

# ZONING BY-LAW AMENDMENT



## 1040 Bank Street | Planning Rationale



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File: 1711

Date: May 12, 2017



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# 1 OVERVIEW

Lloyd Phillips & Associates Ltd. have been retained by Windmill Development Group Ltd. (the 'Client') to prepare a Planning Rationale report for a Zoning By-law Amendment Application with respect to the lands known municipally as 1040 Bank Street (the 'site'). The site is located in Old Ottawa South, contains the Southminster United Church, and is a large lot bounded by Bank Street, Aylmer Avenue, Galt Street, and urban greenspace owned by the National Capital Commission (NCC).

The client's intent is to partially redevelop the site to accommodate residential uses on the site's westerly half and retain the existing church on the easterly half. The existing assembly hall, which is an addition to the principal church, is to be replaced with a mix of townhouse and mid-high rise apartment dwellings ranging from one to three bedrooms in size. The proposed design is sensitive to the existing character and historical elements of the church and the surrounding residential built-form along Aylmer Avenue and Galt Street. This design sensitivity is demonstrated through the proposed articulated stepbacks from Galt Street and Aylmer Avenue which allow for a low-rise to mid-rise transition in scale that works well and fits well within these existing residential streetscapes.

To achieve the proposed redevelopment, an application for a Zoning By-law Amendment is required. This report represents the required Planning Rationale component of the development application.

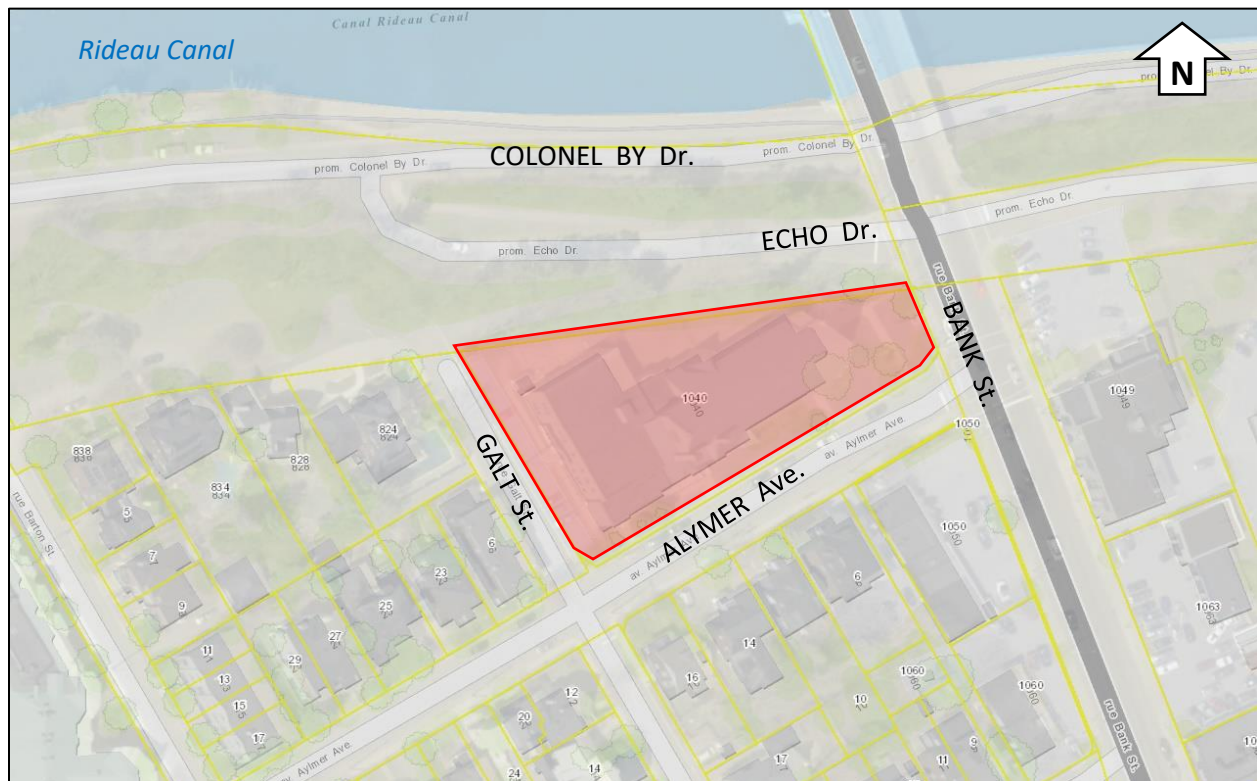


FIGURE 1. PARCEL MAP DISPLAYING THE SUBJECT SITE (1040 BANK STREET) HIGHLIGHTED IN RED



## 2 SITE AND SURROUNDING CONTEXT

### 2.1 Site

The site is located in the Old Ottawa South neighbourhood of Ottawa's urban area, and is bounded by Bank Street to the east, Aylmer Avenue to the south, Galt Street to the west, and NCC land to the north. The lot is wedge-shaped and contains frontage on three streets, with the longest along Aylmer Avenue and the shortest along Bank Street. The property sits atop a portion of escarpment that generally extends along the Rideau Canal from Seneca Street, west of the site, to Mount Pleasant Avenue, east of the site.

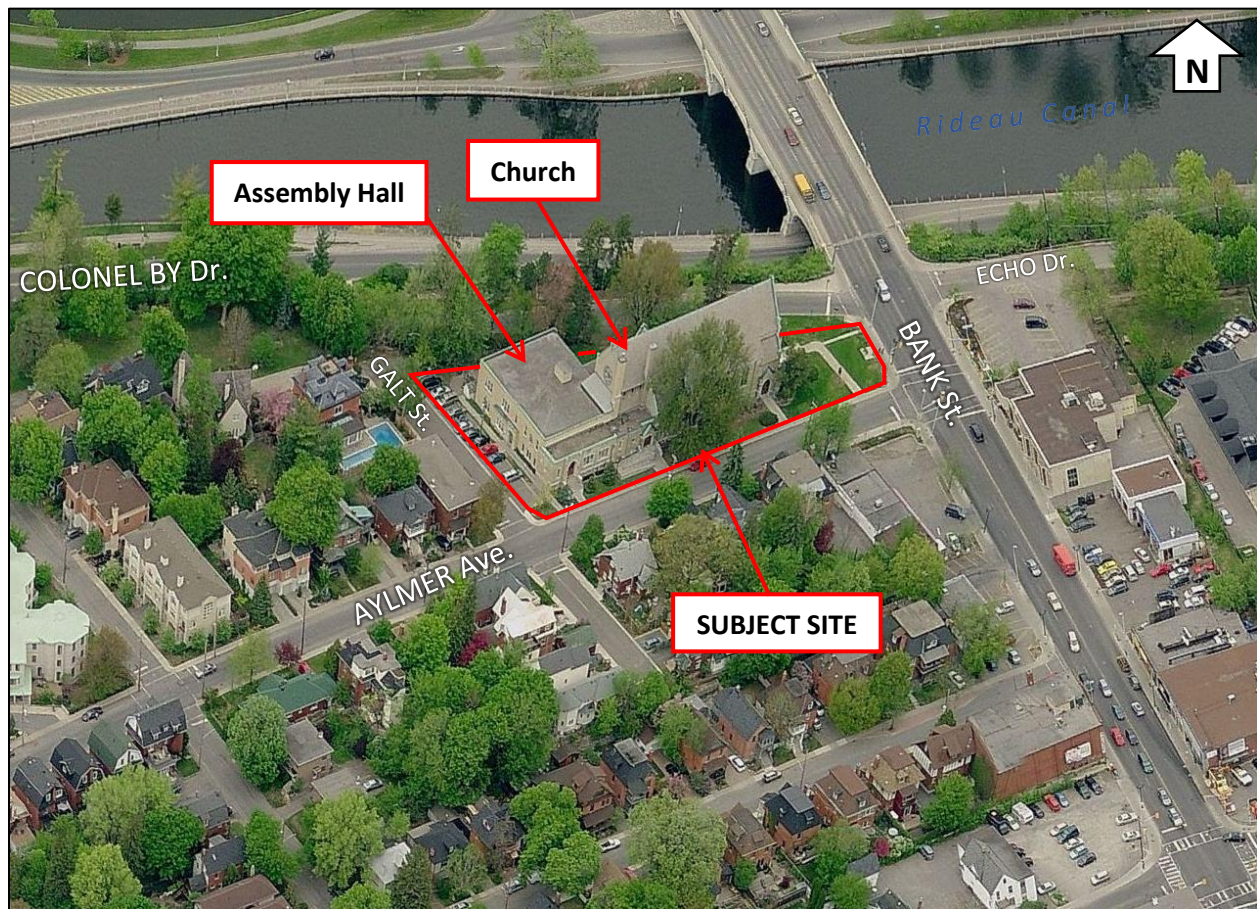


FIGURE 2. BIRD'S EYE VIEW OF THE SUBJECT SITE (FACING NORTH)

The site contains the Southminster United Church, which consists of the principal church building that was constructed in 1932, and Memorial Hall, an attached assembly hall added to the site in 1955 (see Figure 2). Since 1955 the site has undergone a few major changes which include the installation of the Memorial Centennial Carillon in 1966, and the opening of the Walter LeGrow Memorial Library in 1990.

Prior to the existing situation, the site was made up of two separate properties, with the eastern half fronting Bank Street containing the Ottawa South Methodist Church (1908-1932). These properties eventually merged and then became the home of the Southminster United Church in 1932 (see Figure 4).



Along the site's Bank Street frontage is the main access to the church which is separated from the City road allowance by a number of mature trees, shrubs, manicured grass and internal pathways. The site's Aylmer Avenue frontage contains a similar mix of soft and hard landscaping, and provides a wheelchair-accessible ramp that was added in the early 2000s and leads to one of the two Aylmer Avenue entrances into the church (one into the principal church and the other into the assembly hall). Adjacent to the property line along Aylmer Avenue is a City-owned sidewalk followed by parallel street parking.

The site's Galt Street frontage contains a mix of soft and hard landscaping which is subsidiary to the 90-degree parking which extends the majority of the site's western property line. This frontage contains a single entrance into the site's assembly hall and also provides for outdoor access to a side yard amenity area which serves as a daycare/Sunday school space for children.

The site has a total area of 3,188.47 m<sup>2</sup> (0.31 ha) and has frontage of 16.61 m (54.49 ft) on Bank Street, 88.37 m (289.92 ft) on Aylmer Avenue, and 54.58 m (179.06 ft) on Galt Street.

The lot's PIN is 04143-0321, and it is legally known as LOT 8 AND PART OF LOT 6 EAST OF GALT STREET AND PART OF LOT 3 WEST OF BANK STREET REGISTERED PLAN 36 CITY OF OTTAWA.

