



PLANNING RATIONALE REPORT

SUBDIVISION AND ZONING BY-LAW AMENDMENT

January 2019

CAIVAN BRAZEAU LANDS

3809 Borrisokane Road

Con 3, Lot 8 -Geographic Township of Nepean

Caivan Brazeau Development Corporation



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1.0 Introduction

1.1 Background

J.F. Sabourin and Associates has been retained by Caivan Brazeau Development Corporation to prepare this Planning Rational in support of a subdivision application for the Caivan Brazeau Lands located at 3809 Borrisokane Road. This report provides an analysis of relevant policies and summary of supporting technical studies which support the proposed development.

The draft plan for the Caivan Brazeau Lands will establish approximately 500 residential units on full services, one park block, one stormwater management block, 1 collector and 11 local streets and a remaining aggregate pit block.

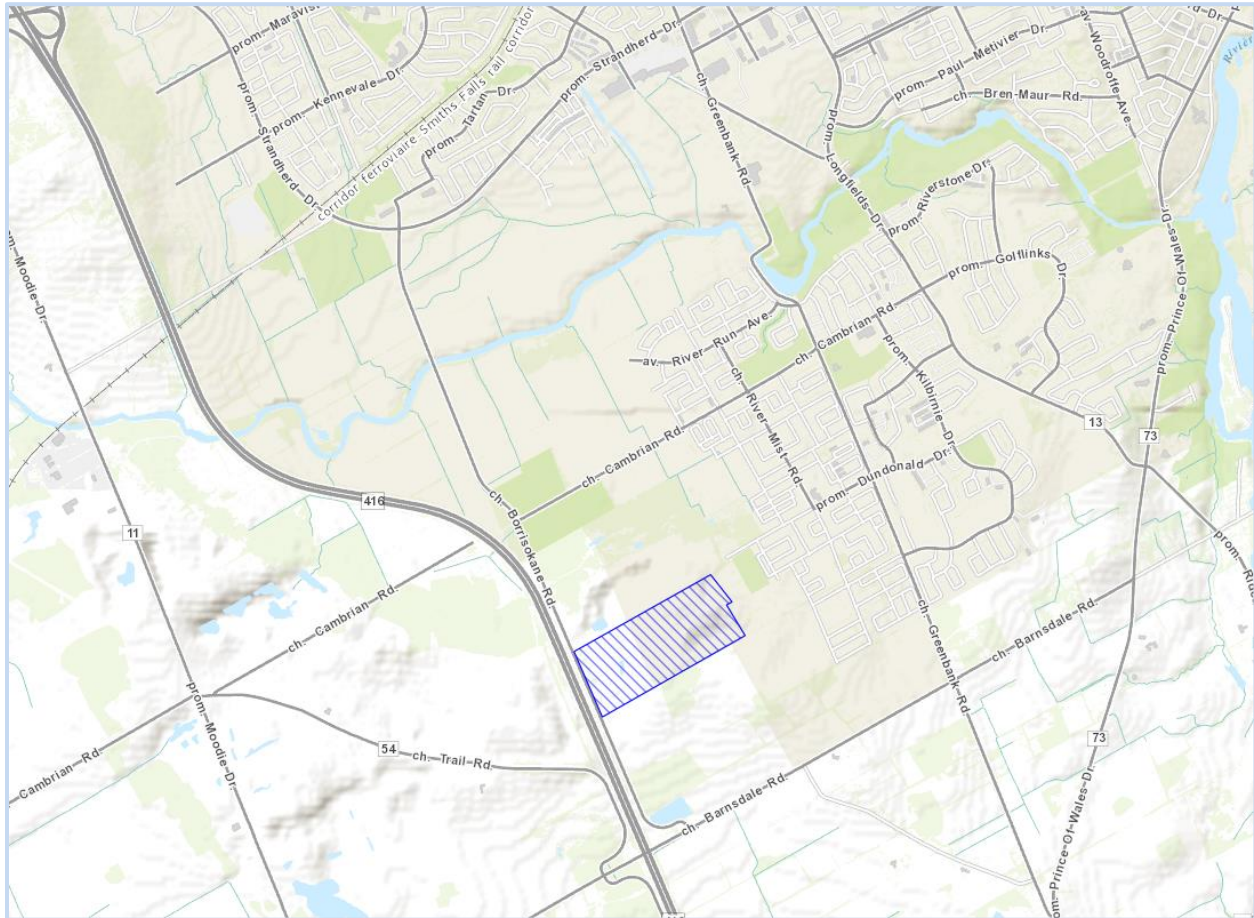
A Zoning By-law Amendment is also required to change the current zoning in the development area from Mineral Extraction Zone to reflect to proposed urban development consisting of mainly residential uses. The park block and stormwater management pond block shall be zoned as open space.

1.2 Location

The site is a 39.73 hectare rectangular parcel currently addressed at 3809 Borrisokane Road, Concession 3, Lot 8 of the former Geographic Township of Nepean (See Figure 1). The land is bounded by the existing Costello (Drummond) Aggregate Pit to the north, adjacent development lands and the realigned Greenbank Road to the east, vacant rural lands and an offset parcel of development lands to the south, and Borrisokane Road and Highway 416 to the west.

The eastern development area of the site is located within the Barrhaven South Urban Expansion Area (BSUEA) and as such is subject to a Community Design Plan developed for the area. The remnant western portion of the site is expected to remain vacant, with a stormwater management pond and one local access road connection to Borrisokane Road.

Figure 1: Location of Site.



1.3 Consultation

A pre-consultation meeting with the municipal planning file lead and review staff was held on May 11, 2018. Confirmation of materials required to support this subdivision and zoning by-law application were provided by email on June 1, 2018 from Sean Moore.

The Ward Councillor is aware of this application.

1.4 Supporting Reports

This application is accompanied by the following reports and plans:

- ⊕ Planning Rationale
- ⊕ Addendums to Figures 3, 7, 8, 9 and 12 of the CDP are include in the Planning Rationale as requested.
- ⊕ Transportation Impact Assessment – CGH Transportation Inc.
- ⊕ Functional Servicing Report – David Schaeffer Engineering Ltd.
- ⊕ Preliminary Grade Control and Drainage Plan – contained in the above.
- ⊕ Preliminary Site Servicing Plan – contained in the above.

- ⊕ Geotechnical Study – Paterson Group
- ⊕ Traffic Noise Feasibility Assessment – Gradient Wind
- ⊕ Noise Feasibility Study (Stationary Noise Component) – Paterson Group
- ⊕ Archeological Resource Assessment – Paterson Group
- ⊕ Phase I Environmental Site Assessment – Golder Associates Ltd.
- ⊕ Tree Conservation Report and Environmental Impact Statement – Kilgour & Associates Ltd.
- ⊕ Mineral Resource Impact Assessment – Paterson Group
- ⊕ Draft Plan of Subdivision plotted 12/20/2018 – J.D. Barnes Ltd.
- ⊕ Concept Plan dated October 30, 2018 – Gerrard Design

2.0 Context

The local context and consultation with municipal staff has influenced the layout of the subdivision and location of the land uses within. The arrangement of the roads and blocks is intended to address the relationship with the adjacent transportation corridor, developing neighbourhoods to the north, east and south, and be sensitive to the adjacent rural lands. The road network is designed as a permeable grid with pathways and walkway blocks, ultimately connecting through to adjacent developments. The park block provides connection between the community and the treed open space adjacent the south boundary. The development features wider frontage singles with shallower depths without compromising overall densities. The townhouses are located closer to the transit corridor at the eastern extent. Future general rural land uses in the western part allow integration at the urban-rural boundary which divides the site, and provides a separation buffer from Highway 416, while utilizing the rehabilitated aggregate land efficiently.

2.1 Surrounding Area

The development site is located at the edge of the urban boundary. The lands to the north and east are designated General Urban Area and Urban Expansion Study Area. To the south and west, the lands are designated General Rural Area and Sand and Gravel Resource Area. As shown in the 2017 air photo, the surrounding lands are characterized by the following land uses (see Figure 2):

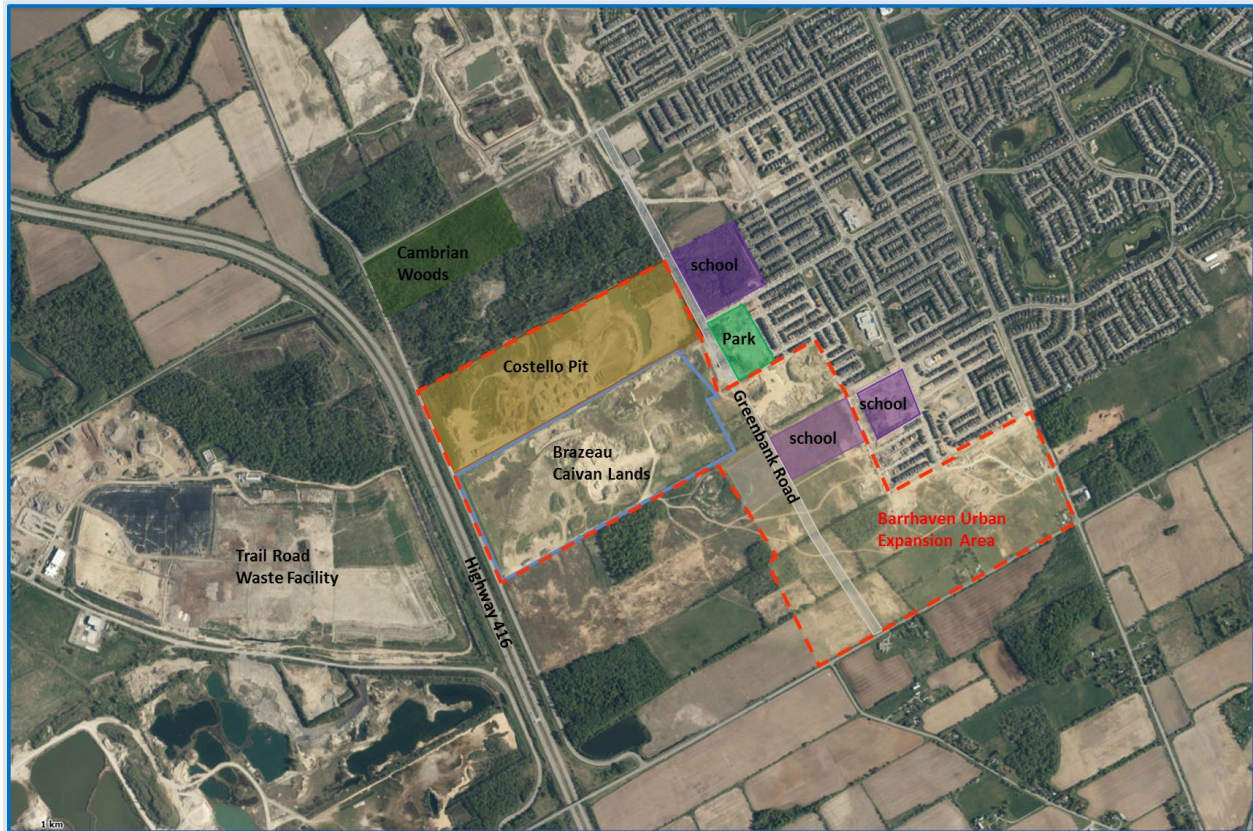
- ⊕ North
The lands immediately north are part of the Drummond/Costello Sand Pit operation. Further north is the Cambrian Road Woods Urban Natural Area.
- ⊕ East
To the east, there is a narrow (< 100 metre wide) parcel owned by others before the future Greenbank Road extension. It is expected to be developed for residential and commercial uses. On the other side of Greenbank Road is the Mattamy Half Moon Bay South Phase 4, 5 & 6 lands, under development as a community park, residential and secondary school site.
- ⊕ South/East
Minto Quinn's Pointe Lands are under development with a residential, Park and Ride facility and elementary school site.
- ⊕ South

Directly south is vacant rural land.

⊕ West

The Highway 416 and the Trail Road Waste Disposal Facility are west of Borrisokane Road.

Figure 2: Context Map.



2.2 Site Conditions

As shown in Figure 3, the site has been occupied by an aggregate extraction pit operation for sand resources and has recently ceased operation. The site has internal informal truck roads, several derelict buildings, and scale house and significant berms to the east and west. The land is significantly disturbed and there are stockpiles of materials at various location and elevations. Elevations on the site vary from 108.0 to 99.0 metres and the drainage is generally toward the west (Borrisokane Road). No surface water features exist on the site other than standing water in excavated depressions. Vegetation is limited to trees along periphery and ground cover growth on surfaces that have not been recently disturbed. Trees on the north and south boundaries are generally precariously attached to the raised edges of the site.

Figure 3: Air Photo of Site.



2.3 Relationship to Landscape

The site is at the urban - rural boundary of Ottawa and will be continuous within the urban fabric from the north and east once development proceeds in these areas. Currently the land is contiguous with the aggregate pit directly to the north and rural vacant land to the south. The Jock and Rideau Rivers are approximately 2 and 3.5 kilometres respectively to the north and east. There are no tributaries near or connected through the site. The Cambrian Woodlot and undeveloped wooded lands further north are not connected to the site. A smaller woodlot, which does not constitute a significant woodland, is adjacent the south boundary, but is surrounded by vacant land former agricultural without corridor connection to other small woodlot patches in the rural area. See Figure 4.

Figure 4: Location of Site in Existing Landscape.



2.4 Transportation Connections

The property is currently accessed by the existing Borrisokane Road along the west boundary which will be accessed by a central east/west local road. Although currently disconnected from the local roads being built in Half Moon Bay to the east, a temporary service road may be considered until the future Greenbank Road extension is constructed. The future Greenbank Road arterial is also expected to support a future transit corridor and cycling route.

Future road, transit and cycling/pedestrian routes delineated in the City's Transportation Master Plan ('TMP'), Pedestrian and Cycling Plans will provide robust connections in the future to adjacent communities, employment and services for future residents. As per the CDP demonstration plan, the road fabric will ultimately connect through to Cambrian Road on a proposed north/south collector. The site has good connections to the Highway 416 by way of the Fallowfield Road interchange and a potential future interchange at Barnsdale Road. Bus Rapid Transit is planned to the Cambrian Road/Greenbank Road intersection and ultimately onwards down Greenbank Road to a BRT Station with Park and Ride adjacent the site.

Figure 5: Transportation Master Plan – Road Network Urban.

