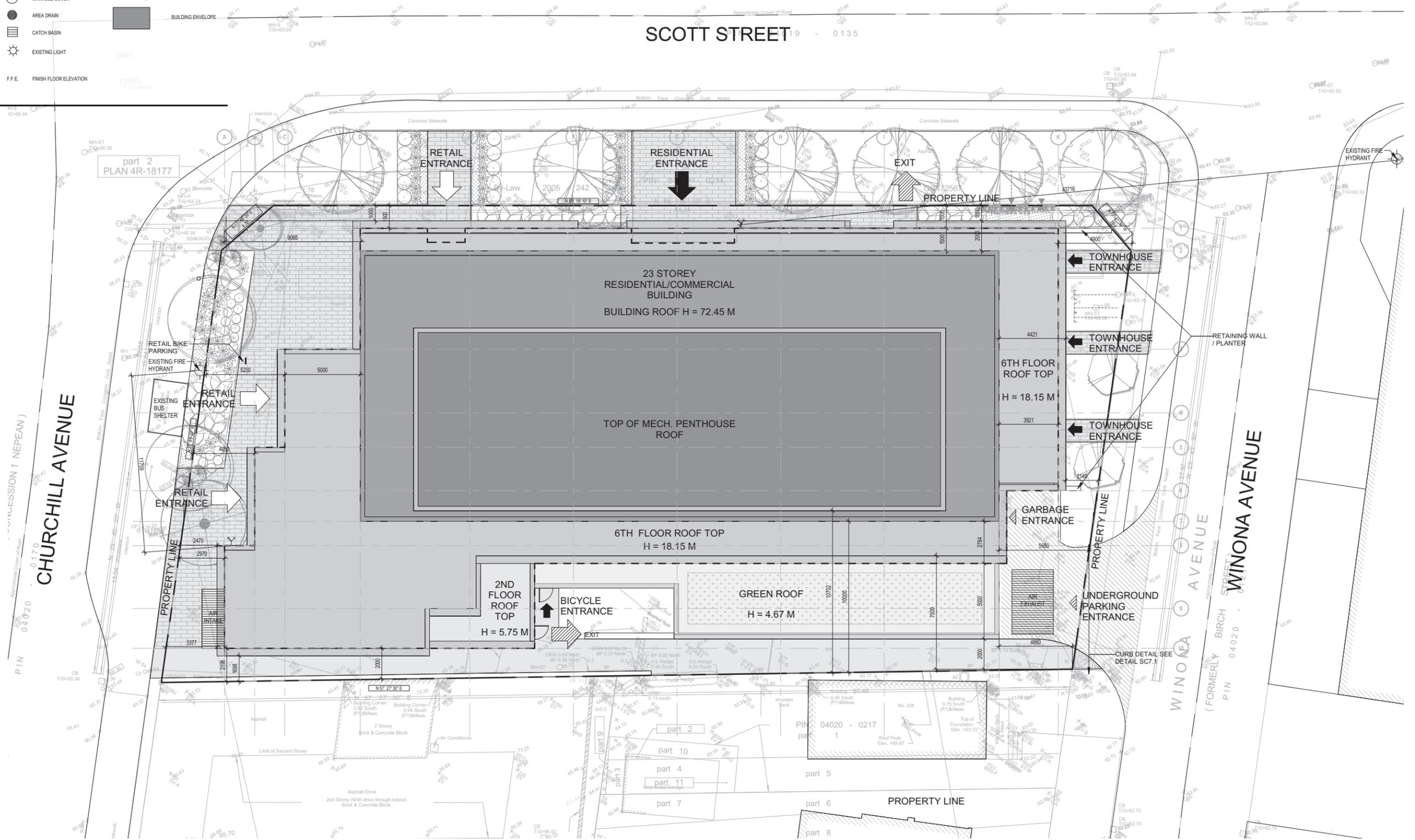
GBA: 423 sm / 4,553 sf  
 GFA Res: 51 sm / 545 sf

