

FOTENN

541 & 545 RIDEAU STREET



January 26, 2018

Planning Rationale
and Design Brief

Zoning By-law Amendment
and Site Plan Control



Prepared for:



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January 26, 2018

Fotenn Consultants Inc., acting as agents for 10311197 Canada Inc. (“Chenier Development Corp.”), is pleased to submit the enclosed application for a Zoning By-law Amendment and Site Plan Control for the redevelopment of their lands municipally known as 541 and 545 Rideau Street (“the subject site”).

The proposed development consists of a nine (9) storey mixed-use building with ground-floor, street-oriented commercial uses and 102 residential units in the upper eight (8) storeys. An existing building on the site is identified as having heritage interest, and is being retained and relocated as a semidetached dwelling through a program of deconstruction and reconstruction on a different part of the site.

The proposed development conforms to the policies and objectives of the Community Design Plan and Secondary Plan for Uptown Rideau Street, as well as to the strategic direction of the Official Plan in general; the purpose of the requested Zoning By-law Amendment is to obtain relief from zoning provisions relating to the maximum height, maximum Floor-Space Index (FSI), and minimum setbacks in effect for this site.

SURROUNDING AREA AND SITE CONTEXT

2.1 The Site

The subject site consists of two parcels known municipally known as 541 and 545 Rideau Street.

541 Rideau is currently the location of a three-storey detached house, most recently in use as a restaurant. This house is identified as having some heritage value, though it is not currently designated. 545 Rideau is located immediately east of 541 Rideau, and currently functions as surface parking lot.

These parcels have a combined frontage of 31.4 metres along Rideau Street, a frontage of 48.1 metres on Cobourg Street, and a combined area of 1332.7 square metres.

2.2 Context

The subject site is located on Rideau Street at the northeast corner of the intersection with Cobourg Street (Figure 1). Rideau Street is an important mainstreet, and functions both as a major thoroughfare and as a main commercial strip linking Vanier and Montreal Road to the east with the Byward Market, Parliament, and Ottawa's central business district to the west. Rideau Street is also the boundary between the neighbourhoods of Lowertown, to the north, and Sandy Hill to the south.

The surrounding area has been subject to significant development interest in recent years, with several active proposals for mid- and high-rise buildings. The present proposal to introduce a 9-storey mixed-use building is timely, as it will form part of the emerging context of a revitalized Rideau Street.



Figure 1: Context

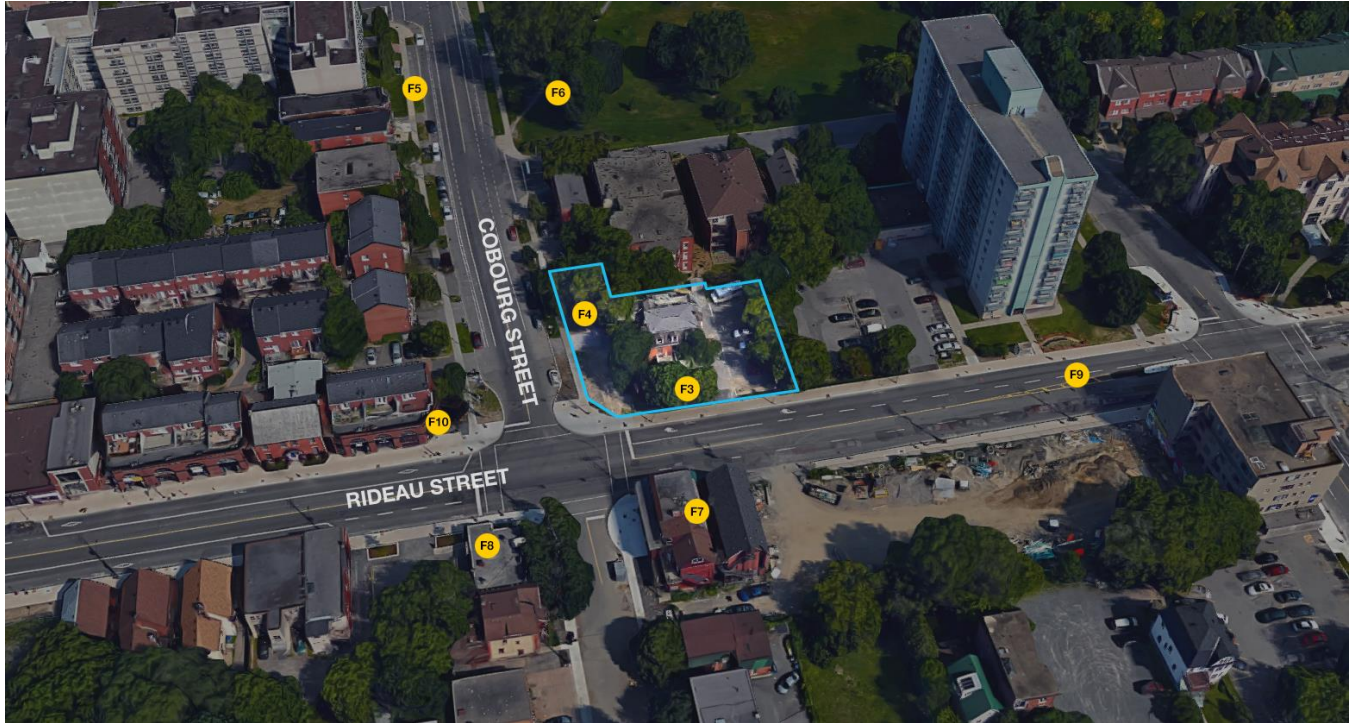


Figure 2: Aerial view of the subject site – see Figures 3 through 10 for site photos

North of the site, sharing a rear lot line with 541 & 545 Rideau, are some low-rise apartment buildings and two-storey houses. Beyond them is MacDonald Gardens, a large neighbourhood park, and a variety of residential uses and building types in the Lowertown neighbourhood.

South of the site is Rideau Street, and beyond it the neighbourhood of Sandy Hill, characterized by a diverse range of low-rise residential uses and occasional mid- or high-rise apartment buildings. The south side of Rideau Street in this area is defined predominantly by low-rise commercial uses, though there are a number of temporarily vacant lots that are testament to future development and intensification of this corridor.

