

# CULTURAL HERITAGE IMPACT STATEMENT

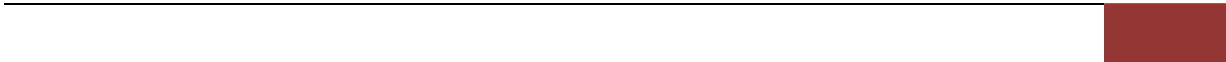
145 Loretta Avenue & 951 Gladstone Avenue,  
Ottawa, Ontario



**SUBMITTED TO: 'TIP Gladstone LP'**

**PREPARED BY: COMMONWEALTH HISTORIC RESOURCE MANAGEMENT**

**Revised November 2018**



## Table of Contents

1.0	INTRODUCTION.....	2
1.2	Present Owner and Contact Information .....	2
1.3	Site Location, Current Conditions, and Introduction to Development Site .....	3
1.4	Digital Images of Heritage Resource .....	4
1.5	Built Context and Street Characteristics .....	6
1.6	Relevant Information from Council Approved Documents.....	7
2.0	HERITAGE RESOURCE DESCRIPTION AND HISTORY .....	7
2.1	Heritage Resource Description.....	7
2.2	Site Development History .....	9
3.0	STATEMENT OF CULTURAL HERITAGE VALUE OR INTEREST .....	11
3.1	Heritage Survey and Evaluation Form .....	11
4.0	DESCRIPTION OF PROPOSED DEVELOPMENT .....	12
4.1	Context .....	12
4.2	Description .....	12
4.3	The Standard Bread Factory .....	14
5.0	IMPACT OF PROPOSED DEVELOPMENT .....	18
5.1	Introduction.....	18
5.2	Official Plan Amendment Review of Development Applications .....	18
5.3	General Standards for Preservation, Rehabilitation and Restoration.....	21
5.4	Additional Standards Relating to Rehabilitation .....	22
6.0	ALTERNATIVES AND MITIGATION STRATEGIES .....	22
6.1	Alternatives .....	22
6.2	Mitigation Measures .....	23
6.3	Conclusions.....	24
7.0	BIBLIOGRAPHY / PEOPLE CONTACTED. ....	24
8.0	AUTHORS QUALIFICATIONS.....	25

## 1.0 INTRODUCTION

---

The City of Ottawa has requested a Cultural Heritage Impact Statement (CHIS) prepared by a qualified heritage consultant to examine impacts from a proposed development on the site of the former Standard Bread Company Building. The factory is identified as a heritage resource and is listed on the City's Heritage Register.

Policy 11 of Section 4.6.1 of the Official Plan states that where development is proposed adjacent to, or across the street from, a building on the Heritage Resource List, the applicant shall demonstrate the proposal's compatibility with that heritage resource and its streetscape. It is the City's intention to proceed with designation of the Standard Bread Building under Section 29 Part IV of the Ontario Heritage Act as a heritage building with the aim of having the designation, and the development considered concurrently by Council.

This Cultural Heritage Impact Statement (CHIS) has been prepared by Commonwealth Historic Resource Management and provides a detailed review of the proposed development as it relates to the Standard Bread Building. The CHIS identifies the cultural heritage resources and values that may be impacted by the proposed redevelopment of the property at 951 Gladstone Avenue.

This CHIS follows the content outline recommended by the City of Ottawa for Cultural Heritage Impact Statements.

The following documents were consulted in the preparation of this report:

- Gladstone Station Community Design Plan 2015;
- Part IV and V of the Ontario Heritage Act;
- City of Ottawa Official Plan Amendment Policies 4.6.1(3) and (9);
- Urban Design Guidelines for High-rise Buildings;
- Guidelines for the Preparation of Cultural Heritage Impact Statements, City of Ottawa;
- Heritage Survey and Evaluation Form – 951 Gladstone Ave.;
- John G. Cooke & Associates Consulting Engineers, 951 Gladstone Avenue, Building Condition Review, May 25, 2018;
- Site Plan, 2018. Hobin Architecture
- Floor Plans, Elevations, Perspectives, 2018 Hobin Architecture; and,
- Standards and Guidelines for the Conservation of Historic Places in Canada, Second Edition, 2010.

### 1.2 Present Owner and Contact Information

**Address:**

145 Loretta Avenue North & 951 Gladstone Avenue, Ottawa ON,

**Current Owner, Representative, and Contact:**

**Aaron Cameron** Development Manager

'TIP Gladstone LP' 77 Bloor Street West, Suite 1601



Toronto, ON Canada M5S 1M2  
t: 416.255.8800 x: 253  
e: acameron@trinity-group.com

### 1.3 Site Location, Current Conditions, and Introduction to Development Site

The development site is located in the southeast corner of the Hintonburg/Mechanicsville neighbourhood and is bound by Gladstone Avenue to the south, Loretta Avenue to the west, the Trillium O-Train corridor to the east, and private property to the north. The development site is surrounded by light industrial and commercial uses fronting onto Loretta and Gladstone Avenues. The site is located within the area defined by Gladstone Station District Community Design Plan, and is next to a future O-Train Station.

The site is a 2.5-acre parcel of land that was initially developed in 1924 with the construction of the Standard Bread Company Factory. The factory is aligned with Gladstone Avenue and consists of a three and four storey concrete structure clad in brick set adjacent to the O-Train corridor, with a one-storey wing extending to Loretta Avenue. The one-storey wing is set back from Gladstone Avenue with a surface parking lot at the street. The northern portion of the development site consists of a surface parking lot and a number of single storey commercial and light industrial buildings, one of which served as a stable for the bread factory.

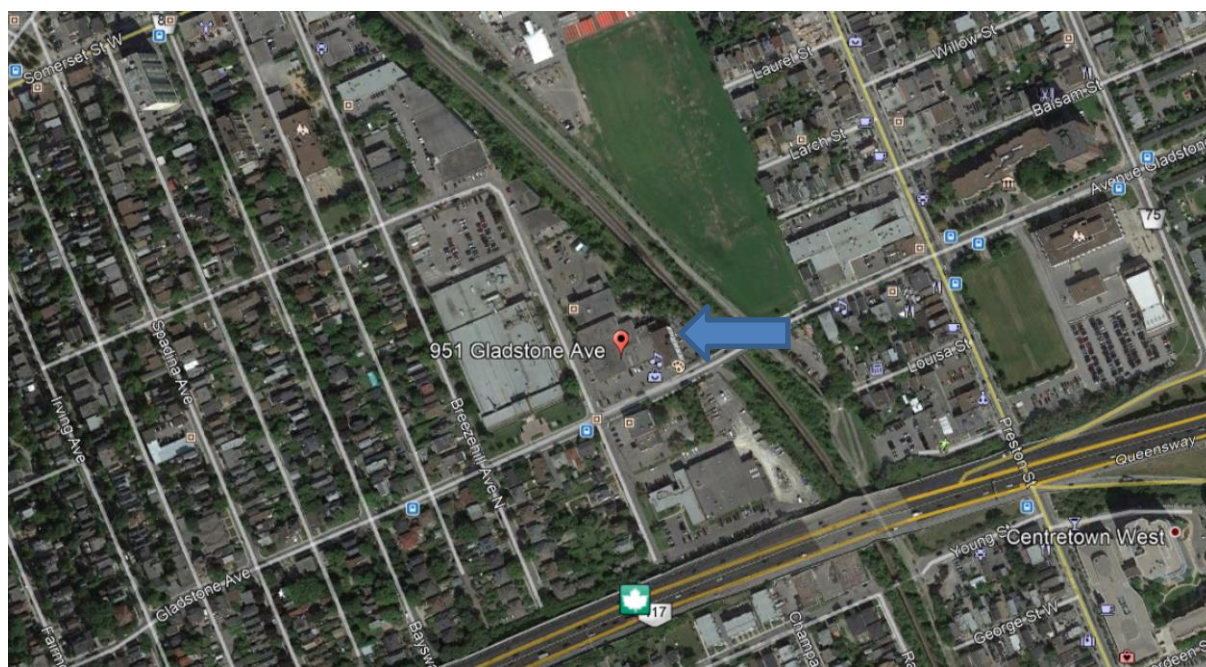


Figure 1: Aerial view of the property showing its context. Site arrowed north top of page. Source: Google Earth.

