



## **788 March Road**

Planning Rationale Addendum  
Site Plan Control  
May 29, 2020



Prepared for 10731854 Canada Inc.

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## Introduction

Fotenn Consultants Inc. have been retained by 10731854 Canada Inc. to submit a Site Plan Control application for the lands municipally known as 788 March Road (“the subject property”), located at the southeast corner of March Road and Klondike Road, in the Kanata North community of the City of Ottawa.

In support of the above application, Fotenn prepared a Planning Rationale, submitted and dated August 2018. The purpose of the original Planning Rationale was to evaluate the proposed development with respect to the applicable policy and regulatory framework and determine if the development is appropriate for the site and compatible with the existing and planned function of the broader area.

The Site Plan Control Application for the subject property (file no. D07-12-18-0128) was submitted in August 2018 and was deemed complete in October 2018 and put on technical circulation. The Ward Councillor held a Town Hall meeting in September 2018, where Fotenn presented the proposed development, sought input, and answered questions from members of the public.

A first round of technical circulation comments was received from the City on October 25, 2018. A resubmission followed in December 2018 and January 2019 in response to the first round of technical circulation comments. A second round of technical circulation comments was received from the City on March 29, 2019.

Fotenn also submitted a Minor Variance Application for the proposed development in December 2018. The Minor Variance Application was submitted to seek relief from Rear Yard Setback, Minimum Visitor Parking Space Rates, and Maximum Building Height provisions in the Zoning By-law. As a result of significant revisions to the original plans, Fotenn requested that the Minor Variance Application be adjourned *sine die*, which was obtained at the Committee of Adjustment hearing on February 6, 2019.

The revisions to the proposed development were discussed with City staff in December 2019. Because the revised proposal is significantly different from the original Site Plan, City planning staff will be recirculating the proposed development material to all technical agencies.

10731854 Canada Inc. is proposing the development of two (2) apartment buildings on the subject property. The apartment buildings will be intricately connected, sharing a small, two (2) storey central entrance block with communal amenity space and access to both buildings; an outdoor surface parking lot; and an indoor parking garage. The low-rise apartment buildings will each be four (4) storeys in height and will provide a total of 92 dwelling units.

The revised plan differs from the previous design and proposes a different building and parking configuration, recognizing the shallow depth of the development lot, the environmental constraints and opportunities for creek frontage for the units, market considerations for the units and building and to respond to previous technical circulation comments.

The following is an Addendum to the submitted Planning Rationale. The Addendum is not intended to replace the original Rationale, but rather itemizes significant revisions to the proposed development and reviews its conformity to applicable policy and its compliance with applicable zoning provisions.

Although the revised proposal generally meets the provisions of the General Mixed Use Zone – GM zoning, minor variances will be required for two (2) landscape buffers and drive aisle and driveway widths. A separate application will be submitted to the Committee of Adjustment, with a supporting planning rationale.

## Revisions to the Proposed Development

Since the submission of the original proposal, the property owner has re-examined the scale, unit mix, and specific building and parking layout of the proposed development.

As demonstrated on the Site Plan and Elevation drawings prepared by Project1 Studio (Figure 1), the proposed development has been revised from two (2) six (6) storey mid-rise apartment buildings to two (2) four (4) storey low-rise apartment buildings. The proposed development has also been altered to recess the apartment buildings to the rear of the developable land on-site in order to offer greater proximity to Shirley's Brook, which runs along the eastern portion of the subject property. The relocation of the proposed buildings closer to Shirley's Brook will provide enhanced views of the creek corridor and related vegetation, and reduce nuisances generated by traffic noise and pollution along March Road.

Whereas the previous proposal consisted of 196 dwelling units (ranging from one (1) bedroom to three (3) bedrooms), the revised proposal will provide a total of 92 dwelling units, broken down as follows:

- / Two (2) bachelor units;
- / 44 one (1) bedroom units; and,
- / 46 two (2) bedroom units.