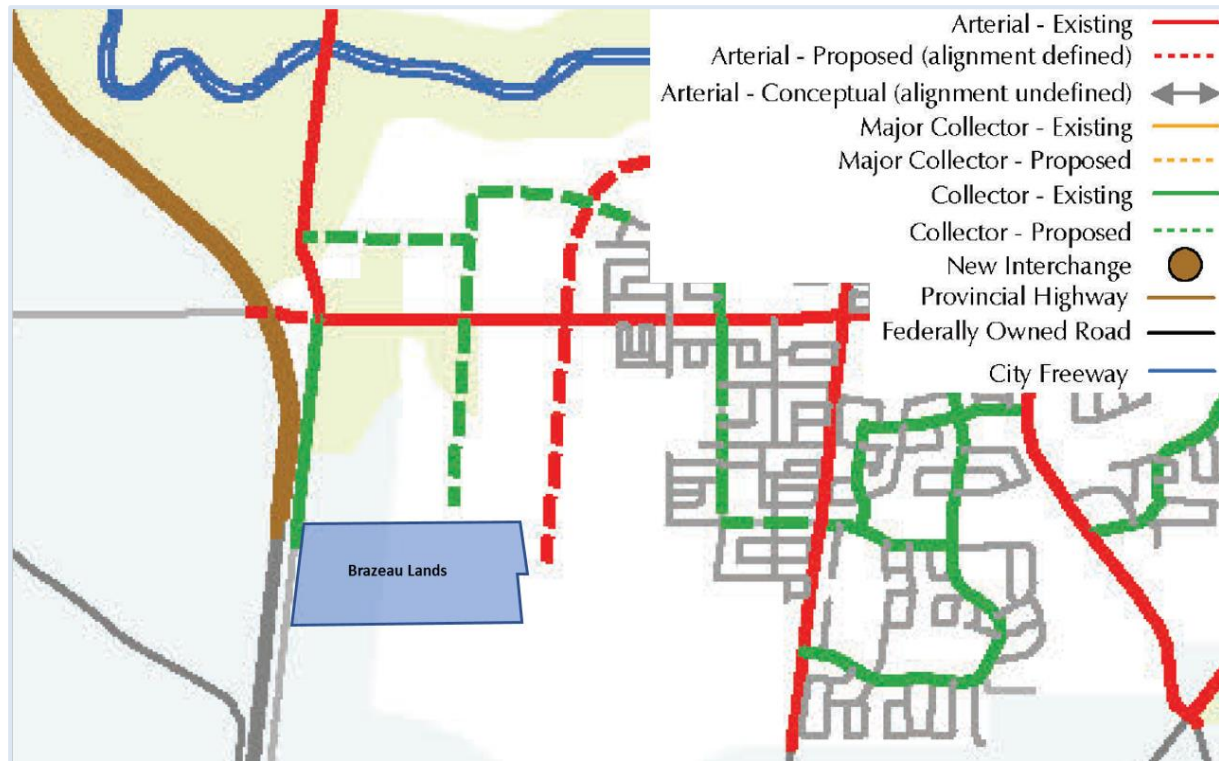


Figure 6: Transportation Master Plan – Road Network Rural.

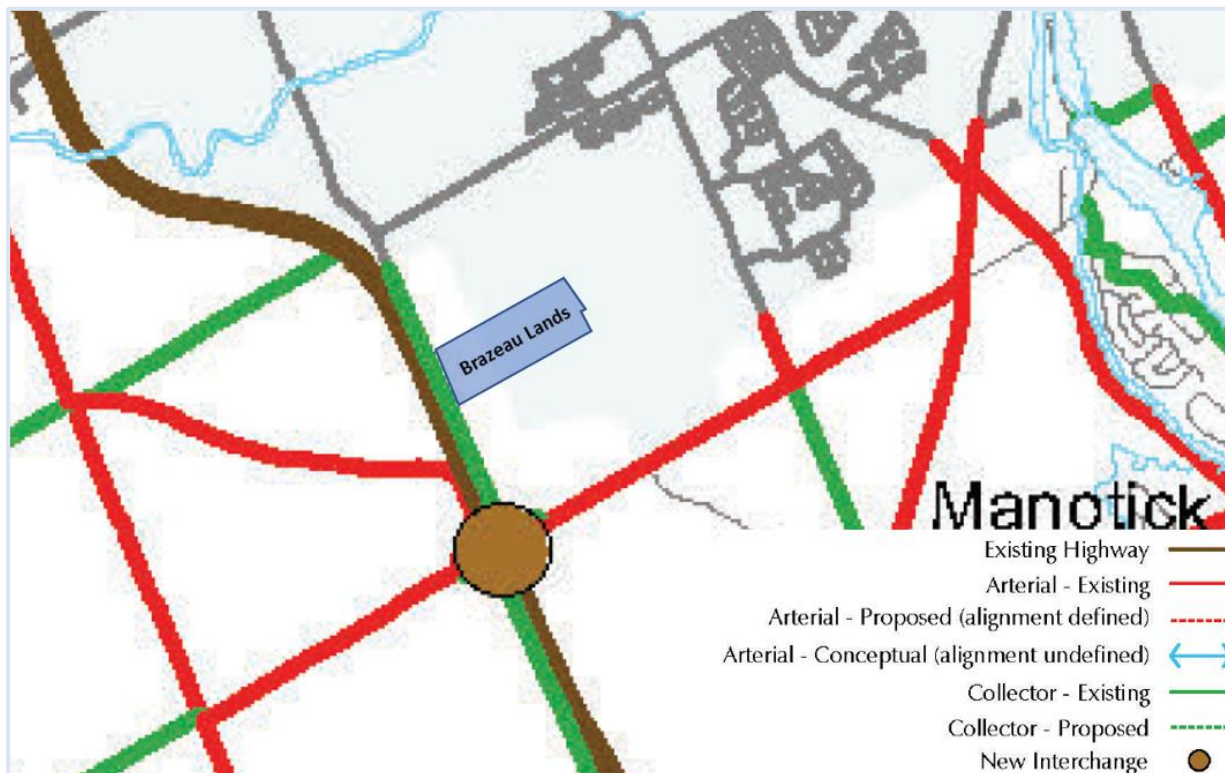


Figure 7: Transportation Master Plan – Rapid Transit and Transit Priority.

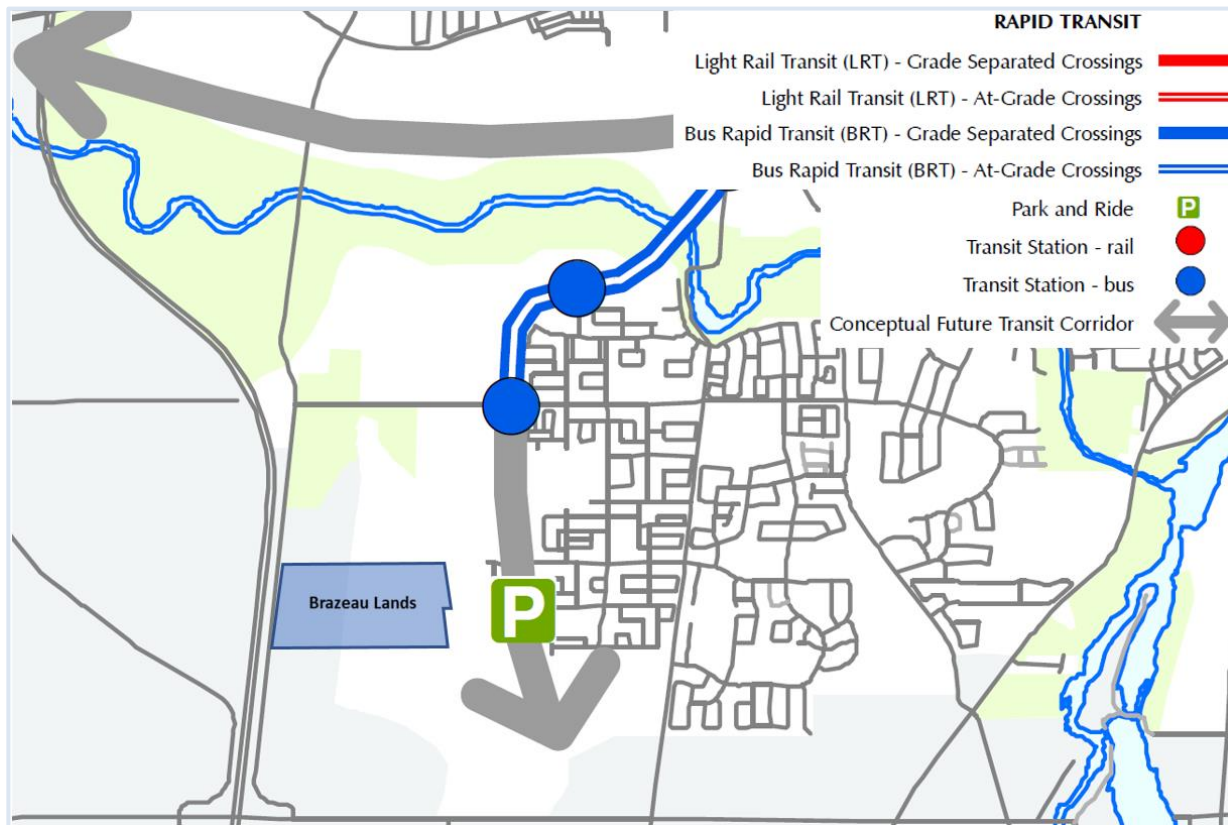
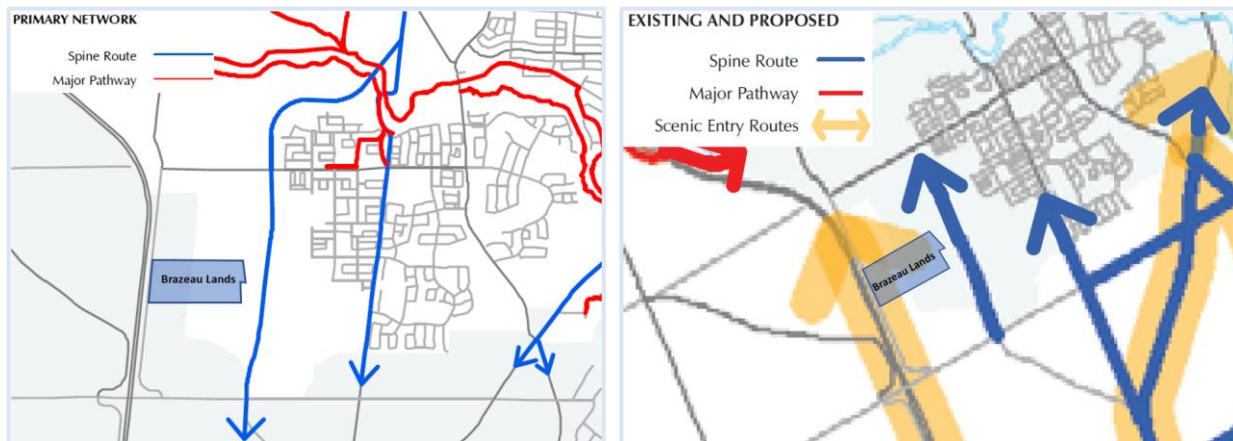


Figure 8: Transportation Master Plan – Cycling Network – Primary Urban & Rural Routes.

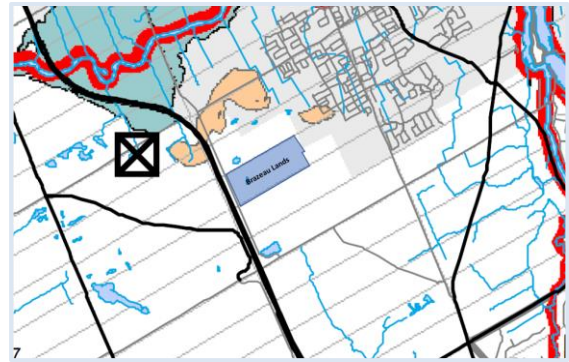
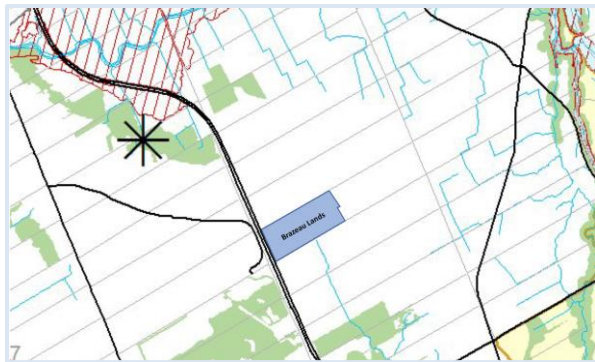


2.5 Natural Heritage System and Parks

The site is not located within/adjacent any features identified in either the Natural Heritage System Schedule L2 or within/adjacent any Environmental Constraints on Schedule K.

The Community Design Plan for BSUEA identifies the location of existing woodlots and future parks. None are designated within the Caivan Brazeau Lands, however a neighbourhood park of approximately 1.64 hectares is proposed as per requirements.

Figure 9: Natural Heritage System (Sched.L2) and Environmental Constraints (Sched.K)



3.0 Proposed Plan of Subdivision

The following section describes the key components of the proposed plan of subdivision.

3.1 Details

3.1.1 Layout

The residential development land within the urban boundary is comprised of single detached and Townhouse units over 24.06 ha of the site. The layout has been designed to integrate into the adjacent residential lands within the urban boundary, in accordance with the CDP.

The structural components of the plan include the following, as shown on Figure 10:

- ⊕ 320 single detached homes
- ⊕ 180 Townhouses
- ⊕ 1 park block (1.63 ha)
- ⊕ 7.79 ha of streets and walkways (one collector and eleven local roads)
- ⊕ On the remaining 15.67 ha, there is a 2.73 ha stormwater management block, vacant land and a road connection out to Borrisokane Road. The balance of the land outside the urban boundary will remain vacant.
- ⊕ A 500 metre buffer from the property boundary of the Trail Road Waste Facility is the western limit of the residential development area of this proposal. This is shown on the accompanying submitted draft plan prepared by J.D. Barnes Ltd.

The site is proposed to be developed in 3 phases with approximately 165 lots per phase starting in late 2019.

Figure 10: Concept Plan for Draft Subdivision.



3.1.2 Statistics: Height/Density/Unit Type/Mix

As illustrated above, the proposal presents a mix of approximately 36 % townhouse and 64 % single detached homes. The lots for both types of units are 27.4 metres in depth, located along 18 metre wide local roads and one 24 metre wide north/south collector. The townhouse components is located in the eastern section, considering proximity to future rapid transit and commercial resources as suggested in the CDP concept plan. The total number of units (500) divided by the total land area devoted to residential use provides a density of 33.16 units per hectare. This is generally in keeping with the CDP guidelines.

The residential remnant blocks along the irregular eastern boundary of the subdivision will be held until the adjacent residential lands move forwards with development.

3.1.3 Landscaping and Streetscaping

As indicated in the Geotechnical Report (dated November 2018), there are no planting setback restrictions for the subject site due to the absence of silty clay deposits within the residential development area. A landscaping plan will be prepared in consultation with City of Ottawa staff and in keeping with geotechnical recommendations.

3.1.4 Parks and Open Space

The park of 1.63 hectares accounts for 6.7 % of the land area and has 199.46 metres of frontage on two streets with sidewalks. The park is located to coincide with the adjacent the woodland to the south as identified in the Environmental Management Plan for the BSUEA to support the linkage with the potential future greenspace network.

The stormwater management block takes advantage of the natural drainage towards the west and is located outside the urban boundary, as had been permitted in other urban/rural boundary threshold development projects.

3.1.5 Parking

Parking will be provided in excess of the Minimum Parking Space Rates By-Law (Section 101). Further, the collector road is expected maintain the BSUEA CDP road cross sections which would include one lane of parking on one side.

3.1.6 Transportation and Access

The proposed internal street network of 18 metre local roads is designed to integrate with future development to the east and north providing connectivity and permeability while limiting cut through traffic potential. The north/south collector road is consistent with the conceptual plan for road networks in the BSUEA CDP. The proposed 24 metre cross section includes a cycle track and sidewalk on both sides with one lane of parking.

Further, traffic calming measures will support pedestrian and cycling movement, as well as encourage more measured vehicle speeds.

Sidewalks are purposed on one side of most local circulation streets, and adjacent the park block. Dedicated cycling tracks are proposed on the north/south collector road.

Figure 11: Concept Pedestrian /Cycle Network.

