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Van Gaal Lands

1039 Terry Fox Drive

& 5331 Fernbank Road

Planning Rationale and Integrated Environmental Review Statement

VAN GAAL LANDS
1039 TERRY FOX DR. & 5331 FERNBANK RD.
OTTAWA, ON

**PLANNING RATIONALE AND
INTEGRATED ENVIRONMENTAL REVIEW STATEMENT
IN SUPPORT OF A DRAFT PLAN OF SUBDIVISION**

Prepared For:

Claridge Homes

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October 10, 2018

Novatech File: 117198

Ref: R-2018-106

October 10, 2018

City of Ottawa
Planning, Infrastructure and Economic Development Department
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Attention: Laurel McCreight, Planner II

Reference: Van Gaal Lands – 1039 Terry Fox Drive and 5331 Fernbank Road
Draft Plan of Subdivision
Planning Rationale and Integrated Environmental Review Statement
Our File No.: 117198

Novatech has prepared this Planning Rationale and Integrated Environmental Review on behalf of Claridge Homes to support a Draft Plan of Subdivision application for 1039 Terry Fox Drive and 5331 Fernbank Road.

Claridge intends to develop a residential subdivision consisting of single-detached and street-oriented townhouses. All proposed uses conform to the current zoning.

Should you have any questions or comments, please do not hesitate to contact me.

Sincerely,

NOVATECH



Ellen Potts, BES (PI)

Planner

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1.0 | INTRODUCTION

Novatech was retained by Claridge Homes ('Claridge') to prepare a Planning Rationale in support of a Draft Plan of Subdivision to allow for the development of a portion of the lands municipally known as 1039 Terry Fox Drive and 5331 Fernbank Road (the 'Subject Site').

An "Integrated Environmental Review Statement" (IERS), as required under Section 4.7 of the *Official Plan*, also forms part of this Rational. It demonstrates how the findings of the supporting studies influence the design of the proposed development with respect to effects on the environment and compliance with the appropriate policies contained in Section 4 of the *Official Plan*.

2.0 | CONTEXTUAL ANALYSIS

2.1 SUBJECT SITE AND LOCAL CONTEXT

1039 Terry Fox Drive and 5331 Fernbank Road (the 'Van Gaal Lands') extend from Fernbank Road in the south to the Trans Canada Trail in the north. Claridge has purchased a portion of the Van Gaal Lands north of Cope Drive and South of Monahan Drain (the 'Subject Site') for the proposed Plan of Subdivision, as shown in Figure 1.



Figure 1: Subject Site Location (Image Source: Google Maps, 2018)

The Subject Site is generally flat and is currently used as agricultural crop land.

The following uses surround the Subject Site, as shown in Figure 2:

North: The Van Gaal Lands currently extend north of the Monahan Drain to the Trans Canada Trail and are used for cattle grazing but are intended for future employment uses. A hydro corridor cuts through this northern portion in the east-west direction.

East: The Subject Site is bounded by the eastern edge of a closed road allowance. Beyond it are linked-detached dwellings with frontage on Northgraves Crescent in the adjacent Soho West Community.

South: Cope Drive, a two-lane collector road, bounds the Subject Site to the south. The Van Gaal Lands extend south of Cope Drive to Fernbank Road. An application for a shopping plaza is contemplated for this southern portion of the Van Gaal Lands by Smart centres.

Southwest: A Smart Centres plaza (5357 Fernbank Road) is located at the southwest corner of Terry Fox Drive and Cope Drive.

West: Terry Fox Drive, a two-lane arterial road, bounds the Subject Site to the west. The Blackstone residential community is located on the west side of Terry Fox and is within the Fernbank Community Design Plan area.



Figure 2: Surrounding Land Uses (Image Source: Google Maps)

2.2 PLANNING CONTEXT

2.2.1 CITY OF OTTAWA OFFICIAL P5512LAN

Per “Schedule B – Urban Policy Plan” of the *Official Plan*, The Subject Site is designated General Urban Area, as shown in Figure 3. Please see Section 4.2 of this report for a description of how the proposed development meets the applicable policies of the General Urban Area.

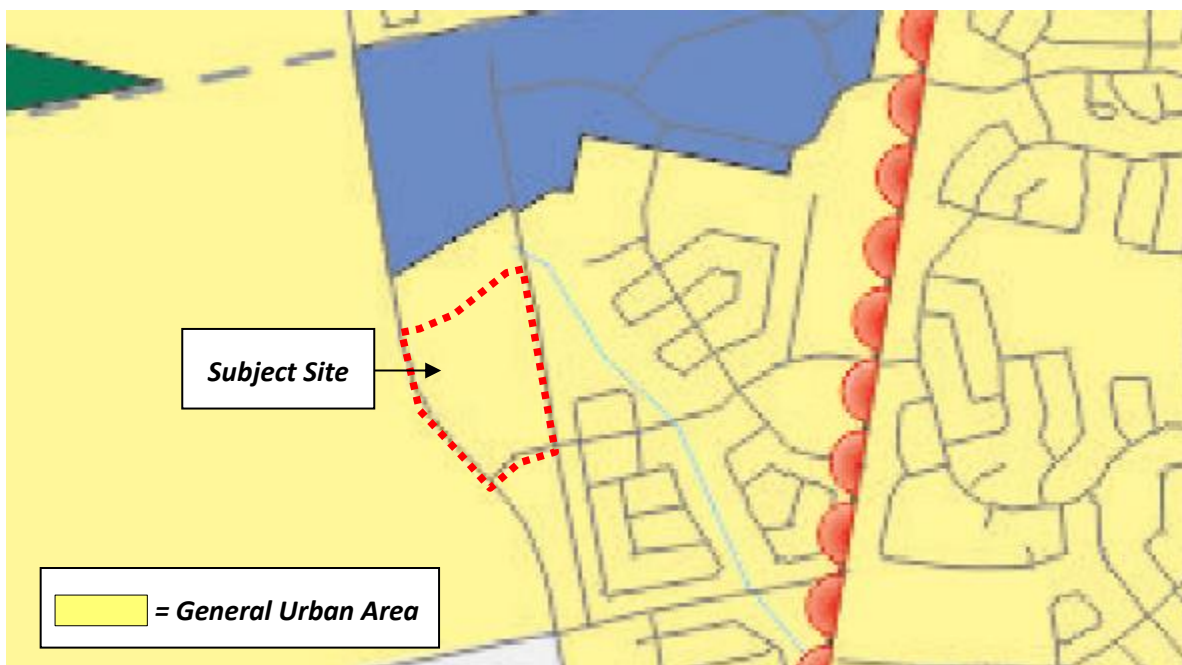


Figure 3: Excerpt of OP Schedule B with Subject Site outlined in red

Per “Schedule C – Primary Urban Cycling Network”, Terry Fox Drive is designated as an on-road cycling route, as shown in Figure 4. A pocket bike lane is provided on the southbound approach to the intersection with Cope Drive.