East of the site is a high-rise apartment building and its associated surface parking. Along Rideau Street east of the subject site are a mix of low-rise commercial uses on the south side of the street, and residential uses of mixed form (some high-rise apartments, some low-rise townhouses). The Rideau River and Cummings Bridge are approximately 430 metres east of the subject site.

West of the site, Rideau Street continues all the way to the Rideau Canal, and is lined with gradually more intense commercial and mixed-use development as it proceeds farther west. Immediately across from the subject site on Cobourg Street is a townhouse planned-unit development with active ground-floor commercial uses fronting on Rideau Street.

2.3 Community Amenities

The proposed development is near many community amenities, including commercial and retail uses along Rideau Street and Montreal Road to the east; MacDonald Gardens Park; the various community assets at the Patro d'Ottawa community centre and adjacent Jules Morin Park; and several schools including Sainte-Anne Catholic Elementary, York Street Public School, Mauril Belanger Public Elementary, and De la Salle Public High School. The subject site is approximately one kilometre away from the University of Ottawa campus.



Figure 3: Existing conditions on the subject site (looking northwest)



Figure 4: Subject site, neighbouring low-rise apartments to the north, and high-rise apartments to the east



Figure 5: North of the subject site – Cobourg Street, with low- and mid-rise residential at left, MacDonald Gardens Park at right (looking north)



Figure 6: North of the subject site – MacDonald Gardens Park



Figure 7: South of the subject site – low-rise commercial uses; at left, a site cleared for future development (looking southeast)



Figure 8: South of the subject site – low-rise commercial and surface parking (looking southwest)



Figure 9: East of the subject site – several high-rise apartments; Rideau Street heading toward Vanier



Figure 10: West of the subject site – low-rise commercial with townhouses behind; Rideau Street heading toward downtown

2.4 Transportation Network

2.4.1 Active Transportation

Both Rideau Street and Cobourg Street are identified as part of on-road cycling routes and part of Ottawa's primary urban cycling network in Schedule C of the Official Plan. The site's location in downtown Ottawa confers it with excellent cycling and pedestrian access.

The subject site also has excellent transit service, with nearby access to Routes 7, 12, 14, 16, 17, 18 and 19 running on Rideau Street, Cobourg Street, and Charlotte Street. This variety of bus routes provides access to many areas of the city.



Figure 11: Excerpt from OC Transpo system map

2.4.2 Road Network

The subject site is well served by the existing road network in its area. It is situated at the intersection of Rideau Street, an arterial road, and Cobourg Street, a collector road. Highway 417 can easily be reached using the Vanier Parkway approximately 1 kilometre east of the subject site.

3.0 PROPOSED DEVELOPMENT

Chenier Development Corp is proposing to construct a nine (9) storey mixed-use building at 541 and 545 Rideau Street. The proposed building is to have commercial uses at grade and 102 residential units in the floors above.

It is proposed to retain the existing house by reconstructing it on the northwest corner of the site, fronting on Cobourg Street, and adapting it for reuse as two (2) semi-detached units.

The proposal was developed with regard for numerous design guidelines and policies, including:

- / City of Ottawa Official Plan
 - o Managing Growth policies (Section 2.2.2)
 - o Building Liveable Communities policies (Section 2.5.1)
 - o General Urban Area policies (Section 3.6.1)
 - o Compatibility policies (Section 4.11)
- / Official Plan Amendment 150
- / Uptown Rideau Community Design Plan
- / Uptown Rideau Street Secondary Plan
- / Urban Design Guidelines for Development along Traditional Mainstreets

Conformity with these policies is discussed in detail in Section 4.