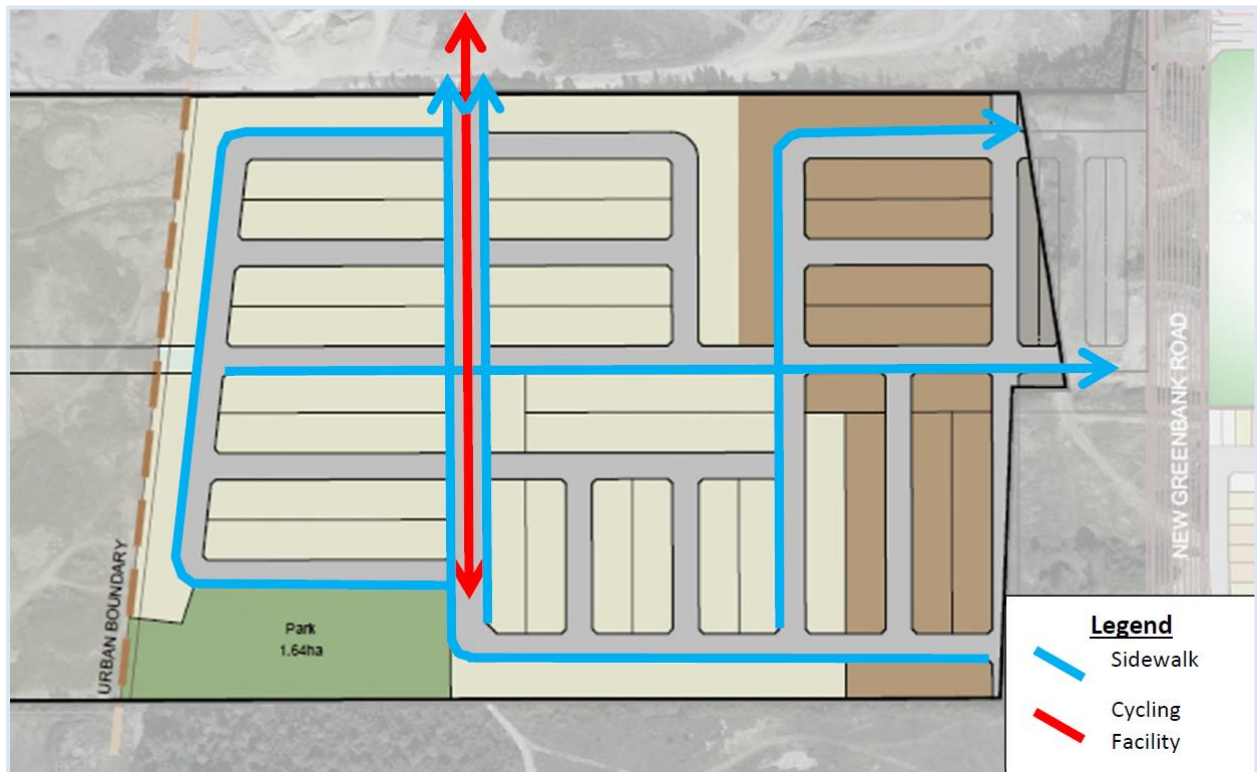


Figure 12: Concept Traffic Calming Plan.

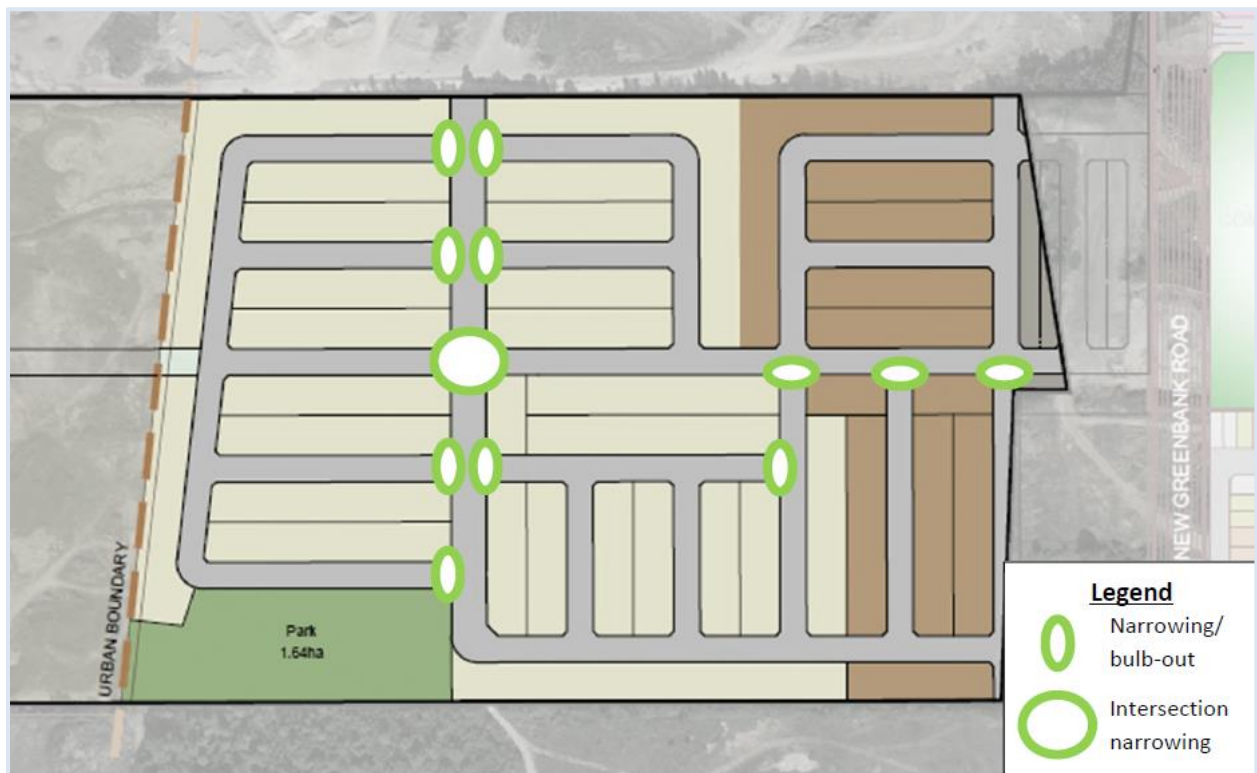


Figure 13: BSUEA CDP Transportation Master Study Collector Plan View.

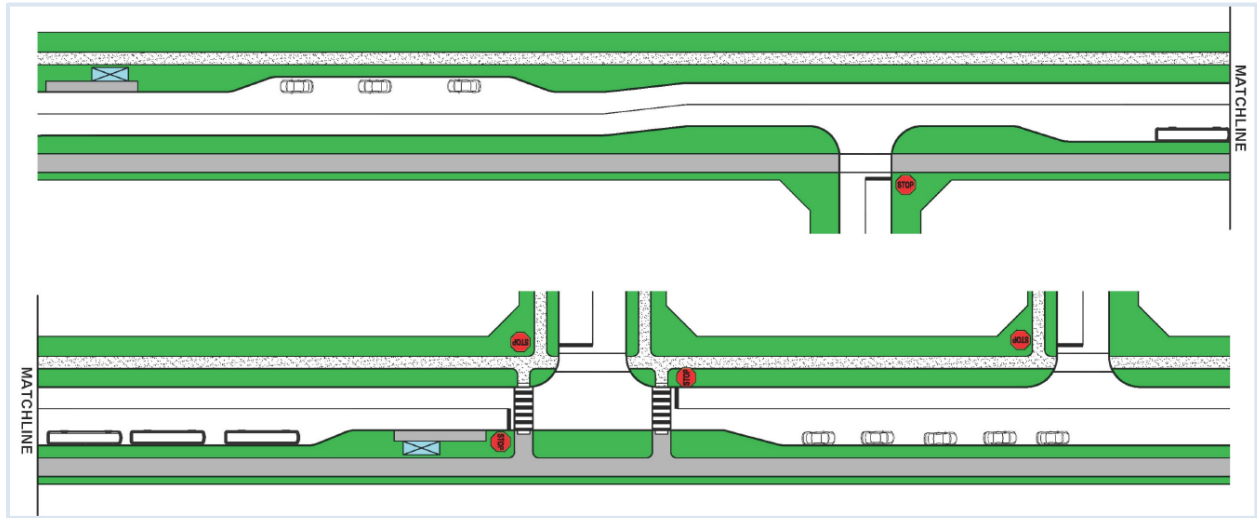
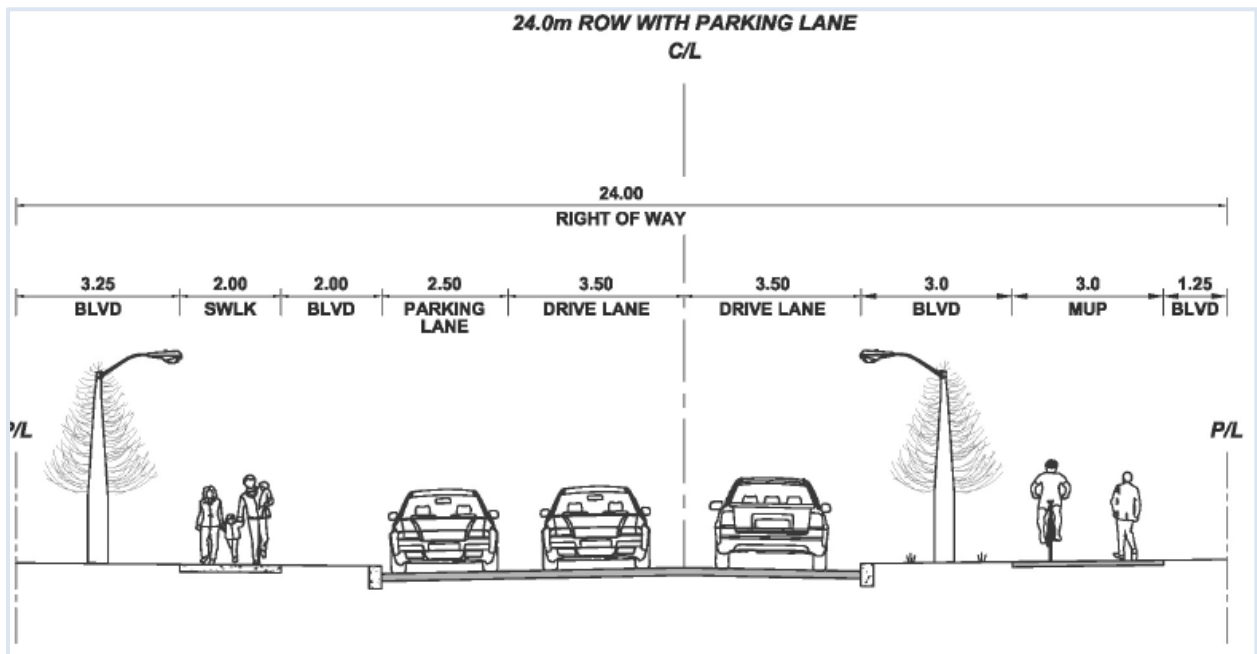
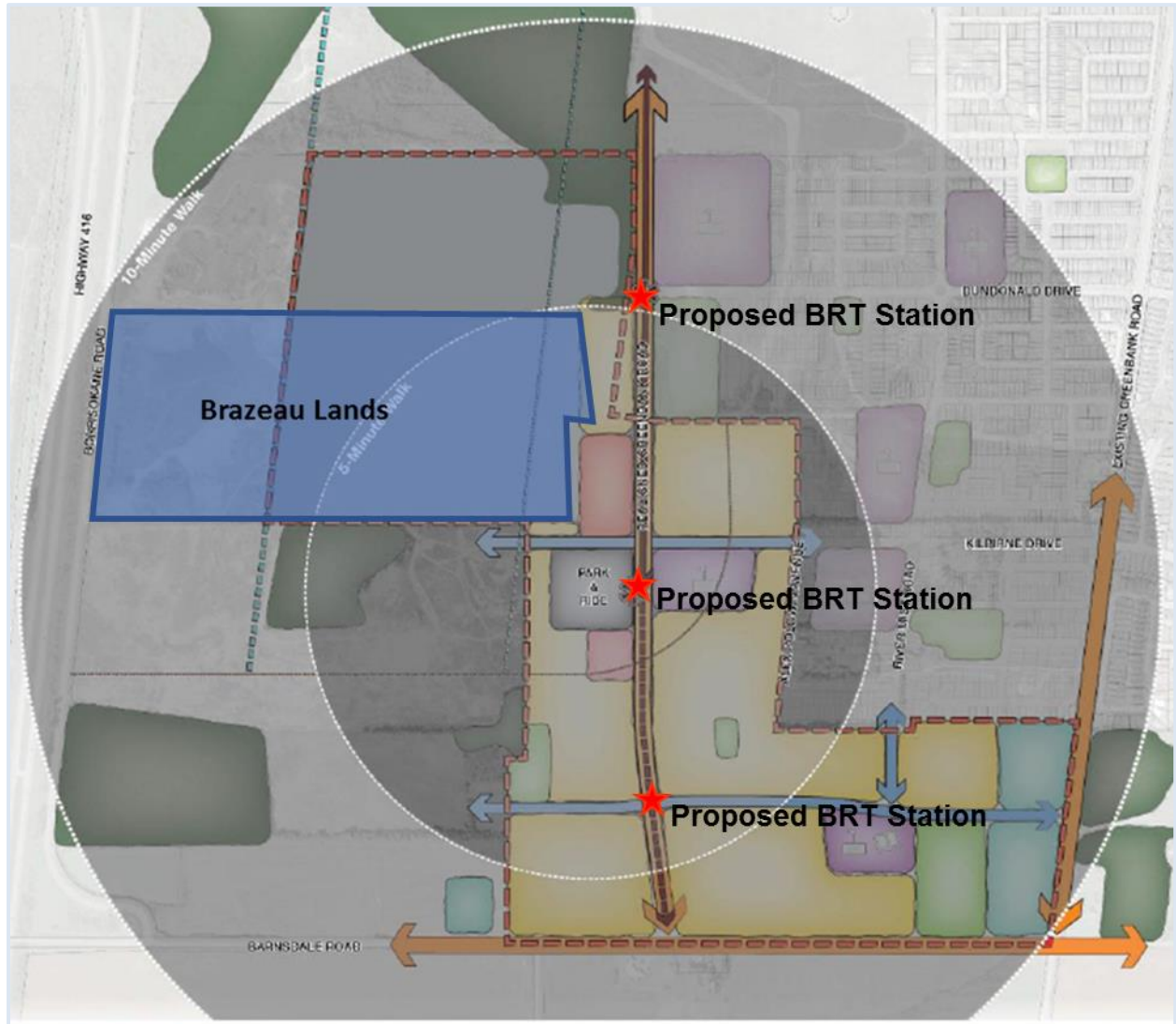


Figure 14: BSUEA CDP Transportation Master Study Collector Cross-Section.



Access to the subdivision shall be initially from Borrissokane Road. It is expected that the Greenbank Road extension, when built, will provide the primary access to the site. The construction of Greenbank Road will also facilitate the proximity of the Bus Rapid Transit (BRT) system and a park and ride with BRT station all within walking distance of the proposed residential area. See Figure 15.

Figure 15: BSUEA CDP Transportation Master Study Collector Cross-Section.