It is important to note that the site is not subject to any heritage designations under Part IV or Part V of the Ontario Heritage Act. However, the intent of the proposed redevelopment is to sensitively conserve the existing principal church building, while replacing the existing assembly hall with residential use buildings that are compatible and complementary to the church and surrounding context. The current building footprint of the church and adjoined assembly hall is approximately 1,374.6 m<sup>2</sup>, which results in a lot coverage of approximately 43%.



FIGURE 3. BIRD'S EYE VIEW OF THE SUBJECT SITE (FACING NORTH)



FIGURE 4. HISTORICAL AIR PHOTOS OF THE SITE (NOTE: 1928 PHOTO SHOWS THE TREE LINE DEMARCATING THE BOUNDARY OF THE FORMER METHODIST CHURCH PROPERTY AND THE WEST PROPERTY BEFORE THEY MERGED)









FIGURE 6. VIEW FACING SOUTHWEST TOWARDS THE CHURCH FROM THE BANK STREET BRIDGE



FIGURE 7. VIEW FACING SOUTHWEST FROM THE INTERSECTION OF BANK STREET AND ECHO DRIVE

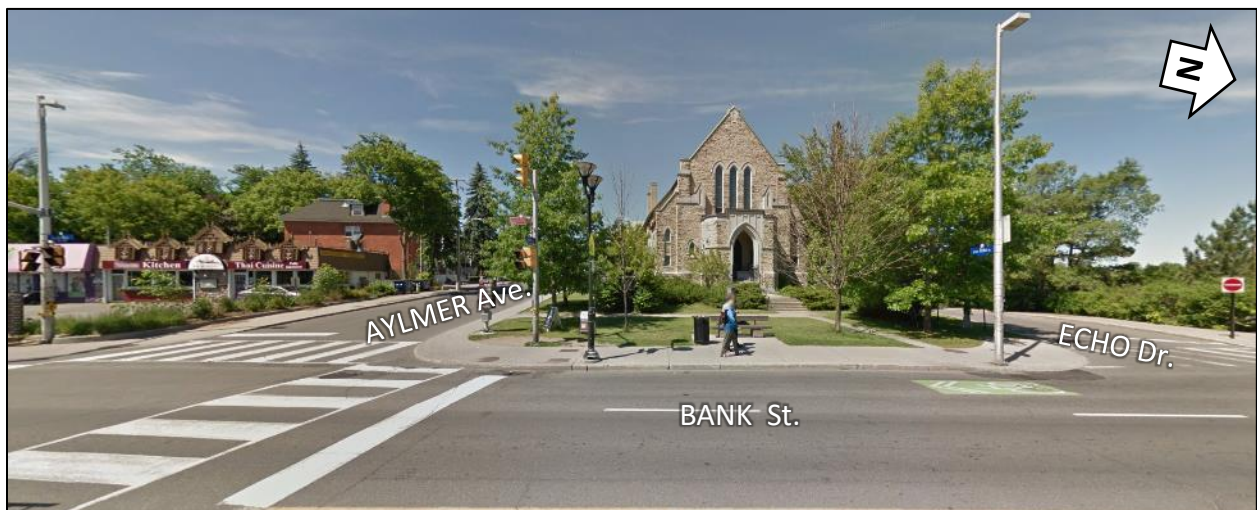


FIGURE 8. VIEW FACING WEST TOWARDS THE FRONT ENTRANCE OF THE CHURCH FROM BANK STREET





FIGURE 9. VIEW FACING NORTHWEST FROM THE INTERSECTION OF BANK STREET AND AYLMER AVENUE



FIGURE 10. VIEW FACING NORTH TOWARDS THE FRONT YARD OF THE CHURCH FROM AYLMER AVENUE



FIGURE 11. VIEW FACING NORTHWEST FROM AYLMER AVENUE





FIGURE 12. VIEW FACING WEST FROM AYLMER AVENUE



FIGURE 13. VIEW FACING NORTH TOWARDS THE CHURCH FROM AYLMER AVENUE



FIGURE 14. VIEW FACING NORTH TOWARDS THE ASSEMBLY HALL AND CHURCH FROM AYLMER AVENUE





FIGURE 15. VIEW FACING NORTHEAST FROM THE INTERSECTION OF GALT STREET AND AYLMER AVENUE



FIGURE 16. VIEW FACING NORTH FROM THE INTERSECTION OF GALT STREET AND AYLMER AVENUE



FIGURE 17. VIEW FACING NORTHWEST FROM THE INTERSECTION OF GALT STREET AND AYLMER AVENUE





FIGURE 18. VIEW FACING NORTHEAST FROM THE INTERSECTION OF GALT STREET AND AYLMER AVENUE



FIGURE 19. VIEW FACING EAST TOWARDS THE ASSEMBLY HALL ENTRANCE ALONG GALT STREET



FIGURE 20. VIEW FACING SOUTHEAST TOWARDS THE SITE FROM GALT STREET

## 2.2 Surrounding Context

The site is located in Old Ottawa South, which falls within Ward 17 – Capital. Generally, the site is situated south of Lansdowne Park and the Rideau Canal, north of the Rideau River, east of Carleton University, and immediately west of Bank Street (see Figure 21).

Old Ottawa South was developed well over 100 years ago as a residential area with Bank Street, a Traditional Mainstreet, cutting through the heart of the neighbourhood. This portion of Bank Street has historically provided commercial, retail and service type land uses along its frontage extending from the Rideau River Bridge in the south of the neighbourhood to the Rideau Canal Bridge at the north of the neighbourhood. Institutional uses in Old Ottawa South, such as churches, schools, libraries and daycares, have historically located around key intersections along Bank Street (i.e., Sunnyside Avenue and Hopewell Avenue).

Notable influences on the neighbourhood include Carleton University which – located immediately west of Bronson Avenue, Lansdowne Park, TD Place Stadium and the Rideau Canal – located immediately north of the site, and Billings Bridge Shopping Plaza and the RA Centre – located just south of the Rideau River.

Within an approximately 800 metre walking distance from the subject site, are a number of residential supportive land uses which include, but are not limited to: The Rideau Canal; Lansdowne Park; Brown's Inlet Park; Sylvia Holden Park; Percy Taverner Park; Brewer Park Complex, Arena and Sports Fields; Windsor Park; Brighton Beach Park; Linda Thom Park; Osborne Park; Ottawa South Community Centre; Ottawa Tennis and Lawn Bowling Club; Royal College of Physicians and Surgeons of Canada; Hopewell Avenue Public School; Ottawa Montessori School – The Element; Astolot Education Centre; and, Ottawa Library – Sunnyside Branch.

The land uses and key features immediately surrounding the site include:

**NORTH:** NCC green space followed by Echo Drive which connects Colonel By Drive to Bank Street.

- The Bank Street and Echo Drive intersection is unsignalized.

**EAST:** Bank Street followed by the Ottawa Public Library – Sunnyside Branch.

- This portion of Bank Street contains 4 lanes of traffic including shared lanes for cyclists, and is serviced by OC Transpo Bus No. 1 and 7.
- The Bank Street and Aylmer Avenue intersection is fully signalized.

**SOUTH:** Aylmer Avenue followed by low-rise residential building types of 2-3 storeys in height.

- Aylmer Avenue contains street parking along its north side.

**WEST:** Galt Street followed by low-rise residential building types of 2-3 storeys in height.

- Galt Street terminates along the northwest corner of the subject site and does not contain any street parking.
- The Galt Street and Aylmer Avenue intersection is unsignalized.



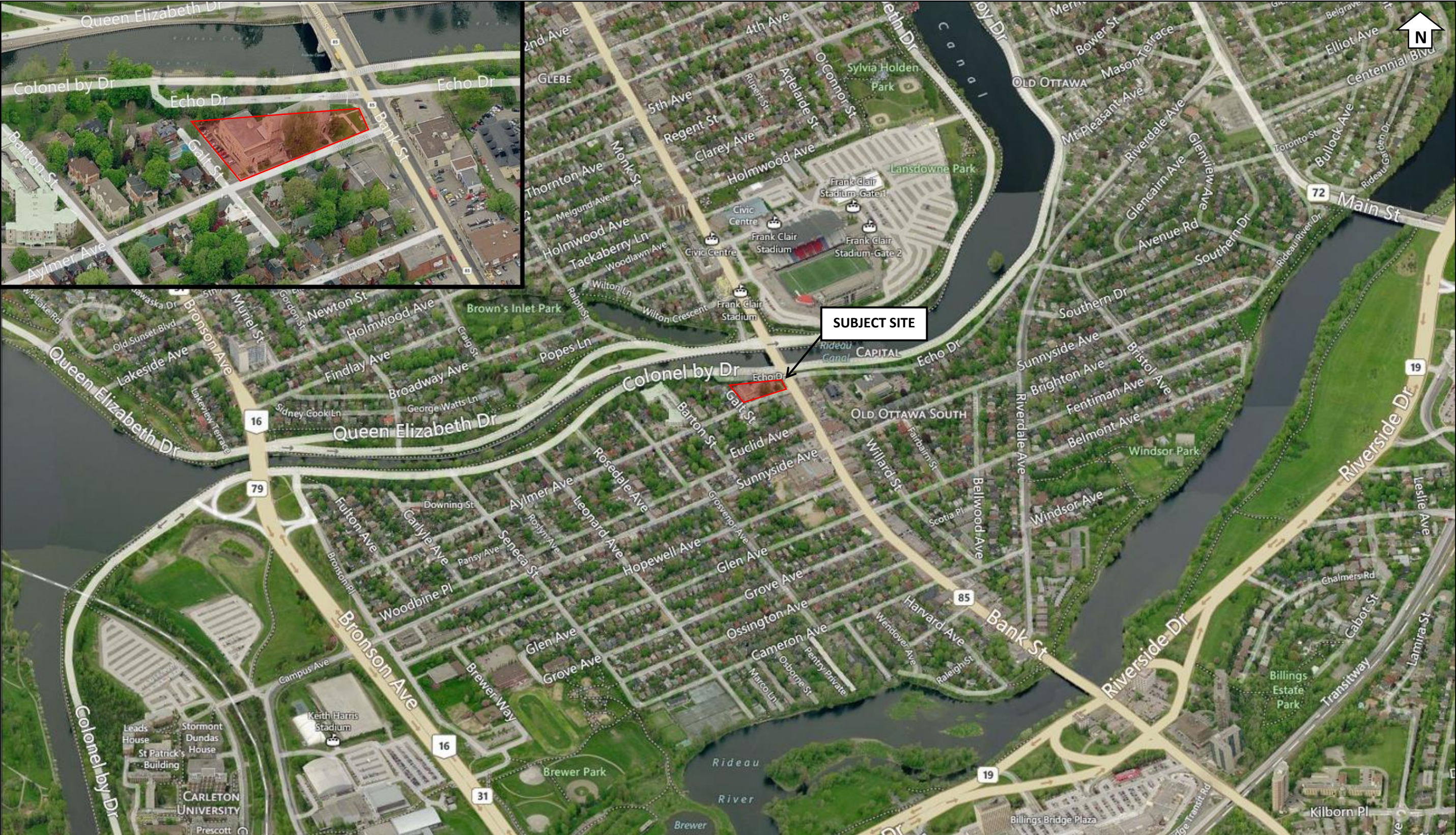


FIGURE 21. LOCATION PLAN



### 3 PROPOSED DEVELOPMENT

The proposal is to partially redevelop the site to accommodate residential uses on the site's westerly half. The redevelopment proposes retention of the existing principal church building that faces Bank Street and the replacement of the existing assembly hall with a mix of townhouse and mid-high rise apartment dwellings (mid-rise condominium) ranging from 1 to 3 bedrooms in size. Figure 22 shows the proposed conceptual site plan for the site.