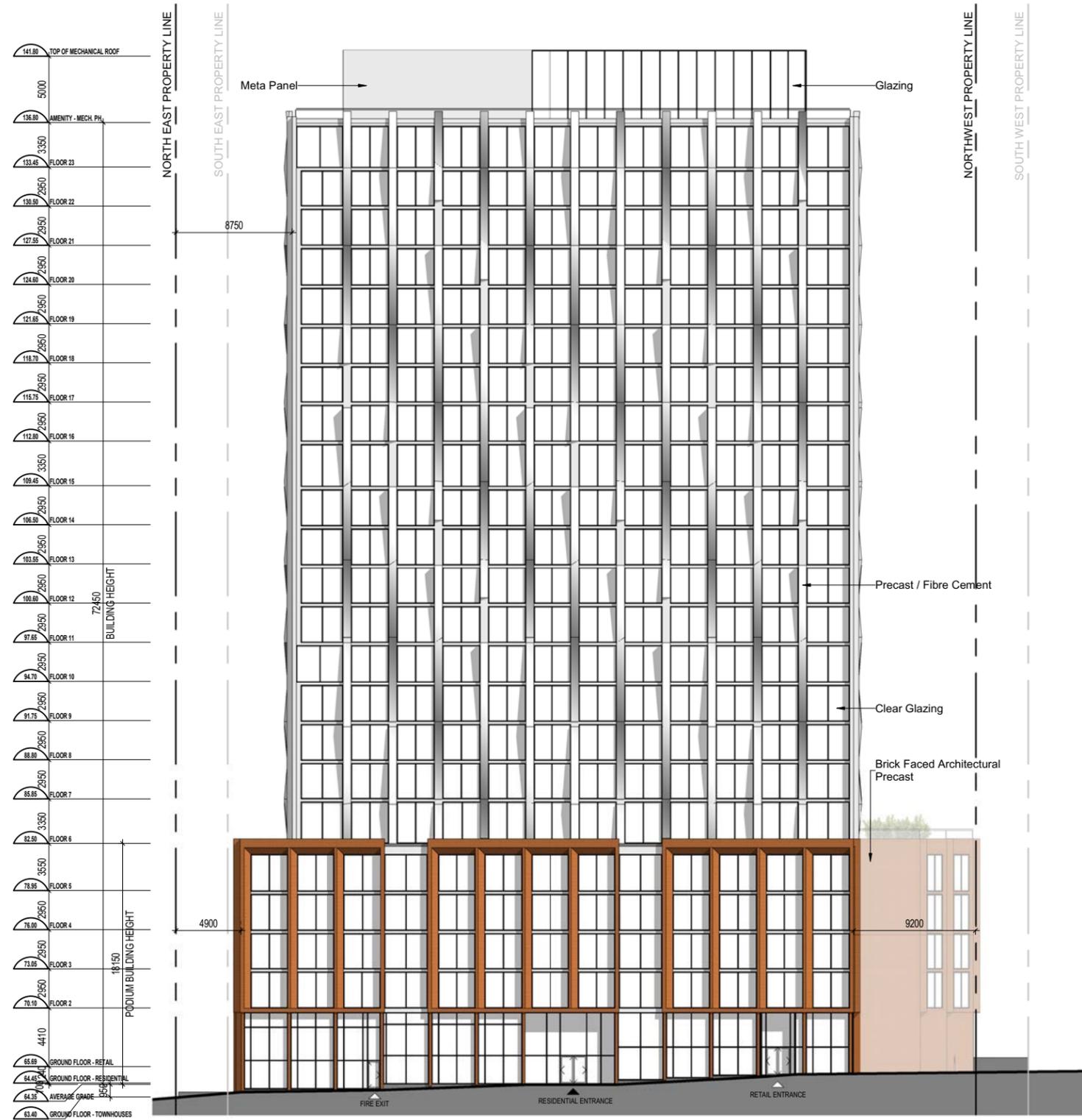
Amenity (Indoor): 156 sm / 1,679 sf  
 Amenity (Outdoor): 311 sm / 3,348 sf