3.1.7 Outdoor Amenity Space

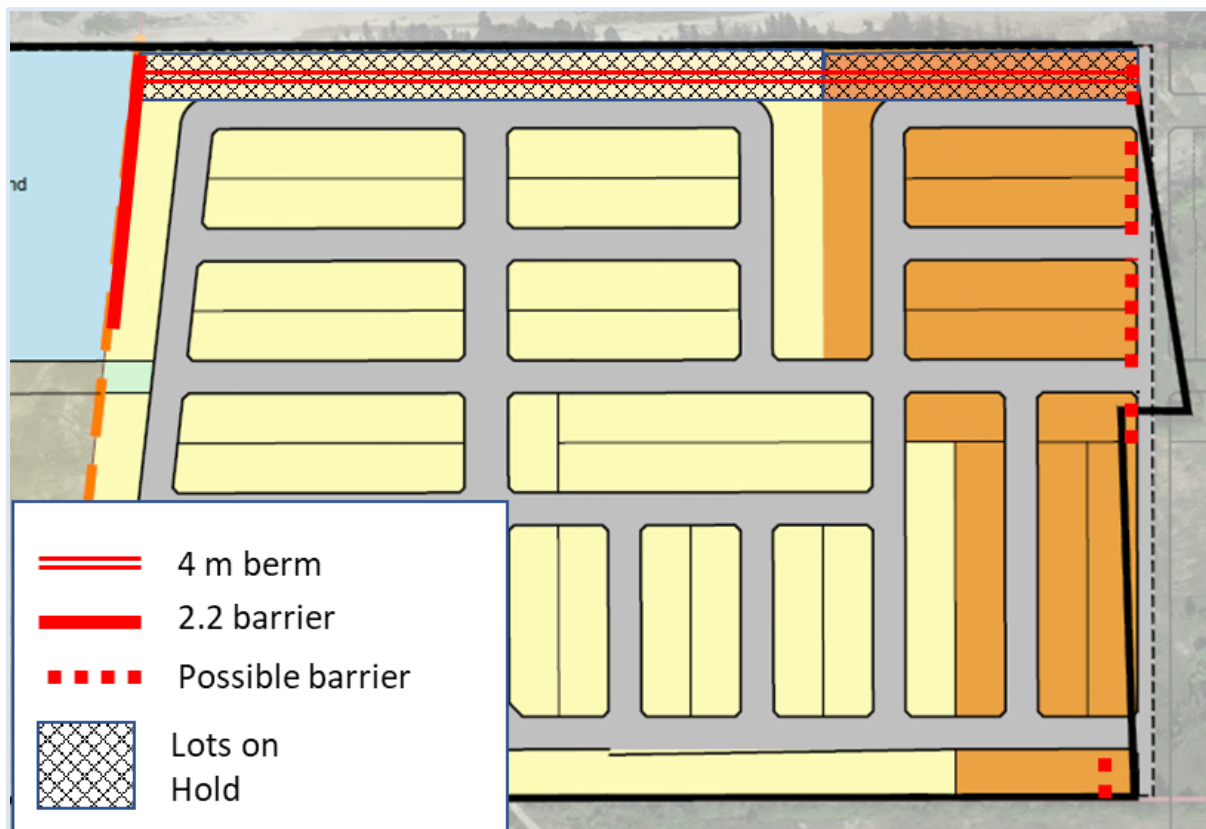
Rear or side yard private outdoor amenity space is provided with each detached and townhouse unit. Consistent with other Caivan Community residential developments in Ottawa, zoning that supports additional privacy for courtyard configurations in the rear yard is proposed. The local park provides additional open space area for the residents.

3.1.8 Noise and Air Quality

A noise feasibility assessment has been performed to consider the potential for noise from adjacent transportation routes (a minor and major collector) and existing aggregate operations to the north. Noise control mitigation measures near the future Greenbank Road may be required to protect outdoor private amenity space. This will be somewhat dependent on the timing and type of development east of the site. Townhouse blocks or other development types which act as barriers may address this item. Development blocks adjacent the north boundary of the site, closest to the existing Drummond/Costello aggregate operation will be subject to a holding zone until the pit ceases operation, expected within a two-year time window. In the interim, a 4 metre high earthen berm, is to be constructed across these lots, and a 2.2 m high sound barrier is to be constructed along the western development limit. See Figure 16.

The proposed concept plan has taken into consideration the proximity of the site to the Trail Road Waste Facility to ensure appropriate land uses within the potential influence area of the Facility. The proposed residential uses are outside of the 500 m buffer (as shown on the draft plan of subdivision).

Figure 16: Proposed Noise Mitigation Measures



3.1.9 Sunlight and Microclimate

The proposed units are all low rise and are not adjacent any existing structures. No impacts related to sunlight or shading are expected. The nature of the proposed residential development, which is characterized by private amenity areas, street trees and open landscaped space is not likely to contribute to microclimatic change in the local area.

3.1.10 Supporting Neighbourhood Services

This additional residential density is in keeping with the larger community structures and amenities planned in the BSUEA CDP. The residential population will have access to, and support the use of parkland, schools and transportation systems that are existing and planned for the adjacent established and developing communities.

3.2 Approvals

No approvals have been applied for in advance of this submission. communication has been initiated with various stakeholders and agencies to ensure support for and orderly process of future applications. Future required approvals will include:

- ⊕ Ministry of the Environment and Climate Change: Environmental Compliance Approval for construction of new sanitary and storm sewers.
- ⊕ Ministry of the Environment and Climate Change: Permit to Take Water for construction of services and basements.
- ⊕ Ministry of the Environment and Climate Change: Record of Site Condition on the eastern/residential portion of this site.
- ⊕ City of Ottawa: Ministry of the Environment Form 1 Record of Watermains Authorized as a Future Alteration for construction of watermains.
- ⊕ Rideau Valley Conservation Authority: Permit under O.reg 174/06 for stormwater outlet connection to the watercourse ditch at the corner of Cambrian and Borrisokane Roads.
- ⊕ Ontario Aggregate Resources Corporation to complete a Surrender Pending Production Report.
- ⊕ Ministry of Natural Resources Major Site Plan Amendment to revise the existing rehabilitation plan for the aggregate site.
- ⊕ Ministry of Natural Resources Surrender of Licence issued under Aggregate Resources Act.

3.3 Aggregate License Procedure

Sand extraction operations have ceased at the Brazeau Pit. Consultation with the Ministry of Natural Resources (MNR) and the assigned Aggregate Resources Technical Specialist-Inspector-Officer, has confirmed that the Ministry is in agreement that the Brazeau Sand Pit has acceptably exhausted the viable resource capacity to continue operations. This, in combination with urban boundary location, adjacent land uses and suitability for residential development, results in support for the owners pursuing the process of license surrender under the Aggregate Resources Act. The following steps shall be undertaken:

- ⊕ Subdivision receives draft approval indicating municipal support for the conceptual plan and land use.
- ⊕ An application for a major site plan amendment (under the Aggregate Resources Act) shall be filed with MNR to revise the existing rehabilitation plan to reflect the approved

draft plan of subdivision. The site plan proposal may be posted on Ontario's Environmental Registry, subject to a decision on significant by MNR.

- ⊕ The site rehabilitation plan shall be changed to reflect the draft approved subdivision.
- ⊕ Licence fees and production reports are submitted to the Ontario Aggregate Resources Corporation (OARC) finalizing the operation closure. OARC provides MNR with confirming that fees and production reports are up to date and compliant for operation closure.
- ⊕ MNR accepts surrender of licence.

4.0 Proposed Zoning By-law Amendment

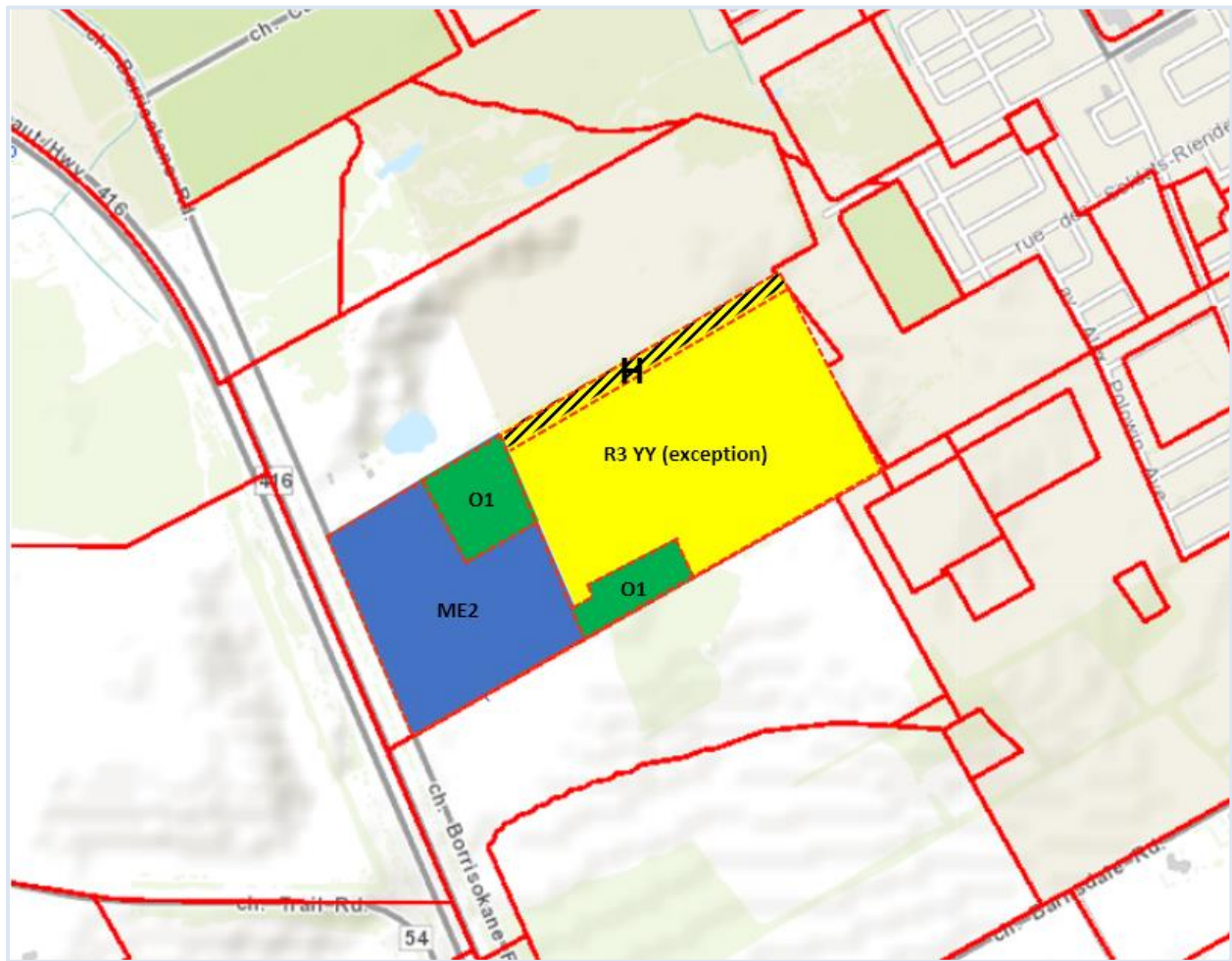
The entire site is currently a Mineral Extraction (ME2) zone in keeping with its legacy use as a local sand pit operation. The proposed rezoning shall reflect the land uses and housing types expected by the draft approved plan of subdivision. The use of a holding (h) symbol is proposed to address the temporary adjacent lands use constraints.

4.1 Details of Proposed Amendment

The lands are composed of a range of housing types and densities, including detached homes and townhouses (yellow), open space (swm facility and park), and vacant rural. The proposed zoning shall support these residential uses and open space, as well as recognize the lands outside the urban boundary, as per Figure 17.

A holding zone (h) shall be implemented on lots within the 30 m buffer adjacent the aggregate extraction to the north until the operations cease in approximately 2 years.

Figure 17: Proposed Zoning.



4.1.1 Residential

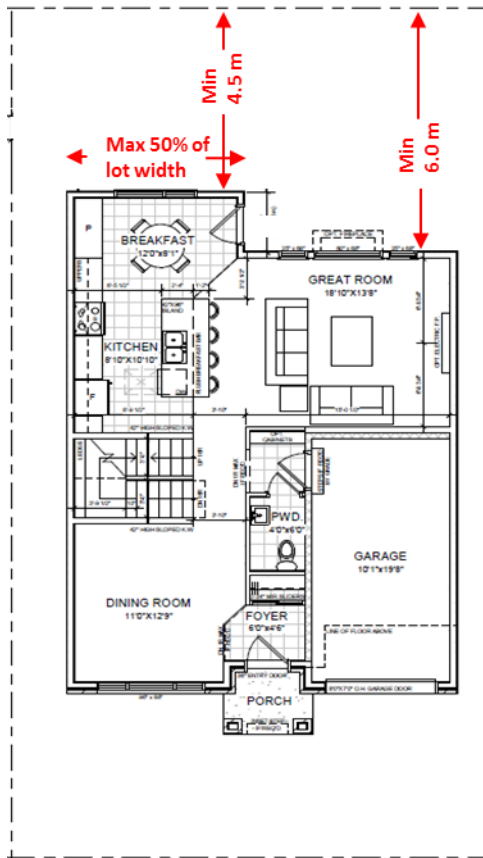
The proposed zoning will implement the detached and townhouse residential dwellings as proposed within the draft plan of subdivision. Such zoning is compatible with the existing and proposed developments adjacent the site. The proposed zoning provisions are based on a similar zone used in other City of Ottawa suburban communities - R3 YY (1909) (By-law 2018-359), with the following additional accommodations:

Detached dwelling interior lots:

To support additional privacy for courtyard configurations in the rear yard (see Figure 18), the following setback accommodations are proposed for all interior lots:

- ⊕ Minimum 6.00 m rear yard setback with minimum 4.50 m setback up to 50% of the lot width.

Figure 18: Detached Interior Lot Rear Yard Setbacks.



Zone requirements for dwellings:

Zone requirements:	Detached Dwelling - interior lot	Detached dwelling on a corner lot	Semi-Detached and Townhouse Dwellings
minimum lot area	220 m ²	220 m ²	137 m ²
minimum lot width	9 m	9 m	5.5 m
minimum front yard setback	3.0 m	3.0 m	3.0 m
minimum front yard setback for an attached garage	3.5 m	3.5 m	
minimum total interior side yard setback	1.8 metres with a minimum of 0.6 metres on at least one side.		
minimum interior side yard setback	1.2 m, with minimum of 0.6 m on at least one side	0.6 m minimum	1.5 m
minimum corner side yard		2.5 m, despite the foregoing, no more than two portions of the building, not exceeding a total floor area of 3m ² , may be located no closer than 2.0 m from the side lot line abutting a street. An active entrance must be provided on the side of the building facing the corner side yard	2.5 m
minimum rear yard setback	minimum 6.00 m setback with minimum 4.50 m setback up to 50% of the lot width.	minimum 2.5 m setback for any part of the building that is no higher than 4.5 m. any part of the building, excluding projections, located less than 6.0 m from the rear lot line must be located at least 4.0 m from any interior side lot line.	
maximum building height	12.0 m	12.0 m	14.0 m
maximum lot coverage	55%	55%	65%

Corner sight triangle:

The proponent also requests that clarification be established regarding corner sight triangles, as shown italicized below:

Despite Section 57(2), for townhouse dwellings, the corner sight triangle *may be* calculated using 57(1) and in the instance of any dwelling listed in (1) including multiples, the distance used to determine a corner sight triangle *may be a minimum* of 2.75 metres and not 6 metres.

4.1.2 Open Space

Consistent with other municipal stormwater management facility blocks and local parks, the O1 zoning is proposed.

4.1.3 Vacant Land

In accordance with Policy 17 of the Mineral Aggregate Resources Section 3.7.4 of the Official Plan, where a pit licence has been surrendered and the lands are predominantly surrounded by designations other than Agricultural Resource, the uses in Section 3.7.2 for the General Rural Area may be permitted. A rezoning from the mineral extraction zone will ultimately be required.

4.1.4 Temporary Setback Constraint

Development blocks adjacent the north boundary of the site, closest to the existing Drummond/Costello aggregate operation will be subject to a holding zone (h) until the pit ceases operation, expected within a two-year time window.

5.0 Policy and Regulatory Framework

5.1 The Planning Act and 2014 Provincial Policy Statement

Under Section 3 of the Planning Act, the Provincial Policy Statement 2014 (2014 PPS) provides for appropriate development and land management while protecting public resources. Specifically, it promotes growth in urban areas for the efficient use of land, resources, infrastructure and public service facilities. The policies also seek to protect and conserve the natural resources that support the long-term health and social well-being of communities, and the sustainability of natural features and systems in the environment. Relevant policies of the 2014 Provincial Policy Statement are discussed below.

5.1.1 Building Strong Communities Section 1.0

The proposed development supports efficient and appropriate development and land use land patterns in accordance with policies under Section 1.1.1.