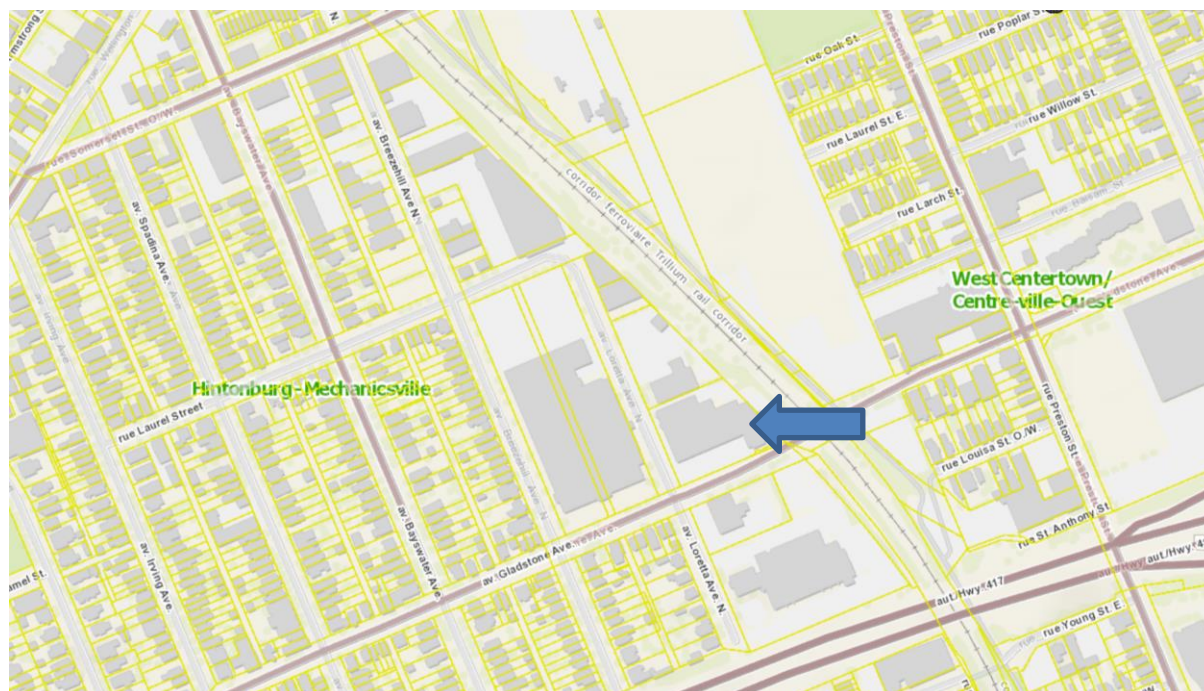


Figure 2: Block plan of the area surrounding the site illustrating the context and lot divisions. Site Arrowed. Source: GeoOttawa.

## 1.4 Digital Images of Heritage Resource

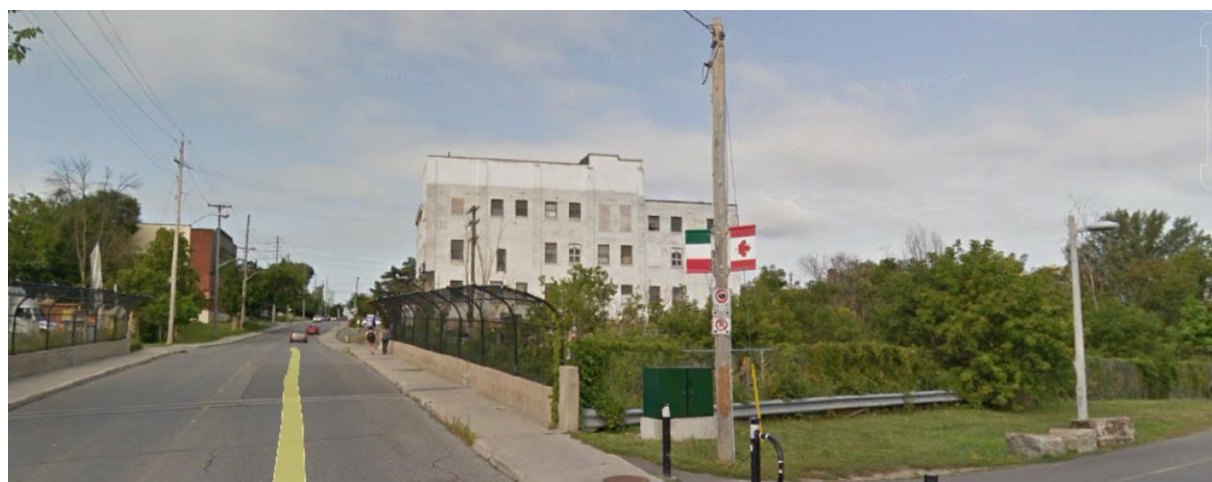


Figure 3: View looking west along Gladstone to the development site. 951 Gladstone is in the centre of the image and the O-Train corridor in the foreground. Source: Google Earth.