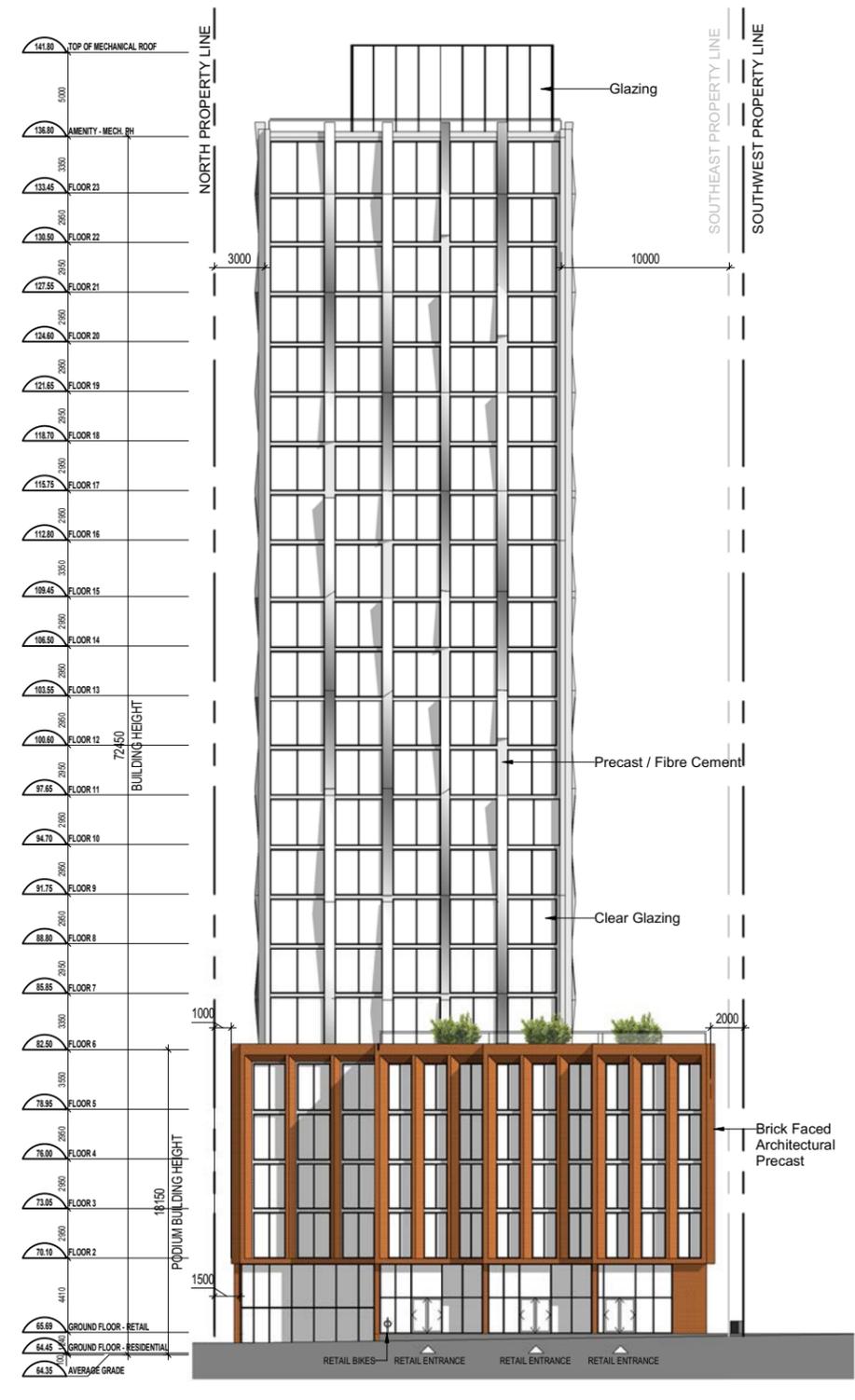
# Site Plan Legend

|  |                                   |  |                               |  |                    |
|--|-----------------------------------|--|-------------------------------|--|--------------------|
|  | PROPERTY LINE                     |  | FIRE HYDRANT                  |  | EXISTING ELEVATION |
|  | LINE OF UNDER GROUND GARAGE BELOW |  | SIAMISE CONNECTION            |  | PROPOSED ELEVATION |
|  | MAIN BUILDING ENTRANCE            |  | MANHOLE COVER                 |  | TOP OF ROOF        |
|  | RETAIL ENTRANCE                   |  | AREA DRAIN                    |  | BUILDING ENVELOPE  |
|  | EXIT                              |  | CATCH BASIN                   |  |                    |
|  | VEHICLE ENTRANCE / EXIT           |  | EXISTING LIGHT                |  |                    |
|  |                                   |  | F.F.E. FINISH FLOOR ELEVATION |  |                    |