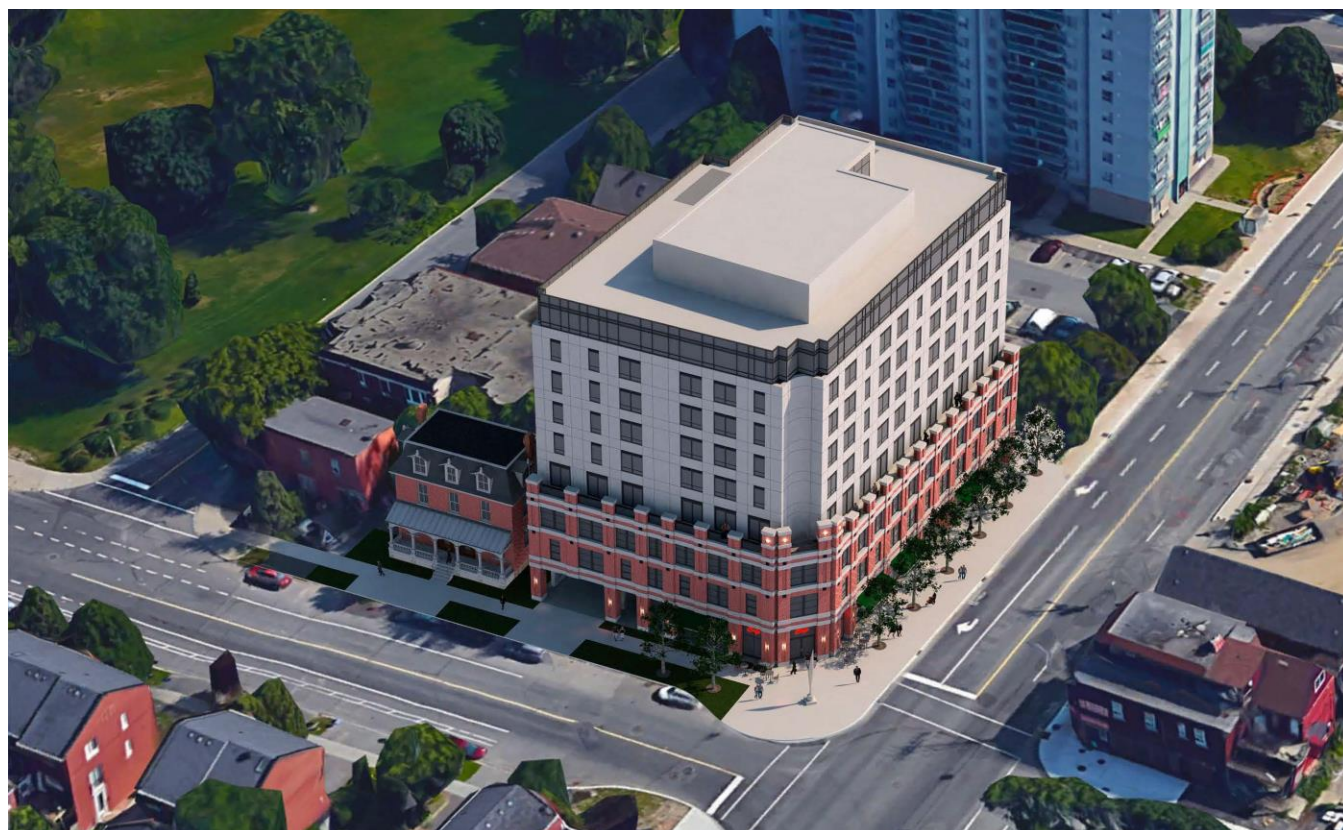


Figure 12: Perspective of proposed development



Figure 13: Perspective of proposed development – as seen from Rideau Street



Figure 14: Perspective of proposed development – as seen from Cobourg Street

The proposed development was designed as a memorable and high-quality addition to the Rideau Traditional Mainstreet. Particular care and attention was paid to the Uptown Rideau Street Community Design Plan and Secondary Plan as the guiding vision of this area's future.

The building is designed with a transparent ground floor with commercial uses to animate adjacent pedestrian spaces. The podium responds to the prevalence of red brick in surrounding low- and mid-rise development, while distinctive banding at the first, second and third floors echo the proportions of the reconstructed house. The upper volumes of the building are distinct from the podium in keeping with the “base-middle-top” approach favored by the CDP and Secondary Plan.

Vehicular access to the site's 53 parking spaces is via a 6 metre driveway from Cobourg Street, which enters at the rear of the building and transitions to a ramp with a 6% slope down to two levels of underground parking. 55 bicycle parking spaces are provided indoors.

Private terraces at the fourth-storey stepback provide 91 square metres of amenity space, while a rooftop patio and interior spaces on the ground floor—including a common room and an exercise room—provide 284 square metres of communal amenity space.

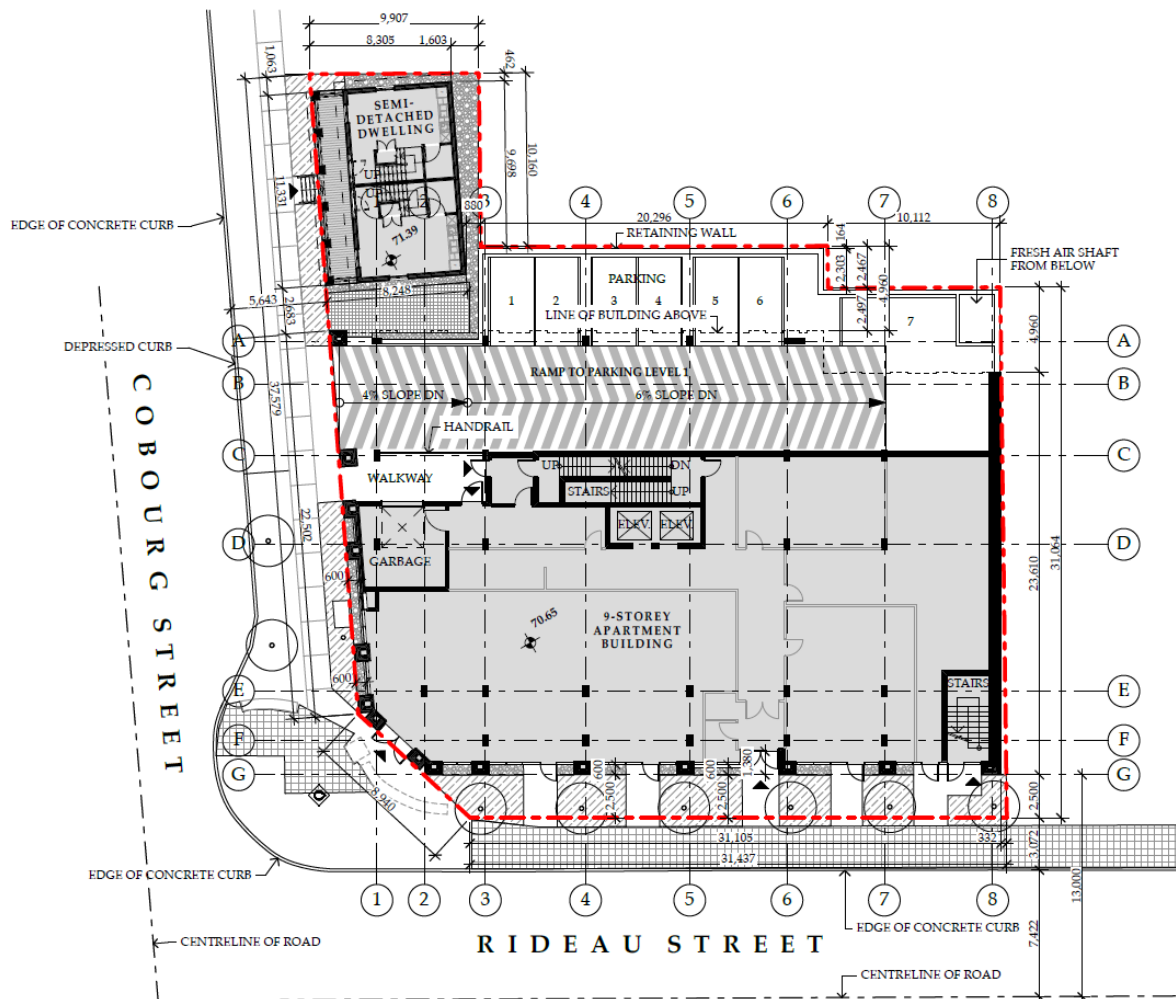


Figure 15: Proposed site plan



Figure 16: Elevation drawings of reconstructed house

While the existing house on the subject site is not formally designated as a heritage building, it is noted as having historic value; as such, the proposal intends to retain and embellish the front façade and a portion of the side elevations. The preservation will be accomplished by carefully deconstructing the building, then reconstructing it at the northwest corner of the site with frontage onto Cobourg Street and adapting it for use as a semi-detached dwelling (Figure 16) which will share parking and amenities with the principal building. The residential building is in keeping with residential uses along Cobourg Street, and the profile of the 3-storey building contributes to an appropriate transition to the neighbourhood to the north.