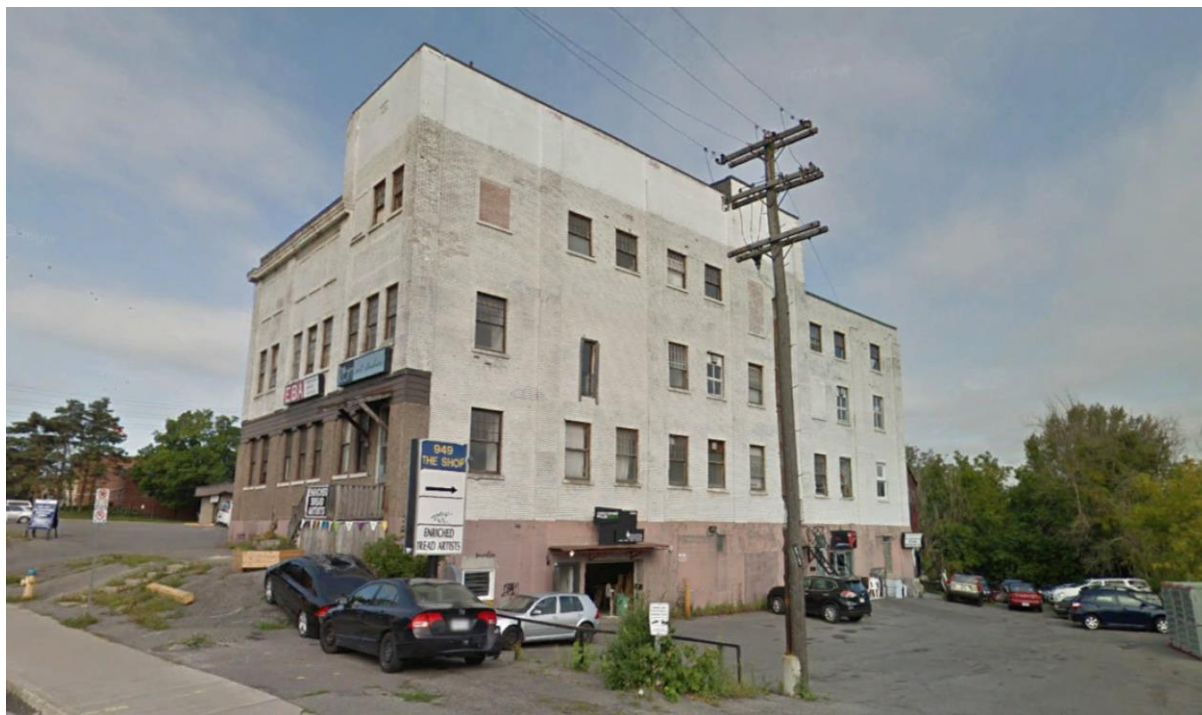


Figure 4: View looking west along Gladstone to the development site. Note the fenestration pattern on the east elevation (left) which characterizes its light industrial use with doors occurring on the second and third-floor levels and the location of an internal hoist to move material between floor levels. Source: Google Earth.



Figure 5: View of the west side of the former Standard Bread Company factory. Note the grade change between the first floor level and the sidewalk along Gladstone. Source: Google Earth.



Figure 6: View looking northeast from the corner of Loretta and Gladstone Avenues to the development site. The standard Bread Building is to the right, and the associated stable is in the left background. Source: Google Earth.

## 1.5 Built Context and Street Characteristics

The Standard Bread factory is set back approximately 5.0m from the property line along Gladstone Avenue and the ground floor level of the building is approximately 2.0m above the level of the sidewalk. The Canadian Bank Note Company is located to the west of the development site along Loretta Avenue.



Figure 7: View looking east on Gladstone to the development site (centre in the foreground of the tower, in the distance). The Canadian Bank Note Company is to the left in the foreground and residential development (right) to the west of Loretta Avenue. Source: Google Earth.



Figure 8: View looking east on Gladstone illustrating the context across the street from the development site. Source: Google Earth.





Figure 9: View looking west along Gladstone from the intersection of Loretta. Canadian Bank Note Company building is to the right, and detached residential neighbourhood to the left. Source: Google Earth.

## 1.6 Relevant Information from Council Approved Documents

The following council approved documents are relevant to the assessment of the proposed development, including:

### Gladstone Station District Community Design Plan (In Process)

*With a future Light Rail Transit Station identified at Gladstone Avenue (near the O-Train corridor), historically large, often underutilized land parcels within the area, the Gladstone Station District will become increasingly attractive to both development and prospective urban dwellers. The District has the potential to emerge as a significant area of high-quality re-urbanisation within the city.*

## 2.0 HERITAGE RESOURCE DESCRIPTION AND HISTORY

### 2.1 Heritage Resource Description

The Standard Bread Factory is significant for its historical association with the Standard Bread Company and Ottawa businesspersons Cecil Morrison and Richard (Dick) Lamothe. The Company opened around 1914 with a small bakery on Hilson Avenue in Westboro. It flourished through the First World War period and; in 1924, the new factory on Gladstone Avenue was built. The L plan building consists of a three and four storey warehouse and bread factory adjacent to the railway line with a single storey wing extending to the west along Gladstone to Loretta. A second single storey building was constructed in the 1920s to the north to house the horses and delivery wagons prior to the age of the automobile; the building remains on the site.

The Standard Bread Company factory is a good example of an early 20<sup>th</sup> century industrial building. It is a flat-roofed three-storey building with a four-storey wing at the east side. The building is a reinforced concrete frame (columns, beams, floor, and roof structures) infilled with terra-cotta tiles between columns, and floor slabs clad in a red brick. The regularly spaced pilasters conceal the concrete columns, which are set on a reinforced concrete foundation. The interior features a grid of massive concrete support pillars throughout the building supporting roof and floor structures.



*Figure 10: A photograph circa 1930 of the Standard Bread Building. Note the railway boxcar on a siding to the right of the photo. Source: City of Ottawa Archives*

Typical of factory buildings, it is very plain with little exterior ornamentation except for a simple metal cornice and parapet on the three-storey portion of the building and a metal cornice and decorative gabled brick parapet on the four-storey section. A plaque near the entrance features a Latin proverb: 'Audaces Fortuna Juvat' meaning Fortune Favours the Bold.

Most of the original single-hung wooden windows remain in the building and vary in size from floor to floor. A number of window and door openings have been bricked in but remain visible on the exterior. Brick has been applied to the exterior walls on the ground floor, and at some point in time; the brick was painted white. The front steps of the building have been altered.

## 2.2 Site Development History

The site is located adjacent to a below grade railway corridor cut into the bedrock constructed circa 1871. The railway corridor led to the former Bayview Yards and the areas adjacent to the corridor developed as a light industrial area early in the 20<sup>th</sup> century. The site was originally part of Nepean Township up until 1907 when the area was annexed by the City of Ottawa. The Gladstone Avenue Bridge over the below grade railway corridor was constructed sometime between 1911 and 1928 when Pine Street to the east of the corridor and Oliver (Willow) Street to the west were connected, and the street renamed Gladstone Avenue. The development site was vacant land. A furniture factory (J. Oliver and Son's) was located on the lot to the south in 1912. By 1928, the site was developed with a number of single storey light industrial and warehouse buildings. The Canadian Bank Note Company building was constructed in 1948 to the west of the development site. The area remains a light industrial area. With the closure of Standard Bread's operations in 1992, the building was leased, including an artist cooperative.

On July 17, 2011, a storm damaged the roof and parapet of the four storey portion of the building. The brick parapets were subsequently removed during the repair of the roof.