The proposal will result in a mix of residential options accommodated on the site's westerly half while the principal church is preserved in both physicality and function on the easterly half. It is intended that the property be subdivided by way of consent at a later date following the rezoning and an application for site plan control. Following rezoning, site plan approval, and the eventual consent, it is intended that the future westerly property containing the proposed mix of residential uses will be subject to a plan of condominium.

The proposed mid-rise residential use building will consist of six storeys stepped back from, and accessed by, Galt Street. The building will include 14 condominium units, below-grade parking for 33 spaces, and a lane for vehicle access to be shared with the proposed adjacent townhouses. There will be three units per floor for levels 1-3, two units per floor for levels 4-5, and a single unit for level six.

The townhouse component of the proposal includes four townhouses of three storeys in height along the corner of Aylmer Avenue and Galt Street. Vehicle access to the townhouses will be provided from a proposed laneway that connects to Galt Street and separates the townhouses from the mid-rise residential use building. Each townhouse will have a driveway connecting to the shared internal laneway.

The proposed residential development to be accommodated on the western half of the existing property has been sensitively designed to provide an extension of the existing residential streetscape along Aylmer Avenue and Galt Street. This streetscape is characterized by residential buildings of typically three storeys with variation in roof types and materiality. To ensure compatibility with the similar streetscape along Galt Street, the proposed mid-rise residential building has been designed with stepbacks that provide a gradual transition in height and massing from three storeys nearest the street to six storeys where the proposed building will abut the church. To reinforce the church's historical presence on the site, the facades on both residential components have been designed to reflect the materiality of the church.

TABLE 1: PROPOSED DEVELOPMENT ATTRIBUTES

Use / Site	Total Land Area	Total GFA	Maximum Height	Total Residential Units	Total Parking
<b>Institutional</b>	1,839 m <sup>2</sup>	TBD m <sup>2</sup>	As per existing height	-	Zero / TBD
<b>Mid-high rise Apartment</b>	905 m <sup>2</sup>	2,611 m <sup>2</sup> (15% space efficiency)	6 storeys (see proposed Schedule yyy)	14	32 resident 1 visitor
<b>Townhouses</b>	445 m <sup>2</sup>	678 m <sup>2</sup>	3 storeys	4	4 resident
<b>TOTAL</b>	3,189 m <sup>2</sup>	TBD	-	18	37



### ***Vehicular Circulation and Parking***

The site is situated along the corners where Aylmer Avenue intersects with Bank Street and Galt Street. Bank Street is a four-lane, two-way existing arterial road that is dedicated as an on-road cycling route, and that contains intermittent areas of street parking. The portion of Bank Street where the site is located is subject to a Right-of-Way (ROW) Protection of 23 metres. Aylmer Avenue is a two-lane, two-way existing local road with an abundance of street parking (20 metre ROW). Galt Street is a two-lane, two-way existing local road that terminates approximately 65 metres north of its intersection with Aylmer Avenue, and that does not contain any street parking (20 metre ROW). The site's street frontage currently provides for 9 public on-street parking spaces, which will increase to a potential 16 spaces with the development.

There are no internal roadways or lanes currently existing on the site. A 90-degree parking lot exists to the west of the assembly hall along Galt Street, and currently provides the site with 17 parking spaces. The parking demand resulting from the site's existing use as a 550-person capacity church, assembly hall, and day care centre, has historically been absorbed by the street parking provided in the surrounding area and the 17 spaces provided on site. The proposal calls for the removal of the assembly hall and the 17 on-site parking spaces. It is anticipated that the available street parking within the area, including 7 additional spaces along Galt Street, will be sufficient to accommodate the church's ongoing parking demands.

Based on the existing site conditions, and to maintain the church on the eastern half of the property, the proposed location for vehicle access was designed along Galt Street approximately 30 metres from the street's intersection with Aylmer Avenue. The vehicle access would be provided by a single laneway that extends approximately 40 metres into the site from the midpoint of its Galt Street frontage and will also function as a courtyard (see Figure 26). The laneway's eastern terminus will turn 90-degrees northwards into a single level of underground parking that will include 33 spaces beneath the western half of the site.

The total amount of parking required for the proposal (including both residential type uses) is four spaces, and the total amount provided is 37 spaces. See Table 2 for the parking analysis.

**TABLE 2. PARKING ANALYSIS FOR PROPOSED LAND USES**

<b>PROPOSED USE</b>	<b>GFA / UNITS</b>	<b>RESIDENT RATE</b>	<b>VISTOR RATE</b>	<b>BICYCLE RATE</b>	<b>PARKING REQUIRED</b>	<b>NOTES</b>
Place of Worship	TBD m <sup>2</sup>	5 per 100 m <sup>2</sup> of GFA of assembly area	NA	1 per 1500 m <sup>2</sup> of GFA	NA – See notes	The church has a grandparented parking rate to permit zero spaces.
Apartment, Mid-High Rise	14	0.5 per dwelling unit	0.1 per dwelling unit	0.5 per dwelling unit	1 resident 0.2 visitor 7 bicycle	First 12 dwelling units are exempt
Townhouse	4	0.75 per dwelling unit	0.1 per dwelling unit	NA	3 resident 0.4 visitor 0 bicycle	-
<b>TOTAL REQUIRED:</b> 4 resident, 0 visitor, 7 bicycle <b>TOTAL PROVIDED:</b> 36 resident, 1 visitor, ≥ 7 bicycle <b>VEHICLE PARKING SURPLUS:</b> 33 spaces <b>BICYCLE PARKING SURPLUS:</b> TBD						

### **Pedestrian Connectivity**

Pedestrian access to and through the site is currently provided by several City-owned sidewalks and private internal walkways. A sidewalk encompasses the site along its Bank Street and Aylmer Avenue frontage, providing barrier-free access to the site's internal pathways and the crosswalks existing at the intersections that bound the site to the east and west. These crosswalks, especially along Bank Street, are well-lit, allow for safe connections across the roads that border the site, and provide access between the site and nearby transit stops and amenities.

The proposal for the westerly half of the site will contain a sidewalk parallel to Galt Street, which will provide connection to the primary ground level entrance into the mid-rise residential building (see Figure 27). This sidewalk along Galt Street will connect to the Aylmer Avenue sidewalk at its south leg and to the NCC urban greenspace lands at its north leg. The existing wheelchair ramp and adjacent staircase along Aylmer Avenue will remain in place and will represent the general eastern limit of the proposed residential portion of the site.

The proposal requires 7 bicycle parking spaces (0.5 spaces per apartment dwelling unit), and will provide for greater than, or equal to the required amount. The details regarding the amount and the location of provided bicycle parking will be provided during the site plan control stage. Although no dedicated cycling facilities currently exist along Galt Street or Aylmer Avenue, Bank Street is dedicated as an on-road cycling route in the City's Official Plan, and the site is well-connected and within proximity to the multi-use pathways along the Rideau Canal.

### **Landscaping**

The existing site contains a mix of soft and hard landscaping along its street frontages. The majority of this landscaping is provided in the yards immediately abutting the existing church (see Figure 23), which are to be retained as part of the east half of the property that is not subject to the proposed residential development.

The proposed development on the westerly half of the property will improve the greenspace along Aylmer Avenue by adding trees and landscaping along the townhouse portion of the frontage (see Figure 24). This mix of retained and improved landscaping will help to seamlessly extend the residential streetscape that exists west of Galt Street, and to frame the portion of the property boundary that will include the townhouses.

Along the northern terminus of Galt Street, the proposal will include an improved greenspace that transitions into the established formal and informal pathways extending throughout the NCC's urban greenspace. The crescent-shaped northwest corner of the proposed mid-rise residential building will allow light and air to pass around it, while also creating a functional area for at-grade greenspace.







Southminster United Church Redevelopment  
**Conceptual View | Aerial View from South**

30 March 2017



FIGURE 23. CONCEPTUAL VIEW – AERIAL VIEW FROM SOUTH. SOURCE: HOBIN ARCHITECTURE





Southminster United Church Redevelopment  
**Conceptual View | Looking Northwest along Aylmer Avenue**

30 March 2017



FIGURE 24. CONCEPTUAL VIEW – LOOKING NORTHWEST ALONG AYLMER AVENUE. SOURCE: HOBIN ARCHITECTURE





Southminster United Church Redevelopment  
**Conceptual View | Looking North along Galt Street**

30 March 2017



FIGURE 25. CONCEPTUAL VIEW – LOOKING NORTH ALONG GALT STREET. SOURCE: HOBIN ARCHITECTURE





Southminster United Church Redevelopment  
**Conceptual View | Proposed Courtyard at Galt Street**

30 March 2017



FIGURE 26. CONCEPTUAL VIEW – PROPOSED COURTYARD AND LANEWAY EXTENDING NORTHEAST FROM GALT STREET. SOURCE: HOBIN ARCHITECTURE





Southminster United Church Redevelopment  
**Conceptual View | Condo Main Entrance at Galt Street**

30 March 2017



FIGURE 27. CONCEPTUAL VIEW – LOOKING NORTHEAST FROM GALT STREET TOWARDS THE MAIN ENTRANCE OF THE MID-RISE RESIDENTIAL BUILDING. SOURCE: HOBIN ARCHITECTURE





Southminster United Church Redevelopment  
**Conceptual View | Looking South along Galt Street**

30 March 2017



FIGURE 28. LOOKING SOUTH ALONG GALT STREET. SOURCE: HOBIN ARCHITECTURE





FIGURE 29. MASSING DIAGRAM DEMONSTRATING A FORM MANIPULATION OF THE PROPOSAL. SOURCE: HOBIN ARCHITECTURE





1. **Preserve the church** and its landscaped approach as a community resource and gateway.



4. **Maintain the existing sense of scale on Galt** by establishing a masonry base of similar mass to the existing church education wing.



2. **Reintroduce the traditional Old Ottawa South streetscape** with parallel parking, street trees and landscaping and a residential architectural rhythm.



5. Use a **stepped approach** to the building mass above the building base to break up mass and preserve scale.



3. **Preserve views of the church** from all abutting streets



6. **Clad the base building with stone and precast concrete accents** to reflect the church and library gateway buildings.