As the reconstructed house is the subject of pending condition assessments, feasibility studies, and a Cultural Heritage Impact Statement (CHIS), the inclusion of this heritage component will be discussed in more detail at a later date. Since the reconstructed house will be sited on a small “peninsula” of this irregularly shaped site and is of a much smaller scale and potential impact than the proposed nine-storey mixed-use building, this rationale will focus primarily on the design and appropriateness of the mid-rise development.

At present it can be noted that the setbacks from the reconstructed house to the property line (0.88m to the east; 0.46m to the north) are similar to the setbacks of the existing two-storey building at the corner of Cobourg Street and Tormey Street and can reasonably be accommodated as further details are confirmed. The spacing and rhythm of the lot and setbacks will be in keeping with the existing neighbourhood pattern.

POLICY & REGULATORY FRAMEWORK

4.1 Provincial Policy Statement (2014)

The Provincial Policy Statement (PPS) was issued under Section 3 of the Planning Act in April 2014. It provides direction on matters of provincial interest related to land use planning and development. The Planning Act requires that decisions affecting planning matters “shall be consistent with” policy statements issued under the Act.

The PPS emphasizes the intensification of built-up areas to promote the efficient use of land, existing infrastructure and public service facilities to avoid the need for unjustified and uneconomic expansion. Planning authorities must identify and promote opportunities for intensification and redevelopment [Policy 1.1.3.3].

The proposed development capitalizes on an opportunity for mixed-use development and intensification within the City’s built-up area. It minimizes the consumption of new land for development and makes efficient use of existing infrastructure and public services, brings residential intensification to an underutilized site and adds housing in an area where public transit is easily available, and promotes densities that contribute to more sustainable land use patterns.

4.2 City of Ottawa Official Plan 2003, as amended

The City of Ottawa Official Plan is composed of eight sections, each addressing a different aspect of the planned function of the City as a whole. Section 2 of the Official Plan provides Strategic Directions for growth and development within the City.

Ottawa’s population is projected to grow by up to 30 percent by 2031. The City plans to meet this growth challenge by managing it in ways that support liveable communities and healthy environments. In other words, the City is striving to create ‘complete’ communities in which residents do not need to drive for everyday activities and where jobs, shopping, recreation and social activities lie within walking or cycling distance.

More specifically, the Plan pursues strategic directions in four key areas, two of which are relevant to the proposal:

1. Managing Growth
 - / The City will manage growth by directing it to the urban area where services already exist or where they can be provided efficiently, including development on previously underutilized lots;
 - / Growth in the urban area will be directed to areas where it can be accommodated in compact and mixed-use development, and served with quality transit, walking and cycling facilities.
2. Creating Liveable Communities
 - / The City will provide opportunities to increase the supply of affordable housing throughout the rural and urban areas;
 - / Growth will be managed in ways that create complete communities with a good balance of facilities and services to meet people’s everyday needs, including schools, community facilities, parks, a variety of housing and places to work and shop;
 - / Attention to design will help create attractive communities where buildings, open space, and transportation work well together.

These strategic directions are developed further in the policies of Section 2.2 (Managing Growth) and 2.5 (Building Liveable Communities), and are discussed below.

4.2.1 Managing Growth

Section 2.2.2 of the Official Plan deals specifically with the management of growth within the urban area and recognizes that intensification is generally the most cost-effective pattern of development for the provision of municipal services, transit, and other infrastructure.

The pattern of intensification described in the Official Plan is described one of nodes and corridors. Mainstreets are the corridors in this scheme. Mainstreets are intended to carry crosstown commuters and attract shoppers from all over the city, and act as corridors for services, meeting places, and residences for people occupying adjacent neighbourhoods.

Policies 1b and 1c of Section 2.2.2 cite redevelopment of underutilized lots in existing built-up areas and infill development as forms of residential intensification. Target areas for intensification, per Policy 4, includes Mainstreets. Policy 13 concerns the promotion of compact, mixed-use development. The proposed multi-storey mixed-use building with below-grade parking and internal amenity spaces furthers the Official Plan's objectives for efficient and compact development.

The proposed development conforms to the intent of the Official Plan policies on managing growth within the City, where growth in Ottawa's designated urban area is to be directed to locations with significant development potential like Mainstreets. It brings residential intensification and new commercial uses to a Mainstreet site that is, in its current state, performing significantly below its planned function in terms of residential density and commercial activity.

4.2.2 Building Liveable Communities

Section 2.5 of the Official Plan describes the basics of liveable communities – good housing, employment, ample greenspace, and a sense of history and culture – and proposes to create more liveable communities by focusing on community design and collaborative community building. Community design engages with the details of how buildings and landscapes relate.

The proposed development relates to key design objectives and principles in Section 2.5.1 as follows:

- / Enhances the sense of community by creating and maintaining places with their own distinct identity;
 - o The proposed development has a distinctive architectural personality, and adds a new high-quality building to an important corner along Rideau Street that is otherwise underdeveloped relative to its potential.
- / Defines quality public and private spaces through development;
 - o The proposed development is an incremental step toward defining and enclosing the street edge of Rideau Street and, in doing so, establishing a better relationship between the building and the street. The new retail uses will support and encourage activity on the street. The relocated building of heritage interest will make a positive contribution to Cobourg Street.
- / Creates places that are safe, accessible and easy to get to, and move through;
 - o Having active ground-floor uses with street-facing windows and front entrances, the proposed development introduces “eyes on the street” and promotes a safe community through natural surveillance.
- / Considers adaptability and diversity by creating places that can adapt and evolve easily over time and that are characterized by variety and choice.
 - o Residential intensification of 541 & 545 Rideau Street helps achieve a more compact urban form over time and contributes to the evolution of the eastern extent of the Rideau Traditional Mainstreet. A mixed-use building provides flexibility in accommodating commercial uses at grade that can serve the community in a variety of ways.

4.2.3 Land Use Designation

541 and 545 Rideau Street are designated Traditional Mainstreet on Schedule B – Urban Policy Plan in the City of Ottawa Official Plan. Mainstreets are, according to Section 3.6.3 of the Official plan, streets with some of the most significant opportunities in the City for intensification. They are planned as compact, mixed-use, and

pedestrian-oriented streets that can be accessed by foot, cycle, transit and automobile. A broad range of uses is permitted on Mainstreets, including retail and service commercial, office, residential, and institutional uses, as well as uses mixed within individual buildings.