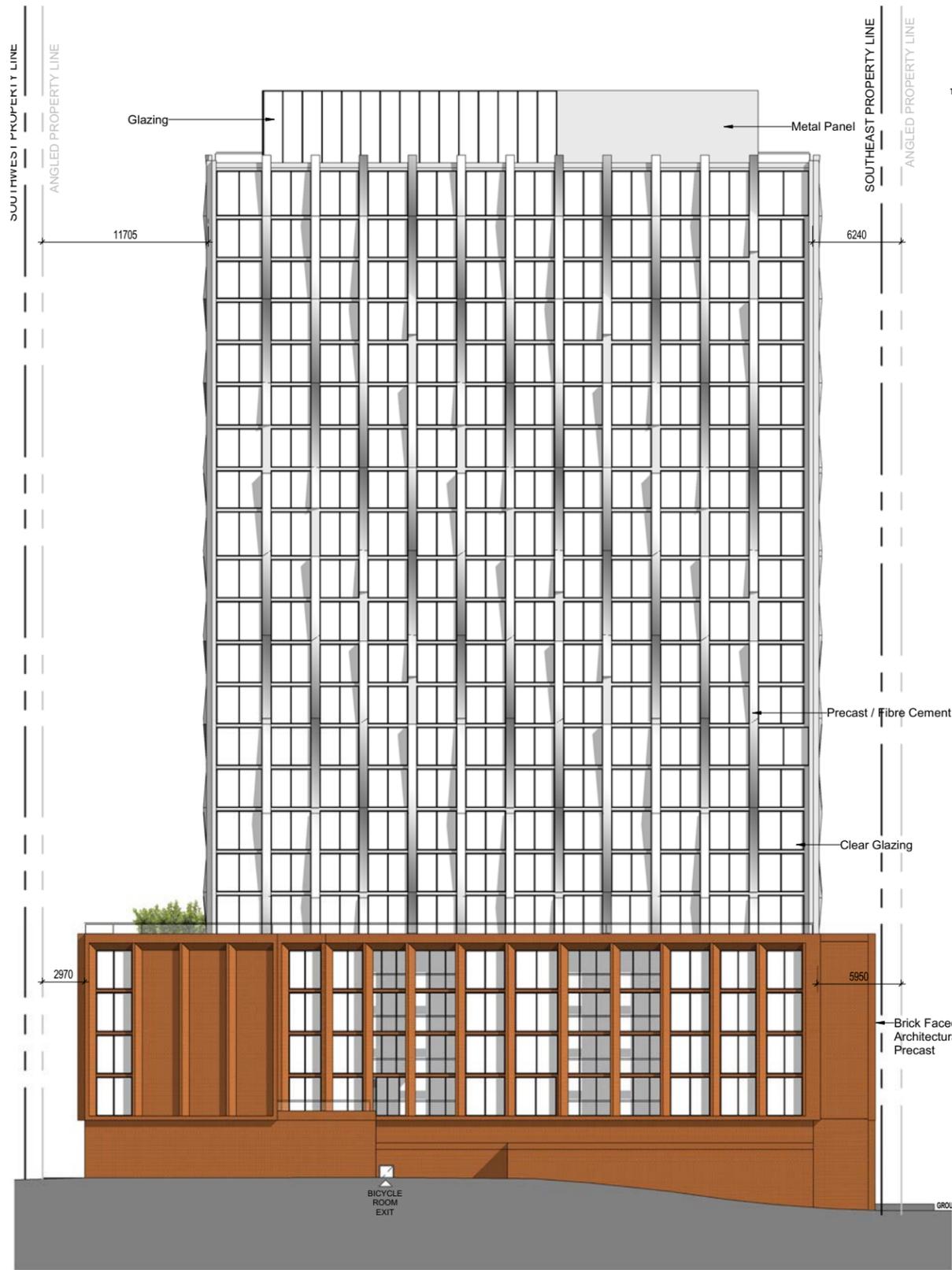




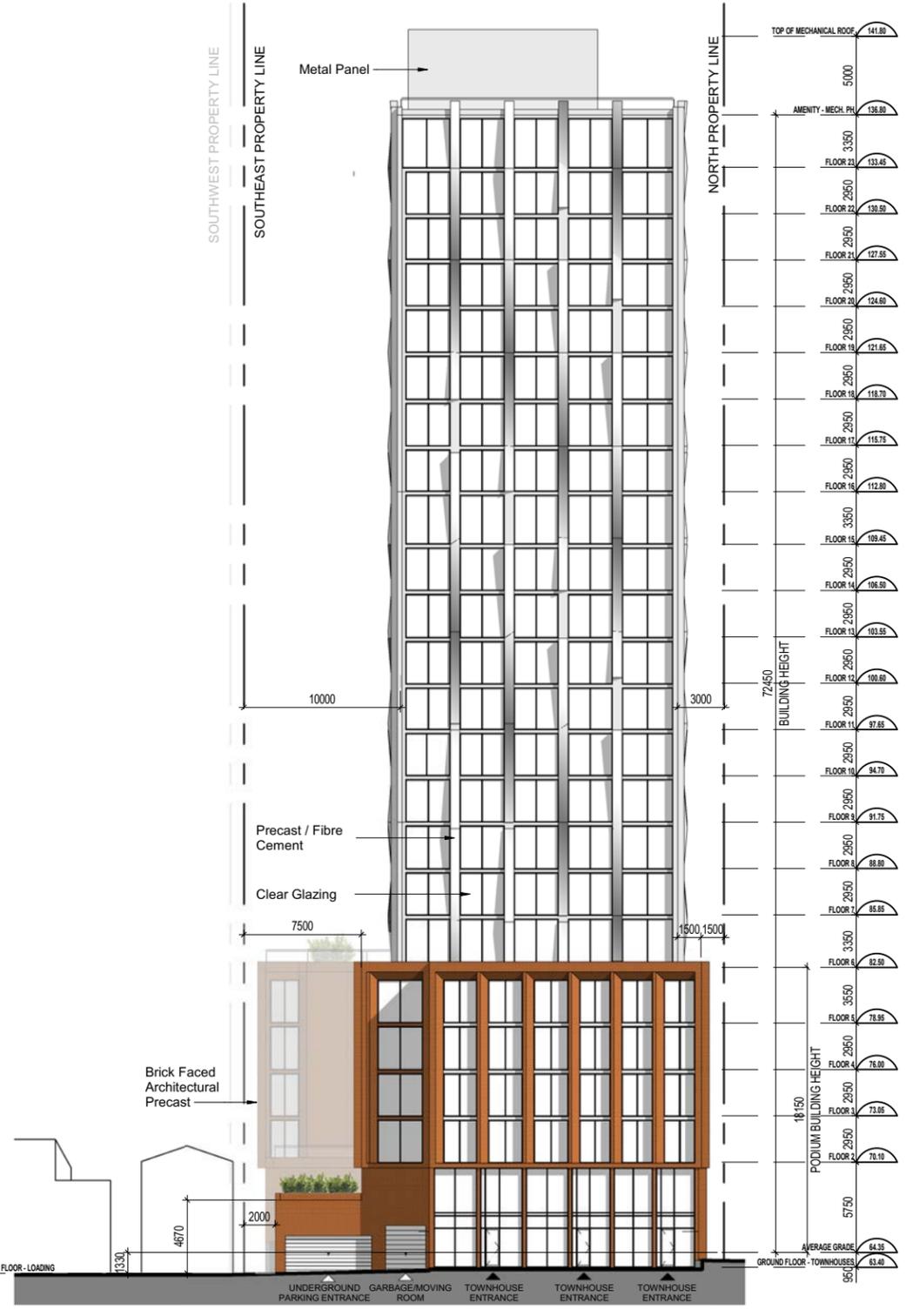
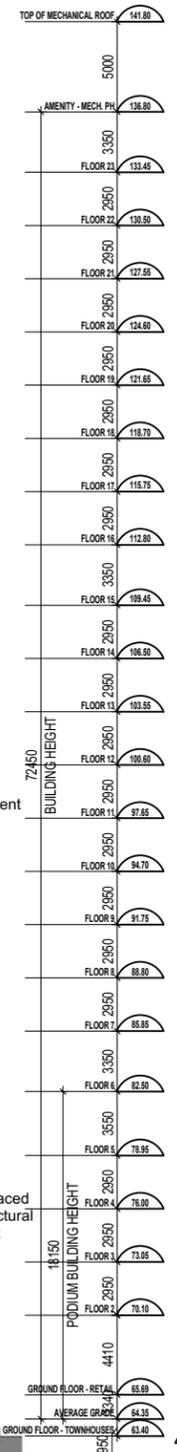
NORTH ELEVATION



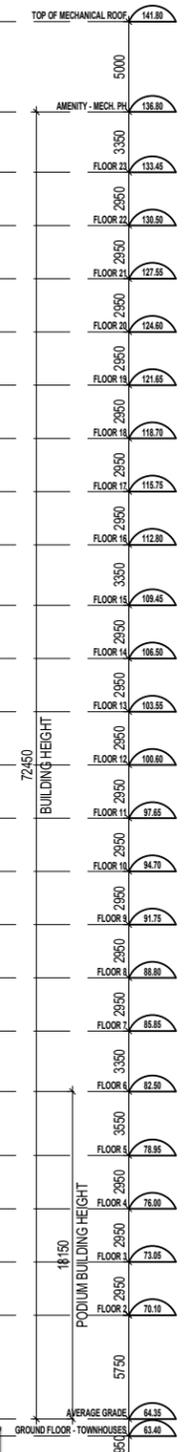
WEST ELEVATION



SOUTH ELEVATION



EAST ELEVATION



## PROJECT STATISTICS SUMMARY

Municipal address: 2070 Scott Street, Ottawa, Ontario

Lot Area: 1,868.5 sm

Zoning By-law 2008-326

|   |  |
|---|--|
| Building Height:                                  | 23 Storeys (excl. Mech Penthouse)  |
| Total GBA:  | 20,124 sm / 216,613 sf   |
| Total GFA Residential:                            | 16,234 sm / 174,743 sf   |
| Total GFA Retail:                                 | 516 sm / 5,554 sf  |
| Floor Space Index (FSI):                          | 9.0  |
| Number of Residential Suites:                     | 241  |
| Total Residential Amenity Space Provided:         | 1,446 sm / 12,215 sf (6.0 sm / unit)   |
| Total Residential Indoor Amenity Space Provided:  | 431 sm / 4,639 sf  |
| Total Residential Outdoor Amenity Space Provided: | 1015 sm / 10,472 sf  |
| Vehicular Parking Total Provided:                 | 144 Spaces ( Res: Min. 0.5 / unit; Min. Visitor 0.1 / unit; Retail: 1.25 - 3.6 / 100 sm) |
| Bicycle Parking Total Provided:                   | 123 Spaces ( Res: 0.5 / unit; Retail: 1/250 sm)  |