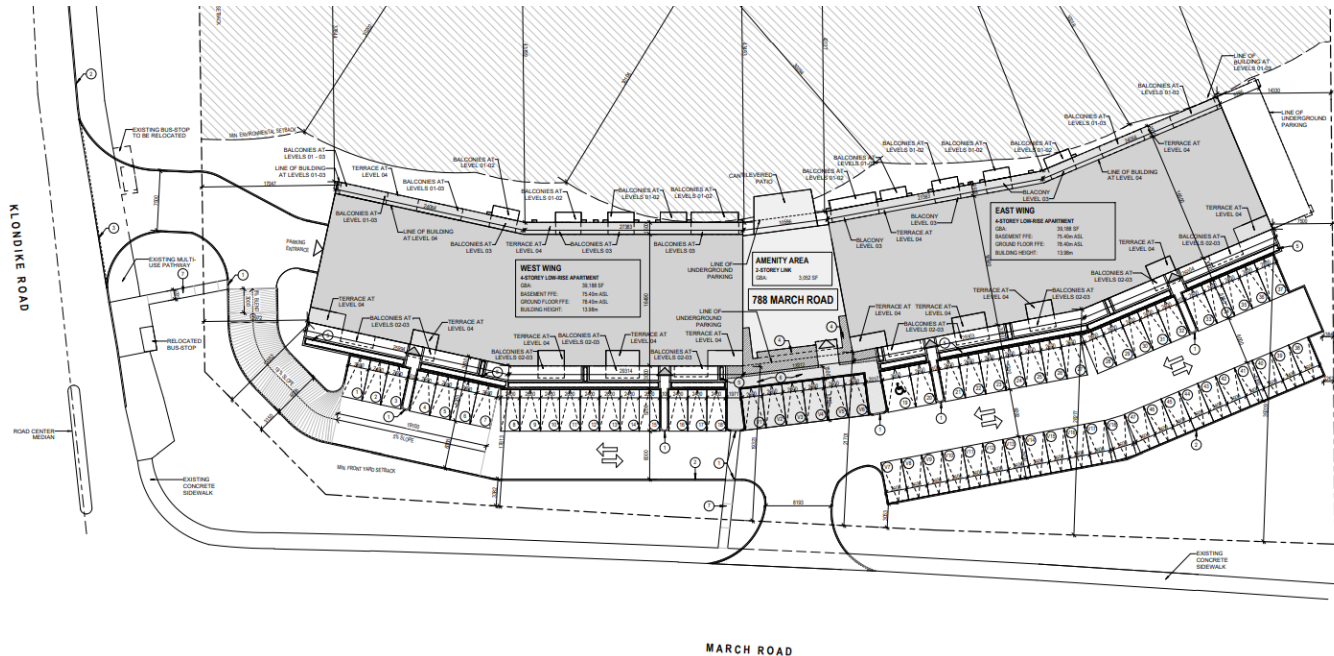


Figure 1: Excerpt from the Site Plan of the proposed development, prepared by Project1 Studio, dated May 12, 2020

A with the previous proposal, two (2) vehicular accesses to the subject property continue to be proposed:

- / The first is at the north of the subject property, from Klondike Road. The intersection of Klondike and March Roads is a signalized full-movement intersection. The driveway from this access into the subject property will lead to an underground parking garage as well as the surface parking lot located between the proposed building and March Road.

- / The second is a right-in/right-out access from March Road. The access is centrally located between the two (2) phases of the development. Both the surface parking lot and the underground parking garage will be accessible to and from this second entrance.

The previous proposal's accesses from March and Klondike Roads separately led to the visitor surface parking lot and the underground parking garage, respectively. The revised proposal's vehicular circulation is more flexible, as it now provides access to the underground parking garage and surface parking lot from both parking accesses.

Pedestrian access to the proposed development will be provided via a walkway and crosswalk connecting the building's main entrance to a sidewalk along March Road. An additional walkway to the north will provide pedestrian access between the subject property and a bus stop along Klondike Road. A pedestrian pathway is also proposed to be constructed at the rear of the proposed development, where it will connect to an existing pathway network to the south.

A total of 128 parking spaces will be provided part of the revised development. A surface parking lot between March Road and the apartment buildings will provide access to 65 surface parking spaces, with the balance (63 spaces) located in a one (1) storey underground parking garage, available from the north side of Building A. The proposed provision of 128 parking spaces complies with the minimum parking rate requirements for resident and visitor parking. A portion of the parking lot is single-loaded, providing opportunities for enhanced landscaping along March Road to adequately screen the surface parking area. Adequate bicycle parking, storage lockers, and garbage rooms servicing the proposed development will also be located in the underground parking garage.