## 4 PROVINCIAL POLICY STATEMENT, 2014

The Provincial Policy Statement 2014 (PPS) provides policy direction on planning matters for the Province of Ontario. Decisions affecting all planning matters shall be consistent with the Provincial Planning Policies. The proposed development at 1040 Bank Street is consistent with the policies of the PPS 2014, as demonstrated below.

### **Policies:**

**Section 1.1.1** of the PPS sets out the criteria whereby healthy, livable, and safe communities are sustained. In short, these are: to promote efficient development and land use patterns which sustain the financial well-being of the Province and municipalities over the long term; avoiding development and land use patterns which may cause environmental or public health and safety concerns; promote cost-effective development patterns; improve accessibility for persons with disabilities and older persons by identifying, preventing, and removing land use barriers which restricts their full participation in society; ensuring there is necessary infrastructure; and, promoting development and land use patterns that conserve biodiversity.

In the above list of criteria, there is a large emphasis on efficient development that is safe, respects the natural environment, uses available infrastructure, and provides for the needs of the community both in the short term and long term.

**Section 1.5.1** of the PPS states that healthy, active communities should be promoted by: planning public streets, spaces and facilities to be safe, meet the needs of pedestrians, foster social interaction and facilitate active transportation and community connectivity.

Old Ottawa South is a safe and walkable neighbourhood, consisting of well-lit intersections, wide sidewalks, ample street parking, a grid pattern street network with clear sight lines, and an abundance of mature street trees and greenspace. The proposal will contribute to and complement these neighbourhood qualities through the following:

- carefully incorporating key aspects of the site's existing conditions into its design by retaining the site's existing church and its yards at the Bank Street and Aylmer Avenue intersection, that together, act as a community resource and gateway;
- preserving and enhancing portions of green space, including the retaining of existing site trees, the planting of new trees, and the provision of rooftop terraces;
- maintaining an existing sense of scale along Galt Street and Aylmer Avenue by providing similar mass and materiality to the assembly hall addition which currently exists there;
- maintaining the existing pedestrian access points into the church from Bank Street and Aylmer Avenue (proximity to nearby public transit along Aylmer Avenue and Bank Street will not be affected);
- encouraging active transportation by providing bicycle parking in well-lit and visible areas on site;
- providing an enhanced pedestrian experience along Galt Street by removing the existing 90-degree parking and providing a connection to the NCC urban greenspace lands to the north from a sidewalk animated with a mix of hard and soft landscaping;
- providing an improved view of the rear gable of the existing church which will align with the laneway that extends into the site from Galt Street;



- promoting community interaction and safety by providing a multi-use space on the site which will function as both a laneway and courtyard. The effect of ample glazing and balconies from both residential type buildings will provide 'eyes on the street' that will encourage community interaction and safety for users of the space. The 90-degree turns into the site from Galt Street and from the underground parking garage, will provide added levels of traffic calming for the multi-purpose laneway.

***Section 1.4.1 of the PPS states that planning authorities shall provide for an appropriate range and mix of housing types and densities required to meet projected requirements of current and future residents of the regional market area.***

***Section 1.6.6 of the PPS states that planning for sewage and water services shall direct and accommodate expected growth or development in a manner that promotes the efficient use and optimization of existing: municipal sewage services and municipal water services.***

***Section 1.7.1 of the PPS states that long-term economic prosperity should be supported by: promoting opportunities for economic development and community investment-readiness; and, maintaining and, where possible, enhancing the vitality and viability of downtowns and mainstreets.***

The proposed mix of residential type uses and unit sizes on a large site within an established residential neighbourhood in proximity to a wide range of complementary uses, supports the PPS policies relating to intensification, density, and long-term housing provisions.

The site represents a large piece of land adjacent to Bank Street, which is a Traditional Mainstreet with established street parking, public transit, on-road cycling routes, and a mix of uses complementary to the residential type uses found throughout the neighbourhood. The site is also adjacent to the NCC urban greenspace lands which contain a mix of formal and informal pathways adjacent the Rideau Canal. Within walking distance to the site are a number of amenities and services, including schools, churches, entertainment venues, food stores, etc. Within this context, the residential proposal fits well and works well, providing an efficient use of land that is compatible with its immediate and surrounding context.

***Section 2.6.3 of the PPS state that planning authorities shall not permit development and site alteration on adjacent lands to protected heritage property except where the proposed development and site alteration has been evaluated and it has been demonstrated that the heritage attributes of the protected heritage property will be conserved.***

The church at 1040 Bank Street is not designated under Part IV or Part V of the Ontario Heritage Act. Irrespective, the intent of the proposal is to enhance views of the existing church and to respect its massing by providing design features and materiality that will complement its built form.

The proposal is consistent with the applicable policies of the PPS 2014.



## 5 CITY OF OTTAWA OFFICIAL PLAN

Schedule B of the City of Ottawa Official Plan designates the site at 1040 Bank Street as Traditional Mainstreet, as shown in Figure 31. The general character of this designation is described in Section 3.6.3 of the Official Plan, and summarized as such:

Traditional Mainstreets are planned as compact, mixed-use, pedestrian-oriented streets that provide for access by foot, cycle, transit and automobile. Mainstreet's offer some of the most significant opportunities in the City for intensification through more compact forms of development, a lively mix of uses and a pedestrian-friendly environment.

Mainstreets generally developed prior to 1945, designated as Traditional Mainstreets, are typically set within a tightly knit urban fabric, with buildings that are often small-scale, with narrow frontages and set close to and addressing the street, resulting in a more pedestrian-oriented and transit friendly environment.

The proposal does not require an amendment to the City's Official Plan.

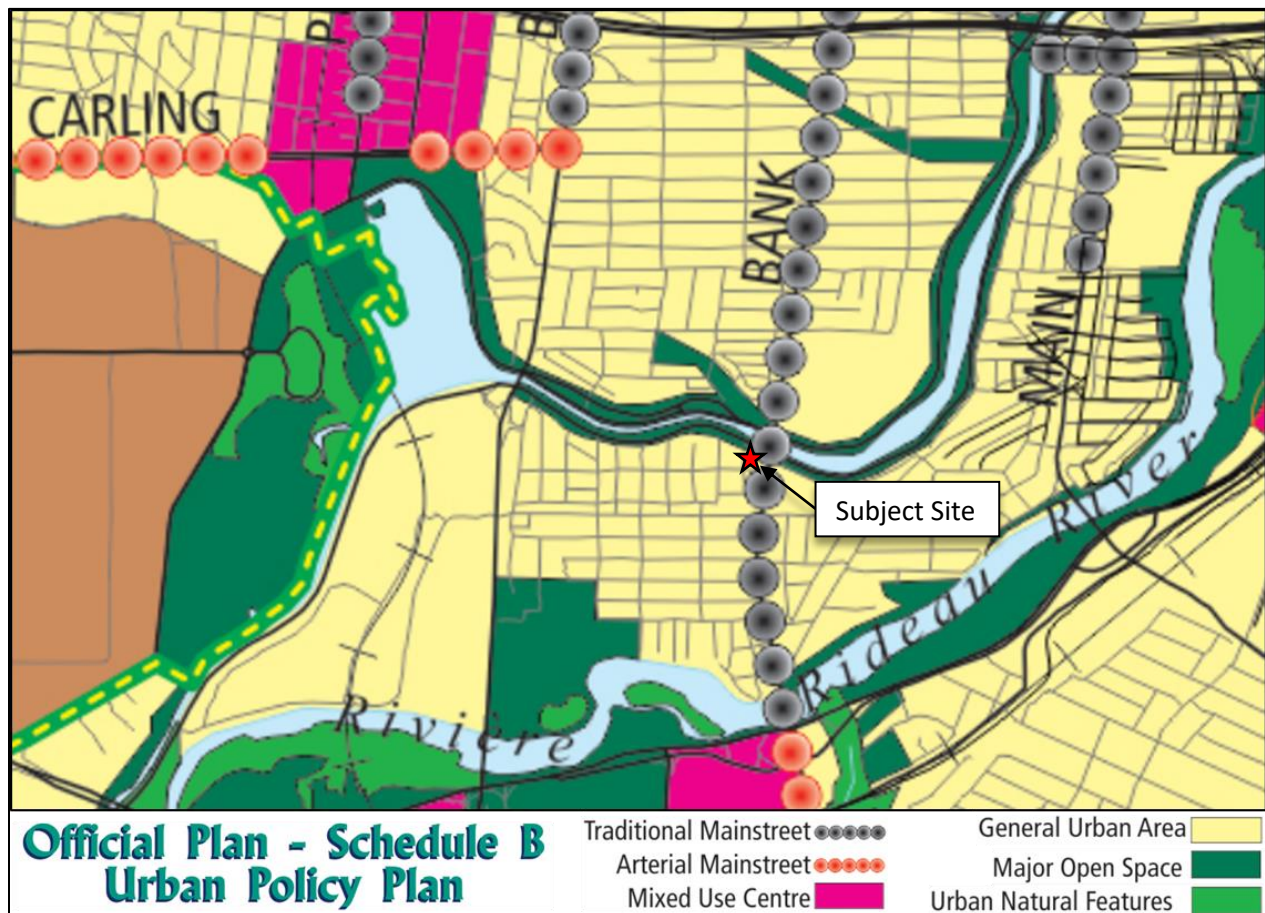


FIGURE 31. OFFICIAL PLAN – SCHEDULE B URBAN POLICY PLAN (TRADITIONAL MAINSTREET)



## 5.1 Section 3.6.3 – Mainstreets

### **Policies:**

1. *Traditional Mainstreets [...] are planned as compact, mixed-use, pedestrian-oriented streets that provide for access by foot, cycle, transit and automobile. [...] Traditional and Arterial Mainstreets will fulfill and take advantage of their multi-modal transportation corridor function.*

The Traditional Mainstreet designation permits the proposed residential uses for the westerly portion of 1040 Bank Street, which will result in a more efficient use of the land by adding residential density to a large property uniquely situated within an established residential neighbourhood and adjacent to Bank Street, which is a multi-modal, mixed-use corridor.

2. *The boundary of the Traditional and Arterial Mainstreet designation is flexible depending on site circumstances and lot configuration [...]. On lots where development has the potential to develop both adjacent to the street and to the rear of the property, the Mainstreet designation will apply to the entire lot [...]. These sites should be planned in a coordinated fashion that will facilitate:*
  - *multi-modal (pedestrian, cycling, transit and vehicular) access between the site and the public street(s),*
  - *attractive, safe and usable pedestrian and cycle connections between the site and adjacent communities,*
  - *an enhanced interconnected pedestrian environment that links individual uses on the site, transit stops and continuous public sidewalks on the adjoining streets, and which is generally distinct from internal vehicle routes,*
  - *measures of sufficient size and quality to relieve the visual impact of surface parking areas,*
  - *the provision of adequate landscaped areas, particularly trees, along the perimeter of the site and street frontages,*
  - *the provision of coordinated signage, and*
  - *over time, a development that is oriented to the Mainstreet.*

The proposed residential area of the site is bounded by street frontage on two sides (Galt Street and Aylmer Avenue), and NCC urban greenspace lands and the abutting church on the remaining sides. A mix of hard and soft landscaping along sidewalks that connect to nearby multi-modal pathways and public transit services will help to animate the street frontage along the site, and will provide for an enhanced pedestrian experience.