June 21 at 8am



June 21 at 9am



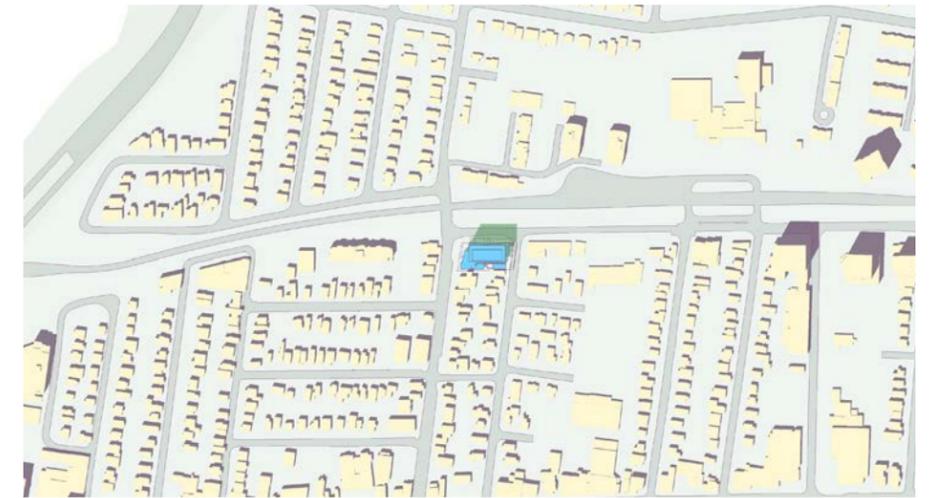
June 21 at 10am



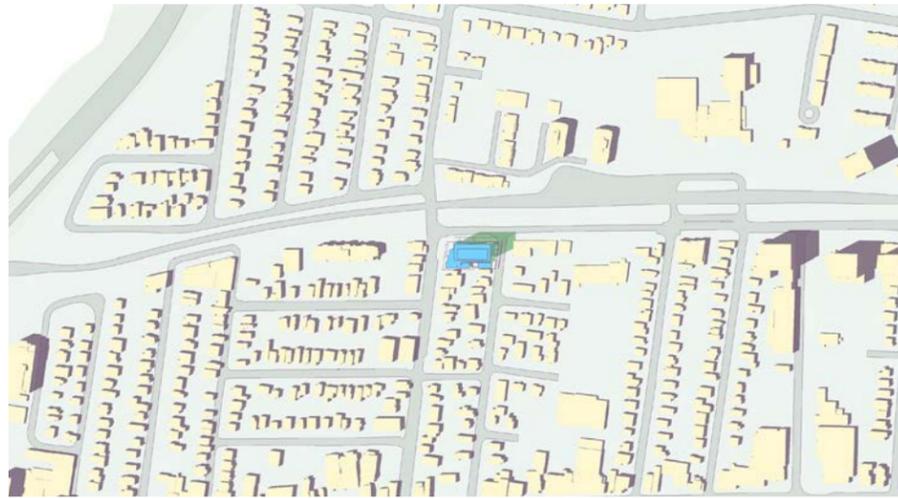
June 21 at 11am



June 21 at 12pm



June 21 at 1pm



June 21 at 2pm



June 21 at 3pm



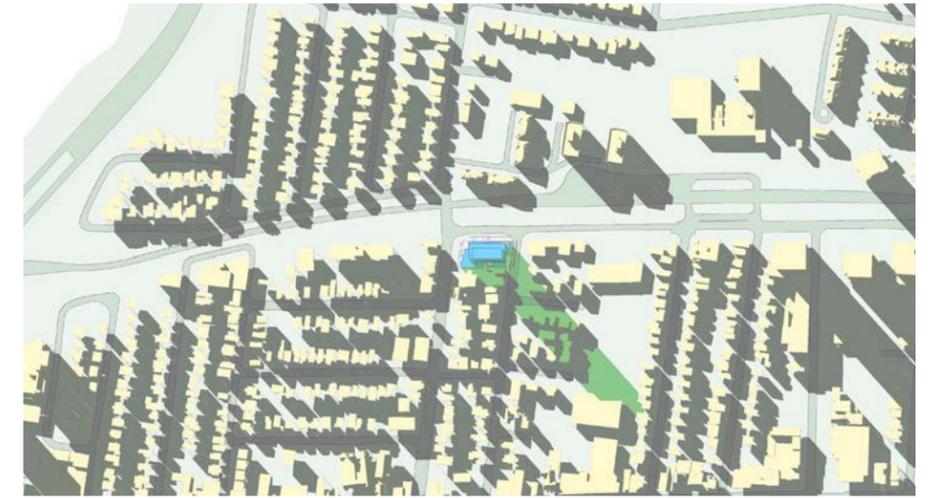
June 21 at 4pm



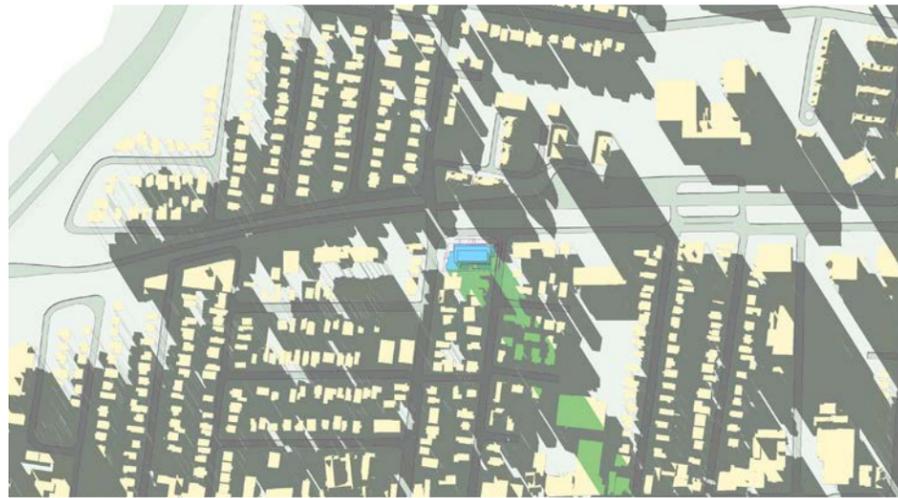
June 21 at 5pm