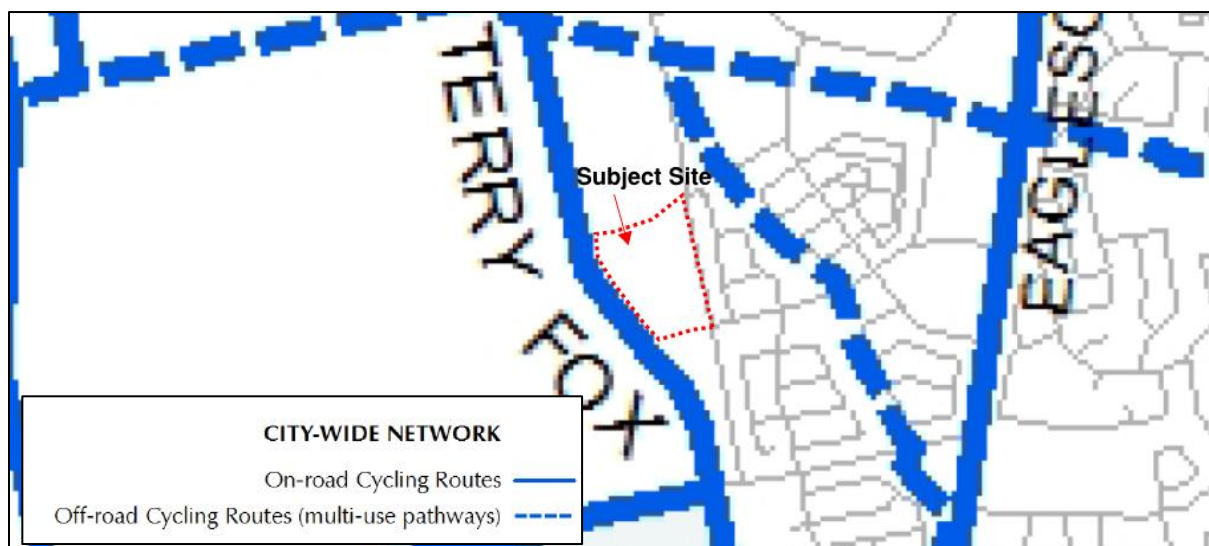


Figure 4: Excerpt of Schedule C with Subject Site outlined in red

Per “Schedule E – Urban Road Network”, Cope Drive is designated as a collector road and Terry Fox Drive is designated as an arterial road, as shown on Figure 5. Sidewalks exist on both sides of Cope Drive. No sidewalks currently exist along Terry Fox Drive, but paved shoulders are provided on both sides.

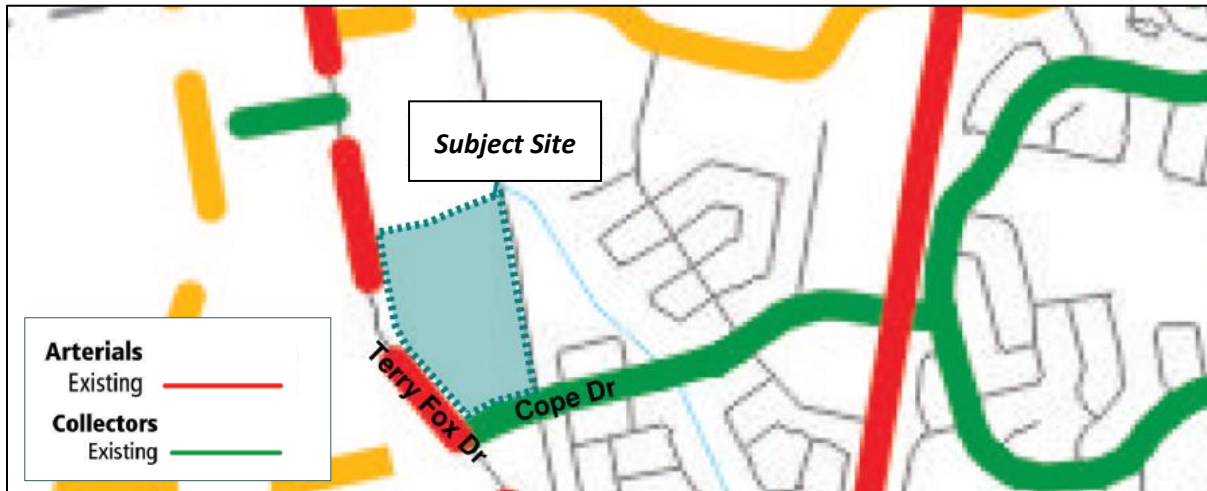


Figure 5: Excerpt of Schedule E - Urban Road Network

Per “Schedule K – Environmental Constraints” in the *Official Plan*, there are no environmental constraints affecting the Subject Site, as shown in Figure 6.