The proposal will not include any surface parking, but will allow for seven new parallel spaces along Galt Street. The site's private parking, which is in excess of the minimum requirement, will be provided on a single, below-grade level for the mid-rise residential building and in individual single-vehicle garages for each of the four townhomes.

8. *Redevelopment and infill are encouraged on Traditional and Arterial Mainstreets in order to optimize the use of land through intensification, in a building format that encloses and defines the street edge*



*and provides direct pedestrian access to the sidewalk. Any proposal for infill or redevelopment will be evaluated in light of the objectives of this Plan. This Plan supports building heights up to six storeys on Traditional Mainstreets [...].*

- 9. On Traditional Mainstreets, the minimum building height will be the equivalent of a two-storey building [...].*
- 12. In order to demonstrate its commitment to development on Mainstreets, the City will consider them to be priority locations for considering [...] the use of techniques such as increased height and density provisions [...].*

The residential proposal for the westerly portion of the site includes building heights of three storeys along the site's Aylmer Avenue and Galt Street frontages (the third storey is stepped back on the townhomes). The townhouse component of the proposal will have a maximum height of three storeys, whereas the mid-rise residential building will contain a number of terraced stepbacks from Galt Street, creating a transition in height from three to six storeys. Although these building heights are permitted by the Official Plan's Traditional Mainstreet designation of the site, the site's existing zoning permits a maximum height of 11 metres, and therefore requires rezoning to permit the proposal.

The proposal conforms with the Policies of Section 3.6.3 of the Official Plan.

## 5.2 Section 2.5.1 – Urban Design and Compatibility

Section 2.5.1 of the Official Plan regarding urban design and compatibility generally deals with patterns and locations of land use, relative densities, street networks, and the allocation of community services and facilities. Urban design is more concerned with the details relating to how buildings, landscapes and adjacent public spaces look and function together. Section 2.5.1 also deals with matters of compatibility regarding new development, and how new development can be made to 'fit well' and 'work well' with the surrounding context.

The following are Design Objectives that are qualitative statements of how the City wants to influence the built environment as the City matures and evolves. These Design Objectives contain Design Principles that have been satisfied throughout the design process that has culminated in the proposed conceptual site plan prepared by Hobin Architecture (Figure 22 displays this plan).

### **Design Objectives**

- 1. To enhance the sense of community by creating and maintaining places with their own distinct identity, design should:*
  - Recognize and reflect on the history of the City or community.
  - Create distinctive places and appreciate local identity in patterns of development, landscape and culture.
  - Reflect a thorough and sensitive understanding of place, context and setting.



The proposal accounts for the existing built form and local identity of Old Ottawa South by providing a design concept focused on complementing the existing church which is to be retained on the easterly portion of the site. The church acts as a gateway and community symbol of Old Ottawa South, and will continue to remain as such.

The new residential buildings (four townhomes and a mid-rise building) have been designed to reflect the existing streetscape along Aylmer Avenue and Galt Street, with a materiality that echoes the historical design of the church, and a scale that is sensitive to the general building heights in the surrounding area. The mid-rise building will contain a number of stepbacks and terraces that will increase the greenspace on site and will provide a transition in height from Galt Street to where the westerly portion of the proposed residential site abuts the church.

The proposed design provides an opening along Galt Street which acts as a laneway into the site, but that also provides for a keyhole view of the church's rear gable and large circular window, which currently are almost entirely blocked by the presence of the assembly hall addition. The proposed main entrance to the mid-rise building along Galt Street will contain large church-like doors, providing an extended essence of the church's presence from the front of the site along Bank Street to the rear of the site along Galt Street.

The design of the proposed residential development reflects a thorough and sensitive understanding of place, context, and setting, that will fit well and work well within this area of the community.

**2. *To define quality public and private spaces through development, design should:***

- Clearly define and connect public and private spaces by:
- Defining and enclosing spaces using buildings, structures and landscaping.
- Encourage a continuity of street frontages. Where continuous building facades are not a dominant feature of the streetscape, the gradual infilling of empty spaces between buildings and between the building and the street edge is promoted to occur over time. Depending on the stage of evolution of the street, it may be appropriate to achieve this principle in a number of ways e.g., building form, landscape treatment, architectural ornamentation.
- Address the relationship between buildings and between buildings and the street.
- Meet the needs of pedestrians as a priority.
- Contribute to attractive public spaces and important vistas.

The proposal clearly defines and connects public and private spaces by enclosing spaces with landscaping and buildings along its street frontages and interior property lines. The proposal addresses the relationship between buildings and between buildings and the street by providing a materiality that demonstrates continuity between the site's existing and proposed buildings, and by providing landscaping and building heights along the street frontage that mirror the church's existing height and character of the adjacent streetscape.



The proposal has been designed to visually reflect a pedestrian dominated residential enclave that is free of surface parking, and contains a single internal laneway to dually function as a courtyard. The proposal enhances the pedestrian experience along Aylmer Avenue and Galt Street by removing the existing 90-degree parking and replacing it with a sidewalk and landscaping that provides a continuous connection along the site between Bank Street and the NCC urban greenspace lands at the northern leg of Galt Street.

The proposal will improve vistas of the site by replacing the existing assembly hall with buildings and landscaping oriented towards the street, and by providing a laneway that will re-open a former view to the rear gable and large circular window of the existing church.

**3. *To create places that are safe, accessible and are easy to get to, and move through, design should:***

- Connect buildings and spaces through a network of roads, sidewalks, and pathways in ways that are understandable. These connections should be accessible to all users and incorporate the principles of universal access and where connections lead into a building, the building and its facilities should be designed so that it can be approached, entered, and used by persons with physical or sensory disabilities.
- Integrate public transit with existing and new development, where feasible.
- Create places and spaces that are visible and safe and can be confidently used at all hours of the day and at night where it is appropriate to do so.

The proposal contains a single vehicle access point that enters the site's internal laneway from Galt Street, which then leads to a single level of underground parking. Sidewalks will surround the site along its Aylmer Avenue and Galt Street frontages, and will provide connection to nearby public transit services (Bus No. 1 & 7 along Bank Street and Bus No. 7 along Sunnyside Avenue), on-road bicycle routes, and a number of informal and formal pathways throughout the NCC urban greenspace lands.

By creating active street frontages along Aylmer Avenue and Galt Street, and by having ample glazing and balcony space within the courtyard, the design and orientation of the buildings has ensured a high-level of safety for pedestrians on the site.

**4. *To ensure that new development respects the character of existing areas, design should:***

- Integrate new development to complement and enliven the surroundings.
- Complement the massing patterns, rhythm, character, and context.

The proposed development will complement and enliven the surroundings by providing a sensitively designed mix of residential uses that will fit well within the immediate streetscape, and that will work well with the surrounding uses and planned function of Bank Street as a Traditional Mainstreet.

The form of the new buildings provides background to the iconic view of the church from various locations along Bank Street, Aylmer Avenue, and from the Rideau Canal. The laneway that enters the site from Galt Street allows for a new view of the church's rear gable with its large, circular glass window. The proposal



will replace the existing 90-degree parking and assembly hall addition along Galt Street with active frontages and building facades consisting of a similar material and design treatment as the existing church.

Through form manipulation exercises, the proposed residential components have been designed to complement the massing patterns within the surrounding context. These patterns include building stepbacks, materiality, and height, from nearby examples that include the Royal College of Physicians and Surgeons of Canada, the Ottawa Library – Sunnyside Branch, and the existing church on the site.

5. *To consider adaptability and diversity by creating places that can adapt and evolve easily over time and that are characterized by variety and choice, design should [OMB decision #2649, September 21, 2006]:*

- Achieve a more compact urban form over time.
- Provide flexibility for buildings and spaces to adapt to a variety of possible uses in response to changing social, economic and technological conditions.
- Accommodate the needs of a range of people of different incomes and lifestyles at various stages in the life cycle.

The proposal includes a new mid-rise residential building and four townhouses on the westerly portion of the site. These compact residential forms will replace the existing assembly hall addition to the church. Despite the replacement of the assembly hall, 80% of the church's community services and functions will continue following the proposed redevelopment. The result of the proposal is the filling in of a uniquely large site intended for mixed uses, as per the site's Official Plan designation as Traditional Mainstreet. The proposal will assist in achieving the planned function of Traditional Mainstreets, as well as the Province and City's desire for more compact and dense development within existing urban neighbourhoods.

The proposal consists of varied residential type uses (mid-rise residential and townhouse) and unit sizes, which will help to accommodate the needs of a range of people of different incomes and lifestyles. The design of the mid-rise residential building also provides flexibility for social and demographic changes within the Old Ottawa South neighbourhood, as well as the City of Ottawa as a whole. By designing the building so that each unit is on a single floor accessed from the ground by an elevator, the building provides an accessible environment for local residents who wish to age in place, and be within walking distance from the large range of community amenities along and adjacent to Bank Street.

6. *To understand and respect natural processes and features in development design, design should:*

- Protect, integrate and enhance the urban forest, vegetative cover, green spaces and corridors, environmental features and landscapes, and existing topography, where possible and appropriate.

The proposal will have no impact on the existing vegetation of the easterly half of the site which will continue to contain the existing church. Existing trees on the westerly portion of the site are limited in number, and located primarily along Aylmer Avenue (three trees) and adjacent to the NCC lands to the



north. The proposal will introduce 12 new trees to be located along Aylmer Avenue, Galt Street, and the internal laneway of the westerly portion of the site. A number of shrubs and other vegetation will frame the new buildings, their accesses, and the street frontages, which will greatly improve the existing pedestrian environment and provide a continuous green canopy from the NCC lands at the north to the intersection of Galt Street and Aylmer Avenue at the south.

**7. *To maximize energy-efficiency and promote sustainable design to reduce the resource consumption, energy use, and carbon footprint of the built environment, design should:***

- Orient development to maximize opportunities for passive solar gain, natural ventilation, and use energy efficient development forms and building measures.
- Maximize opportunities for sustainable transportation modes (walking, cycling, transit facilities and connections).
- Reduce hard surfaces and maximize landscaping and site permeability on site.
- Consider use of innovative green spaces such as green roofs, and measures that will reduce the urban heat island effect.
- Utilize green building technologies and rating systems such as LEED.