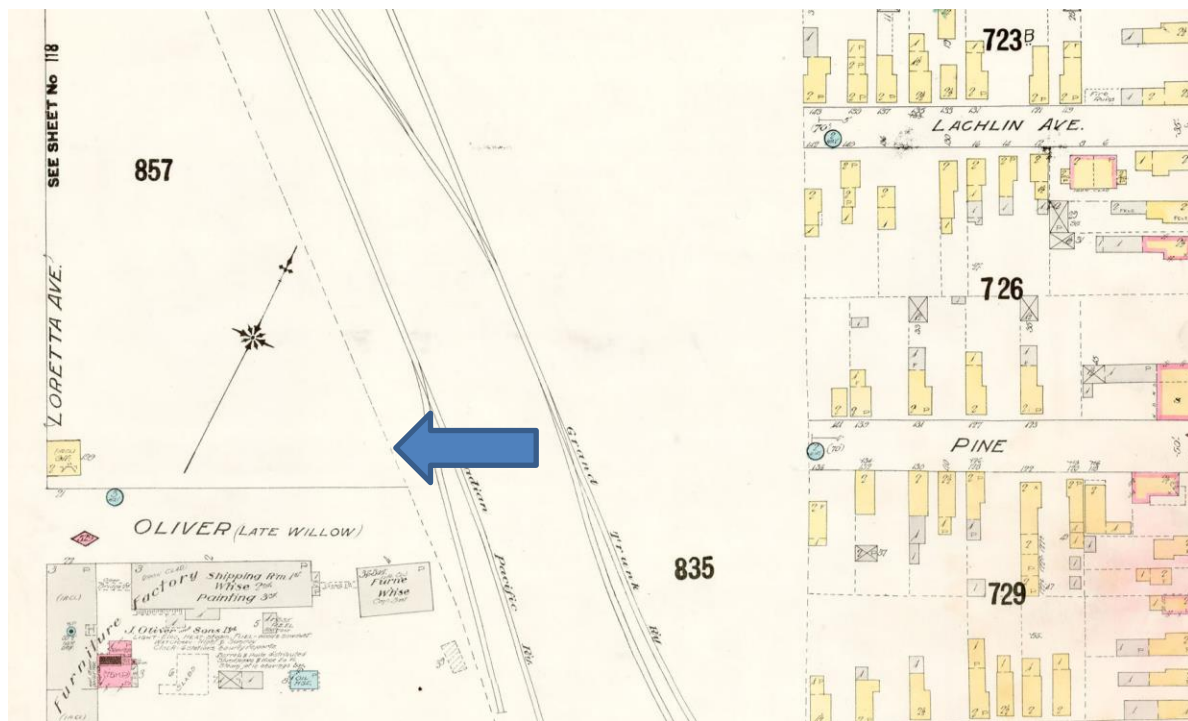
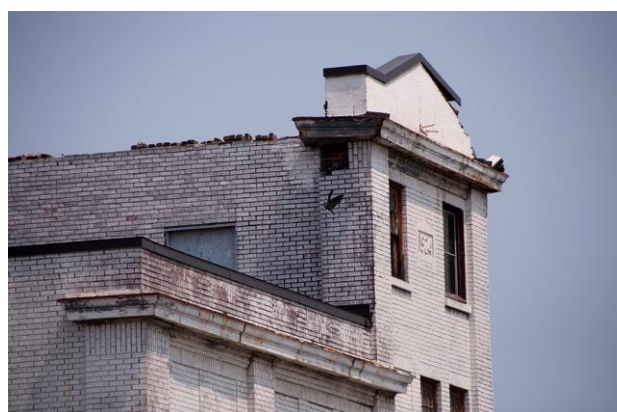


Figure 11: 1901 Revised 1912 Fire Insurance Plan Sheet 121 Volume II. The development site (arrowed) was largely undeveloped by 1912 with a furniture factory located to the south of the development site on Gladstone Avenue (formerly Oliver and Willow Streets). A bridge spanning the railway corridor connecting Pine and Oliver Streets was constructed sometime between 1912 and 1928, and the street renamed Gladstone Avenue. Source: Library and Archives Canada.





Figure 12: 1928 aerial view of the area in which the development site and the Standard Bread Building are located shortly after being constructed in 1924. The area developed as a light industrial neighbourhood served by the adjacent railway spur lines (Grand Trunk and Canadian Pacific) to the Bayview Yards. Site arrowed. Source: GeoOttawa.



Figures 13 and 14: Views of the building following the July 2011 storm that damaged the roof, and parapets on the four-storey wing. Source: Enriched Bread Artist 2011 Archive.

## 3.0 STATEMENT OF CULTURAL HERITAGE VALUE OR INTEREST

---

### 3.1 Heritage Survey and Evaluation Form

The Standard Bread Building has been listed on the City of Ottawa's heritage register. The Heritage Survey and Evaluation Form identifies the primary heritage values and attributes of the building. The associated stable building on site is not included in the listing to the heritage register.

#### **Design or Physical Value – Architecture, Craftsmanship, Technical - Summary**

The Standard Bread Company Factory is a good example of an early 20<sup>th</sup> century industrial building featuring simple decorative details.

#### **Historical and Associative Values**

The Standard Bread Company is significant for its association with Ottawa's entrepreneurs Cecil Morrison and Richard Lamothe. The building is also significant as a representative example of the work of architect, Sydney Comber. The area abutting the railway tracks was developed with light industrial buildings, and the Standard Bread Company Factory is one of the last remaining buildings representing the early 20<sup>th</sup> century industrialization of Hintonburg.

#### **Contextual Values – Community Character, Context, Landmark - Summary**

The Standard Bread Company Factory is an important building in this part of Hintonburg, serving both as a reminder of the area's industrial past set next to a railway spur line as well as a landmark in the local community.

### **CHARACTER-DEFINING ELEMENTS**

Character defining elements that contribute to the heritage value of the Standard Bread Building include:

- Its materials, including the reinforced concrete frame or structure infilled with terra cotta tiles and clad in brick;
- Its modular grid of bays separated by pilasters reflecting the structural grid or building module;
- Its fenestration pattern on the Gladstone façade with symmetrically arranged windows in groups of two and three, varying in size between floor levels with a single light lower sash and a 15 or 10 light upper sash configuration;
- Its fenestration pattern on the east elevation overlooking the railway corridor with a simpler pattern of windows in each bay with doors on the second and third floors reflecting its light industrial use;
- Its parapeted flat roofs with a false gable fronting onto Gladstone on the four storey wing, separated by simple metal cornices from the walls below;
- The main entrance set in the four storey wing; and,
- Its location next to a railway corridor.

## **4.0 DESCRIPTION OF PROPOSED DEVELOPMENT**

---

### **4.1 Context**

The proposed development is located adjacent to a future transit station on lands formally operating as the Standard Bread Factory - a prominent local landmark along Gladstone and the LRT corridor. At present, the area is characterized as industrial with no nearby registered heritage assets to consider in proximity to this development. The historic building on-site is being integrated into the proposal and will undergo conservation and rehabilitation. The City intends to designate the building.

Gladstone Avenue is a prime east-west arterial road, acting as a Traditional Mainstreet within the Centretown neighbourhood and providing access through the Hintonburg-Mechanicville, West Centretown, and Centretown neighbourhoods. South of the subject property is a further extension of the light industrial uses, including the City of Ottawa's Traffic Operations Centre and storage yard and a three-storey building at the intersection. To the southwest, on the west side of Loretta Avenue are low-rise residential dwellings. On the west side of Loretta Avenue and extending across the entirety of the block to Breezehill Avenue is the Canadian Bank Note Company. Further north at Breezehill Avenue and Somerset Street west, Claridge Homes is proposing two towers at 1040 and 1050 Somerset. A pedestrian/bicycle pathway along the train corridor will link Somerset with Gladstone. With the planned O-train station, the subject property has been identified as an appropriate location for high-rise buildings given its proximity to the LRT station.

The proposal for OPA is submitted in anticipation of the completion of the City-initiated Gladstone Station District Community Design Plan and Secondary Plan, targeted for Council consideration in 2019. The Trinity OPA will complement the emerging policy direction of the CDP and will be in keeping with the Official Plan policies related to transit stations, as implemented by Official Plan Amendment No. 150, a statutory five (5) year amendment to the Official Plan.