The proposed development will provide a variety of communal and private amenity areas. Communal amenity areas including a lounge and a fitness area will be provided in the two (2) storey entrance pavilion, as well as in front and at the rear of the amenity building. In particular, a partially cantilevered terrace at the rear of the building will provide quality views of the abutting creek corridor. Private amenity spaces will be provided in the form of a mix of ground floor terraces and balconies.

The first three (3) storeys of the proposed development will be finished with red brick, where as the fourth storey will be finished with dark grey fibre cement panels, which will help break up the building's massing. The fourth storey will also be stepped back slightly from the north (on the side of the buildings abutting the creek corridor) and its balconies on the south face of the development will be flush with the main building wall, again helping to mitigate massing concerns.

Due to the significant grade difference between the proposed development's location on the subject property and its frontage abutting March Road, the visual impact of the building from viewpoints across March Road is lessened. The grade difference and the implementation of landscaping along the subject property's frontage on March Road will also help screen the surface parking lot from the public right-of-way and other viewpoints.

The proposed development will be set back 30 metres from the normal high-water mark of Shirley's Brook, and existing vegetation within the 30 metre setback is proposed to be retained. This setback will ensure the preservation of Shirley's Brook and its riparian corridor and mitigate any flooding concerns.





Figure 2: Excerpt from the South Elevation – West Wing drawing of the proposed development (as seen from March Road)

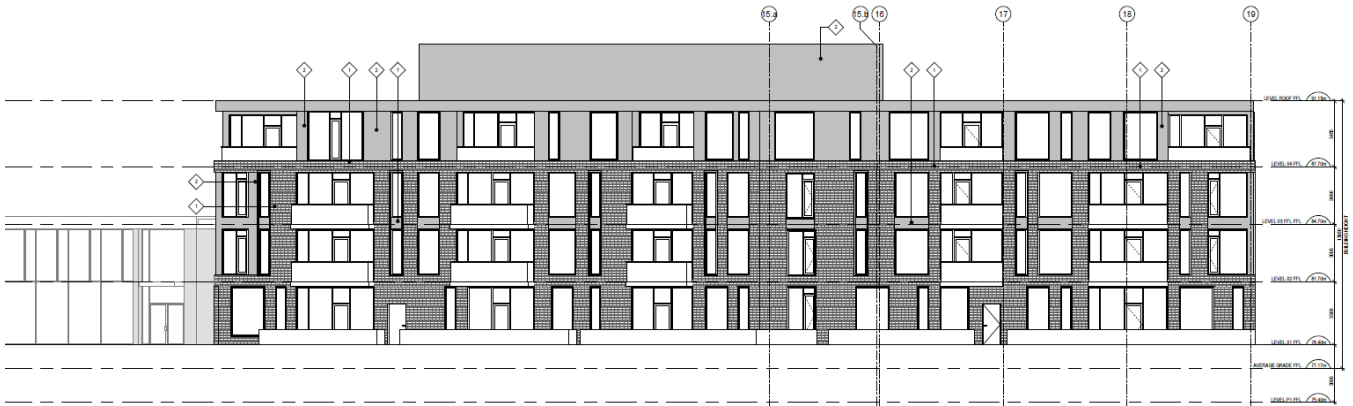


Figure 3: Excerpt from the South Elevation – East Wing drawing of the proposed development (as seen from March Road)



Figure 4: Excerpt from the North Elevation – West Wing drawing of the proposed development