The proposed development would provide a site that is well-accessed by both pedestrians and vehicles, that will provide the visual appearance of a vehicle-free environment, and that is in proximity to public transit and pedestrian pathways.

Pedestrian access points to the site as well as the sidewalks along Aylmer Avenue and Galt Street will be framed with improved landscaping that will enhance the pedestrian experience. The orientation of the buildings and the provision of an internal courtyard, ample glazing and balconies, will all provide opportunity for passive solar gain, natural ventilation, and ‘eyes on the street.’ Additionally, green terraces have been included in the design of the mid-rise residential building to provide additional green coverage. Specific green building technologies to be used in the proposed design of the buildings will be determined during the site plan control process.

The proposal is compatible with and enhances the established community, and can co-exist with existing adjacent development without causing undue adverse impact. The proposal achieves the intent and purpose of the Design Policies, Objectives, and Principles of Section 2.5.1 of the Official Plan.

### 5.3 Section 4.11 – Urban Design and Compatibility

Section 4.11 of the Official Plan regarding urban design and compatibility generally deals with the criteria of height, massing, scale relationship, and building/lot relationships within the context of built form and operational characteristics, such as traffic, access, and parking.

The following are a number of policies regarding urban design and compatibility that have been satisfied throughout the design process that culminated in the proposed Concept Plan prepared by Hobin Architecture (Figure 22 displays this Plan).

The City will evaluate the compatibility of development applications on the basis of the following compatibility criteria.

**a. Traffic:** Roads should adequately serve the development, with sufficient capacity to accommodate the anticipated traffic generated. Generally, development that has the potential to generate significant amounts of vehicular traffic should be located on arterial or major collector roadways so as to minimize the potential for traffic infiltration on minor collector roadways and local streets.

The roads servicing the development are Galt Street, Aylmer Avenue, and Bank Street (arterial). Vehicle access to the proposed below-grade parking garage will be provided from a laneway that connects to Galt Street. It is anticipated that the majority of traffic drawn to the site will result from the future residents of the site and their visitors, who will be accommodated by the 33 spaces provided in the below-grade parking garage. Traffic resulting from ongoing church services will continue to rely primarily on street parking along Aylmer Avenue, with the potential to also park along Galt Street where space for seven new parallel stalls will be made available by the development.

Given the site's proximity to an abundance of amenities and multiple transit modes (city sidewalks along the site's frontage, on-road cycling routes on Bank Street, formal and informal pathways along the Rideau Canal, public transit along Bank Street) it is expected that active and public transit will substantially offset site-generated vehicle traffic.

**b. Vehicular Access:** The location and orientation of vehicle access and egress should address matters such as the impact of noise, headlight glare and loss of privacy on development adjacent or immediately opposite. Vehicular access and egress for development that has the potential to generate a significant amount of vehicular traffic should be oriented on streets other than local streets, wherever the opportunity exists, considering traffic safety and other transportation objectives of this Plan [Amendment #76, OMB File #PL100206, August 18, 2011].

The garage bays leading to the below grade parking face south and are oriented away from Galt Street, as are the north-south driveways of the townhouses. The building immediately west of Galt Street and across from the laneway into the site is a two-storey apartment building. Currently, the east side of Galt Street adjacent to the site is occupied by 17 east-west parking spaces. The proposal will narrow and limit light glare and vehicle noise to the width of the internal laneway (4 metres) as opposed to the existing 90-degree parking situation which spans approximately 50 metres. The laneway will also be framed by the landscaping and building walls of both the mid-rise residential building and the townhouses, further decreasing noise and light impacts.

Given the existing and proposed site design, the surrounding context, and the proposed residential uses, it is expected that the proposed development will have minimal impact on surrounding development, and that it will improve the existing situation with regards to vehicle noise and light resulting from the location of the site's vehicle access point.



**c. Parking Requirements:** The development should have adequate on-site parking to minimize the potential for spillover parking on adjacent areas. A range of parking forms, including surface, decked, and underground, should be considered taking in account the area context and character. Opportunities to reduce parking requirements and promote increased usage of walking, cycling and transit will be pursued, where appropriate, particularly in the vicinity of transit stations or major transit stops in accordance with the provisions of Section 4.3 [Amendment #76, OMB File #PL100206, August 18, 2011].

The proposed development provides 32 resident spaces and one visitor space for the mid-rise apartment dwelling, whereas each of the four townhouses have a single parking space within an enclosed garage. Parking access and locations for the site have been carefully designed to eliminate the visual presence of vehicles. The building treatments along the heavily landscaped frontages will provide variation in depth and materiality, which in addition to the minimized visual presence of vehicles, will help to encourage active modes of transportation.

**d. Outdoor Amenity Areas:** The development should respect the privacy of outdoor amenity areas of adjacent residential units and minimize any undesirable impacts through the siting and design of the buildings and the use of screening, lighting, landscaping or other mitigative design measures.

The proposal consists of two residential building types; a row of four, three-storey townhouses along Aylmer Avenue containing a stepback and pitched roof design, and a 6-storey residential building along Galt Street that is setback from the street and contains stepbacks on the fourth, fifth, and sixth storey. These design measures have been accounted for in the proposed zoning for the site which will contain both a land use schedule and a heights and setbacks schedule. These zoning schedules will ensure the residential type uses and their building designs are confined to the proposed maximum heights and minimum setbacks of the proposed conceptual site plan.

**e. Loading Areas, Service Areas, and Outdoor Storage:** The operational characteristics and visual appearance of loading facilities, service areas (including garbage), parking, and areas for the outdoor storage of goods or materials should be mitigated using a variety of methods (e.g., location, containment, screening, berms, and/or landscaping). These uses and activities should be located away from residences where possible.

The proposed development consists of residential type uses which will not require the provision of any loading spaces. Garbage storage for the mid-rise building will be located in the below grade parking garage within a large space adjacent to the elevator and vehicle access ramp.

**f. Lighting:** The potential for light spill over or glare from any lighting source onto adjacent light-sensitive areas should be avoided or mitigated.

The townhouse component of the proposal on the north side of Aylmer Avenue, from front building wall to front building wall, will be located approximately 23 metres from the two-storey detached dwellings

on the south side of Aylmer Avenue. The townhouses will be 3-storeys in height with top floor stepbacks and pitched roofs, and will also be screened from the adjacent houses by three front yard trees.

The mid-rise residential component of the proposal on the east side of Galt Street, from front building wall to front building wall, will be located approximately 20 metres from the 2.5-storey detached dwelling on the west side of Galt Street. The mid-rise building will have stepbacks at the fourth, fifth, and sixth storey that will further increase the distance and the angle of sightlines between the two residential buildings. The proposal also includes a number of trees along the Galt Street frontage as well as green terraces at each stepback.

The residential type uses and the building designs of the proposal are not expected to generate any adverse impacts regarding light spill over and glare onto adjacent light-sensitive areas.

**g. Noise and Air Quality:** The development should be located and designed to minimize the potential for significant adverse effects on adjacent sensitive uses related to noise, odours, and other emissions.

The proposed building designs, locations, landscaping, and separation distances will minimize the potential for adverse effects on adjacent sensitive uses related to noise, odours, and other emissions.

**h. Sunlight:** The development should minimize shadowing on adjacent properties, to the extent practicable, particularly on outdoor amenity areas, through the siting of buildings or other design measures.

The proposed building designs, locations, and landscaping will minimize shadowing on adjacent properties which will also be separated from the site by the Galt Street and Aylmer Avenue road allowances.

**i. Microclimate:** The development should be designed to minimize adverse effects related to wind, snow drifting, and temperature on adjacent properties.

The proposed building designs, locations, landscaping, and separation distances will minimize the potential for adverse effects on adjacent properties related to microclimate.

**j. Supporting Neighbourhood Services:** The development should contribute to or be adequately served by existing or proposed services and amenities such as health facilities, schools, parks and leisure areas. Where the proposed development itself is to contribute such services and amenities, they should be of a scale appropriate to the needs and character of the area. [Amendment #28, July 13, 2005] [OMB decision #2649, September 21, 2006]

The proposed development will be adequately served by existing and future services and amenities within the neighbourhood of Old Ottawa South, and particularly, those that are located along and adjacent to Bank Street. The site is complemented by the presence of nearby green space (Rideau Canal, NCC greenspace, parks, etc.), community facilities (schools, places of worship, a library, a community centre,



etc.), and a variety of service, retail and entertainment type uses. All of these complementary uses are within walking distance of the site. It is also important to note that the future residents of the site will have optimal access to the many services and programs hosted at Southminster United Church due to the adjacency of the sites.

The proposal achieves the intent and purpose of the Policies of Section 4.11 of the Official Plan.

## 5.4 Official Plan Amendment No. 150

On December 11, 2013, City Council adopted OPA 150 to implement the completion of the five-year review of the Official Plan. While not technically in full force and effect pending Ontario Municipal Board proceedings, the implications of OPA 150 remain relevant to the proposal.

### 2.1 – Patterns of Growth

#### Managing Growth

- Growth in the urban area will be directed to areas where it can be accommodated in compact and mixed use development, and served with quality transit, walking and cycling facilities.
- The Central Area, designated Mainstreets, Mixed Use Centres and Town Centres will be compact liveable, and pedestrian-oriented with a vibrant mix of residential uses, and social, cultural and economic activity.

### 2.2 – Managing Growth

Growth will be distributed through the urban area to strengthen the City's liveable communities through:

- Intensification and infill
- Building new communities on vacant land

The Central Area, Mixed-Use Centres, Town Centres and Mainstreets are all identified in this Plan as design priority areas, where both the public and private sectors will be required to achieve higher standards of design.

#### 2.2.1 – Urban Area and Village Boundaries

##### *Policies*

##### Urban Boundaries

1. The City will accommodate approximately 90 per cent of its growth in urban areas shown on Schedule B, where urban services exist or can be efficiently provided or upgraded.
2. Sufficient land will be provided in the urban area to meet the City's projected requirement for housing [...].

#### 2.2.2 – Managing Intensification Within the Urban Area

This Plan supports intensification throughout the urban area where there are opportunities to accommodate more jobs and housing and increase transit use. Intensification is directed to Target Areas

[i.e., Traditional Mainstreets] for Intensification which have the potential to develop at moderate to high densities in a compact form.

### ***Policies***

#### **Definition of Intensification**

1. Residential intensification means the development of a property, building or area that results in a net increase in residential units or accommodation and includes:
  - a. Redevelopment (the creation of new units, uses or lots on previously developed land in existing communities), including the redevelopment of brownfield sites;
  - b. The development of vacant or underutilized lots within previously developed areas, being defined as adjacent areas that were developed four or more years prior to new intensification.

#### **Target Areas for Intensification**

4. Target areas for intensification are the Central Area, Mixed Use Centres, Mainstreets, and Town Centres defined on Schedule B, and the Community Core in Riverside South.