June 21 at 6pm



June 21 at 7pm



June 21 at 8pm



September 21 at 8am



September 21 at 9am



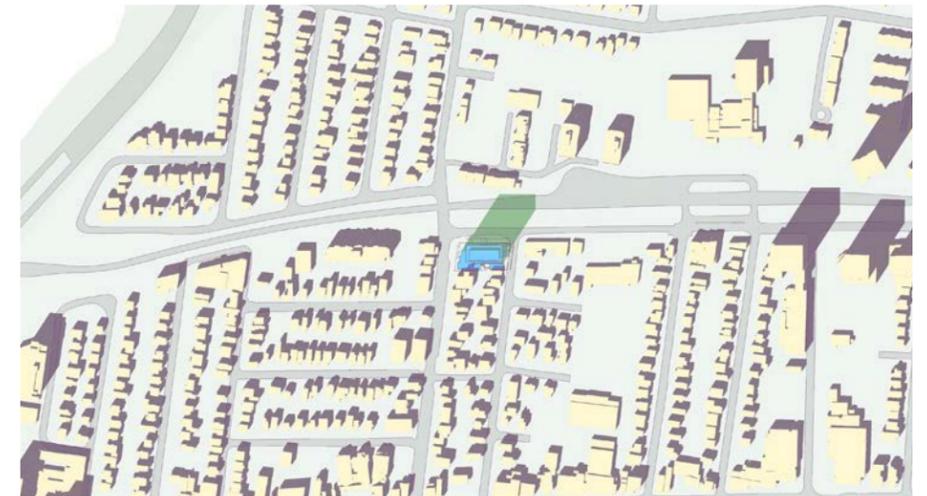
September 21 at 10am



September 21 at 11am



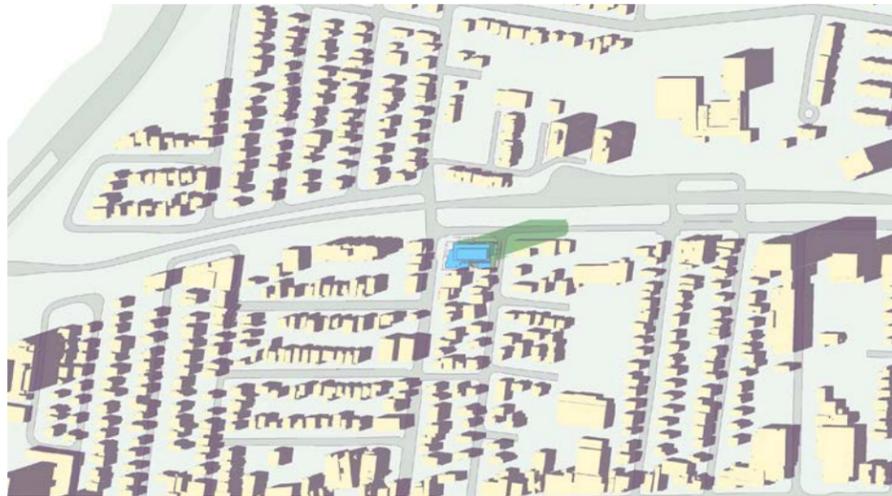
September 21 at 12pm



September 21 at 1pm



September 21 at 2pm



September 21 at 3pm