### **4.2 Description**

The development represents a significant intensification of the property into a mixed-use centre that will include office, retail, and residential uses. The proposed development will reinforce and further establish the character and built-form for Gladstone Avenue, rehabilitating the Standard Bread Building and integrating it within a six-storey podium with ground floor retail and office uses above, consistent with a Traditional Mainstreet character. The proposed massing respects the Standard Bread Building and reflects a pedestrian-scale along the street. The three proposed high-rise towers are set well back from the adjacent streets to acknowledge this scale and character.

The Standard Bread Building is retained in place, and the ground floor serves as a gateway internally linking the development with a new pedestrian bridge connection. The bridge will link the southeast corner of the Standard Bread Building to an added second storey of the Gladstone Station. The connection to the second storey will provide opportunities for transit users to access both the northbound and southbound platforms quickly and easily. As well, the development's internal



pedestrian street provides access through the retail and office podium from Gladstone Avenue to the north end of the site and the additional residential towers. Retail uses will animate this internal street.

The proposed development consists of three related components. The first is the Standard Bread Building and the overhead link to the station. Looking west along Gladstone the two-storey station creates a visual foreground to the heritage building and introduces the stepping or terracing that helps establish a pedestrian precinct. Along Gladstone Avenue a six-storey podium along the street contains retail and office components, anchored by the retained Standard Bread Building on the east side. The podium uses a mix of red brick and concrete together with black panels referencing the industrial character of the surrounding area. The scale of the podium provides a pedestrian scale along Gladstone Avenue, reinforced by at-grade retail units with direct access to the sidewalk. The podium wraps the corner of Gladstone and Loretta where the industrial character is less evident.

Above the retail and office podium is the tallest of the proposed residential towers at 41 storeys. The middle of the tower is set well back from the podium below on all sides and features a tower floor plate approximately 820 square metres. The top of the tower is articulated with an additional step back on all sides and the integration of the rooftop mechanical systems within architectural elements of the building.

Along Loretta Avenue, two additional residential towers focus on a central green space and drop-off court. Residential tower 2 is located centrally on the site, abutting the rail corridor and features a four-storey base articulated with punch balconies. The building has a total height of 35 storeys. Residential tower 3 with total height of 30 storeys is located at the north end of the site, and features a podium connected to tower 2.

The podiums of towers 2 and 3 are articulated using materials and through an inset fifth storey. The podiums are connected with access to a common outdoor amenity terrace along the rail corridor. A staircase at the edge of the terrace provides access to the future multi-use pathway along the west side of the O-Train corridor.

A total of 921 residential dwelling units are proposed within all three residential buildings including a range of bachelor, one, two, and three bedroom units. Along with the residential, 2,000 square metres of retail space and 13,170 square metres of office space are proposed. 548 parking spaces will serve the proposed development, including 14 surface parking spaces in the central courtyard and 534 spaces within the two-storey underground parking garage. Access to the parking garage is from two locations on Loretta Avenue with a third retail access from an existing driveway adjacent to the Standard Bread Building off Gladstone Avenue. The majority of the bike parking spaces will be provided within the underground garage, with some spaces provided at-grade for retail and office users, and for residential visitors.

Amenity space for the residential units is dispersed throughout the development, including 2,234 square metres of private balconies, and 1,943 square metres of communal amenity space within the building

and on the rooftops. The landscaped area at-grade providing amenity for the residents, has a total area of 1,616 square metres. In total, approximately 5,800 square metres of amenity area is proposed, in addition to the bicycle and walking pathway connections.

### 4.3 The Standard Bread Factory

The development proposal includes the rehabilitation of the heritage building, and the conservation of the exterior form and materials, except for the single storey wing extending to Loretta Avenue. The link between the train station to the new development will occur on the first floor level only and will require opening up portions of the east wall on the ground floor between the columns. The recently installed brick veneer at the first floor of the Gladstone elevation will be removed, and the original fenestration pattern restored with the removal of brick infill from the windows. The gabled brick and metal parapets will be re-instated to their original form and design. The intent is to repair the deteriorated exterior concrete and spalled brick as necessary and strip the white paint from the exterior revealing the original redbrick exterior. Deteriorated concrete windowsills will be replaced and new insulated single hung windows installed with a light configuration similar to the original window patterns.

The fenestration pattern on the east elevation overlooking the O-Train corridor will be altered to reflect the fenestration pattern on the south (Gladstone) elevation, with banks of paired windows. An existing basement level loading door will be repurposed to a garage access door through the basement of the building to the below grade parking structure. The paint will be stripped and spalling brick repaired on the north elevation of the building. It will be left as a blank brick clad wall with pilasters. The west façade will be incorporated into the new development.

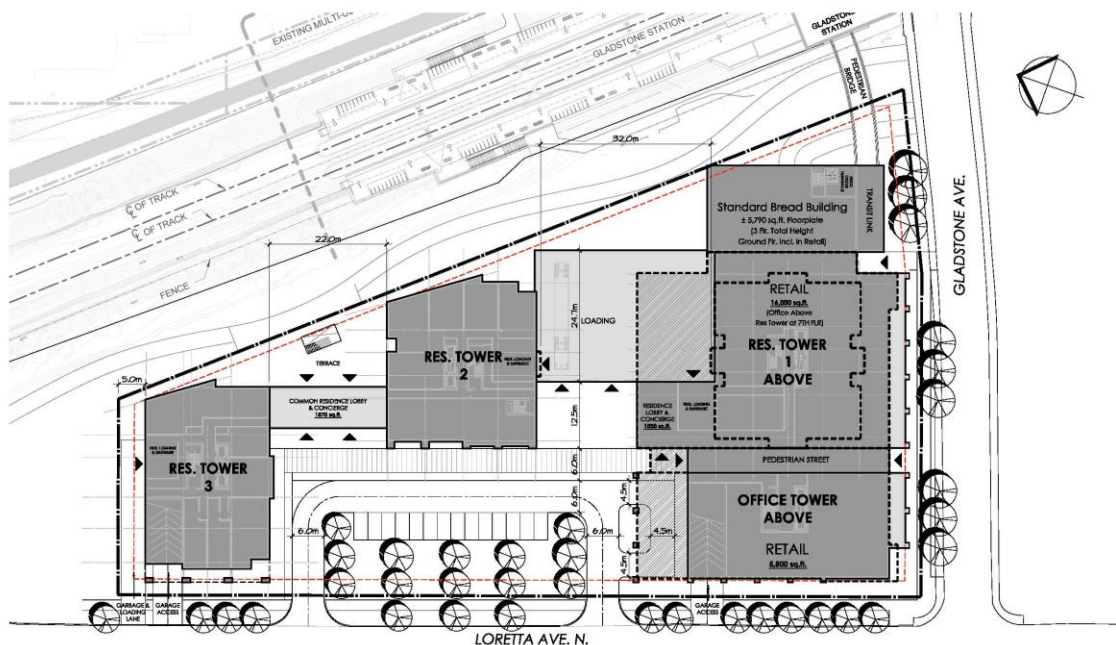


Figure 15: Site plan of the proposed development illustrating its relationship to Gladstone and Loretta Avenues. A future O-Train Station with an elevated pedestrian link to the Standard Bread building is being proposed. Source: Hobin Architecture 2018.