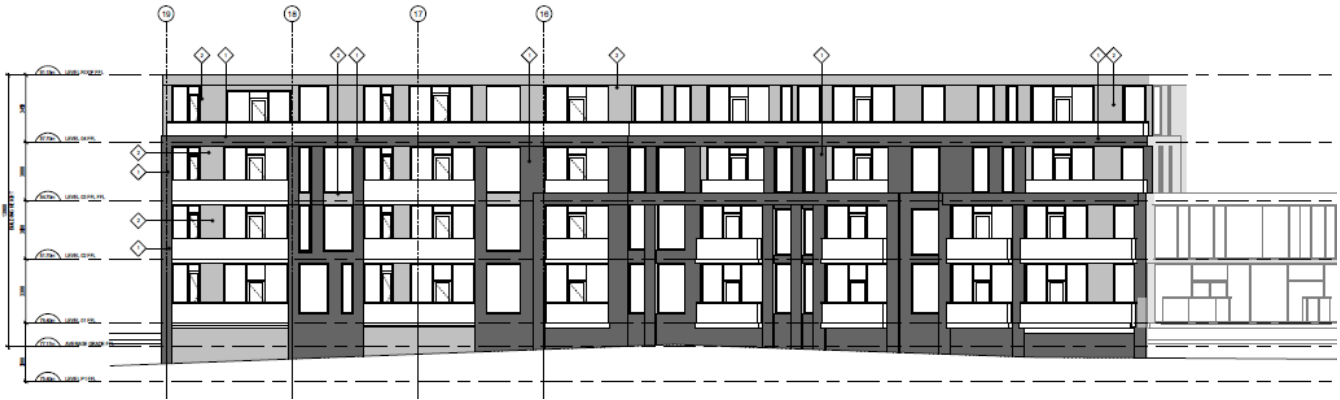


Figure 5: Excerpt from the North Elevation – East Wing drawing of the proposed development



Figure 6: Excerpt from the East Elevation drawing of the proposed development





Figure 7: Excerpt from the West Elevation drawing of the proposed development

## Proposed Site Plan Control Application + Discussion

The original Site Plan Control Application (file no. D07-12-18-0128) for the subject property was submitted to the City of Ottawa in August 2018. Because the revised proposal is significantly different from the original Site Plan, City planning staff will be recirculating the proposed development material to all technical agencies.

PDFs of the following materials are being submitted for the Site Plan Control Application recirculation:

- / This **Planning Rationale Addendum**, prepared by Fotenn Consultants Inc., dated May 29, 2020;
- / Revised **Traffic Impact Assessment Study**, prepared by Parsons, Addendum 3, dated March 19, 2020;
- / Revised **Functional Servicing and Stormwater Report**, Revision No. 3, prepared by David Schaeffer Engineering Ltd., dated May 2020;
- / Revised **Geotechnical Investigation Report**, prepared by Geofirma Engineering Ltd., Revision No. 3, dated April 9, 2020;
- / Revised **Traffic Noise Assessment**, prepared by Gradient Wind Engineering, dated February 10, 2020;
- / Revised **Traffic Noise Addendum Memo**, prepared by Gradient Wind Engineering, dated March 27, 2020;
- / Revised **Meander Belt Width Assessment**, prepared by Matrix Solutions Inc., Version 2.0, dated January 9, 2020;
- / Revised **Combined Environmental Impact Statement & Tree Conservation Report**, prepared by McKinley Environmental Solutions, dated May 2020;
- / Revised **Site Plan**, prepared by Project1 Studio, Issue No. 3, dated May 12, 2020;
- / Revised **P1 Level Floor Plans**, prepared by Project1 Studio, dated May 12, 2020;
- / Revised **Floor Plans**, prepared by Project1 Studio, dated May 12, 2012;
- / Revised **Elevation Drawings**, prepared by Project1 Studio, dated May 12, 2012;
- / **Renderings**, prepared by Project1 Studio, dated May 7, 2020;
- / **Landscape Plan** (including Landscape Plan, Planting Plan, and Landscape Details), prepared by Fotenn Planning + Design, Revision No. 00, dated May 27, 2020;
- / Revised **Stormwater Management Plan**, prepared by David Schaeffer Engineering Ltd., Revision No. 4, dated May 28, 2020;
- / Revised **Erosion Control Plan**, prepared by David Schaeffer Engineering Ltd., Revision No. 4, dated May 28, 2020;
- / Revised **Grading Plan**, prepared by David Schaeffer Engineering Ltd., Revision No. 4, dated May 28, 2020;

- / Revised **Site Servicing Plan**, prepared by David Schaeffer Engineering Ltd., Revision No. 4, dated May 28, 2020; and,
- / Revised **Existing Conditions Plan**, prepared by David Schaeffer Engineering Ltd., Revision No. 4, dated May 28, 2020.

A table containing the City's technical circulation comments and a response to each comment is also enclosed as part of this resubmission. Please note that several of the circulation comments no longer apply, given that they were issued in response to the previously proposed development.

### 3.1 Provincial Policy Statements (2014, 2020)

The Provincial Policy Statement (PPS), issued under the authority of Section 3 of the Planning Act and in effect since April 30, 2014, provides direction on matters of provincial interest related to land use planning and development. The Planning Act requires that decisions affecting planning matters 'shall be consistent with' such policy statements issued under the Act.