Policy 8 of Section 3.6.3 encourages infill development and other intensification along Traditional Mainstreets in a building format that “encloses and defines the street edge and provides direct pedestrian access to the sidewalk.” Heights up to six (6) storeys are generally permitted on Traditional Mainstreets, and greater building heights may be considered in accordance with Section 4.11.

4.2.4 Compatibility


Section 4.11 of the Official Plan sets out criteria which are used to evaluate the compatibility of proposed developments. These criteria include: traffic, vehicular access, parking requirements, outdoor amenity areas, loading areas, service areas and outdoor storage, lighting, noise and air quality, sunlight, microclimate, supporting neighbourhood services.

Policy 1 of this section states that when evaluating compatibility of development applications, the City will have regard for the site’s land use designation. The compatibility of the proposed development, therefore, should be evaluated with Rideau Street’s Traditional Mainstreet status in mind.

Policy 2 enumerates evaluation criteria for the compatibility of development applications, as follows:

Table 1. Compliance with Section 4.11 of the Official Plan

COMPATIBILITY CRITERIA	PROPOSED DEVELOPMENT
TRAFFIC:	<p>The subject site is located near the heart of Ottawa and benefits from proximity to employment, community services and amenities, as well as transit and cycling facilities which can easily accommodate the proposed development. Because of this proximity and the availability of public and active transportation options, it is anticipated that the proposed development will generate fewer vehicular trips.</p> <p>As per the Transportation Impact Assessment prepared by Parsons, vehicular traffic generated by the proposed development can be accommodated by Rideau Street, Cobourg Street and the surrounding road network at an acceptable level of service. The additional traffic demand generated by the proposed development is considered negligible.</p>
VEHICULAR ACCESS:	Vehicular access is provided via a 6 metre driveway from Cobourg Street, which is safely located approximately 24 metres from the intersection with Rideau. Situating the access on Cobourg Street is intended to minimize vehicles making turns from Rideau Street, which is identified as a transit priority corridor.
PARKING:	Two (2) levels of underground parking are provided, as well as a small amount of surface parking spaces at the rear of the site in keeping with Traditional Mainstreet policy discouraging parking between the building and the street. The supply of parking spaces meets the requirements established in the Zoning By-law.
OUTDOOR AMENITY AREAS:	Outdoor at-grade amenity areas associated with the low-rise developments abutting the subject site to the north are either minimal or non-existent; no undue impacts on privacy are anticipated. The proposed development does not include balconies, which further reduces impact on the privacy of adjacent outdoor areas.

SUNLIGHT:	<p>Shadow analysis carried out by Barry Padolsky Associates has shown that the proposed building would not cast shadows significantly more impactful than those that would occur under the existing as-of-right zoning.</p> <p>The following graphic (Figure 17) is a sample from the shadow analysis. Note how the shadow of the height and massing permitted by the existing zoning, which is shown in green, already encompasses the rear yards of adjacent sites.</p>  <p>Figure 17: Sun shadow study for December 21 at 1:00pm.</p> <p>Moreover, the proposed development fits within the height limit anticipated by the CDP and Secondary Plan; impacts arising from mid-rise development on Rideau Street were contemplated during the planning process for the above policy documents.</p> <p>No undue adverse impacts are anticipated. Please consult the full shadow analysis for more comprehensive detail.</p>
SUPPORTING NEIGHBOURHOOD SERVICES:	<p>The proposed development is located in close proximity to several neighbourhood amenities including schools, parks, and community centres. The site is located on a Traditional Mainstreet and has easy access to the many retail uses and commercial services located on Rideau Street.</p>

Greater building heights than those typically considered on Traditional Mainstreets are justified by Policy 9 of Section 4.11, which states that taller buildings are appropriate within areas characterized by existing high-rise buildings that have direct access to an arterial road (Policy 9a.) or where a community design plan or secondary plan identifies locations suitable for a community focus on a strategic corner lot (Policy 9c). The Rideau Street Secondary Plan and CDP support a nine (9) storey building on this site.

The proposed development conforms to the City of Ottawa Official Plan and the policies therein. It significantly advances the goals of the Traditional Mainstreet land use designation in this area, adds infill housing that contributes to a more compact, mixed-use development pattern, and contributes to a liveable community with a compatible development form.

4.3 Official Plan Amendment 150

In 2013, the City of Ottawa reviewed its Official Plan resulting in numerous policy changes. Ottawa Council adopted Official Plan Amendment (OPA) 150 in December 2013, receiving Ministry of Municipal Affairs and Housing approval April 24, 2014. As OPA 150 is currently under appeal, the current policies of the City of Ottawa Official Plan 2003, consolidated May 2013 remain in full force and effect. Despite this, the relevant policies of OPA 150 have been considered below because this is a Council-approved document.

4.3.1 Section 2.2.2

In OPA 150, Section 2.2.2 emphasizes the role of Secondary Plans in determining appropriate building heights. Per Policy 12 of this section, Secondary Plans “may specify greater or lesser building heights than those established in Section 3 where those heights are consistent with the strategic directions of Section 2.”

Where the Official Plan generally supports six-storey heights on Traditional Mainstreets, Section 2.2.2 determines that greater heights for the subject site are justifiable because the Uptown Rideau Street Secondary Plan specifies a height of up to nine storeys.

4.3.2 Sections 2.5.1 and 4.11

Section 2.5.1 remains largely unchanged in OPA 150 except to provide more flexibility in how its objectives are addressed. It clarifies that “compatible development” is not necessarily the same as or similar to existing buildings in its vicinity, and can enhance the established community through good design and innovation. Proponents are “free to respond in creative ways to the Design Objectives” and are not limited to approaches suggested in the Official Plan.

Section 4.11 has been significantly revised. As such, Table 2 assesses the proposed development with regards to the relevant policies of the revised Section 4.11.