- ⊕ Promotes long term and sustainable economically efficient development and land use patterns;
- ⊕ Accommodates an appropriate range and mix of land uses to meet long-term needs;
- ⊕ Avoids environmental or public health and safety concerns;
- ⊕ Avoids limiting the appropriate location of future settlement area expansion, minimizing land consumption and servicing costs;
- ⊕ Improves ease of access and mobility, supporting participation in society for all;
- ⊕ Ensures that necessary infrastructure and public service facilities are available to meet current and projected needs; and
- ⊕ Sited and designed to conserve biodiversity and consider climate change impacts.

The subject area is located in a designated settlement area, in accordance with the municipal direction provided in the approved master planning documents, as directed under Section 1.1.3.

- ⊕ Focuses growth and development in the settlement area;
- ⊕ Efficiently and appropriately establishes a land use pattern that supports land, resources, infrastructure and public service facilities which are planned or available, including public and active transit;
- ⊕ Minimizes negative impacts to air quality and climate change, and promotes energy efficiency;
- ⊕ Occurs directly adjacent to the existing built-up area in a compact form, meshing with and contributing to the mix of uses and densities that allow for the efficient use of land, infrastructure and public service facilities.

Section 1.1.4 addresses land use considerations and compatibility in rural areas. The proposal demonstrates an appropriate interface at this rural-urban boundary by establishing a transitional area across the vacant rehabilitated rural lands, and through connecting the neighbourhood's functional edge to Borrisokane Road and Highway 416.

The proposed development area has been established within a comprehensive master planning exercise, which conforms to the strategic planning of the approval authority. This is compliant with the coordinated, integrated and comprehensive approach required by Section 1.2. In addition, as per Section 1.2.6, the remnant vacant rural land further buffers the urban residential lands from potential adverse effects associated with major facilities and the adjacent highway.

The residential use of land adheres to the Section 1.4 housing provisions.

- ⊕ Supports residential growth in the settlement area as designated to meet projected land requirements;
- ⊕ Supports a range of appropriate housing types and densities;
- ⊕ Directs the new housing to areas where existing infrastructure and services are currently or are planned to be available.

The subdivision layout promotes public health and activity through the inclusion of open space and connectivity as per Section 1.5.

- ⊕ Streets and walkways are designed to support community interaction and active transportation, provide connectivity and allow for passive and active recreational activities.
- ⊕ Park space is located to be publicly accessible and adjacent a natural area for active and passive recreation, trails and linkages.

Infrastructure and services are available and provided to the development in accordance with Section 1.6.

- ⊕ Existing municipal sewage and water services are economically and physically optimized;
- ⊕ Efficient use is made of existing and planned transportation and transit systems;
- ⊕ The systems protect human health and the natural environment;
- ⊕ The storm water management system shall protect surface and ground water quality and quantity through design to support the local water balance and in accordance with the standards of the receivers.

Long term economic prosperity is reinforced as per Section 1.7 by efficiently using land, resources, infrastructure and facilities. Further, energy conservation, air quality and climate change are supported by compatibility with the land use and development pattern as set out in the Community Design Plan.

5.1.2 Wise Use and Management of Resources Section 2.0

The development has been designed to protect the natural heritage features and functions for the long term as per Section 2.1 and 2.2.

- ⊕ The diversity, connectivity and long-term function of natural features in the area is maintained, restored and, where possible, improved by the subdivision layout and coordination with adjacent land uses.
- ⊕ Connections between natural heritage features and areas, surface water features and ground water features are supported by locating open space amenities and land uses appropriately and designing infrastructure accordingly.
- ⊕ The proposed development design protects water by maintaining catchment area boundaries, the groundwater budget and providing appropriate quality and quantity controls, all which support related functions among natural heritage and surface water features.

Consistent with the requirements of Section 2.4 to protect mineral resources for the long term, the available mineral aggregate resource on this site has been exhausted to its economic and physically viable potential.

- ⊕ The adjacent mineral aggregate operation will be protected from development and activities which could interference with operations until such a time as the licence surrendered.

- ⊕ In known deposits of mineral aggregate resources and on adjacent lands, development and activities which would preclude or hinder the establishment of new operations or access to the resources shall only be permitted if the proposed land use or development serves a greater long-term public interest and issues of public health, public safety and environmental impact are addressed. The establishment of the urban expansion area, including and adjacent the identified aggregate resource area, is a result of the municipalities determination that compact urban growth serves the public interest in the long-term. Further, the existing and approved developments within the immediate area are, at this time, already imposing impacts on the remaining resource opportunities.
- ⊕ The rehabilitation of the site shall be comprehensive and compatible with adjacent existing and proposed land uses designations.

5.1.3 Protecting Public Health and Safety Section 3.0

The subdivision location and design is consistent with the policies protecting public health and safety by avoiding natural and human made hazards.

- ⊕ Rehabilitation and other measures to address and mitigate any potential hazards associated with the interim condition of the adjacent mineral aggregate operations are under way or have been completed.

5.1.4 Summary

The proposed development is consistent with the applicable PPS 2014 policies as described above. The residential development site is located within the urban boundary and respects the direction provided in the master planning documents for the area as approved by the municipality. The design comprehensively and efficiently services the community, extending and connecting the development fabric in a logical manner. The proposal addresses hazard lands and supports the natural heritage and water features of the site and adjacent lands.

5.2 City of Ottawa Official Plan (2003, as amended)

The property is divided between the rural and urban lands.

5.2.1 Urban and Rural Lands

The proposed residential development portion of the property is shown as 'Sand and Gravel Resource Area' within the 'Urban Expansion Study Area' overlay on Schedule B in the Official Plan, as shown on Figure 19. The urban expansion area designation was intended to accommodate urban growth and be developed primarily for residential purposes. As required by the Official Plan, the lands were considered and include in a Community Design Plan and the supporting studies.

An Official Plan Amendment was passed June 13, 2018 which revised the urban boundary to encompass the proposed residential development lands on the east side of the site, designating them as 'Sand and Gravel Resource Area' and 'Developing Community Expansion Area'. Policies were also established with in Section 3.12 stating that the lands will fulfill the requirements for inclusion in the Barrhaven South Urban Expansion Study Area Community Design Plan (BSUEA CDP).

The west portion of the property, outside the urban boundary as shown on Figure 20, remains at this time designated as a Sand and Gravel Resource Area. As the sand pit operating has ceased and the license will be surrendered, the land will be designated as General Rural in a future comprehensive update or general amendment to the official plan.

Figure 19: Schedule B 'Urban Policy Plan'

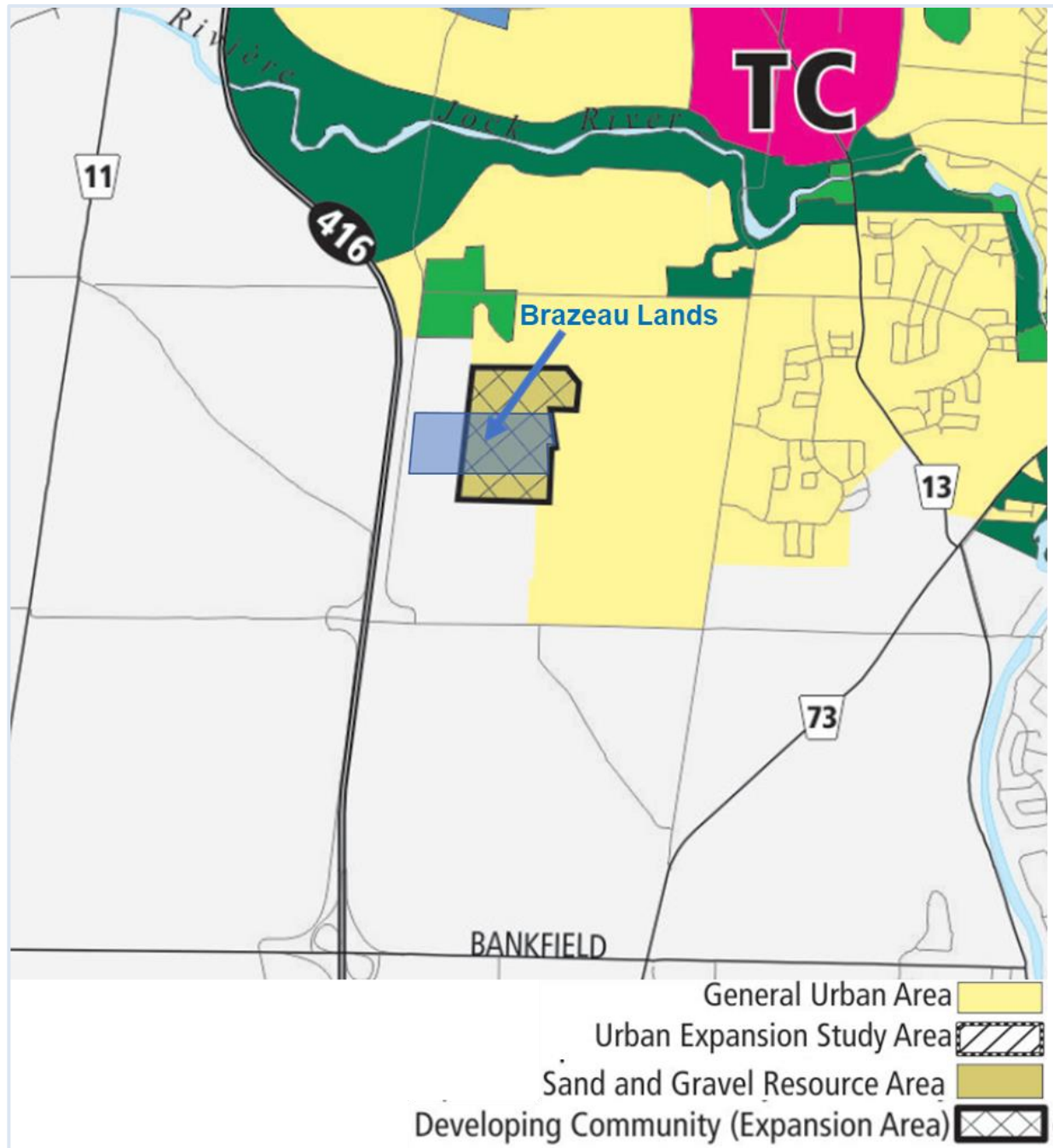
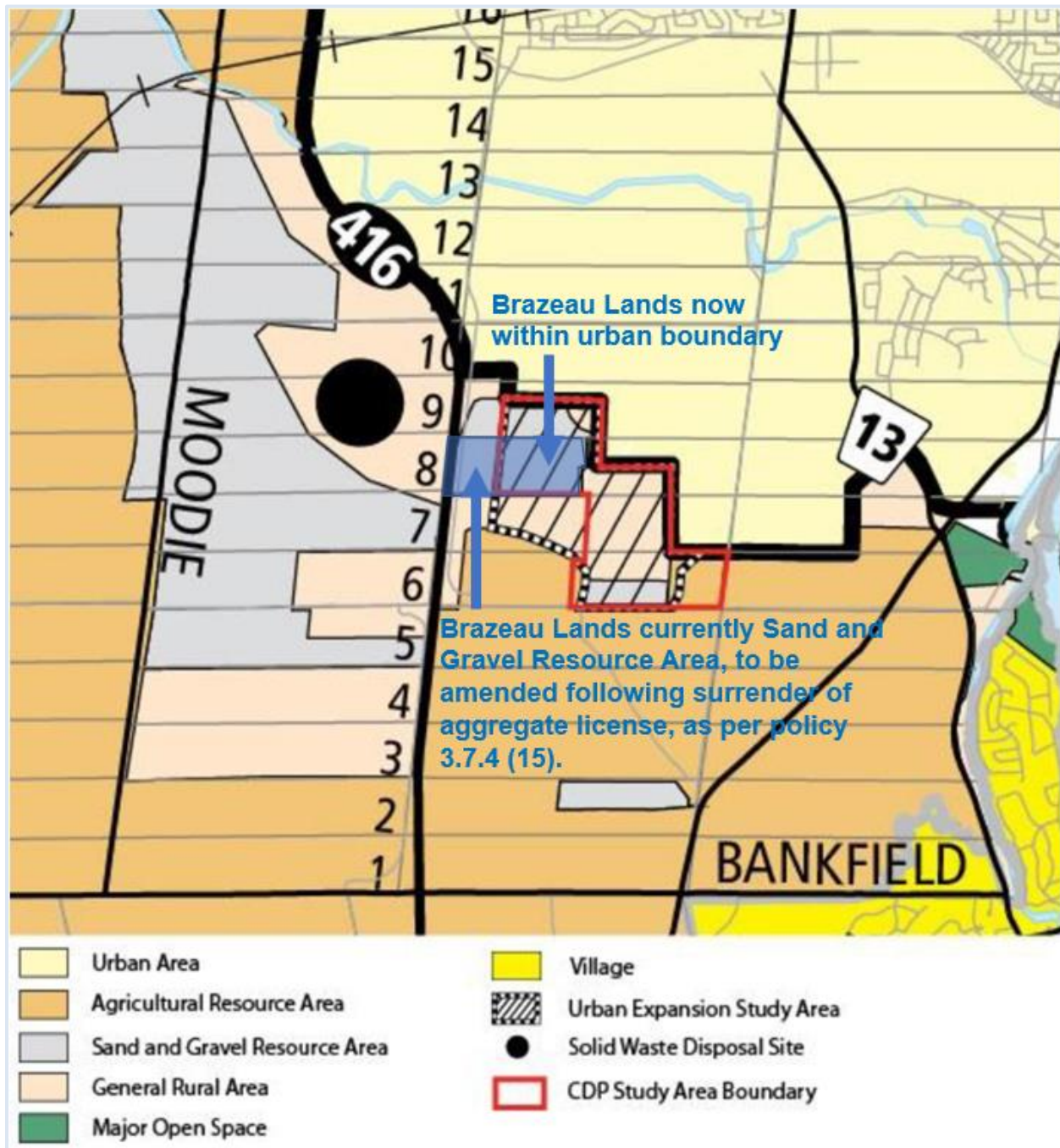


Figure 20: Schedule A 'Rural Policy Plan'



5.2.2 Designations and Land Use

Developing Community (Expansion Area) – Section 3.12

The designation of Developing Community (Expansion Area) on Schedule B is expected to support projected urban population growth and associated residential requirements. These lands shall be developed primarily for residential purposes, although minor, non-residential neighbourhood related uses may also be located here.

Policy 7 establishes a site-specific exception for these lands which states that:

“Notwithstanding Policy 3a), b), and c) above, the lands designated Sand and Gravel Resource Area at 3713 and 3809 Borrisokane will fulfill the requirements for inclusion as described in the Barrhaven South Urban Expansion Study Area Community Design Plan, Master Servicing Study, Transportation Master Study, Area Parks Plan and Environmental Management Plan”.

Thus, policy 3 (a), (b) and (c) are met through conformity with the Barrhaven South Urban Expansion Study Area Community Design Plan (BSUESA CDO) and associated Master Servicing Study, Transportation Master Study, Area Parks Plan and Environmental Management Plan.

Policy 2 directs that each plan of subdivision establishes the mix and location of residential dwelling which, as a minimum, will constitute the following:

At least 45% single detached but not more than 55% single detached, at least 10 per cent apartment dwellings and the remainder multiple dwellings, other than apartments. The split between single detached and townhome units is proposed at 64% to 36%. This development does not include any apartment dwellings as it is not adjacent the transit route nor the Park and Ride as directed in the BSUEA CDP. The percentage split between single detached and townhouses established the CDP has been considered and adapted to suite the location of the subject site. Located on the outside boundary of the urban area, directly adjacent the rural area, the land must function as a transition zone. Further, in consideration of the future densities expected on the parcel of land (owned by others) directly east adjacent the Greenbank Road Transit Corridor, the functional neighborhood will have a density that appropriately contributes to the overall housing mix established by the CDP.