#### **Intensification and Building Height**

10. Intensification may occur in a variety of built forms from low-rise to high-rise provided urban design and compatibility objectives are met. Taller buildings should be located in areas that su

OPA 150 includes supportive policies and language regarding increased residential density in the urban area, especially where it is in proximity to mixed use development, public transit, and pedestrian and cycle facilities. These policies are central to the proposal, as the development will provide for a respectful residential density within buildings compatible to the surrounding context, and located on a large underutilized site that benefits from multi-modal connectivity and optimal access to existing amenities and services. As noted above in the previous section, the proposal also meets the urban design and compatibility objectives of Section 4.11 and 2.5.1. of the Official Plan.

The proposed development conforms to the policies of the Official Plan 2013 and OPA 150.

## **6 URBAN DESIGN GUIDELINES: TRADITIONAL MAINSTREETS**

The purpose of the Urban Design Guidelines for development along Traditional Mainstreets (2006) is to provide urban design guidance at the planning application stage in order to assess, promote and achieve appropriate development along Traditional Mainstreets.

Mainstreets are defined in the Official Plan as “streets that offer some of the most significant opportunities in the city for intensification through more compact forms of development, a lively mixed of uses and a pedestrian-friendly environment.” Although the proposed development will not immediately abut Bank Street, its visual presence and land use will complement the planned function of Bank Street as a Traditional Mainstreet, while maintaining compatibility with its surrounding context, which includes low-rise residential uses and non-residential uses provided by the site’s existing church.



## Objectives

*To promote development that will enhance and reinforce the recognized or planned scale and character of the streets.*

- The proposed development maintains the church's community and cultural presence at the corner of Bank Street and Aylmer Avenue. Through stepbacks and materiality, the design of the proposed buildings complements the scale and character of the church, and provides a mirror-like transition in massing to the existing development east of Bank Street where the Royal College of Physicians and Surgeons of Canada is located.

*To promote development that is compatible with, and complements its surroundings.*

- The proposal provides a mix in residential density and building types within an established residential area that is complemented by access to a wide range of amenities, public transit, and pedestrian and cycling facilities.

*To achieve a high-quality built form and strengthen building continuity along Traditional Mainstreets.*

- The proposed development is designed to complement the materiality and built form of nearby existing landmarks within Old Ottawa South (Southminster United Church, Ottawa Library – Sunnyside Branch, and the Royal College of Physicians and Surgeons of Canada). The presence of the townhouses along Aylmer Avenue and the three-storey front wall of the mid-rise building along Galt Street help to frame the street frontage and provide continuity along the streetscape.

*To foster compact, pedestrian-oriented development linked to street level amenities.*

- The proposal breaks up the westerly portion of the site by proposing two different residential building types to be separated by a multi-purpose laneway. The site maintains a pedestrian dominated presence by eliminating any view of parking from the street, and by providing a well-landscaped internal laneway to function as both a courtyard and an access into the below-grade parking garage.
- This compact yet open built form includes ample glazing, balconies and green terraces that allow for a more interactive relationship with the street.

*To accommodate a broad range of uses including retail services, commercial uses, offices, residential and institutional uses where one can live, shop and access amenities.*

- The proposal introduces a respectful residential density on the periphery of a Traditional Mainstreet where residents can enjoy optimal access to nearby amenities and multi-modal connections.

The proposed development meets the Objectives of the Design Guidelines for Traditional Mainstreets by incorporating the applicable design guidelines in its design, and by creating a development that will function safely and efficiently, and will be compatible with and complementary to its surrounding context.

## 7 ZONING BY-LAW

In the City of Ottawa Zoning By-law 2008-250 the subject site is zoned I1A – Minor Institutional, Subzone A. This zoning reflects the site's existing land use as a place of worship for the Southminster United Church. The I1A zone does not permit the residential uses proposed for the westerly portion of the site. The intent of the I1A zone is to permit a range of community and institutional uses, as well as complementary ancillary uses.



FIGURE 32. DEMONSTRATION OF THE SITE'S EXISTING ZONING (SITE OUTLINED IN WHITE)



## 7.1 Proposed Zoning Amendment

The proposal intends to retain the principal church building on the easterly portion of the property and to maintain the site as one lot for zoning purposes, regardless of any potential future severance. As such, two separate approaches were explored for rezoning the site:

1. Create two zones, one for the residential component on the westerly portion of the site, and one for the ongoing institutional use of the easterly portion of the site.
2. Create one new zone which is designed to accommodate mixed use development.

Through consultation with City of Ottawa staff it was determined that the most appropriate approach for rezoning the site would be to propose a single zone for the site that is designed to accommodate mixed use development.

To ensure that the portions of the site intended for specific land uses and performance standards are regulated as such, the proposed rezoning includes the following two zoning schedules:

1. Schedule yyy – Maximum Building Heights and Minimum Setbacks
2. Schedule zzz – Permitted Land Uses

The proposal is to rezone the site from I1A to a form of TM – Traditional Mainstreet that will account for the proposed mix of uses and will align the site's zoning with its existing designation in the Official Plan as a Traditional Mainstreet. The proposed TM zone will contain a subzone, a site-specific exception, a schedule relating to permitted land uses, and a schedule relating to building heights and setbacks.

Table 4 provides a zoning matrix comparing the existing and proposed zoning for the site. Figures 33 and 34 display the proposed zoning schedules to accompany the site's proposed TM zoning.

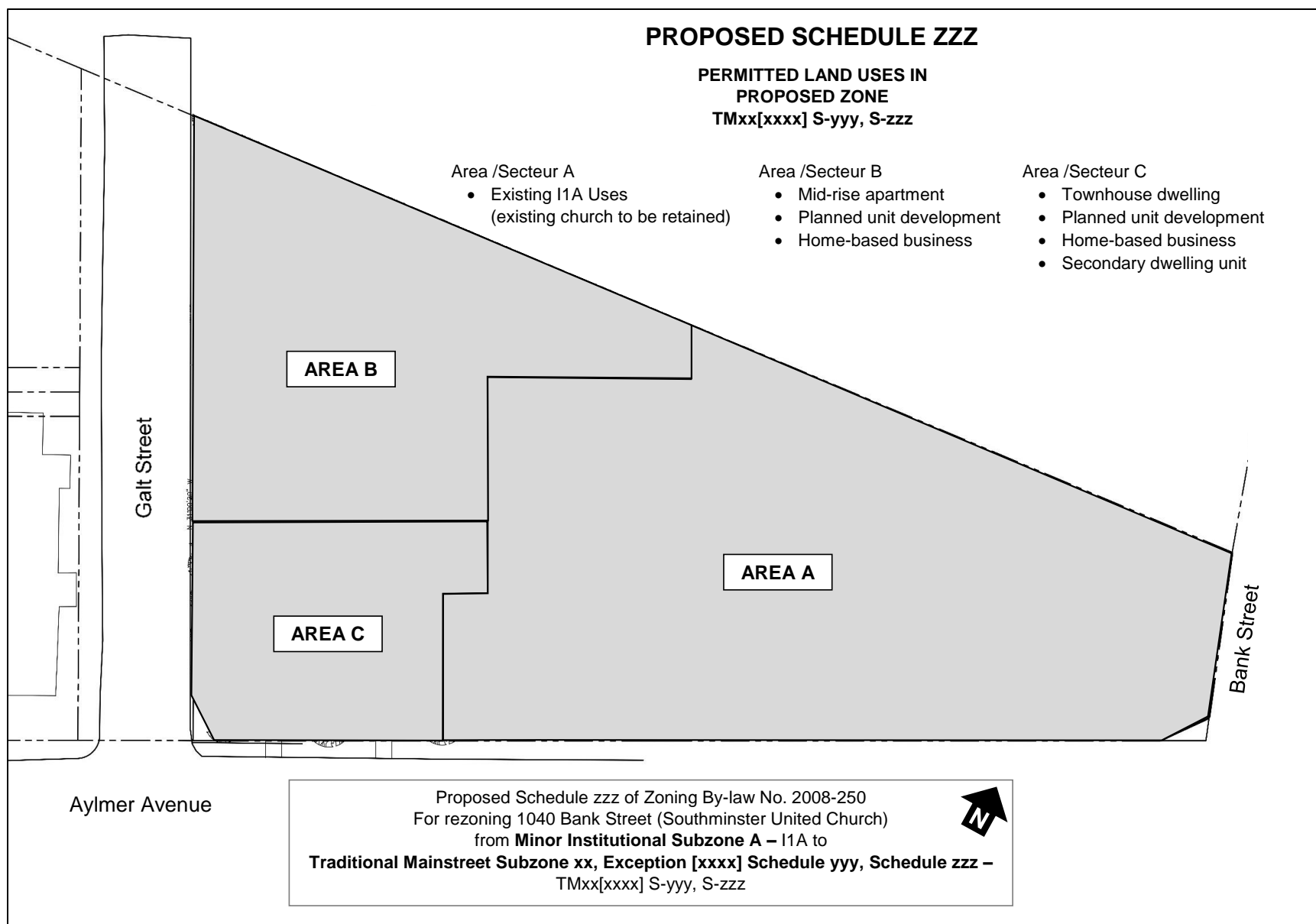
It is of our opinion that the proposed zoning format for the site is an appropriate approach for accommodating the residential development while maintaining the intent and purpose of the Zoning By-law and retaining the church and its ongoing functions.

Through this approach, the proposed scale, massing, density, and use of land will be detailed and regulated to prevent the rezoning from creating a situation that would permit over-development that is not in line with the general intent and purpose of the Zoning By-law.