**The proposed development continues to be consistent with the policies of the PPS as it contributes to providing an appropriate range and mix of residential uses, and efficiently uses land, resources, and the infrastructure and public service facilities which are planned or available.**

A new Provincial Policy Statement (PPS), issued under the authority of Section 3 of the Planning Act, will come into effect on May 1, 2020. The new PPS will continue to provide policy direction on matters of provincial interest related to land use planning and development. As with the 2014 PPS, decisions affecting planning matters "shall be consistent with" policy statements issued under the Act.

**The proposed development is consistent with the policies of the new PPS as it contributes to providing an appropriate range and mix of residential uses, and efficiently uses land and resources and available infrastructure and public service facilities in a built-up area of the City.**

### 3.2 City of Ottawa Official Plan (2003, as amended)

#### 3.2.1 General Urban Area Designation

The subject property is designated General Urban Area in Schedule B – Urban Policy Plan of the City of Ottawa Official Plan. The General Urban Area designation permits the development of a full range and choice of housing types to meet the needs of all ages, incomes and life circumstances, in combination with conveniently located employment, retail, service, cultural, leisure, entertainment and institutional uses. This will facilitate the development of complete and sustainable communities. Low-rise residential apartment buildings are a permitted use in this designation.

**The proposed development continues to conform with the Official Plan policies for the General urban Area designation by proposing a residential use that will contribute to the provision of a full range and choice of housing types in Kanata North. The proposed housing is located in an area characterized almost exclusively by single detached and townhouse dwellings and will provide apartments as a housing option. The proximity of housing to the existing service commercial and employment uses also contributes to the creation of a complete community – where residents can meet most of their daily needs close to their homes.**

#### 3.2.2 Urban Design and Compatibility (Section 2.5.1)

Section 2.5.1 of the Official Plan contains objectives and principles for urban design and compatibility. The proposed development meets these objectives and associated principles as follows:

- / To enhance the sense of community by creating and maintaining places with their own distinct identity.

Guiding principles of the former City of Kanata include connectivity to the natural environment and features. The proposed development creates a distinctive place that appreciates and respects its relationship to Shirley's Brook to the east and the character of the surrounding community.

The proposed amenity pavilion's cladding and proportion will be similar to that of the March House across the street from the subject property, one of Kanata's more recognized heritage buildings.

- / To define quality public and private spaces through development.  
The proposed development's location near the Shirley's Brook corridor creates quality private amenity spaces and views for the buildings' residents. Landscaping along the subject property's frontage on March Road will help enhance the space along the public right-of-way by screening the surface parking lot.
- / To create places that are safe, accessible and are easy to get to, and move through.  
The proposed development provides connections between buildings and spaces that are understandable and universally accessible. A pathway and crosswalk will connect March Road to the central entrance and amenity pavilion, which will provide access to both buildings. The building walls are located in proximity to both the surface parking lot and the proposed pathway at the rear of the development, enhancing site safety by providing several "eyes" on the nearby outdoor areas.
- / To ensure that new development respects the character of existing areas.  
The proposed development is designed to respect nearby Shirley's Brook by providing a 30 metre setback from its normal high water mark. The building is set well back from nearby uses along Klondike Road. The proposed amenity pavilion will be designed to provide similar elements to the nearby heritage March House.
- / To consider adaptability and diversity by creating places that can adapt and evolve easily over time and that are characterized by variety and choice.  
The proposed apartment building development contributes to the variety of housing choices that are located in the Kanata North community, which currently consists primarily of single detached, semi-detached, and townhouse dwellings.
- / To understand and respect natural processes and features in development design.  
The proposed development respects the natural feature that is Shirley's Brook in its design, by providing an adequate setback from the watercourse and its riparian corridor and highlighting this natural feature for the proposed development's residents by locating the buildings near the creek corridor.

### 3.2.3 Urban Design and Compatibility (Section 4.11)

Policy 2 of Section 4.11 contains criteria for evaluating the compatibility of development applications. The proposed development meets the applicable criteria, including:

Compatibility Criteria	Proposed Development
Traffic	An addendum to the Transportation Impact Study (TIS) prepared by Parsons for the proposed development concludes that the impacts of the proposed development can be accommodated by the existing road network.