Table 2. Compliance with Section 4.11 of OPA 150

COMPATIBILITY CRITERIA	PROPOSED DEVELOPMENT
VIEWS:	The building will not impact any protected views. [Policies 2, 3, 4];
BUILDING DESIGN:	<p>The proposed development, being a nine-storey mixed-use building with minimal setback from the street, upper-storey stepbacks, and a transparent ground floor with active entrances, fits with the desired character of a Traditional Mainstreet, and represents an improvement to built form on Rideau Street [Policy 5];</p> <p>The principal façade and entrances are oriented to the street, and windows are visible from the public realm [Policy 6];</p> <p>Intersections of arterial and collector roads support land uses that are focal points for a community, and are required to have strong architectural design elements that emphasize the corner and capitalize on design possibilities for both facades [Policy 7];</p>
MASSING AND SCALE:	<p>The proposed nine (9) storey building fits within the scale prescribed in the CDP and Secondary Plan [Policy 9];</p> <p>A shadow analysis was conducted to evaluate potential impacts of the proposed development, these impacts are not qualitatively different from impacts that would result from the as-of-right zoning envelope [Policy 10];</p>

	<p>The building makes use of architectural detailing and a distinct podium base to provide a transition in scale from adjacent development [Policy 11];</p> <p>The location on a strategic corner of a Traditional Mainstreet justifies additional height to make it a community focus; it includes an upper-storey step-back on the Rideau Street and Cobourg Street façades that is in keeping with CDP guidance [Policy 12];</p>
OUTDOOR AMENITY AREAS:	The proposed development will have no undue impact on the amenity spaces of adjacent residential units. [Policy 15].

4.4 Uptown Rideau Community Design Plan

The Uptown Rideau Community Design Plan (CDP) was approved by City Council in 2005 and updated in 2015. The CDP provides policy direction for the evolution of Rideau Street as a vibrant downtown mainstreet, a gateway and transition to Ottawa's Central Area and Parliament Hill, a gathering place between the neighbourhoods of Lowertown and Sandy Hill, and a street of generally mid-rise buildings featuring continuous active commercial uses at grade with places for people to live and work above.

Four core principles are identified in Section 4.2.2 of the CDP:

1. Establish a vibrant, successful mainstreet that is part of Ottawa's downtown and one of the entryways to the Central Area and Parliament Hill
2. Locate new buildings and taller buildings strategically and with transition to the surrounding neighbourhoods
3. Improve the existing mobility network, prioritizing pedestrian, cyclist, and transit users and create an inviting environment
4. Enhance and connect the open space network

The CDP is divided into four (4) character areas to respond to the variety of development patterns, block sizes, land uses, and building heights and ages present in the area. The subject site is part of Character Area D, which is the quadrant north of Rideau Street and east of Augusta Street.

The subject site is limited to a baseline height of nine storeys and an FSI of 5.0 (Figure 18). The CDP states that this height and density is granted in areas "where reasonable intensification can be accommodated or where a higher density will help retain heritage assets." The proposed development satisfies both of these conditions, in that it is located on a strategic corner site and is retaining an existing building of heritage interest.

The proposed development conforms to the CDP's recommended approach to mid-rise design with its base-middle-top approach, as per Section 4.3.6. The base of the building is designed to relate to and animate the sidewalk and pedestrian realm with commercial uses, as well as to echo elements of the reconstructed house on Cobourg Street. The middle of the building is distinct from the base and contributes to a future street wall. The top of the building is articulated to break up the building's mass and lends interest to where the building profile meets the sky.

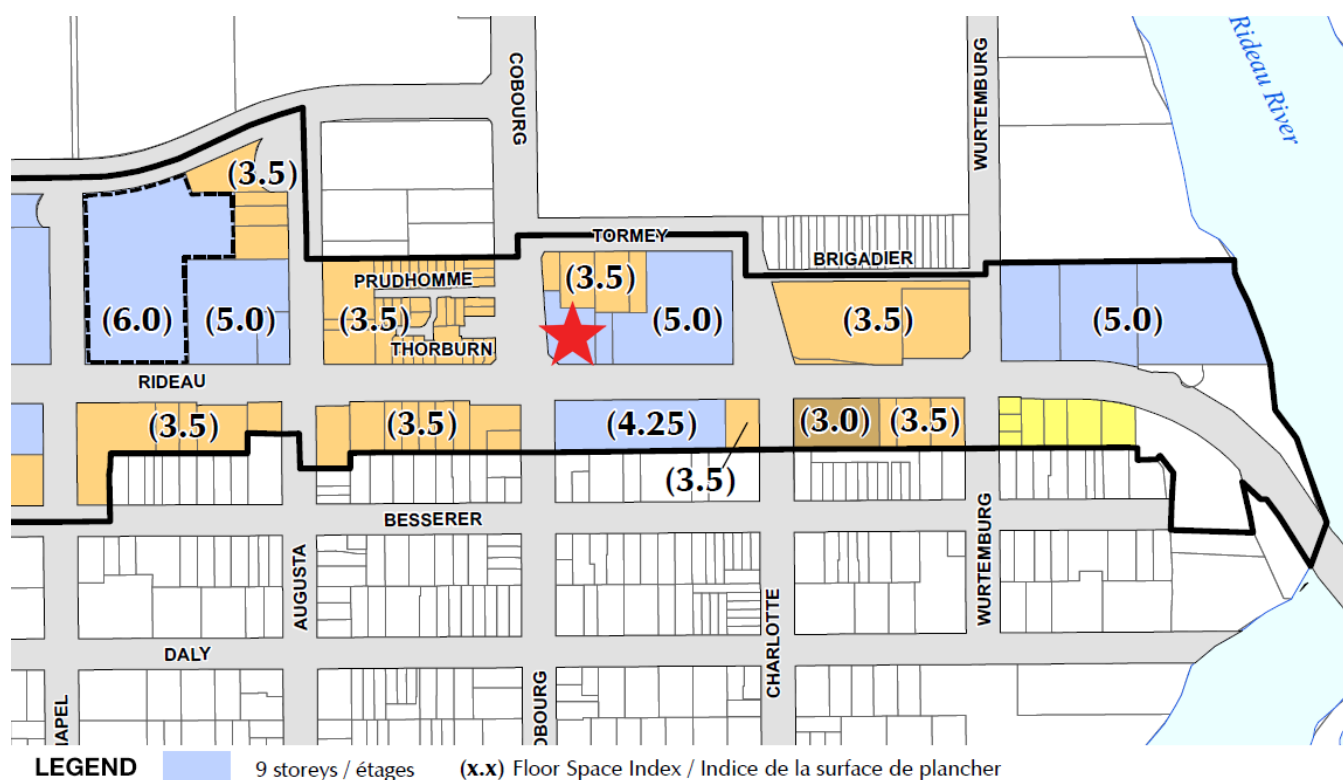


Figure 18: Uptown Rideau CDP and Secondary Plan – baseline heights and densities

The CDP states that stepbacks should be provided on front and corner elevations to maintain access to light, support a pedestrian scale, and provide a transition in scale from abutting buildings. The proposed development has stepbacks of 1.5 metres at and above the fourth floor to respond to and replicate the proportions of desirable context in the area. As this is a corner building, the CDP encourages that the corner should be given special prominence and articulation, which the proposed building does through the orientation of its commercial spaces and the detailing of its upper storeys.