Figure 16: Birds eye view of the parkette on Loretta. Hobin Architecture 2018

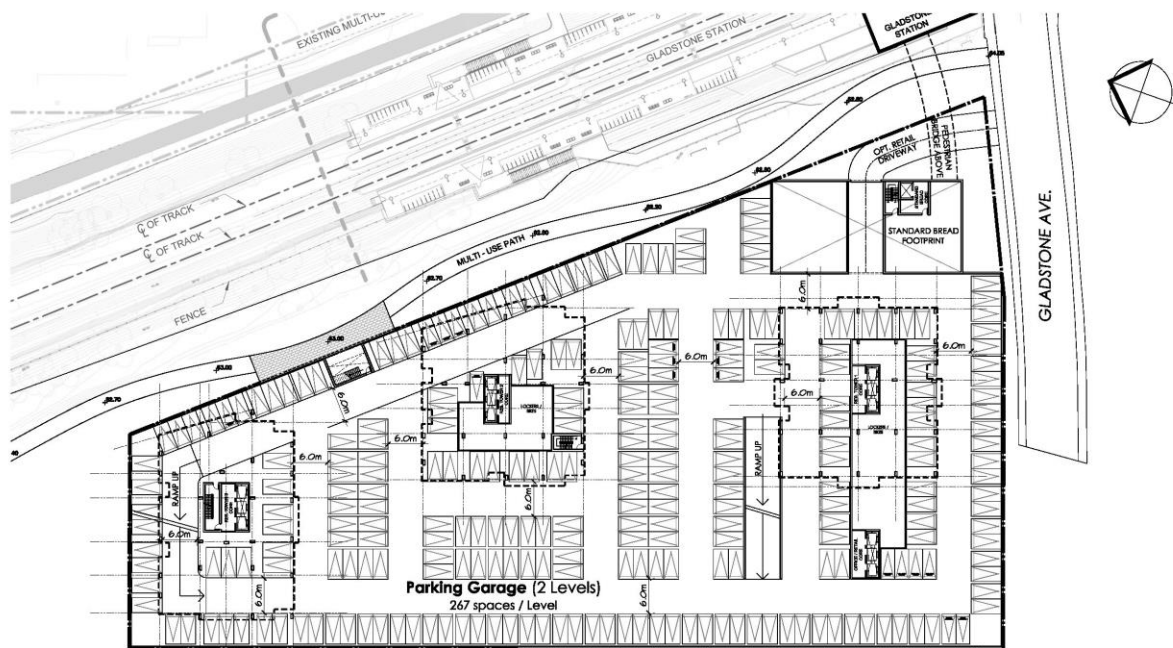


Figure 17: Garage plan of the development illustrating a future basement level access corridor through the Standard Bread building's basement level, and a future multi-use pathway adjacent to the railway corridor that will be accessible from a terrace adjacent to the common lobby of towers 2 and 3. Source: Hobin Architecture 2018.





Figure 18: Rendered perspective view of the development site with the Standard Bread Building illustrating its relationship to the proposed development, with the elevated pedestrian linkage to a future Gladstone Station along the O-Train corridor, and an access driveway from Gladstone Avenue to the below grade parking garage. Source: Hobin Architecture 2018



Figure 19: Rendered perspective view of the development site at the intersection of Gladstone and Loretta. The Standard Bread Building and the future Gladstone Station to the right. Source: Hobin Architecture 2018.



Figure 20: Rendered perspective view of the development site with the Standard Bread Building (left) and the future Gladstone Station with the three towers overlooking the rail corridor. Source: Hobin Architecture 2018.



Figure 21: Bird's eye view of the entrance to the development from the multi-use trail system. Source: Hobin Architecture 2018.



## 5.0 IMPACT OF PROPOSED DEVELOPMENT

---

### 5.1 Introduction

This section specifically addresses the impacts of the development proposal on the identified cultural heritage values of the Standard Bread Building. The heritage attributes of the building are itemized in Section 3.0.

### 5.2 Official Plan Amendment Review of Development Applications

*The assessment of cultural heritage resources is required as part of the development application in accordance with 4.6.1 (3) and (9) when a development has the potential to adversely affect any designated resource (Amendment #76 June 2009). These policies are based on the presumption in favour of the retention of heritage resources in their original location and construction.*

#### a. Section 4.6.1(3-a)

##### Positive Impacts

- The Standard Bread Building is retained in place, and the ground floor serves as a reception area internally linking the development with the train station through a new pedestrian bridge connection.
- The development proposal includes the rehabilitation of the Standard Bread Building as part of the development with a program to conserve its industrial character, integrate both the structure and its character into the planned development, and interpret its historic use.
- The architecture of the six-storey podium along Gladstone and Loretta punctuates the corner and reflects the warehouse and industrial heritage of the area.
- The character of neighbouring buildings along this section of the Gladstone Streetscape are acknowledged using building materials, the podium height and with the retained Standard Bread Building.
- The development does not immediately abut any residential properties. It is adjacent to light industrial and office uses as well as the rail corridor, thereby mitigating concerns with respect to privacy and overlook.
- The subject property is not within any protected view corridors. The proposed development will contribute to the evolving skyline of Little Italy and the Trillium Line O-Train corridor. The building design introduces architectural elements to make a positive contribution to the skyline.
- The heritage building maintains its prominence in views from the Gladstone Bridge and from the adjacent railway corridor.
- The lower podium height of towers 2 and 3 helps to carry the stepping and terracing through the site and relate to the Canada Bank Note Building across the street.

##### Adverse Impacts

- The development will result in an introduction of just under 1000 new apartment units with an increase in traffic and concentration of population.



- This development will set a precedent for other vacant or under developed sites in the area.
- The towers will dramatically alter the skyline within the neighbourhood. The podium height of six-storeys is appropriate for the character of the community and mitigates the high-rise buildings above. All three of the residential towers are set well back from the nearest residential uses.
- Because the heritage building is set back from the street line, views from the east will be partially obscured by the new build.
- The three (3) high-rise residential buildings on the subject property have large floor plates that could affect the sunlight penetration to public spaces and create wind or shadow impacts. Mitigating these impacts is the fact that *A Shadow Analysis* has been prepared by Hobin Architecture as part of the applications to assess the impact of the proposed development on adjacent properties. As shown in the study, the majority of the shadows will extend over the Canadian Bank Note Building to the west and over the rail corridor to the east. The shadows will not result in undue adverse shadow impacts on surrounding properties. Longer shadows in the shoulder seasons will extend over the Gladstone Village Ottawa Community Housing lands.
- The treatment of the podiums of the two towers on Loretta is less successful in establishing a comfortable pedestrian realm and the industrial character. Continuing the Gladstone material and colour palette would help integrate the podiums of towers 2 and 3 with the rest of the development.
- The mini park needs a stronger interface between public and private and perhaps a focal feature to provide a foreground to the towers.



#### **b) Actions Required to Prevent, Minimize or Mitigate the Adverse Impacts**

- Retain the heritage building in its original location, ensure that the adaptive reuse is compatible with and does not negatively affect the character defining attributes.
- Undertake a Phase II CHIS as part of the Application to Alter under the Ontario Heritage Act once the building is designated. This should include a detailed conservation plan addressing the brick repairs, fenestration and doors, the interface between the heritage building and the links required on both the east and west elevations. The gabled brick and metal parapets will be re-instated to their original form and design; repair the deteriorated exterior concrete and spalled brick as necessary and strip the white paint from the exterior revealing the original redbrick exterior. Deteriorated concrete windowsills will be replaced and new insulated single hung windows installed with a light configuration similar to the original window patterns.
- Undertake further analysis of the Loretta Avenue streetscape to help integrate the impact of development on the lands adjacent to the development.