<b>Compatibility Criteria</b>	<b>Proposed Development</b>
<b>Vehicular Access</b>	The proposed development is served by two (2) vehicular access points – one (1) from March Road and one (1) from Klondike Road. The accesses have been reviewed and discussed by Parsons in their TIA.
<b>Parking Requirements</b>	The proposed development provides a total of 128 parking spaces, which complies with the requirements of the Zoning By-law.
<b>Outdoor Amenity Areas</b>	A communal, outdoor, partially cantilevered terrace is proposed to be provided at the rear of the entrance pavilion. Additional outdoor amenity spaces are provided in the form of private balconies and terraces.
<b>Loading Areas, Service Areas and Outdoor Storage</b>	Garbage rooms are proposed to be located in the building basement parking area. Refuse will be brought out to a garbage pick-up area behind the building, with access to Klondike Road.  No loading, services areas, or outdoor storage are proposed as part of the development.
<b>Lighting</b>	Site lighting will be designed to meet minimum City Standards and to mitigate any negative impacts on adjacent properties.
<b>Noise and Air Quality</b>	A noise study has been prepared by Gradient Wind Engineering Inc. to assess potential impacts of noise from nearby arterial roadways. The Study makes design recommendations and concludes that if these recommendations are implemented, noise levels within the building will be within acceptable levels.
<b>Sunlight</b>	The low-rise building will not have any significant shadowing impacts on abutting properties.
<b>Microclimate</b>	No microclimate impacts are anticipated as a result of the proposed low-rise development.
<b>Supporting Neighbourhood Services</b>	The proposed development is located in close proximity to two (2) existing shopping centres, which provide a range of services including a retail food store and other retail stores, restaurants, banks, and medical facilities.

### 3.3 Official Plan Amendment No. 150

In 2013, the City of Ottawa reviewed its Official Plan which resulted in several changes to policy references and land use designations. Ottawa City Council adopted Official Plan Amendment (OPA) 150 to implement the changes in December 2013 and it was subsequently approved by the Ministry of Municipal Affairs (MMAH) in April 2014. At the time the Site Plan Control Application was originally submitted, several appeals of OPA 150 were before the Ontario Municipal Board (OMB) with portions having been approved.

OPA did not propose any changes that would have any impact on the proposed development. The subject property retains its General Urban Area designation and continues to satisfy the criteria for urban design and compatibility of Sections 2.5.1 and 4.11.

### 3.4 City of Ottawa Comprehensive Zoning By-law (2008-250)

The following table summarizes the proposed development's compliance with the existing General Mixed Use Zone - GM zoning. Areas of non-compliance are noted with an "X".

Zoning Mechanism	Required	Provided	Compliance
<b>Minimum Lot Area</b>	No minimum	12,210.01 m <sup>2</sup>	✓
<b>Minimum Lot Width</b>	No minimum	64.62 m	✓
<b>Minimum Front and Corner Side Yard Setbacks</b>	3 m	Front yard setback (Klondike Road): 12.97 m Corner side yard setback (March Road): 17.6 m	✓
<b>Minimum Interior Side Yard Setback</b>	For a residential building higher than 11 m in height: 3 m	37.8 m	✓
<b>Minimum Rear Yard Setback</b>	For a residential use building: 7.5 m	7.5 m	✓
<b>Maximum Building Height</b>	18 m	13.98 m	✓
<b>Maximum Floor Space Index</b>	2	< 2	✓
<b>Minimum Width of Landscaped Area</b>	Abutting a street, or residential or institutional zone: 3 m  Other cases: no minimum	1.26 m	X
<b>Minimum Setback from Watercourses</b>	30 m from normal high-water mark of any watercourse	> 30 m	✓
<b>Amenity Area</b>	<b>Total:</b> 6 m <sup>2</sup> per dwelling unit (92 units) = 552 m <sup>2</sup>	<b>Total:</b> 1,015.76 m <sup>2</sup>	✓
	<b>Communal Area:</b> A minimum of 50% of the required total amenity area = 276 m <sup>2</sup>	<b>Communal Area:</b> 283.56 m <sup>2</sup>	✓
	<b>Layout:</b> At least one amenity area must be a minimum of 54 m <sup>2</sup>	<b>Layout:</b> At least one communal amenity area is more than 54 m <sup>2</sup> in area	✓
<b>Minimum Landscaped Area in a Parking Lot</b>	15% (perimeter or interior landscaped area)	> 15%	✓
	Abutting a street: 3 m	3 m	✓