Section 4.6.1 of the CDP identifies the existing house at 541 Rideau Street as a candidate for inclusion on the City of Ottawa heritage register and designation under Part IV of the Ontario Heritage Act for its cultural heritage value. In view of this, efforts are being made to retain the building on-site through reconstruction and repurpose it with a residential use.

The proposed development enlivens the surrounding neighbourhood with its active ground floor and high-quality architecture; it is consistent with the goals of the CDP in promoting context-sensitive mid-rise development with high-quality architectural design, active ground-floor uses, and respect for the area's historic characteristics.

4.5 Uptown Rideau Street Secondary Plan

The Uptown Rideau Street Secondary Plan was adopted in 2016 and is intended to provide area-based policy direction to implement the Uptown Rideau Street CDP. As such, the core principles of the Secondary Plan are the same as those enumerated in Section 4.2.2 of the CDP, discussed in the previous section.

Section 5 of the Secondary Plan addresses land use and built form. The Plan's approach is to establish mid-rise building heights in the corridor and to provide design principles as general guidance for built form.

It is acknowledged in Section 5.2 that baseline building heights and maximum FSI established in the Secondary Plan “are generally greater than the heights and FSI allowances under the current Zoning By-law” and that Council may grant such baseline heights and densities through an amendment to the Zoning By-law. As in the CDP, the subject site is identified as suitable for 9-storey development up to a maximum FSI of 5.0. The proposed development conforms to this policy with its 9-storey height and FSI of 4.54.

Section 5.4 of the Secondary Plan addresses built form principles – namely, creating a positive ground-floor experience and ensuring quality architecture in mid-rise development. As described in the previous section, the proposed development conforms to the vision of the CDP and Secondary Plan with its active, transparent ground floor and interpretation of desirable mid-rise characteristics (e.g. the base-middle-top approach to built form, massing that enhances the character of the public realm, and special treatment for the corner.)

As per the vision of the Uptown Rideau Street Secondary Plan, the proposed development advances the evolution of Rideau Street as a vibrant downtown mainstreet, as an entryway to Ottawa’s Central Area and Parliament Hill, and as a gathering place at the boundary of two established neighbourhoods.

4.6 Urban Design Guidelines for Development along Traditional Mainstreets

The Urban Design Guidelines for Development along Traditional Mainstreets were approved by City Council in 2006 and were initiated to provide urban design guidance at the planning application stage in order to assess, promote and achieve appropriate development along Traditional Mainstreets.

Key objectives of the Urban Design Guidelines for Development along Traditional Mainstreets are:

- / To promote development that will enhance and reinforce the recognized or planned scale and character of the street;
- / To promote development that is compatible with, and complements its surroundings;
- / To achieve high-quality built form and strengthen building continuity along Traditional Mainstreets;
- / To foster compact, pedestrian-oriented development linked to street level amenities;
- / To accommodate a broad range of uses including retail, services, commercial uses, offices, residential and institutional uses where one can live, shop and access amenities.

The proposed building is sited at the front of the site to begin to define a continuous streetscape on its block (Guideline 1); street trees and generous sidewalks are being accommodated (Guideline 2&3); the focus at the corner of Rideau Street and Cobourg Street will create an attractive public amenity space (Guideline 6); the building design is rich in architectural detail and highly attuned to the rhythm and pattern of the street (Guideline 8), with a highly-transparent ground floor (Guideline 11) and upper-storey stepbacks (Guideline 12).

Residential uses are located above the ground floor with a shared entrance clearly accessible from the street (Guideline 13). The building concentrates height and mass at a local node, the intersection of Rideau and Cobourg Street, where it highlights the corner site and includes detailing on both facades (Guideline 14&16). Awnings provide pedestrian weather protection on Rideau Street (Guideline 17).

The proposed development significantly advances the key objectives of the Traditional Mainstreet design guidelines.

4.7 City of Ottawa Comprehensive Zoning By-law (2008-250)

The subject site is zoned Traditional Mainstreet, subzone 6 (TM6) in the City of Ottawa Zoning By-law. The purpose of the TM zone is to accommodate a broad range of uses; promote compact, mixed-use, pedestrian-oriented development; recognize the function of Business Improvement Areas as primary business or shopping areas; and impose development standards to ensure that street continuity, scale and character is maintained, and that uses are compatible and complementary to surrounding land uses.

The TM zone permits mid-rise apartments, as well as a variety of commercial uses including bank, convenience store, medical facility, office, personal service business, restaurant, and retail uses.

The zoning for this site includes a maximum FSI of 3.5 and a height limit of 19 metres. These provisions are more restrictive than the applicable CDP and Secondary Plan, which support a FSI of 5.0 and a height of nine (9) storeys on the subject lands.

The following table summarizes the proposal's compliance with the current zoning provisions:

Table 3: Zoning Compliance for TM6 F(3.5) H(19) Zone (s.197 and Table 197A)

Provision	Required	Proposed	Compliance
Minimum Lot Area (m²)	No minimum	1,345.3m ²	✓
Minimum Lot Width (m)	No minimum	31.4m	✓
Building Height (m)	6.7m minimum, 19m maximum	28.8m	✗
Front Yard (m)	Minimum 1m, maximum 2m (s.198(6)(e)(i) and (ii))	2.5m; 0m after ROW dedication	✗
Additional Front Setbacks	Additional 2 metre setback from front lot line and corner side lot line required at & above 6 th storey(s.198(6)(e)(i))	Additional 1.5m at 4 th storey	✗
Corner Side Yard (m)	Minimum 1m, maximum 4m (s.198(6)(e)(iii) and (iv))	0m	✗
Interior Side Yard (m)	No minimum, no maximum (s.198(6)(e)(iii) and (iv))	0m (main building)	✓
Rear Yard (m)	Minimum 7.5m (s.198(6)(e)(v))	4.96m (main building) 0.46m (reconstructed house)	✗
Additional Rear Setbacks	10.5m above 3 rd storey (s.198(6)(e)(v))	4.96m	✗
Maximum FSI	3.5	4.5	✗
Parking (s.101)	0.5/unit, excluding first 12 = 44	44	✓