Overall residential development is expected to meet a minimum average density target of 34 units per net hectare. Net residential density has been calculated for the urban area of the site at 33.16 units per hectare. The proposed density of this development is generally in keeping with the City of Ottawa and CDP guidelines.

Infrastructure projects associated with this development were identified through the master planning process for the BSUEA. A Municipal Class Environmental Assessment was undertaken and integrated through the planning process.

If requested, the developer shall prepare a Financial Implementation Plan and commit to providing the on-site and off-site servicing systems described above through development charges or at the expense of the developer as per policy 4.

Note that policy 5 states that an amendment to the Official Plan will not be required to remove the designation of Developing Community (Expansion Area) and replace it with General Urban Area. Development may proceed once the City is satisfied that the requirements of this section have been met and the City has approved the plan of subdivision.

General Urban Area – Section 3.6.1

Policy 13 is a site-specific exception for the community design plan for the Barrhaven South Community to guide future development. It states that development may proceed in keeping with the community design plan and policies elsewhere in this Plan:

In order to achieve the intent of the objectives of Section 3.7.4 Mineral Resources, the City will not approve any subdivision, zoning (potential exception could be a holding zone) or site plan control application for lands within the Barrhaven South Community Design Plan study area that are located within 300 metres of the Sand and Gravel Resource Area to the west of the community in the rural area, until the conditions set for these lands in the community design plan have been satisfied.

The extraction operation on the subject site has ceased and the Costello operation to the north is expected to wind down in the next 2 years. The Community Design Plan indicates that these lands may be developed once the extraction of the mineral aggregate ceases (as in the case of the Brazeau Pit), or a study is completed to the satisfaction of the City, which demonstrates that proposed development is compatible with the aggregate operations (as has been prepared to address noise and other operational concerns associated with the remaining limited operation time for the Costello Pit.

To demonstrate on the plan that it has been established that the mineral aggregate resource is depleted, an Official Plan Amendment shall be required for an alternate land use on the Sand and Gravel Resource Area. No alternate land use is proposed on the vacant rural lands at this time, although revised rehabilitation plans shall be prepared and implemented to address the requirements of the Ministry of Natural Resources.

General Rural Area – Section 3.7.2

The intent of the General Rural Area is to accommodate a variety of land uses that are appropriate for a rural location and to limit the amount of residential development such that development will not preclude or resist continued agricultural and or other non-residential uses. No development is proposed on the remnant rural land blocks other than the rehabilitation of the site. Discussion regarding the status and opportunities on these isolated lands, caught between Highway 416 and the urban boundary will take place in the future with city staff.

Mineral Aggregate Resources - Section 3.7.4

Sand and Gravel Resource Area policies are intended to protect non-renewable mineral aggregate resources from incompatible activities, so they are close to markets and available for future use. Further, the designation is intended to protect operations while minimizing their effects on communities and the environment.

Development Restriction on Adjacent Lands

Policy 10 states that new development will not be approved within 300 metres of a Sand and Gravel Resource Area, unless it can be demonstrated that such development will not conflict with future mineral aggregate extraction. Further, policy 11 allows the approval of new development within 300 metres of an existing sand and gravel pit if it can be demonstrated that the existing mineral aggregate operation, and potential future expansion of the operation in depth or extent, will not be affected by the development.

The Costello Pit operation to the north shall be protected from conflict during its final phase of operation by implementing a holding zone on lots within the potential impact area. The aggregate license shall be surrendered, and no further sand resource extraction activities are expected.

As per policy 12, the Ministry of Natural Resources has been consulted and has indicated their support for the closing of operations at both the subject site and the adjacent Costello Pit within the proposed development horizon.

It is expected that in accordance with policy 13, that the city will approve the development of land in accordance with policies above, subject to the proposed conditions to ensure the development provides adequate buffering and/or separation between the new proposed use and the mineral aggregate area/operation.

Rehabilitation and New Use of Depleted Sites

Policy 15 states that where the sand, gravel or bedrock mineral aggregate resources of a property have been fully extracted, the site fully rehabilitated and an aggregate license surrendered, the property may be used for other purposes. Under this circumstance the City will not require the proponent to amend the Official Plan; instead the Plan will be amended to accurately reflect the new use at the time of the next comprehensive Official Plan update or through a general Official Plan amendment. It is expected that the Land will be designated as General Rural in a future comprehensive update or general amendment.

Solid Waste Disposal Site – Section 3.8

The Trail Road Landfill is identified on Schedule 'A'.

Human health and safety may be affected within the area of influence of an operating or non-operating solid waste disposal site. All residential development is proposed beyond the 500 metre perimeter 'influence area' of the site.

Protection of Vegetation Cover – Section 4.7.2

The site has very limited existing natural vegetation cover. Aggregate removal operations have removed all trees. Plantings will be provided in the park and residential lands through the development process. There will be a net gain in tree cover realized through the proposed development.

Erosion Prevention and Protection of Surface Water – Section 4.7.3

There is no surface water/watercourses on the site. Stormwater leaving the site will be treated and controlled to meet the receiver quality and quantity control requirements.

5.3 Compatibility Analysis

As per Policy 2 of the 'General Urban Area' designation, new development applications must reflect Urban Design compatibility objectives in Section 2.5.1 and the Urban Design and Compatibility policies in Section 4.1.1. and more specifically in Urban Design Guidelines for Greenfield neighbourhoods. The components in these documents have also been applied and expanded upon in greater detail in the BSUEA CDP.

5.3.1 Urban Design and Compatibility - Section 2.5.1

To enhance the sense of community by creating and maintaining places with their own distinct identity.

- ⊕ The proposal complements the fabric of the existing Barrhaven South Community but is laid out to establish a local neighbourhood intimacy.
- ⊕ The design and quality of the architecture

To define quality public and private spaces through development.

- ⊕ The development layout connects the public spaces with the private residential blocks through a network of streets which include pedestrian supportive design, dwellings which directly address the street front with variety and interest and public ROW frontage for the parkland.

To create places that are safe, accessible and are easy to get to, and move through.

- ⊕ The development is well connected through a street network that links easily through the neighbourhood and to adjoining transportation corridors and communities. A common central north-south and east-west street layout makes the neighbourhood easily understandable.
- ⊕ Sidewalk connections and intersection bump-outs allow for pedestrian movement.
- ⊕ Buildings are street oriented and street frontage is substantial on the public space blocks which support 'eyes on the street' for safety and relationships between residents in the community.

To ensure that new development respects the character of existing areas.

- ⊕ The development design integrates well with the existing areas.
- ⊕ The frontage on Chapman Mills Drive continues the housing forms in terms of massing, pattern, rhythm and character expected along this corridor.
- ⊕ The combination of detached and townhouses is consistent with proposed adjacent community to the north.
- ⊕ The fabric of streets and open space is linked to the adjacent developed and future development areas.

To consider adaptability and diversity by creating places that can adapt and evolve easily over time and that are characterized by variety and choice.

- ⊕ The choice of detached and townhouse units provided, combined with public spaces, transit, additional housing types and amenities available in the adjacent developments, provides a contribution to the diverse larger community and supports different housing and transportation choices for residents.

To understand and respect natural processes and features in development design

- ⊕ Servicing supports natural function through maintaining natural catchment areas established drainage patterns.
- ⊕ The adjacent forested area is connected to the parkland.

To maximize energy-efficiency and promote sustainable design to reduce the resource consumption, energy use, and carbon footprint of the built environment.

- ⊕ The provision of residential density along transit corridors and walkable distances to park space and public transportation reduces automobile use.
- ⊕ Restoration of vegetation cover through additional plantings contributes to tree cover and carbon sequestration in urban areas.

5.3.2 Urban Design and Compatibility – Section 4.11

Section 4.11 of the Official Plan provides policy to be used in the evaluation of the compatibility of development application. The policies do acknowledge that determination of compatibility will vary depending on the use proposed and the immediate planning context and that not all criteria

are applicable. The policies of the applicable Community Design Plan have been applied and respected, further, in the context of this application, the following criteria have been considered.

- ⊕ Vehicular Access: The higher density townhouse residential uses are closer to the ultimate Greenbank Road extension and transit corridor, minimizing the potential for traffic infiltration on minor collector roadways and local streets. The road network has sufficient capacity to accommodate the proposed development.
- ⊕ Parking Requirements: The development has adequate on-site parking with a minimum of one space per unit provided. On-street parking supply is achieved through driveway spacing and one side of parking on the collector road. Walking, cycling and transit is promoted through proximity to transit stations and roadway design.
- ⊕ Outdoor amenity space on residential lots is designed to provide privacy to residents through building configuration, structure placement on lots and setbacks.
- ⊕ Lighting, sunlight, microclimate, noise and air quality matters are addressed through the development layout and shall be mitigated, where required, by landscaping and building design.
- ⊕ The development will be well serviced by existing and proposed services and facilities available in the community of Barrhaven south.

Figure 22: BSUEA School Area Plan [CDP Figure 4 -revised]



Parks

A neighborhood park is located to complement the park distribution across the CDP area. In keeping with Section 6.1.4.1, the majority of residential units will be within a five-minute walking distance.

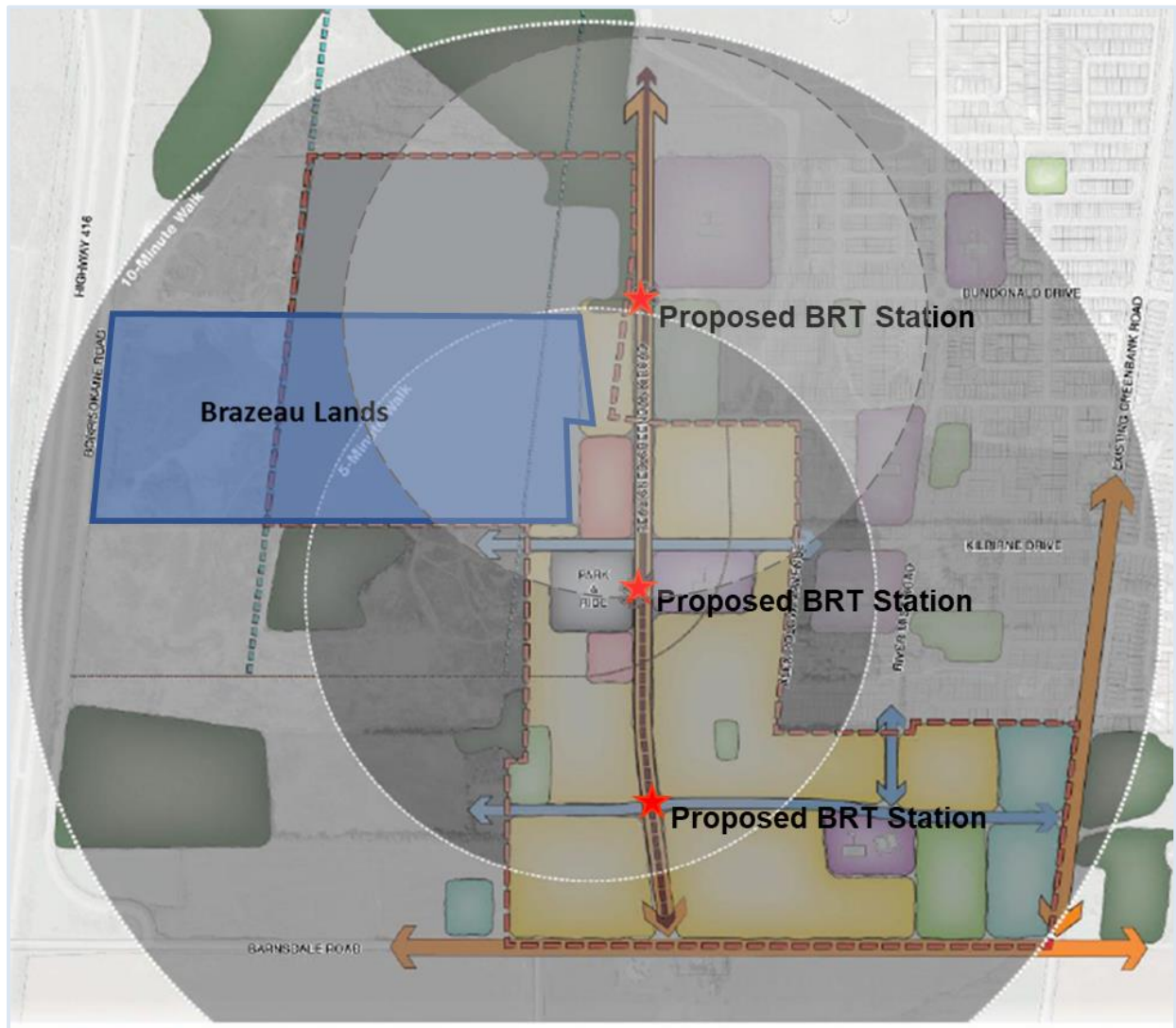
Stormwater Management Facilities

The stormwater management strategy to service the Caivan Brazeau Lands maintains the natural drainage catchment area. Detailed design will follow the direction of Section 6.1.5 and the Master Servicing Study.

Park and Ride and Transit Stations

The realigned Greenbank Road and Transitway Extension will be located in close proximity to the east end of the development. The majority of residents will benefit from a five-minute walk to one of two proposed BRT Stations as per Section 6.1.6.

Figure 23: BSUEA Park and Ride Area Plan [CDP Fig 7 -revised]



5.4.2 Demonstration Plan

The CDP Demonstration Plan reflects the desired connections, layout and land uses. The plan does not provide a preferred local layout for these lands; however, the plan respects the overall vision and intent of the CDP with regard to road and pathway layout, connections and the location of facilities. Figure 24 shows the Caivan Brazeau Lands relationship with the CDP Demonstration Plan, and Figure 25 extends the overlay to include the adjacent developing neighbourhoods of Halfmoon Bay South Phase 4 & 5 and Quinn's Pointe Phase 2. The street network, cycling and pedestrian facilities are in conformity with the direction in the CDP and shall be designed accordingly. See Figures 26 & 27.