Zoning Mechanism	Required	Provided	Compliance
<b>Minimum Width of Landscaped Buffer Around a Parking Lot (for a parking lot containing more than 10 but fewer than 100 spaces)</b>	Not abutting a street: 1.5 m	1.26 m (south/rear lot line)	✗
<b>Location of Storage</b>	Storage must be completely enclosed within a building	No outdoor storage is proposed	✓
<b>Minimum Required Vehicle Parking Spaces</b>	<b>Resident</b> (1.2 per unit): 110	<b>Resident:</b> 110	✓
	<b>Visitor</b> (0.2 per unit): 18	<b>Visitor:</b> 18	✓
	<b>Total:</b> 128 spaces	<b>Total:</b> 128 spaces	✓
<b>Minimum Required Bicycle Parking</b>	0.5 per dwelling unit: 46 spaces	46 spaces	✓
<b>Bicycle Parking Space Provisions</b>	A maximum of 50% of the required bicycle parking spaces may be vertical spaces, and the rest must be horizontal spaces.	22 of the proposed bicycle parking spaces are vertical = 48%	✓
<b>Minimum Driveway Width</b>	<b>Parking Lot:</b> 6.7 m	<b>Parking Lot:</b> 6.0 m	✗
	<b>Parking Garage:</b> 6.0 m	<b>Parking Garage:</b> 6.0 m	✓
<b>Minimum Aisle Width</b>	<b>Parking Lot:</b> 6.7 m	<b>Parking Lot:</b> 6.0 m	✗
	<b>Parking Garage:</b> 6.0 m	<b>Parking Garage:</b> 6.0 m	✓
<b>Minimum Parking Space Dimensions</b>	5.2 m by 2.6 m  Up to 40% of required resident spaces may be reduced to 4.6 m by 2.4 m	18 of 110 (16%) required resident spaces are reduced to 5.2 m by 2.4 m  All other spaces are 5.2 m by 2.6 m	

The proposed development meets the majority of the applicable zoning provisions for this site with four (4) exceptions. A Minor Variance application will be submitted after receiving technical circulation comments to address these performance standards, and any others which may arise through the comments.

As noted previously, although the revised proposal generally meets the provisions of the General Mixed Use Zone – GM zoning, minor variances will be required for two (2) landscape buffers and drive aisle and driveway widths. A separate application will be submitted to the Committee of Adjustment, with a supporting planning rationale.

The areas of the landscape buffer minor variances relate not to the entirety of the parking area but rather a single parking space and a portion of the drive aisle within the parking area. The variances are abutting the Residential Fourth Density (R4) zoned property currently owned by Minto to be developed with



stacked townhouse units. There are no negative adverse impacts to the reduction in landscape buffer abutting the last parking space and vehicle turn-around area of the parking drive aisle.

The reduction in drive aisle and driveway width is to reduce the surface asphalt area, and to increase the depths of the landscaped buffers. The proposed driveway and drive aisle widths are sufficient for safe and efficient vehicle movements in the parking lot; a significant portion of the parking spaces are designed as a single loaded drive aisle.

## Conclusions

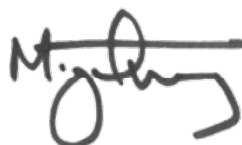
Based on the information presented throughout this Planning Rationale and the original Planning Rationale prepared by Fotenn and dated August 29, 2018, it is our professional planning opinion that the revised Site Plan Application is appropriate and desirable when evaluated against the applicable policies and regulations, within the context of the subject property. In summary:

- / The revised development proposal is consistent with the policies of the 2014 and 2020 Provincial Policy Statements. More specifically, the proposed development consists of dense housing that will be built in a compact form and that will efficiently use land and infrastructure in a settlement area, and is located a safe distance away from an existing natural feature and erosion hazard;
- / The revised development proposal conforms to the Official Plan policies for the General Urban Area designation, contributing to the diversity of housing choices in Kanata North;
- / The revised development proposal conforms to the City's urban design and compatibility criteria established in Sections 2.5.1 and 4.11 of the Official Plan;
- / The revised development proposal conforms to the new policy direction set out in the Official Plan Amendment No. 150 with regards to land use, building design and compatibility; and,
- / The proposed development meets the intent of the Zoning By-law and complies with the majority of the provisions applicable to the subject property. A future Minor Variance application will address performance standards that require a variance.

Sincerely,



Nico Church, M. Pl.  
Planner



Miguel Tremblay, MCIP RPP  
Partner