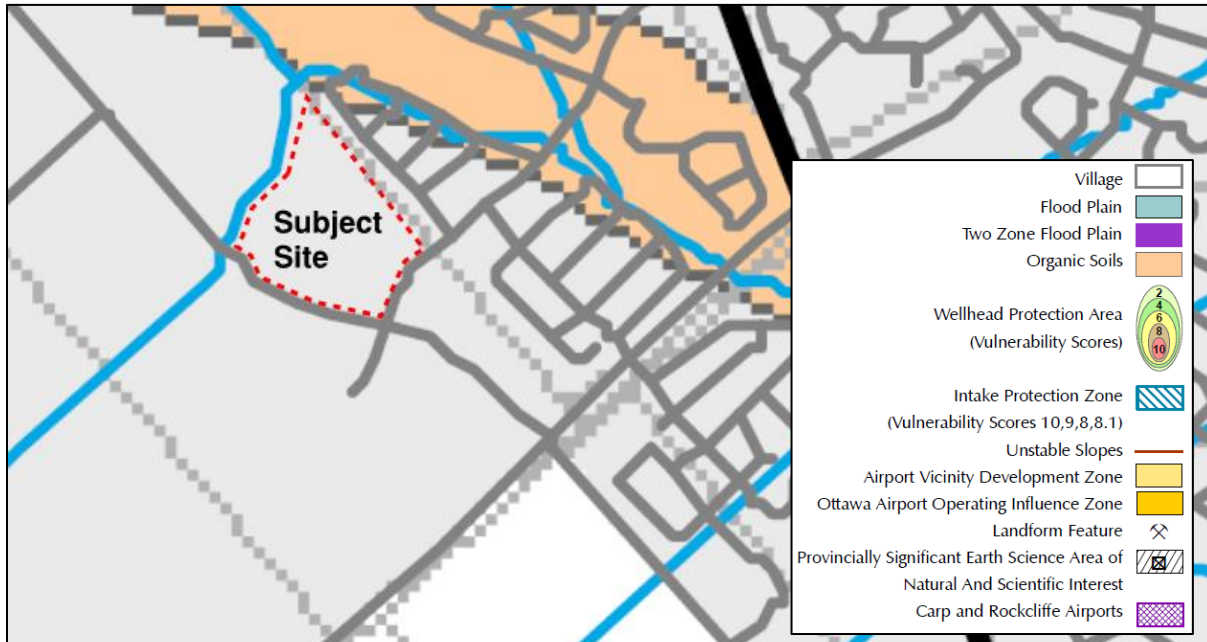


Figure 6: Excerpt from Schedule K showing Subject Site outlined in red

Per “Schedule L3 – Natural Heritage System Overlay (West)”, as shown in Figure 7, no Natural Heritage System Features exist on or adjacent to the Subject Site.

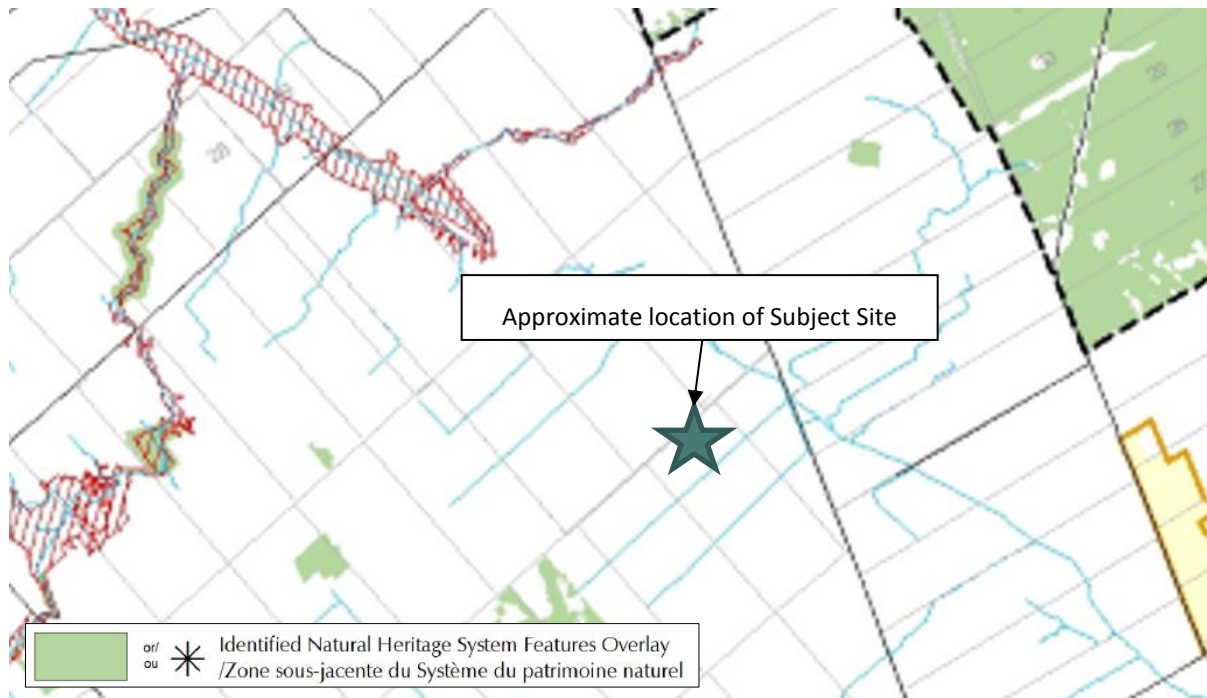


Figure 7: Excerpt from OP Schedule L3 indicating approximate location of Subject Site with a star

2.2.2 ZONING BY-LAW 2008-250

The Subject Site is zoned Residential- R3X[2410]-h, Residential-R4Y[2412]-h and Open Space- O1. A portion of the Subject Site along the Monahan Drain is zoned Environmental Protection - EP3, as shown in Figure 8.

The two residential zones are subject to holding provisions which restrict all forms of development until lifted. They may only be lifted when *it has been demonstrated to the satisfaction of the General Manager of Planning, Infrastructure & Economic Development:*

1. *That the proposed infrastructure for the subject lands being water, sanitary and stormwater management and sewers can be designed in accordance with the City Guidelines and Standards; and*
2. *That the existing infrastructure, including but not limited to the Monahan Drain, has the existing residual capacity to accommodate the subject lands and its proposed land use.*
3. *A Traffic Impact Study has been completed and approved for the subject land and supports the proposed development of the subject lands.*

As part of this Draft Plan of Subdivision application, Novatech prepared a Site Servicing and Stormwater Management Report (Report No. R-2018-116, dated September 7, 2018) and a Transportation Impact Assessment (Report No. R-2018-028, dated September 2018), which are summarized in the IERS in Section 5.0 of this Rationale.



Figure 8: Existing Zoning (Image Source: GeoOttawa, 2018)

3.0 | PROPOSED DEVELOPMENT

The proposed development is generally consistent with the Concept Plan (prepared by Fotenn, dated October 14, 2014) introduced through the 2017 Zoning By-law amendment application, which is included as Appendix A of this rationale. The design is informed by the constraints and opportunities identified in the supporting studies, which are summarized in the IERS in Section 5 of this Rationale. See Appendix B for the proposed Draft Plan of Subdivision.