**b) Review of Development Applications Section 4.6.1(9)**

*When reviewing applications for zoning amendments, site plan control approval, demolition control, minor variance, or the provision of utilities affecting lands/properties adjacent to or across the street from a designated heritage resource, adjacent to or across the street from the boundary of a heritage conservation district, or within a heritage conservation district, the City will ensure that the proposal is compatible by: Amendment 14, September 8, 2004] [Amendment #76, OMB File #PL100206, August 18, 2011]. The following comments focus on those issues, which are relevant to this proposed development.*

- a) *Respecting the massing, profile and character adjacent to or across the street from heritage buildings; [Amendment #76, August 04, 2010]*

The proposed train station on the east side of the rail corridor has been redesigned to a two-storey building in order to provide a pedestrian link and connect the station to the Standard Bread Building.

- b) *Approximating the width of nearby heritage buildings when constructing new buildings facing the street;*

To the south on Gladstone Avenue, the light industrial uses, including the grade changes along the street and the City of Ottawa's Traffic Operations Centre and storage yard offer vistas looking south. The four-storey brick and metal sided building at the corner at Loretta and Gladstone is referenced with the use of brick and in the treatment of the podium at 951 Gladstone. The setback of towers 2 and 3 is respectful of the one-storey red brick industrial building across the street.

- c) *Approximating the established setback pattern on the street;*

The heritage building is to remain in situ and will establish the line of the streetscape. There is a recess to help distinguish the original building from the new development. Its facade is broken up with an applique brick pattern and slight reveals separating two building masses along a continuous podium that wraps around the corner onto Loretta Avenue.

- d) *Being physically oriented to the street in a similar fashion to existing heritage buildings.*

The subject property is a corner site with public realm frontage on three sides and buildings set close to the street line on Gladstone. A continuous ground floor retail edge has direct access to the sidewalk and creates animation at street level. Standard Bread will be a gateway for the site's connection to transit through a bridge link to the future Gladstone O-Train Station.

- e) *Minimize shadowing on adjacent heritage properties.*

There are no adjacent heritage properties. Hobin's office has undertaken sun/shade studies, which demonstrate minimal impact to surrounding lands.

- f) *Having minimal impact on the heritage qualities of the street as a public place in heritage areas.*

This section of Gladstone is an area in transition shifting from an underutilized light industrial use to a high-density commercial residential centre focusing on the transit services. The impact to the street and surrounding area will be positive and very dramatic.

- h) Ensuring that parking facilities (surface lots, residential garages, stand-alone parking and parking components as part of larger developments) are compatibly integrated into heritage areas.*

Parking facilities include two levels of underground parking allocated between residential and commercial users. As well surface parking is provided for drop off and visitor parking. The development is approximately 200 m from a rapid transit line. The plan supports a multi-use bike trail linked to the citywide system.

### 5.3 General Standards for Preservation, Rehabilitation and Restoration

This proposal is assessed using the Standards and Guidelines for the Conservation of Historic Properties in Canada and are in *italic*.

- 1. Conserve the heritage value of a historic place. Do not remove, replace or substantially alter its intact or repairable character-defining elements.*

**Discussion:** The existing reinforced concrete frame, including columns, beams, floor and roof structures on the 3 and 4 storey portions of the building will be retained; the interior columns at ground level are one of the building's defining features as well as the exterior wall assemblies, including terra cotta back-up walls and the brick veneer.

The fenestration pattern on the east elevation overlooking the rail corridor is noted to be a character-defining feature reflecting its industrial use adjacent to a railway siding when constructed in 1924. The proposal is to alter the fenestration pattern on the east elevation to reflect the fenestration pattern on the south elevation with banked pairs of windows. The rationale for the proposed alteration of the fenestration pattern is to introduce a modern sense of aesthetics. The proposed alteration will visually alter the building reducing the industrial character. It will also have an impact on the integrity of the east wall where new openings will have to be cut into the existing infill walls that consist of terra-cotta tiles with a brick veneer.

- 5. Find a use for a historic place that requires minimal or no change to its character-defining elements.*

**Discussion:** The developer is considering a number of uses in an effort to minimize changes to the character defining elements. Standard Bread will be a gateway for the site's connection to transit through a bridge link to the future Gladstone O-Train Station. The design for the new construct incorporates a contemporary industrial flavour that acknowledges the areas past. A continuous interior ground floor walkway will link retail and residential retail activity to the entire property.

- 7. Evaluate the existing condition of character-defining elements to determine the appropriate intervention needed.*

**Discussion:** The developer retained John G. Cooke and Associates to undertake a visual condition survey of 951 Gladstone Avenue formerly the Standard Bread Building. A summary of the conditions noted in the survey is included below.

- The structural system appears to be in good condition.



- The structural system consists of reinforced concrete columns and floor slabs with a terra cotta infill between columns to which the exterior brick is secured.
- The foundations are reinforced concrete walls, of unknown depth, with parging.
- In general, the exterior brick veneer is in good condition but does have localized areas of eroded mortar joints and face spalled bricks. These conditions were typically observed below the windowsills and other areas of high exposure to water, such as on the pilasters on the west and east elevations.
- The gabled brick and metal parapets will be re-instated to their original form and design.
- At the roof level on the west elevation, in the northern four bays, bulges in the brick veneer were observed. The method of anchoring the brick has not been investigated yet.

## 5.4 Additional Standards Relating to Rehabilitation

*10. Repair rather than replace character-defining elements. Where character-defining elements are too severely deteriorated to repair, and where sufficient physical evidence exists, replace them with new elements that match the forms, materials and detailing of sound versions of the same elements. Where there is insufficient physical evidence, make the form, material and detailing of the new elements compatible with the character of the historic place.*

**Discussion:** The proposal includes the reinstatement of the decorative brick gabled parapet and metal cornice on the building. The height and form of the parapet will be guided by photographic evidence and a site inspection. The proposal also includes the installation of new windows in the form and pattern of the existing single hung units with a similar light configuration.

*11. Conserve the heritage value and character-defining elements when creating any new additions to a historic place or any related new construction. Make the new work physically and visually compatible, subordinate to, and distinguishable from the historic place.*

**Discussion:** The redbrick building maintains its prominence in views from the Gladstone Bridge and from the adjacent railway corridor. The four-storey redbrick structure is set against a modern curtain wall backdrop in the adjacent six-storey podium that sets it apart visually from the new construction. Visual compatibility is achieved with the use of brick panels on the Gladstone façade of the podium, and the facades of the new construction facing the railway corridor. The Standard Bread building will be distinguishable from the adjacent development as it is set against a modern architectural backdrop.

## 6.0 ALTERNATIVES AND MITIGATION STRATEGIES

### 6.1 Alternatives

#### Alternatives include:

The east elevation of the Standard Bread Building should read as an industrial building in character. Consider retaining the existing fenestration pattern on the east wall overlooking the railway corridor.

The design of the landscaping / streetscape in front of the Standard Bread building on Gladstone and its interface with the building needs further development. Historic photographs document the location of the main entrance to the building as a character-defining feature. If possible, the historic entrance should be interpreted from the street with stairs leading to the entrance from the public sidewalk.