Provision	Required	Proposed	Compliance
Visitor Parking (s.102)	0.1/unit, excluding first 12 = 9	9	✓
Bicycle Parking (s.111)	0.5/unit + 1/250m ² retail = 51 spaces	55	✓
Amenity Area (s.137)	6m ² /unit = 624m ² Communal: 50% of total = 312m ²	372m ² total amenity 284m ² communal	✗

As previously noted, the proposed building's 28.8 metre height corresponds to the 9-storey height described in the policy context for this site. A 4.5-metre ground floor (as specified in the CDP) and eight, 3-metre residential floors (the standard height of a residential storey described in OPA 150) result in a 28.5 metre height. The additional 0.3 metres of height are necessitated by the step-back at the fourth floor, which requires thicker structural elements and weatherproofing between floors.

Similarly, while the proposed FSI of 4.5 exceeds the zoned maximum of 3.5 FSI, the Uptown Rideau Street Secondary Plan specifies that an FSI of 5.0 is appropriate for this site.

The proposed setback and stepback reductions are minor relative to the requirements, and are necessary and reasonable for the following reasons:

- / The proposed built form is a result of a commitment to remain within the height limits contemplated in the CDP and Secondary Plan while approaching the level of density (FSI 5.0) proposed in the same documents.
- / The upper-storey stepbacks of 1.5 metres are less than the 2 metre requirement of the TM6 subzone, but they conform to the stepback recommended in the CDP and Secondary Plan.
- / Rideau Street is subject to right-of-way widening that will result in a 2.5 metre conveyance from the front of this site. The building is shifted 2.5 metre to the north to accommodate the widening. The right-of-way widening will be taken through the Site Plan Control process, and the proposed building will have a front yard setback of zero metres. This is less than required by the TM6 subzone, a 0 metre setback is common along Traditional Mainstreets. Building to the front lot line is contemplated as appropriate for mainstreets in the general sense, and it is our view that the front setback reduction is appropriate from a design perspective and in light of the pending right-of-way widening.
- / Shifting the building 2.5 metres north to accommodate the right-of-way widening results in a reduced rear yard setback of approximately 5 metres, compared to the minimum required setback of 7.5 metres. Further to this, no upper-storey stepback is proposed at the rear of the building, whereas the Zoning By-law requires a 3-metre stepback beyond what is required as a rear yard setback meaning a total 10.5 metre setback above the 3rd storey. Whereas the intent of such setbacks and stepbacks is generally to protect the sunlight and privacy of amenity areas belonging to adjacent low-rise communities at the rear of a site, it should be noted that the site backs onto a different planned context. The abutting sites are zoned R5, and the CDP and Secondary Plan describe the sites as appropriate for up to 6 storeys and an FSI of 3.5. As such, it is reasonable to expect that the lots north of the subject site could be redeveloped with a denser built form in the future. The proposed development respects the existing development on those blocks but responds also to the planned context of those blocks.

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- / The reconstruction of the heritage house as a semi-detached dwelling at the northwest corner of the site results in a rear yard setback of 0.46 metres. In practice, the yard in question will function as more of a side yard for the 2.5-storey dwelling, and its impact on adjacent development is minimal.
 - / The proposed development has a 0 metre corner side yard setback, whereas a minimum 1-metre setback is required. This is mitigated by the wide bulb-out at the corner of Cobourg Street and Rideau Street and the generous boulevard beside the sidewalk to ensure sufficient space for pedestrians. A corner sight triangle has already been taken in this location, and provides sufficient visibility for turning vehicles.

The reduced on-site amenity area is remedied by the abundance of nearby public amenity space including MacDonald Gardens, Strathcona Park, Riverain Park, and the Patro d'Ottawa community centre, as well as the site's immediate access to a lively mainstreet.

The proposed development maintains the general intent of the Zoning By-law by bringing mid-rise, mixed-use development to a Traditional Mainstreet in a manner that promotes compact, pedestrian-friendly urban form and enhances the continuity, scale and character of the street. The proposed amendments to the Zoning By-law will have minimal impact on Rideau Street or the surrounding community.

The proposed development is consistent with the objectives of the Provincial Policy Statement including the provision of a range of residential uses to meet current and projected needs, the efficient use of land, infrastructure and public service facilities that will meet the long-term needs of the community, and is proposed in a settlement area that is adjacent to existing built up areas and is accessible by existing active transportation and transit networks.

The proposed development conforms to the policies of the Official Plan with regards to achieving infill and intensification goals in target areas and specifically along Traditional Mainstreets. The proposed development also conforms to urban design and compatibility policies in the Official Plan and Secondary Plan. The proposed building is consistent with the height, massing and scale called for along Rideau Street by the CDP and Secondary Plan and represents an incremental step toward a more attractive, urbanized, and mixed-use street.

The proposed development furthers the objectives of the Uptown Rideau Community Design Plan and Secondary Plan with a context-sensitive, high-quality design that adheres to the recommended height and density for the site, has an attractive mid-rise massing that follows built form principles, enlivens the adjacent pedestrian realm with an active, transparent ground floor, and promotes a community focus on an important street corner with appropriate setbacks and architectural detail.

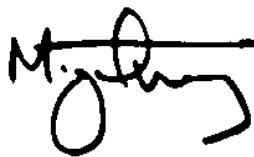
The proposed development complies with the general intent of the Zoning By-law, and the amendments that will be required for compliance are appropriate and in conformity with policy goals for the area.

Overall, the proposed development advances several key policy objectives at the Provincial and Municipal levels, including optimizing the use of serviced lands within the existing urban boundary, promoting residential intensification within the urban boundary, and improving the streetscape in a manner consistent with the planned function and character of Traditional Mainstreets. Based on the above analysis, it is our professional opinion that the proposed development represents good planning and is therefore in the public interest.

Sincerely,



Jeff Nadeau, MPL
Planner
Fotenn Consultants Inc.



Miguel Tremblay, MCIP RPP
Director, Planning and Development
Fotenn Consultants Inc.