The proposed development consists of 55 single detached dwellings, approximately 129 street-oriented townhouses, and a 0.91-hectare neighbourhood park in the northeast corner of the Subject Site abutting the Monahan Drain and the closed road allowance.

The public street layout follows a modified grid pattern to facilitate pedestrian and vehicular movement within the proposed subdivision and into the adjacent existing development. A window street along Terry Fox Drive provides an inviting view into the community while minimizing the need for noise attenuation barriers. All streets have a proposed standard right-of-way width of 18 metres except the proposed window street, which has a proposed width of 16.5m. Two vehicular accesses are provided to the Subject Site: a right-in/right-out access along Terry Fox Drive at proposed Street 4 and a full movement access along Cope Drive at proposed Street 1. Sidewalks will be provided on west side of Street 1 and the north side of Street 4.

4.0 | POLICY JUSTIFICATION

The following is a review of the existing planning policy framework. It demonstrates the consistency of the proposed Plan of Subdivision with the *Provincial Policy Statement, 2014* (PPS), the policies of the *Official Plan*, and the applicable urban design guidelines.

4.1 PROVINCIAL POLICY STATEMENT

The PPS provides policy directions on land use planning and development matters of provincial interest, as set out in Section 2 of the Planning Act. All decisions affecting planning matters are required to “be consistent with” the PPS.

Section 1.1 of the PPS speaks to managing and directing land use to achieve efficient and resilient development and land use patterns.

Policy 1.1.1 states that healthy, liveable and safe communities are sustained by promoting efficient land use patterns with a range and mix of uses to minimize land consumption and servicing costs. Further, development patterns should avoid environmental or public health and safety concerns, conserve bio-diversity, and consider the impacts of a changing climate.

The Subject Site is designated as Settlement Area. The proposed development is designed to accommodate a variety of dwelling types and compatible land uses. The proposed uses are arranged in a modified grid street and pathway pattern, integrated with surrounding development to minimize land consumption, promote active transportation, and enhance connectivity. The proposed development will not impact any significant environmental features and does not contain any natural, anthropogenic or potential hazards.

Policy 1.1.3.2 requires land use patterns within settlement areas to be based on:

b) densities and a mix of land uses which:

- 1. efficiently use land and resources;*
- 2. are appropriate for, and efficiently use, the infrastructure and public service facilities which are planned or available, and avoid the need for their unjustified and/or uneconomical expansion;*
- 3. minimize negative impacts to air quality and climate change, and promote energy efficiency;*
- 4. support active transportation;*
- 5. are transit-supportive, where transit is planned, exists or may be developed, and;*
- 6. are freight-supportive.*

The proposed development provides a mix of single-detached and townhouse dwellings that is appropriate for the existing and planned infrastructure and public service facilities in the area and compatible with the existing adjacent development.

Section 1.1.3.6 of the PPS states that new development should occur adjacent to the existing built-up area and allow for the efficient use of land.

The proposed development is located within the City of Ottawa's designated General Urban Area and is adjacent to the Soho West community.

Public Spaces, Recreation, Parks, Trails and Open Space

Policy 1.5.1 states that healthy, active communities should be promoted by planning public streets, spaces and facilities to be safe, meet the needs of pedestrians, foster social interaction and facilitate active transportation and community connectivity.

Streets and pathways have been arranged in a modified grid pattern, connected to existing and planned adjacent developments. This arrangement builds on the network of pedestrian connections to adjacent communities, open spaces and future planned bus and cycling facilities.

Infrastructure and Public Service Facilities

Section 1.6 of the PPS states that infrastructure shall be provided in a coordinated, efficient and cost-effective manner to accommodate projected needs.

The proposed development will be served by extensions of municipal services. The Site Serviceability and Stormwater Management Report (Report No. R-2018-116, dated September 7, 2018), discussed in Section 5.5 of this rationale, demonstrates that there is adequate infrastructure to support the proposed subdivision.

Transportation Systems

Section 1.6.5 of the PPS promotes safe, energy efficient transportation systems that facilitate the movement of people and goods.

The Transportation Impact Assessment (Report No. R-2018-028, dated September 2018), discussed in Section 5.6 of this rationale, addresses interaction with the adjacent community and confirms that the proposed subdivision can be safely and adequately accommodated by the surrounding transportation network.

Wise Use and Management of Resources

Section 2.0 of the PPS speaks to protecting natural heritage, water, agriculture, mineral aggregate, petroleum, cultural heritage and archaeological resources.

Policy 2.1.1 states that *natural features and areas shall be protected for the long term*; and Policy 2.1.2 states that the *diversity and connectivity of natural features in an area, and the long-term ecological function and biodiversity of natural heritage systems, should be maintained, restored or, where possible, improved, recognizing linkages between and among natural heritage features and areas, surface water features and ground water features*.

Significant natural heritage features are not present on or adjacent to the Subject Site, other than the intermittent fish habitat in the Monahan Drain which will be transferred to the City. This Drain is zoned

Environmental Protection and will be protected with a 40-metre wide corridor, as detailed in Section 5.1 of this rationale.

Policy 2.1.6 states that *development and site alteration shall not be permitted in fish habitat except in accordance with provincial and federal requirements.*

The Monahan Drain contains intermittent fish habitat and will be protected from development with a 40-metre wide natural buffer which will be transferred to the City.

Policy 2.1.7 states that *development and site alteration shall not be permitted in habitat of endangered species and threatened species, except in accordance with provincial and federal requirements.*

No habitat of Endangered or Threatened Species exists on or around the Subject Site, as noted in Section 5.1 of this rationale.

Policy 2.1.8 states that *development and site alteration shall not be permitted on adjacent lands to the natural heritage features and areas identified in policies 2.1.4, 2.1.5, and 2.1.6 unless the ecological function of the adjacent lands has been evaluated and it has been demonstrated that there will be no negative impacts on the natural features or on their ecological functions.*

No *natural heritage features* exist on or adjacent to the Subject Property, as noted in Section 5.1 of this rationale.