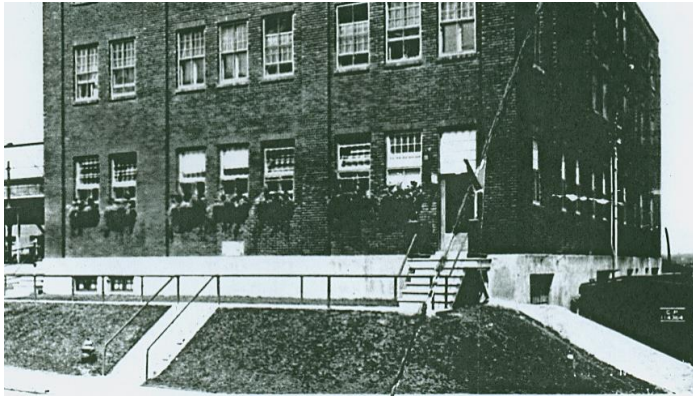


Figure 22: The 1930 view of the front entrance with a stairway leading from Gladstone and an entrance on the right-hand side of the building are defining features.

The removal of the paint finish from the exterior brick will be considered depending upon the quality and surface texture of the original brick. The brick has a surface texture that will retain residual paint in surface irregularities making it difficult to remove all of the paint from the surface. The building may have been painted to protect a relatively porous brick that was deteriorating; alternatively, it may have been for aesthetic reasons. A test of various methods of paint removal and their relative success should be undertaken to determine the appropriate conservation approach. Depending on the outcome of tests, the removal of the paint or painting the exterior brick in a tone similar to the colour of the original brickwork may have to be considered.

## 6.2 Mitigation Measures

Mitigation measures may include:

- The original layout as a bakery should be documented in plans and included as part of City's record of the building's history.
- An interpretive plaque describing the building and its significance as part of the City's industrial heritage should be installed.
- The treatment along Loretta should continue the industrial character.
- The preparation of a conservation strategy will detail the work required to rehabilitate the building, and ensure an appropriate interface between the heritage fabric and the new development. The strategy will include the condition of the exterior wall assemblies, including the exterior brickwork and mortar, the metal ties anchoring the brick to the terra cotta backup walls, and steel angles (assumed) supporting the brick veneer at the various floor levels will need to be determined.
- The visual condition assessment undertaken by John Cooke and Associates notes a number of areas on the upper floors where the exterior brickwork has bowed out between columns/pilasters due to assumed deterioration of the metal ties in the wall assemblies. Assuming the exterior walls of the building will be thermally upgraded a number of representative bricks should be tested for properties, including compressive strength, water

absorption, and freeze thaw performance. The compressive strength of the mortar should also be determined.

### **6.3 Conclusions**

Gladstone Avenue will become the primary east-west commercial corridor within the Gladstone Station District, with a built form predominantly reflective of a traditional mainstreet. A maximum of six-storeys of height is permitted. The proposed development wraps around the Gladstone Loretta intersection with a podium height of six-storeys. Its industrial character is appropriate for the character of the community and helps to ground the high-rise buildings above. Having all three of the residential towers set well back from the podium edge lessens their impact at the pedestrian level. The development will dramatically alter the Little Italy and Hintonburg skyline.

The proposed development responds to policy objectives of protecting heritage resources while implementing development reflective of a traditional mainstreet. A plaque near the entrance of the Standard Bread Building features a Latin proverb: ‘Audaces Fortuna Juvat’ meaning Fortune Favours the Bold and is an appropriate sentiment for this development. The adaptive reuse and the integration of the Standard Bread Building as a gateway are positive outcomes to the proposed development. As is the proposed train station on the east side of the line, which will be redesigned to a two storey building in order to provide a pedestrian link over the tracks and to connect the station to the Standard Bread Building. Capitalizing on the proximity to a rapid transit network and easy access to the pedestrian and cycling network will reduce traffic and encourage active transportation alternatives.

As the Trinity submission illustrates the rehabilitated Standard Bread Building will be a prominent component of the proposed redevelopment of the site. The four-storey redbrick building is set against a modern architectural backdrop maintaining its status as an early industrial building next to the O-Train corridor. The pedestrian link between the Standard Bread Building and the O-Train is an effective interpretation of the reason why historically the building was placed next to the tracks and dependent on that link for its ongoing operation. Exterior signage should clearly identify the building as the Standard Bread Building and some sort of interpretive explanation would be well received by daily commuters and residents.



## 7.0 BIBLIOGRAPHY / PEOPLE CONTACTED.

---

### Bibliography

- Part IV and V of the Ontario Heritage Act;
- Guidelines for the Preparation of Cultural Heritage Impact Statements (City of Ottawa);

### List of People Contacted

Sally Coutts Heritage Planner City of Ottawa

Leslie Collins Heritage Planner City of Ottawa

## 8.0 AUTHORS QUALIFICATIONS

---

**Commonwealth Resource Management** is an integrated consulting and management firm that offers a full range of professional services related to conservation, planning, research, design, and interpretation for historical and cultural resources. A key focus of the practice is planning and development for heritage resources. The firm was incorporated in 1984.

**John J. Stewart**, B.L.A., O.A.L.A., C.S.L.A., CAHP, a principal of Commonwealth is a specialist in the planning and design of cultural resources, building conservation, and commercial area revitalization. A graduate of the University of Guelph, he received additional training at Cornell University (USA) and Oxford University (UK) and holds a diploma in the Conservation of Monuments from Parks Canada, where he worked as Head, Restoration Services Landscape Section. Before Commonwealth's formation, Stewart served for four years as the first director of Heritage Canada's Main Street Program.

Stewart is a founding member of the Canadian Association of Heritage Professionals. He has served as the Canadian representative of the Historic Landscapes and Gardens Committee of ICOMOS and the International Federation of Landscape Architects. Stewart is a panel member with the Ottawa Urban design Review Panel and a board member of Algonquin College Heritage Carpentry Program.

Commonwealth has completed a number of Cultural Heritage Impact Statements for the private and public sectors, including the following:

185 Fifth Avenue, Mutchmor Public School Addition, Ottawa, Ontario.

2489 Bayview Avenue, CFC Canadian Film Institute, Toronto, Ontario.

1015 Bank Street, Lansdowne Park, Ottawa, Ontario.

Algoma District Wind Farm Proposal, Lake Superior Shoreline, Ontario.

1040 Somerset Street West, Ottawa, Ontario.

Laurier Friel Redevelopment Sandy Hill, Ottawa, Ontario.

Cumberland /Murray Streets, Lowertown West, Ottawa, Ontario.

1120 Mill Street, Manotick, Ottawa, Ontario.

Ontario Place, Waterfront Park and Trail Toronto, Ontario.

Fort William Historical Park, Thunder Bay, Ontario.

Allen/Capitol Theatre 223 Princess St., Kingston, Ontario.

101-109 Princess Street and 206-208 Wellington Street Kingston, Ontario.

Greystone Village, Oblate Lands Redevelopment, 175 Main Street Ottawa, Ontario.

Bradley/Craig Barn 590 Hazeldean Road, Ottawa, Ontario.

LeBreton Flats, IllumiNATION LeBreton Redevelopment, Ottawa Ontario.