TABLE 3. ZONING MATRIX

EXISTING			PROPOSED				
Permitted Uses			Permitted Uses				
<ul style="list-style-type: none"><li>• Community centre</li><li>• Community garden</li><li>• Day care</li><li>• Emergency service</li><li>• Group home</li><li>• Library</li><li>• Museum</li><li>• Municipal service centre</li><li>• One dwelling unit ancillary to a permitted use</li><li>• Park</li><li>• Place of assembly</li><li>• Place of worship</li></ul>		<ul style="list-style-type: none"><li>• Recreational and athletic facility</li><li>• Residential care facility</li><li>• Retail food store, limited to a farmer’s market</li><li>• Retirement home</li><li>• Retirement home, converted</li><li>• Rooming house</li><li>• Rooming house, converted</li><li>• School</li><li>• Shelter</li><li>• Sports arena</li><li>• Training centre</li></ul>	AREA A	AREA B	AREA C		
			<ul style="list-style-type: none"><li>• Same uses as currently permitted under the site’s existing I1A Zone.</li></ul>	<ul style="list-style-type: none"><li>• Mid-rise apartment</li><li>• Planned unit development</li><li>• Home-based business</li></ul>	<ul style="list-style-type: none"><li>• Townhouses dwelling</li><li>• Planned unit development</li><li>• Home-based business</li><li>• Secondary dwelling unit</li></ul>		
Performance Standards			Performance Standards				
Maximum height		91.02 metres above sea level (m.A.S.L.)		Maximum height		92.1 metres above sea level (m.A.S.L.)	
Minimum front yard setback		22.01 metres		Minimum front yard setback		22.01 metres	
Minimum Rear yard setback		6.48 metres		Minimum rear yard setback		1.65 metres	
Minimum corner side yard setback		6.02 metres		Minimum corner side yard setback		3.12 metres	
Minimum interior side yard setback		1.48 metres		Minimum interior side yard setback		1.3 metres	
Parking			Parking				
Uses & Units	Required	Provided	Uses & Units	Required	Provided		
All site uses	0 spaces	17 grandparented spaces	Mid-rise Apartment (14)	1 resident 0 visitor 7 bicycle	32 resident 1 visitor ≥ 7 bicycle		
			Townhouse (4)	3 resident 0 visitor 0 bicycle	4 resident 0 visitor 0 bicycle		





**FIGURE 33. PROPOSED ZONING SCHEDULE ZZZ**

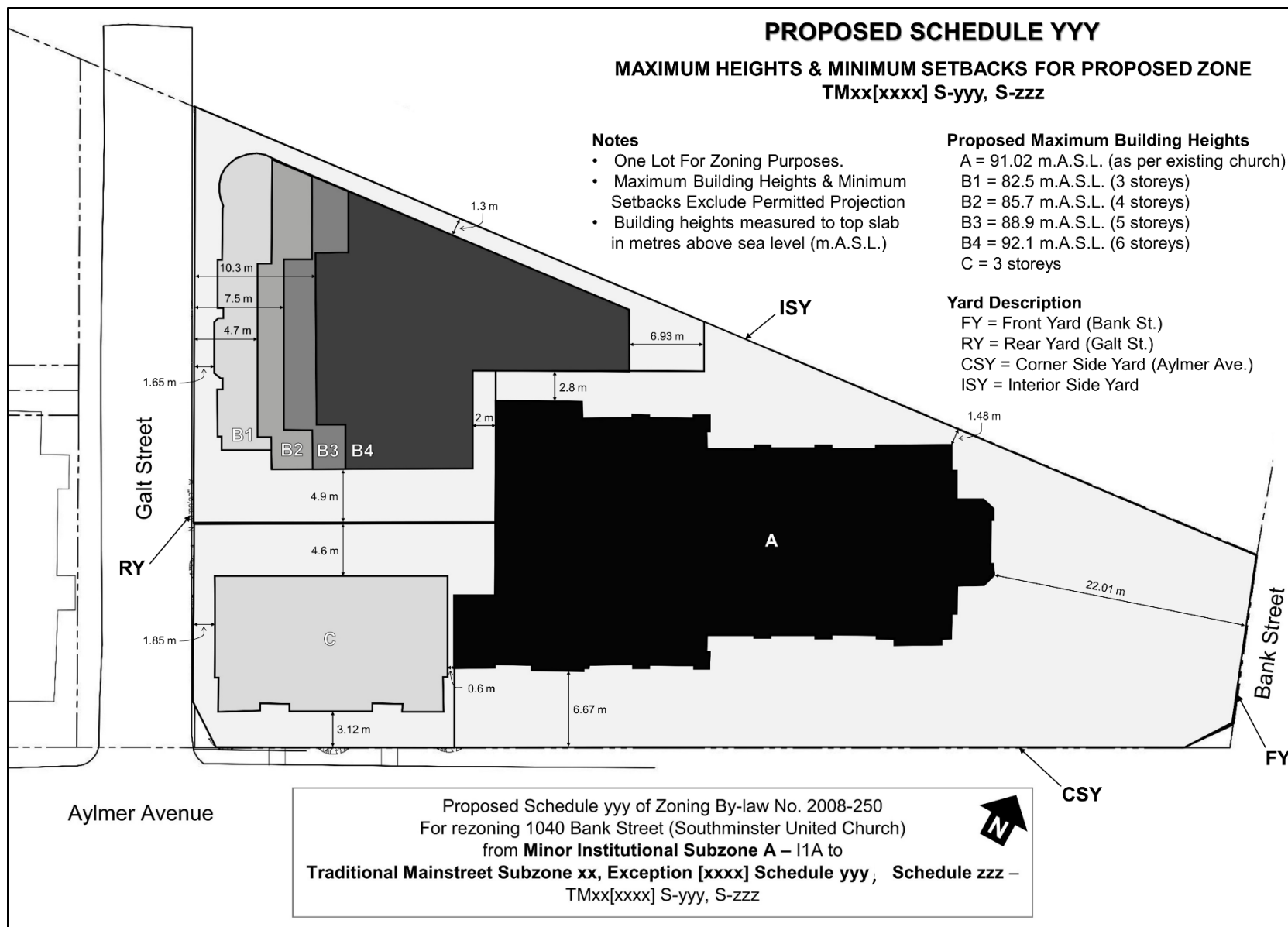


FIGURE 34. PROPOSED ZONING SCHEDULE YYY



## 8 SUPPORTING STUDIES & MATERIAL

### 8.1 Planning Considerations

The proposed development has been carefully and sensitively designed to respect the existing site conditions and the prevailing context and character of the area. The design is inspired by a number of principles focused around developing a residential enclave that will fit well and work well within the context of the neighbourhood of Old Ottawa South. The proposed development and Zoning By-law Amendment is consistent with the Provincial Policy Statement, conforms to the Official Plan, and complies with the intent and purpose of the Zoning By-law. The proposal is supported from a land use perspective.

### 8.2 Assessment of Adequacy of Public Services & Storm Water Management Brief

An Assessment of Adequacy of Public Services report was prepared by David Schaeffer Engineering Ltd. (DSEL) in May of 2017. The report found that the contemplated water supply design conforms to all relevant City Guidelines and Policies, that the municipal system is capable of delivering water within the Water Supply Guidelines pressure range, and that based on the FUS calculation, the site has adequate fire protection.

Regarding wastewater, the report states that the site is tributary to the Rideau River Interceptor, and that based on the existing sanitary analysis, the local sanitary sewers have the capacity to convey the additional flow anticipated from the proposed development. The proposed wastewater design conforms to all relevant City Standards.

Regarding stormwater, the report found that the post-development allowable release rate was calculated as 19.5 L/s, and that an estimated 33.3 m<sup>3</sup> will be required to meet this release rate. The proposed stormwater design conforms to all relevant City Standards and Policies for approval, and it is anticipated that quality controls are not required as there is no contemplated surface parking.

Please see the report for further details.

### 8.3 Geotechnical Study / Slope Stability Study

A Geotechnical Investigation report was prepared by WSP Canada Inc., in March of 2017. The report included a review of the geotechnical conditions and contained recommendations relating to the proposal.

The report provided a number of design and construction works recommendations relating to the following:

- Frost protection
- Seismic considerations
- Site grading
- Slabs on grade

- Lateral earth pressures
- Basement wall backfill and drainage
- Backfilling and compaction
- Excavations and groundwater control
- Site services
- Pavement design
- Corrosion and cement type
- Slope stability

Please see the report for further details.

## 8.4 Roadway Traffic Noise Feasibility Assessment

A Roadway Traffic Noise Feasibility Assessment was prepared by Gradient Wind Engineering Inc. dated May 8, 2017. The assessment found that the major source of transportation noise is Bank Street, Colonel By Drive and Queen Elizabeth Drive. The noise levels predicted due to roadway traffic exceed the criteria listed in Section 4.2.1 for building components on the 6-storey residential building.

As per City of Ottawa requirements, the completion of detailed STC calculations will be required prior to a building permit application for each unit type.

Please see the report for further details.

## 8.5 Phase I/II ESA

A Phase One Environmental Site Assessment (ESA) was prepared by WSP Canada Inc. in February of 2017. The purpose of the ESA was to research the past and current use of the subject site and adjacent properties and identify any environmental concerns with the potential to have impacted the subject site.

The Phase One ESA recommended that a Phase Two ESA be completed for the site, to investigate soil and groundwater quality in the vicinity of the identified areas of potential environmental concern (APEC) and assist in the preparation of a remedial or risk management strategy for the development of the site, if required.

A Phase Two ESA was completed by WSP Canada Inc. in February of 2017. The following conclusions are based on the results of the Phase Two ESA:

- No contaminants were identified to be present in groundwater samples.
- It is interpreted that elevated SAR in surficial soil samples collected on site is related to road salting activities during winter months. According to Section 43(3) of O. Reg. 153/04, this is not considered to be contamination.
- No further contaminant investigations are recommended to be completed to support the [proposal].

Please see the report for further details.



## 8.6 Environmental Impact Statement

An Environmental Impact Statement (EIS) was prepared by Kilgour & Associates Ltd. dated April 26, 2017. The report stated that no species-at-risk or their habitat or natural heritage features are likely to be impacted by the proposed development, nor would it impact adjacent wildlife habitat. Three ornamental trees on site (along Aylmer Avenue) will need to be removed during project development and mitigation measures will be implemented to protect retained trees on site and wildlife species that may use them.

Please see the report for further details.

## 9 CONCLUSION

Located in the Old Ottawa South neighbourhood, the site at 1040 Bank Street contains frontage along Bank Street, Aylmer Avenue, and Galt Street. The existing church on the site acts as a visual and cultural pillar of the community, and is to be complemented by the proposed development of a sensitively designed residential enclave on the westerly half of the site. The site is within walking distance to a number of community facilities, schools, parks, transit stops, amenities, and pedestrian pathways and linkages.

The proposed development consisting of a mid-rise residential building, four townhouses, and an internal laneway dually serving as a courtyard, will retain the presence of the Southminster United Church and enhance the surrounding area with a compatible and efficient use of the land. The residential building types, configurations, and locations have been tailored specifically to the characteristics of the site and its immediate surroundings to ensure the proposal respectfully fits well and works within this area of the neighbourhood.

The proposal requires a Zoning By-law Amendment to rezone the site from I1A – Minor Institutional, Subzone A, to TMxx[xxxx] S-yyy, S-zzz – Traditional Mainstreet, Subzone xx, Exception xxxx, Schedule yyy, Schedule zzz. The rezoning is required to permit the proposed land uses and performance standards for the residential development on the westerly half of the site. As demonstrated within this report, and supported by the required studies and plans for the rezoning, the proposal is consistent with the Provincial Policy Statement, conforms to the Official Plan and Official Plan 150, conforms to the Urban Design Guidelines for Traditional Mainstreets, and complies with the intent and purpose of the Zoning By-law.

The proposal is compatible and desirable within the context of the proposed land uses, the proposed building and landscape design, and the immediate and surrounding area. The proposed development for the site is supported from a land use perspective.

*Prepared by:*



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Barrett L. Wagar, Planner, M.Pl

*Reviewed by:*



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Lloyd Phillips, Principal, MCIP RPP