Section 2.2.1 states that *Planning Authorities shall protect, improve or restore the quality and quantity of water;* and Section 2.2.2 states that *Development and site alteration shall be restricted in or near sensitive surface water features and sensitive ground water features such that these features and their related hydrologic functions will be protected, improved or restored.*

The Monahan Drain will be protected with a 40-metre wide corridor, as detailed in the Stormwater Management Report (Report No. R-2018-116, dated September 7, 2018) and discussed in Section 5.5 of this rationale.

Policy 2.3.1 states that *Prime agricultural areas shall be protected for long-term use for agriculture.*

The Subject Site is designated General Urban Area and not prime agricultural lands.

Policy 2.4.1 states that *Minerals and petroleum resources shall be protected for long-term use.*

The Subject Site is not identified as an area of mineral or petroleum resources.

Policy 2.5.1 states that *Mineral aggregate resources shall be protected for long-term use and, where provincial information is available, deposits of mineral aggregate resources shall be identified.*

The Subject Site is not identified as an area of mineral aggregate resources.

Policy 2.6.1 states that *Significant built heritage resources and significant cultural heritage landscapes shall be conserved.*

The Subject Site is not identified as having areas of significant built heritage resources or significant cultural heritage landscapes.

Health and Safety

Section 3.0 of the PPS provides policies related to reducing the potential public cost and protection of residents from natural or human-made hazards.

Regarding natural hazards, Section 3.1 directs development to generally occur outside of *hazardous sites*, such as those containing unstable soils or unstable bedrock.

As stated in Section 2.2.1 of this rationale, “Schedule K” of the *Official Plan* does not identify any environmental constraints, including unstable slopes, on the Subject Site. Furthermore, the Geotechnical Investigation (Report No. PG4466-1, dated July 12, 2018), discussed in Section 5.3 of this rationale, does not identify the Subject Site as hazardous or unsuitable for development.

Regarding human-made hazards, Policy 3.2.1 states that *Development on, abutting or adjacent to lands affected by mine hazards; oil, gas, and salt hazards; or former mineral mining operations, mineral aggregate operations or petroleum resource operations, may be permitted only if rehabilitation or other measures to address and mitigate known or suspected hazards are under way or have been completed.* Policy 3.2.2 states that *Sites with contaminants in land or water shall be assessed and remediated as necessary prior to any activity on the site associated with the proposed use such that there will be no adverse effects.*

The Phase 1 ESA (dated September 28, 2015 and revised on July 26, 2018), discussed in Section 5.2 of this rationale, confirms that there is no evidence of potential environmental concerns with respect to the Subject Site and that a Phase 2 ESA is not warranted.

4.2 CITY OF OTTAWA OFFICIAL PLAN

As stated in Section 2.2.1 of this Rationale, the Subject Site is designated General Urban Area per Schedule B of the *Official Plan*. Section 3.6.1 of the *Official Plan* contains policies for this land use designation.

Policy 1 states that the *General Urban Area designation permits all types and densities of housing, as well as employment, retail uses, service, industrial, cultural, leisure, greenspace, entertainment and institutional uses.*

Policy 2 states that the *evaluation of development applications, studies, other plans and public works undertaken by the City in the General Urban Area will be in accordance with Section 2.5.1 and Section 4.11.*

OFFICIAL PLAN SECTION 2.5.1

Section 2.5.1 of the *Official Plan* presents Design Objectives in the form of statements which express how the City wants to influence the built environment as the city evolves. The following is a list of the Design Objectives with a description below each regarding the proposed development:

To enhance the sense of community by creating and maintaining places with their own distinct identity.

The proposed development enhances the sense of community maintaining the residential nature of the adjacent Soho West community while providing features that create its own distinct identity from the surrounding land uses.

To define quality public and private spaces through development.

The orientation of the proposed single detached and townhouse dwellings towards the local public streets clearly delineates the public realm from the private amenity spaces.

To create places that are safe, accessible and are easy to get to, and move through.

The proposed development is based upon a modified grid system to promote pedestrian movement through the subdivision and surrounding communities. Buildings will be oriented towards the street to encourage passive surveillance.

To ensure that new development respects the character of existing areas.

The proposed dwellings will be of a consistent quality and character to establish a community identity and sense of coherence. The proposed street layout and widths, pedestrian network, and lot layout are consistent with the adjacent Soho West community.

To consider adaptability and diversity by creating places that can adapt and evolve easily over time and that are characterized by variety and choice. [OMB decision #2649, September 21, 2006]

The proposed development provides a mix of single-detached and townhouse dwellings.

To understand and respect natural processes and features in development design.

The proposed development has been designed with understanding of the existing natural conditions as discussed in the Integrated Environmental Review in Section 5 of this rationale.

To maximize energy-efficiency and promote sustainable design to reduce the resource consumption, energy use, and carbon footprint of the built environment.

The proposed development has been oriented to maximize solar gain to the greatest extent possible.

OFFICIAL PLAN SECTION 4.11

In accordance with Section 4.11 of the *Official Plan*, development applications will be evaluated against compatibility criteria in policy 2 of this section to measure their appropriateness within the surrounding local context. The following is a compatibility analysis of the proposed development based on these criteria:

Traffic

The proposed development is adequately served by the existing public roads, as determined in the Traffic Impact Assessment that was prepared as part of this development application and discussed in Section 5.3 of this rationale.

Vehicular Access

Vehicular access to the Subject Site will be provided via a right-in/right-out access along Terry Fox Drive at Street 4, approximately 240 metres north of Cope Drive and a full movement access along Cope Drive at Street 1, approximately 140 metres east of Terry Fox Drive and 80 metres west of Northgraves Crescent.

Parking Requirements

The previously approved zoning for the proposed single-detached and townhouse dwellings ensure that each dwelling unit can accommodate two cars, one in the garage and one in the driveway, without encroaching on the public sidewalk.

Outdoor Amenity Areas

The proposed lots south of Street 4 and east of Street 1 which abut the existing lots along Northgraves Crescent are oriented so that proposed rear yards abut the existing rear yards thereby respecting the privacy of these existing private outdoor amenity areas. Similarly, the proposed park is adjacent the to remaining existing rear yards along Northgraves Crescent which provides additional buffering between the private amenity areas and the proposed residential dwellings.

Loading Areas, Service Areas, and Outdoor Storage

The proposed development does not include a loading area. Garbage and recycling bins are screened with an enclosure and situated at the back of the Subject Site away from adjacent residential dwellings.