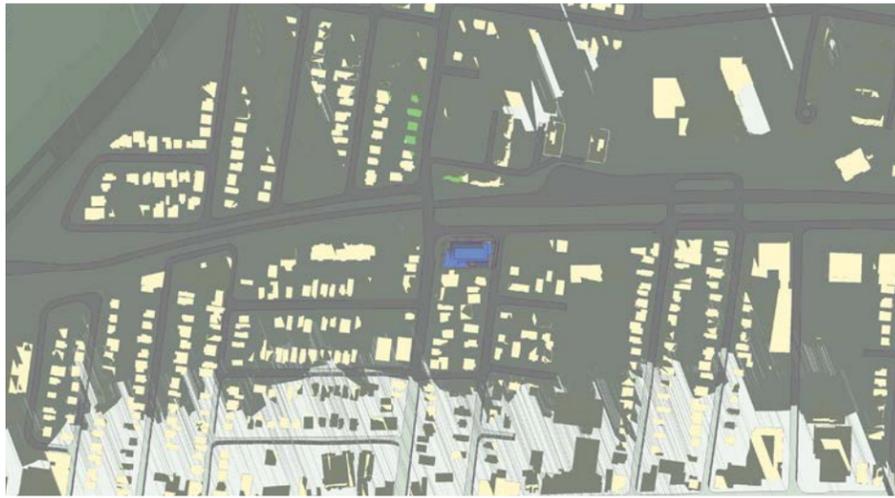
September 21 at 4pm



September 21 at 5pm



September 21 at 6pm



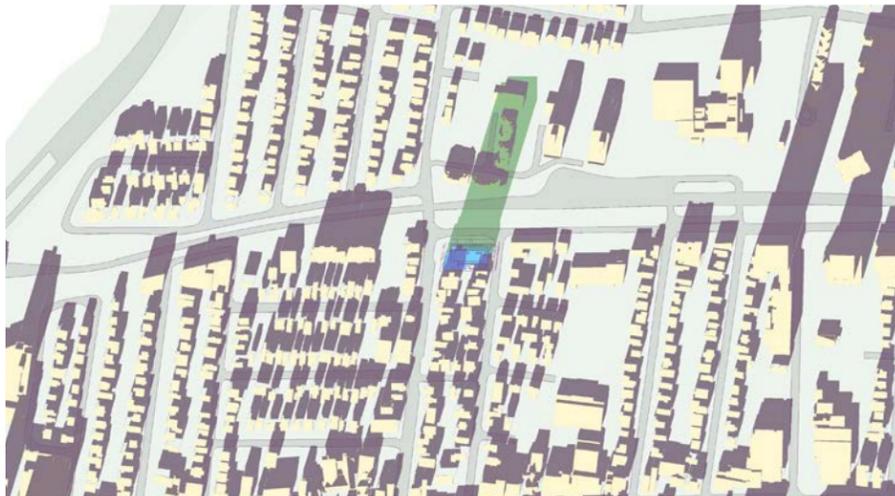
December 21 at 9am



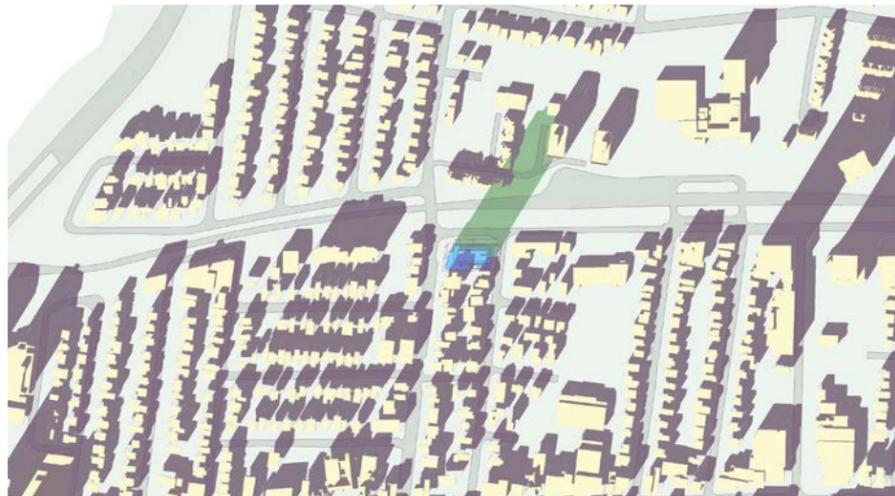
December 21 at 10am



December 21 at 11am



December 21 at 12pm



December 21 at 1pm



December 21 at 2pm



December 21 at 3pm