Figure 24: BSUEA Demonstration Plan with the Caivan Brazeau Lands Concept Plan [CDP Figure 8 -revised]

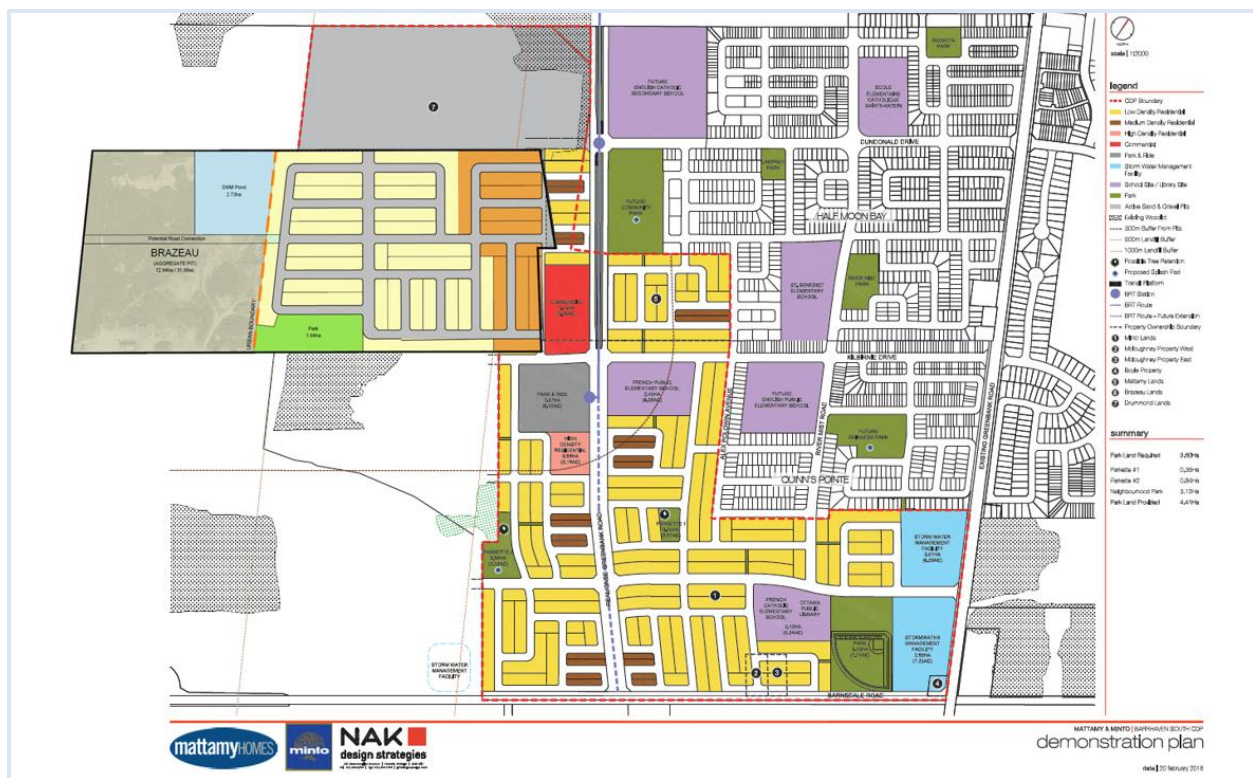
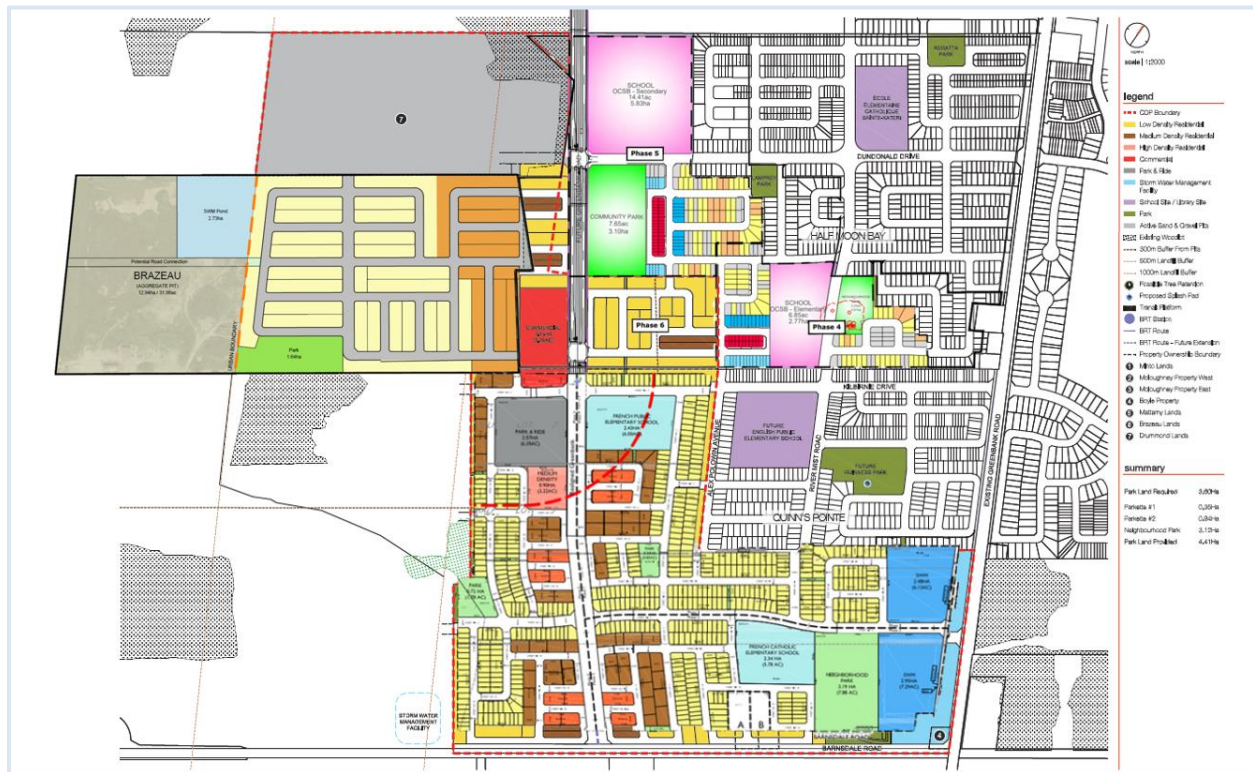


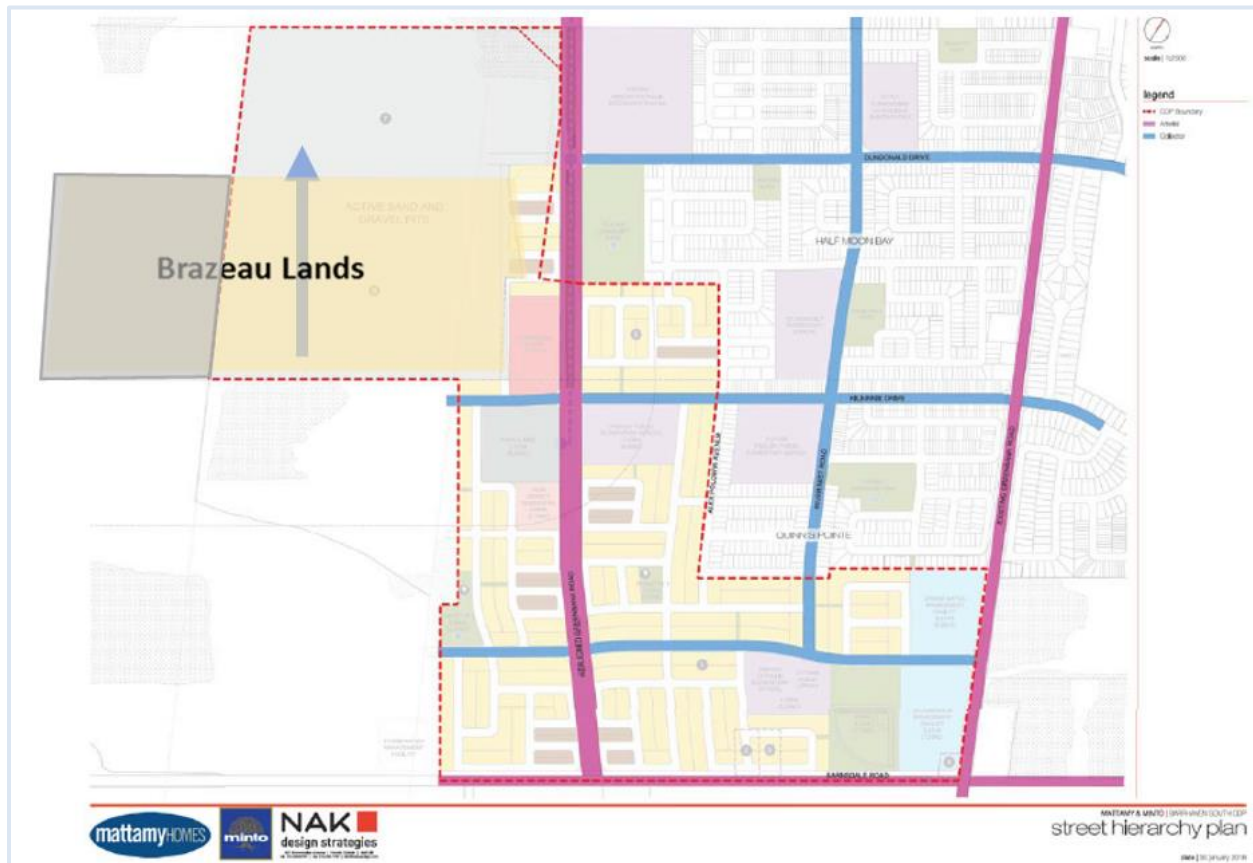
Figure 25: BSUEA Demonstration Plan showing all adjacent developing lands concept plans.



5.4.3 Street Hierarchy Plan

The proposed local roads reflect the modified grid pattern established in the existing community and CDP. The north-south collector street connects the proposed development to adjacent future neighborhoods.

Figure 26: BSUEA Street Hierarchy Plan [CDP Fig 9 -revised]



5.4.4 Cycling and Pedestrian Facilities

The cycling and pedestrian considerations in the CDP are extended through the subdivision lands by way of a cycle track in the north-south collector and sidewalks provided in accordance with direction established in the Ottawa Transporting Master Plan.

Figure 27: BSUEA Cycling Facilities Plan [CDP Fig 12 -revised]

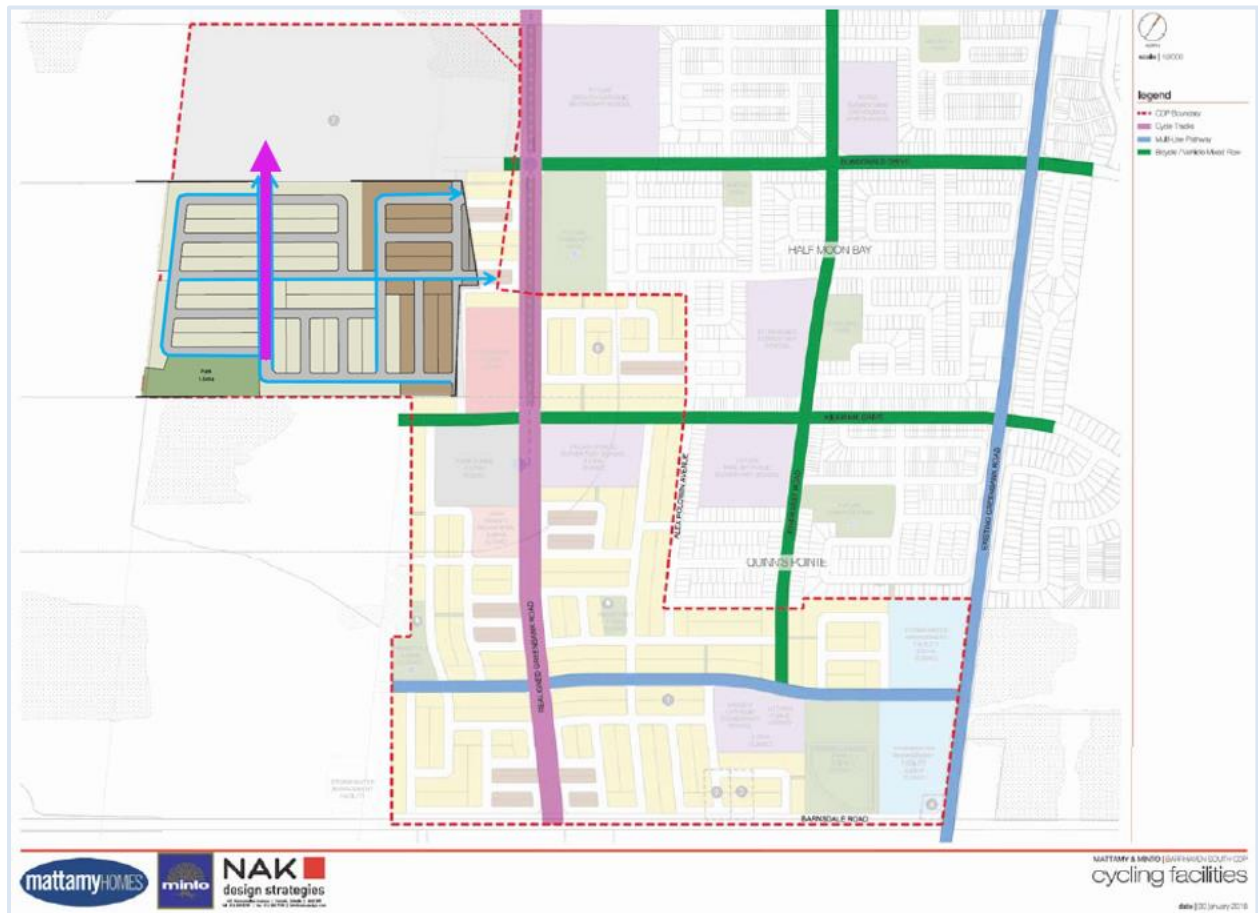
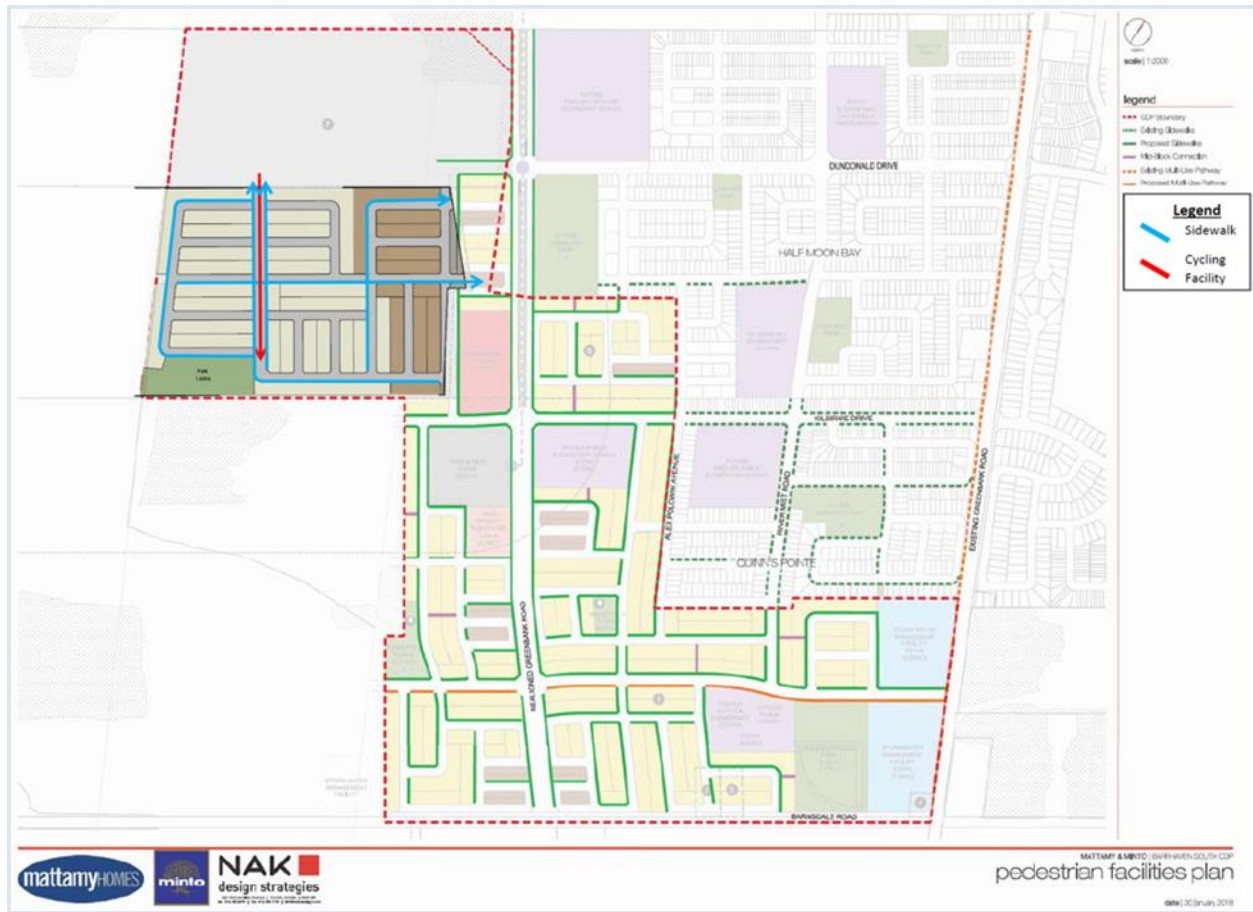