Lighting

The potential for light spill-over or glare from proposed lighting sources is mitigated by the orientation of the proposed streets and dwelling units.

Noise and Air Quality

The nature of the proposed use (i.e. residential) avoids the production of significant adverse effects related to noise, odours, and emissions; nevertheless, the design of the proposed subdivision is such that any amount of adverse effects on the adjacent residential development are minimized through the orientation of the proposed streets and dwellings and the inclusion of parkland and street streets.

Sunlight

The proposed height of the proposed two-storey residential dwellings is consistent with the adjacent two-storey residential dwellings along Northgraves Crescent. Furthermore, the proposed dwellings are setback from the rear yards of the existing dwellings to reduce any potential shadowing.

Microclimate

The proposed development will not produce adverse effects related to wind, snow drifting and temperature on the adjacent residential development.

Supporting Neighbourhood Services

The proposed development is adequately served by existing and future services and amenities.

4.3 BUILDING BETTER AND SMARTER SUBURBS GUIDELINES

The *Building Better and Smarter Suburbs: Strategic Directions and Action Plan* was approved by Planning Committee in March 2015. This document is part of a larger initiative and action group working to address the challenge of supporting land efficiency and functionality in new suburban subdivisions, while at the same time improving urban design and long-term cost effectiveness. The document considers broad, strategic planning and design issues that contribute to the quality, safety, and functionality of a community.

Street Network and Land Use Objectives

The proposed subdivision meets the following objectives for Street Network and Land Use as set out in the BBSS Strategic Directions and Action Plan:

- *Implement a network of street typologies that complements the land uses, densities and built form within a community.*
- *Create a highly connected street and block pattern with short blocks to support efficient routing of transit, short distances to transit stops and stations, and intuitive wayfinding.*
- *Design the street network to respond to and respect natural and cultural features.*
- *Design the street network to enhance access to public facilities and services; prioritize pedestrian and cycling access for short trips, and walking/cycling connections to transit stations and Park and Ride lots for longer distance trips.*
- *Integrate the street network with the park and open space system.*
- *Design the street network and block lengths to include a diversity of routes for vehicular and active transportation in order to minimize bottleneck locations.*
- *Reduce vehicle operating speeds, particularly on local streets, in order to improve safety by reducing vehicular and pedestrian/cyclist conflicts.*

- *Create a street system that promotes passive traffic calming and includes traffic calming features built in to the initial designs for local and collector roads.*

The proposed subdivision creates a connected street and block pattern based on a modified grid pattern with short blocks supporting efficient and intuitive wayfinding. Sidewalks will support active transportation and contribute to the walkability of the area. Street trees and on-street parking will provide passive traffic calming.

Parks and Open Space Objectives

The proposed subdivision meets the following objectives for Parks and Open Space as set out in the BBSS Strategic Directions and Action Plan:

- *Achieve an accessible, connected and safe network of open spaces.*
- *Aim to achieve an urban tree canopy linking the green space system.*
- *Provide access to a range of parks and open space features within reasonable walking distances.*

A neighbourhood park is proposed in the northeast corner of the proposed development. The park location was approved through the previous Zoning By-law Amendment process and has frontage on one local street and open space along the Monahan Drain. A T-shaped intersection will terminate Street 1 at the park, providing a green vista along the street. Specific tree locations and species will be determined through detailed design.

Stormwater Management

The proposed subdivision meets the following objectives for Stormwater Management as set out in the BBSS Strategic Directions and Action Plan:

- *Re-consider use of parks and open space areas for emergency storage and conveyance of stormwater.*
- *Plan and design beautiful stormwater management ponds that are integrated into the open space system.*

Details of the proposed stormwater management approach are discussed in the Site Servicing and Stormwater Management Report (Report No. R-2018-116, dated September 7, 2018). See Section 5.5 of this rationale for a summary.

Parking

The proposed subdivision meets the following objectives for Parking as set out in the BBSS Strategic Directions and Action Plan:

- *Accommodate two cars per ground-oriented dwelling (one in-garage and one in-driveway in single-detached, semi-detached and townhouse units with driveways) while ensuring the visual predominance of front entrances and the inhabited parts of the residence.*
- *Minimize the potential for conflicts between sidewalk users and vehicles in driveways.*
- *Minimize driveway widening and lot area dedicated to driveways in order to maximize space for tree planting, landscaping, and stormwater retention.*
- *Use on-street parking as a traffic calming measure on streets already wide enough to accommodate on-street parking.*

The previously approved zoning for the proposed single-detached and townhouse dwellings ensure that each dwelling unit can accommodate two cars, one in the garage and one in the driveway, without encroaching on the public sidewalk. Where possible, driveways are paired, leading to single car garages to maximise the visual prominence of entrances and maximise the space available for street parking, landscaping and tree planting. On-street parking will be provided where possible between paired driveways.

Road Right-of-Way

The proposed subdivision meets the following objectives for Road Right-of-Way as set out in the BBSS Strategic Directions and Action Plan:

- *Balance the needs of all elements within the street right-of-way.*
- *Create complete streets that accommodate all modes of transportation.*
- *Create street environments that enhance safety and livability.*
- *Create beautiful tree-lined streets as a key component of the public realm.*

The street rights-of-way have been designed to accommodate all required streetscape elements including travel lanes, on street parking, sidewalks, landscaping and utilities. All streets within the development will use standard City street cross-sections for 18-metre or 16.5-metre window streets. Streetscape plans will be prepared at the detailed design stage.

Trees

Details of species and tree location will be determined at the detailed design stage in accordance with the City's *Tree Planting in Sensitive marine Clay Soils – 2017 Guidelines* and the following objectives for Trees as set out in the BBSS Strategic Directions and Action Plan:

- *Ensure sufficient space for healthy trees in the ROW.*
- *Expand the urban forest and enhance its biodiversity.*
- *Select appropriate tree species for the local environment.*

- *Achieve suitable conditions to ensure mature tree development.*

Utility Placement

The proposed subdivision meets the following objectives for Utilities Placement as set out in the BBSS Strategic Directions and Action Plan:

- *Balance the needs of all elements within the street right-of-way.*

The proposed road cross-sections are designed consistent with the City's standard 18-metre and 16.5-metre right-of-way cross-sections.

As demonstrated above, the proposed subdivision has been designed to support the objectives of the Building Better and Smarter Suburbs guidelines. These will continue to be considered through the detailed design process.

5.0 INTEGRATED ENVIRONMENTAL REVIEW STATEMENT

Section 4.7 of the Official Plan states that *design components will be considered as inputs into the development process and must be assessed and considered prior to establishing an initial design or lot pattern*. The intention is to ensure that development design complies with the environmental policies contained in Section 4, and that the principles of 'design with nature' have been applied.

The following technical have been prepared in accordance with Section 4.7 of the Official Plan and per the City's recommendations at the pre-consultation meeting held on February 6, 2018.

5.1 ENVIRONMENTAL IMPACT STATEMENT AND TREE CONSERVATION REPORT

An Environmental Impact Statement and Tree Conservation Report (EIS/TCR), dated September 28, 2015 was prepared by Muncaster Environmental Planning Inc. for the previous Zoning By-law Amendment application. The report was updated for the Draft Plan of Subdivision on July 26, 2018. It determines the features and functions of the on-site and adjacent natural environment and assesses the anticipated impacts associated with the proposed development.

As in 2015, the report concludes that there are no Species at Risk or significant Natural Heritage features, as defined in the *Provincial Policy Statement, 2014* (PPS), on or adjacent to the site excluding the intermittent fish habitat in the Monahan Drain. Please see the 2015 and 2018 reports for details.

5.2 PHASE 1 ENVIRONMENTAL SITE ASSESSMENT

Section 4.8.4.2 of the *Official Plan* requires that all applications for plans of subdivision be supported by a Phase 1 Environmental Site Assessment (Phase 1 ESA) completed in accordance with Ontario Regulation 153/04.

Paterson Group was retained by Claridge to prepare a Phase 1 ESA (Report No. PE4263-1, dated May 8, 2006) to investigate the potential for site contamination by current or past uses. The assessment concluded that there is no evidence of potential environmental concerns with respect to the Subject Site and that a Phase 2 Environmental Site Assessment is not warranted.

5.3 GEOTECHNICAL INVESTIGATION

A Geotechnical Investigation (Report No. PG4466-1) was prepared by Paterson Group Inc. dated July 12, 2018 to determine the subsoil and groundwater conditions at the Subject Site and to provide geotechnical recommendations for the design of the proposed development. Generally, the soil conditions consist of topsoil overlaying a thin silty sand layer followed by a deep silty clay deposit. The depth to bedrock is expected range from 25 to 50 metres. The silty clay deposits are classified as low to medium plasticity (i.e. does not exceed 40% plasticity). The report supports the proposed development and provides several options for how the proposed development can proceed on the Subject Site. Please see the full report for details.

5.4 NOISE FEASIBILITY REPORT

A Noise Impact Feasibility Report (Report No. R-2018-118, dated September 7, 2018) was prepared by Novatech. The report concludes that noise attenuation measures – including noise barriers, forced air ventilation, air conditioning units, and a building façade analysis – will be required for some units along Terry Fox Drive and Cope Drive including proposed lots 31 to 36 and 37 to 44, and blocks 57 (portion of block), 58, 60, 61, and 67 (portion of block). Please see the report for details.

5.5 SITE SERVICEABILITY AND STORMWATER MANAGEMENT REPORT

Novatech prepared a Servicing and Stormwater Management Brief (Report No. R-2018-116, dated September 7, 2018) in support of the Draft Plan of Subdivision application for the Subject Site. The report concludes that the proposed development can be adequately serviced by the proposed sanitary sewers and watermain; the Monahan Drain stormwater management facility will provide the necessary quantity and quality control for stormwater; and erosion and sediment control measures will be implemented before construction starts and will remain until vegetation is established.

5.6 TRANSPORTATION IMPACT ASSESSMENT

A Transportation Impact Assessment (Report No. R-2018-028, dated September 2018) was prepared by Novatech in support of the Draft Plan of Subdivision application for the Subject Site. This report reviewed the design elements along the Terry Fox Drive and Cope Drive boundary streets and included the proposed accesses to the Subject Site and the intersection of Terry Fox Drive and Cope Drive. Based on analyses of the weekday AM and PM peak traffic hours (which represents the ‘worst-case’ combination of site-generated traffic and adjacent street traffic), the report demonstrates that the proposed development can be safely and adequately accommodated at this location. Please see the Executive Summary in the TIA for a summary of the general conclusions and recommendations.

6.0 | CONCLUSION

It is our assessment that the proposed development is consistent with the PPS and conforms to the City of Ottawa Official Plan and other relevant guidelines.

This planning rationale, along with the associated technical studies, supports the development of the proposed subdivision. The mix of single-detached and townhouse dwelling units is compatible with existing and planned uses for the area, and functions well within the surrounding context.

The proposed development is an appropriate and desirable addition to the community and represents good planning.

Sincerely,

NOVATECH

Prepared by:



Ellen Potts, BES (PI)

Planner

Reviewed by:



Greg Winters MCIP, RPP

Senior Project Manager

Planning and Development

Appendix A: Concept Plan



OTHER FUTURE DEVELOPMENT

OWNER(S) DEVELOPMENT

EMPLOYMENT AREA (including Optional School Site): 13.16 ha
PARK: 0.83 ha
RESIDENTIAL: 7.54 ha
COMMERCIAL: 3.95 ha
TOTAL: 25.49 ha

NOTE: IN ORDER TO CALCULATE TOTAL LAND AREA, LAND USE AREAS ARE MEASURED TO CENTRELINE OF ROADS WHERE APPLICABLE (FOR EXAMPLE, THE PARK AREA NOTED AS 0.83 INCLUDES A PORTION OF THE ROAD. THE AREA OUTSIDE THE ROAD ROW IS 0.75 ha, AS NOTED ON THE PLAN).