Figure 28: BSUEA Pedestrian Facilities Plan [CDP Fig 10 -revised]

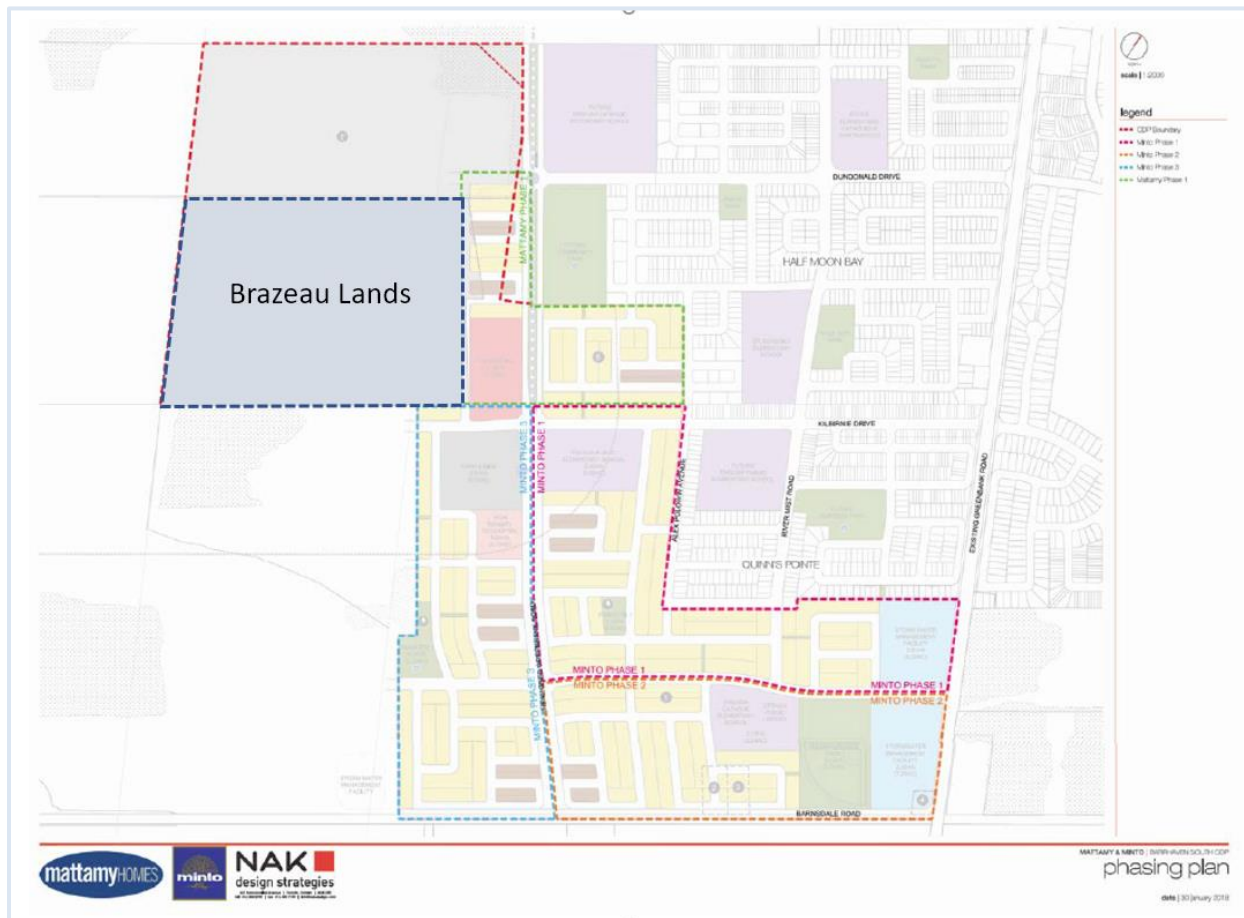


5.5.5 Phasing Plan

The Phasing Plan for the BSUEA will need to be updated to recognize the surrender of the Aggregate License on the Caivan Brazeau Lands and the proposed development coming on line. The subdivision and the servicing are directly adjacent the Phase 1 Mattamy Lands and will access the extension of roads and services from the existing development to the north.

On the lands which are considered by the impact study to be within the influence area surrounding the operating aggregate pit to the north, restrictions and warning clauses may be registered on title until such a time as the operation ceases and the aggregate extraction license surrendered.

Figure 29: BSUEA Phasing Plan [CDP Fig 13 -revised]



5.5.6 Community Design Policies and Guidelines

The proposed Plan of Subdivision has been designed to be consistent with the policies and guidelines established for the BSUEA CDP layout and land uses.

Streetscape Policies:

- ⊕ There is no direct access from the local roads to arterials;
- ⊕ Residential dwellings face the collector road and local streets; and
- ⊕ The design of the collector street is consistent with City guidelines.
- ⊕ Acoustic fencing will be avoided if possible and there are no window streets along the collector street.

Streetscape Guidelines:

- ⊕ The collector street is designed with a 24-metre right-of-way which accommodates bump outs, cycle tracks, sidewalks and other traffic safety measures.
- ⊕ Services and utilities will be designed to limit their obvious presence.
- ⊕ The local street pattern is designed as a fully-connected offset permeable grid. An 18 metre right of way on local streets shall be designed to including sidewalks, street trees and safe crossing points for pedestrians.
- ⊕ Coordination and location of trees, fixtures and other appurtenances will facilitate non-vehicular access and circulation.

Parks Policies:

- ⊕ The Neighbourhood Park is 1.63 hectares in area and shall be designed in consultation with city staff.
- ⊕ Sidewalks adjacent the park along street frontage shall be provided as per the policy direction.
- ⊕ The intersection adjacent the park shall be narrowed to facilitate safe pedestrian crossing.

Stormwater Management Policies:

- ⊕ The stormwater Pond facility shall be generally naturalized and designed to be compatible with adjacent residential land use.

Policies and Guidelines for Linkages and Pathways:

- ⊕ Local street blocks are short to facilitate pedestrian and cyclist access to other parts of the neighborhood and adjacent schools, parks, and transit facilities.
- ⊕ Sidewalks are provided to support pedestrian movement between residential neighbourhoods (existing and future) and parks and schools.

Policies and Guidelines for Residential Site Design and Building:

- ⊕ A variety of housing densities and designs shall be provided to enhance the streetscape.
- ⊕ The proposed zoning standards including setbacks to allow dwellings to be located close to the street and are designed with detailed and varied frontages and nonprojecting garages.
- ⊕ Residential lots are oriented to face the street and corner side yards present articulated frontages to the adjacent streets and reduce fencing edges.
- ⊕ Townhouse elevations and massing shall be varied and end units shall have architectural features incorporated for interest.

- ⊕ Driveways shall be paired where feasible with at least one on-street spot available between each pair.

5.5.6 Implementation

The aggregate pits were brought into the urban area through the implementing Official Plan Amendment for this CDP. A site specific policy, referred to in Section 5.2.2 of this report, allows the land to be integrated into the CDP at the time the license for the operation is surrendered. The pit has ceased operation and is commencing the process for surrendering the license through the Ministry of Natural Resources. However, the license surrender will not be completed by the Ministry until the draft plan is approved, in order to establish the plan as the rehabilitation strategy for the site. No sensitive land uses are proposed within the area of potential impact surrounding the aggregate pit operation to the north.

9.1.2 Major Changes

No major changes to the policies or land use expectations in the CDP are proposed by this development application, however the figures within the CDP must be updated as the land uses are established on the subject property. The amendments to these figures are provided in this planning rational, and formal revision figures can be prepared should they be required.

6.0 Relevant Documents

6.1 Urban Design Guidelines for Greenfield Development

The Urban Design Guidelines for Greenfield Neighbourhoods apply to lands that have not been previously developed and are creating a new neighbourhood of lots fronting on public roads. In this respect the Caivan Brazeau Lands qualify as greenfield development. The guidelines address neighbourhood design relevant to the relationship between adjacent sites or in relation to the public street. They are intended to complement the BSUEA CDP, and have been considered in its creation.

The subdivision design and layout address the following:

- ⊕ The site layout, including grading and drainage considers the soils and adjacent landforms, watercourses and climate in the design of the community.
- ⊕ The layout responds to the adjacent proposed and existing network of parks, stormwater ponds, and community uses through appropriate distribution of parkland and future connections.
- ⊕ Natural features on adjacent lands are supported by maintaining/re-establishing natural drainage patterns and situating the park adjacent the potential retained wooded area
- ⊕ The grading of the site is highly altered due to previous aggregate extraction use. It will be modified to reflect a more natural appropriate topography
- ⊕ The stormwater management pond will contribute to re-establishing a natural area with vegetation as part of the site rehabilitation.
- ⊕ Higher density is located closer to the Greenbank Road extension and proposed transit stations.
- ⊕ Streets are laid out in short blocks with sidewalks to connect across the development and into future adjacent neighbourhoods. The collector street will ultimately be a continuous connection up to Cambrian Road and into the neighbourhoods to the south-east.

- ⊕ The neighbourhood park is located along a collector and local street. It is sized as per direction in the CDP and rectangular shaped with the long edge fronting along the local street.
- ⊕ The proposed zoning supports residential building location and articulation to enhance the relationship with the local streets. Corner lots and end units will present architecturally interesting facades to both front and side streets. The dwelling frontages are not dominated by garages and the wider lots provide space for landscaping and snow storage.
- ⊕ The soils are conducive to tree planting and the collector street is wide enough to accommodate the inclusion of streetscape elements, sidewalks on both sides and utilities.
- ⊕ Streets are designed with bump-outs to mitigate vehicular speeds and improve pedestrian safety.
- ⊕ Cycling routes are clearly defined along the collector road by the inclusion of cycle tracks.
- ⊕ Sidewalks are included in the local road network to provide a safe and universally accessible pedestrian routes.
- ⊕ Transit stops, community mailboxes, and streetscaping amenities shall be located in activity nodes to allow convenient access and encourage relationships between residents.
- ⊕ Utilities shall be located, as much as possible, away from public view lines and incorporated into structural and landscaping features where possible.

6.2 Building Better and Smarter Suburbs (BBSS) and Infrastructure Standards Review (ISR)

The strategic directions and principles established in the Building Better Suburbs initiative are incorporated into the development of the BSUEA CDP. Through this plan of subdivision, the BBSS policy direction and ISR engineering standards are largely reflected in the subdivision servicing and design layout, including:

- ⊕ Improved neighbourhood connectivity through short blocks and a modified grid street network.
- ⊕ Traffic calming and pedestrian priority measures along pedestrian routes.
- ⊕ Cycle tracks on the collector street.
- ⊕ Design considerations to increase supply of on-street parking.
- ⊕ Inclusion of updated stormwater management standards in serving design.

6.3 Parks and Pathways Manual

The park classification system has been applied through the CDP and ultimately translated into the location, shape and size of the proposed neighborhood park. The design of the park, including the tree planting and amenities shall be undertaken in keeping with the manual's direction on process and design, and in consultation with city staff.

7.0 Supporting Studies Summary

The following summarizes the analysis and conclusions/recommendations of various studies undertaken in support of the application.

7.1 Transportation Impact Assessment – dated November 2018 prepared by CGH Transportation Inc.

The assessment reports on the existing conditions and development generated travel demand. It identifies Borrisokane Road as the temporary site access road until the Greenbank Road extension is constructed. A set of recommended design elements are presented including the signalization of the Cambrian and Borrisokane Road intersection due to anticipated heavy turning movements and increase in northbound/southbound volumes through this area. No boundary street improvement are recommended.

The development design establishes the street cross sections, pedestrian and cyclist features and access intersection considerations.

7.2 Functional Servicing Report – David Schaeffer Engineering Ltd.

Water supply: The lands can be serviced by City of Ottawa infrastructure. At detailed design, a watermain analysis will consider any adjacent system expansions and confirmation of any staged infrastructure that may be required.

Sanitary service is to be provided by local sewers along the Greenbank Road extension and existing sewers along Cambrian Road. There is residual capacity in the downstream sewers providing sufficient capacity for peak sanitary flows from the subject property.

Stormwater shall be collected in a new on-site stormwater management facility and outletted to a watercourse receiver at the corner of Cambrian and Borrisokane Roads. Quality and quantity controls shall be designed accordingly. Pre infiltration conditions shall be achieved.

7.3 Geotechnical Investigation – dated December 21, 2018 prepared by Paterson Group

The report anticipates that the proposed buildings will be supported by shallow footings placed over an engineered granular fill pad and an approved fill layer, onto an undisturbed bearing surface. No grade raise limits were identified. High groundwater is anticipated in the stormwater management pond area during initial excavation.

The report also provide recommendation for site grading and preparation, foundation design, seismic considerations, frost protection, excavations, site servicing practices and materials, pavement structure, ground water control, winter construction, landscaping consideration and the stormwater management pond.

7.4 Traffic Noise Feasibility Assessment – dated December 18, 2018 prepared by Gradient Wind.

Results of this study indicate that noise levels due to traffic will range between approximately 46 and 62 dBA during daytime period. The highest roadway noise levels will occur nearest to the Greenbank Road extension. Result indicate that outdoor living areas with this exposure will likely require noise control measures. Mitigation measures along the eastern boundary are technically, economically and administratively feasible.

7.5 Noise Feasibility Study (Stationary Noise Component) – dated December 21, 2018 prepared by Paterson Group.

The report acknowledges that the Costello Pit extraction operations are expected to cease during the later phases of this development. In the interim, the development of residential lots along the northern boundary shall be placed on hold and a berm will be constructed across these lots. A sound barrier is also advised along the western limit of the subdivision. A warning clause on deeds of sale is recommended. Once operations cease, no mitigation measures are required.

7.6 Archeological Resource Assessment – Paterson Group

The report concludes that due to the extensive disturbance of the site from the legacy aggregate extraction operations, no further archeological investigations are required.

7.7 Phase I Environmental Site Assessment – dated December 2018 prepared by Golder Associates Ltd.

The Phase 1 ESA determined that since the site has only been used for quarrying to excavate unconsolidated aggregate, no Phase 2 ESA was recommended for the site. Given the land use of the site was industrial since first developed and that the site will be developed for residential use, the change of property use from less sensitive to more sensitive will warrant the filing of a Record of Site Condition.

7.8 Tree Conservation Report and Environmental Impact Statement – dated December 3, 2018 prepared by Kilgour & Associates Ltd.

The report inventories and assesses the existing natural features on the site and concludes that the proposed development will have no negative impacts to listed SAR or other natural heritage features. Standard mitigation measures are advised.

7.9 Mineral Resource Impact Assessment – dated December 21, 2018 prepared by Paterson Group.

The report concludes that the proposed residential development will not negatively impact the current and future operation of the aggregate resource operation to the north, provided mitigation measures as outlined in the environmental noise study are implemented.

7.10 Integrated Environmental Review

A full Integrated Environmental Review (IER) will be prepared by Kilgour and Associates as a condition of draft approval for this subdivision. The IER will demonstrate that the supporting studies and design of the subdivision meet Section 4.7.1 requirements in the Official Plan and as such consider the environmental features of the site and integrate a design with nature approach.

8.0 Conclusion

Based on the applicable policies and guidelines presented in this report, the draft plan of subdivision represents good land planning and is in the public interest. It is consistent with Provincial Policy Statement intent and objectives and in conformity with Ottawa Official Plan and Community Design Plan for the Area. The introduction of development at this time on the subject site is appropriate.

The proposed zoning by-law implements the regulations required to support development that meets design guideline objectives and sensitivity to the natural environment and adjacent land uses.

Should you have any questions, please do not hesitate to contact me at your convenience.



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