EMPLOYMENT AREA MUST BE MIN. 25.49 x .5 = 12.75 ha
EMPLOYMENT AREA NORTH OF DRAIN* = 13.16 ha

*Does not include Hydro Corridor

No.	REVISION	DATE	BY

PROJECT
VAN GAAL LANDS
TERRRY FOX DRIVE, OTTAWA

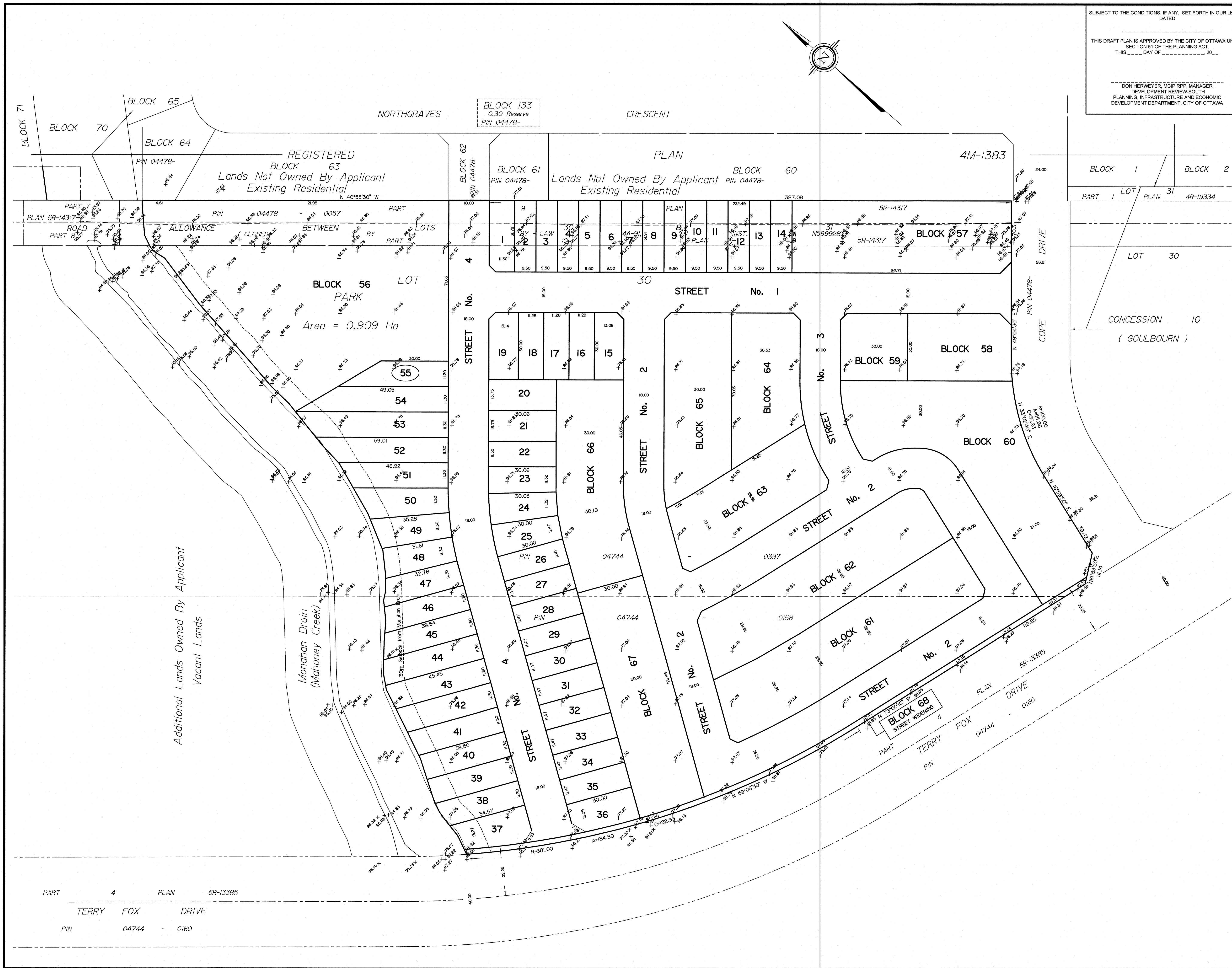
DRAWING
**PRELIMINARY
CONCEPT PLAN -
REVISED**

FOTENN PLANNING &
URBAN DESIGN
223 McLeod Street Ottawa ON K2P 0Z3
Tel: 613 730 5709 Fax: 613 730 1136
www.FoTenn.com

DESIGNED	DB	DRAWING No.
DRAWN	DB	
DATE	2014.10.14	
SCALE	1:2000	

S2

Appendix B: Draft Plan of Subdivision



SUBJECT TO THE CONDITIONS, IF ANY, SET FORTH IN OUR LETTER DATED _____

THIS DRAFT PLAN IS APPROVED BY THE CITY OF OTTAWA UNDER SECTION 51 OF THE PLANNING ACT. THIS _____ DAY OF _____ 20____.

DON HERVEYER, MCIP RPP, MANAGER
DEVELOPMENT REVIEW-SOUTH
PLANNING, INFRASTRUCTURE AND ECONOMIC
DEVELOPMENT DEPARTMENT, CITY OF OTTAWA

KEY MAP
NOT TO SCALE

DRAFT PLAN OF SUBDIVISION OF
PART OF LOT 30
And
PART OF THE ROAD ALLOWANCE
BETWEEN LOTS 30 And 31
(As Closed by By-Law 44-99, Inst. N599928)
CONCESSION 10
Geographic Township of Goulbourn
CITY OF OTTAWA
Prepared by Annis, O'Sullivan, Vollebakk Ltd.

Scale 1 : 750
30 22.5 15 7.5 0 15 30 Metres

Metric
DISTANCES SHOWN ON THIS PLAN ARE IN METRES AND
CAN BE CONVERTED TO FEET BY DIVIDING BY 0.3048

SURVEYOR'S CERTIFICATE

I CERTIFY THAT:
The boundaries of the lands to be subdivided and their relationship to
adjoining lands have been accurately and correctly shown.

Oct 11/2018
Date
Andre Roy
ONTARIO LAND SURVEYOR

OWNER'S CERTIFICATE

This is to certify that we are the agents for the owners of the lands to be subdivided and
that this plan was prepared in accordance with our instructions.

Date
Jim Burghout
CLARIDGE HOMES INC.
I have the authority to bind the corporation.

ADDITIONAL INFORMATION REQUIRED UNDER
SECTION 51-17 OF THE PLANNING ACT

(a) see plan
(b) see plan
(c) see plan
(d) multi-family residential housing
(e) see plan
(f) see plan
(g) see plan
(h) City of Ottawa
(i) see soils report
(j) see plan
(k) sanitary, storm sewers, municipal water, bell, hydro, cable and
gas to be available
(l) see plan

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