



Site Plan Control **DESIGN BRIEF** April 11, 2016

Apartment Building 559 Riverdale Avenue **Ottawa**

Property and stakeholders information

Development address:
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Ottawa ON K1S 1S3

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Design brief sheets

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project
Apartment Building
559 Riverdale Ave
Ottawa ON K1S 1S3

reference
14-10

title

General Information

scale
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Site Plan Control

date
2016/04/11

sheet revision

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This DESIGN BRIEF has been prepared in support to a Site Plan Control Application regarding the construction of a Twenty-Two Apartment Building on 590 Kirkwood Avenue Ottawa ON K1Z 5X4

City documents

The site is zoned Residential Fourth Density Subzone N (R4P) per City of Ottawa Zoning By-law 2008-250 and is designated General Urban Area per Schedule B of the City of Ottawa Official Plan.

The development complies with the Official Plan policies as follows :

The infill development meets the City's goal for residential intensification through redeveloping an existing residential property within the urban area (Section 2.2.2 Policy 1a)

The development encourages walkability and safety of pedestrians through creating connectivity between the public sidewalks and building entrances, as well as providing an accessible entrance at the rear of the building for pedestrians coming from Westhaven Crescent. (Section 2.3.1 Policy 7)

Six bicycle parking spaces are proposed at the front door entrance of the building to maintain visibility of bicycles while stored (Section 2.3.1 Policy 13)

The underground parking minimizes the land devoted to parking (Section 2.3.1 Policy 46c)

The property is serviced by municipal water and sewer that have sufficient capacity to service the eleven proposed residential units. (Section 2.3.2 Policy 2)

The development has been designed with a drainage and stormwater management plan which meets the City's water quality and quantity requirements (Section 2.3.3)

The site is designated General Urban Area per the Official Plan. The development meets the intent of this designation through providing diversified housing types within the urban area. The development is considered to be compatible with adjacent land uses and housing types. A range of commercial and recreational uses are offered in proximity to the development site. The development promotes the use of active, public and private modes of transportation and is designed to be accessible. (Section 3.6.1)

The development meets the Urban Design and Compatibility objectives from a planning / architectural perspective. (Section 4.11)

The site is also located within the Special Infill Provision Area; consideration of the Urban Design Guidelines for Low-Medium Density Infill Housing has been carried out during the design of the site and building. The development is considered to meet the design objectives identified within the Guidelines.

Site description

The project site is a rectangular lot of 511m² and is presently occupied by a single detached house. The existing house is meant for demolition. The site's main access is on Riverdale Avenue, an existing collector road per Schedule E of the Official Plan, which is used for both pedestrian and vehicular purposes.

The site is bound by a mix of residential uses; to the south is a low rise apartment building; to the east and north is a planned unit development.

Building mass and character

The development consists of a three storey apartment building with a total building footprint of 210m² and a Gross Floor Area of 995m². The building is oriented from East to West, with the building entrance on Riverdale Avenue.

Its mass is composed of one volume that is sculpted at top and bottom to create shadow lines and terraces. The planes interplay is emphasized by the difference in materials – the main volume is clad with light grey metal, whereas the cutouts are masonry. Thus, the metal surface seems to float as a roughly pixelated cloud. The masonry that lines the exterior spaces gives a warmer feel and a more detailed texture. Besides, it helps the new building to integrate to its surroundings. The windows are arrayed in a staggered in fashion and are part of the same concept – every opening is a single or a combination of pixels.

The underground parking ramp is located outside the building in order to reduce the first floor mass and open the view to depth of the lot.

In the front yard, the upper volume projection is aimed to create a protective and welcoming environment for the main entrance in conjunction with the other functions that are present in the same area.

Outdoor areas

Except for the parking ramp, the exterior space around the building is dedicated to amenity areas and pedestrian / bicycle circulation. The soft landscaping prevails.

A bicycle parking area is proposed in front yard, juxtaposed by the main entrance door of the building for convenient accessibility.

Outdoor seating / gathering furniture is strategically located in the front and the rear yard to encourage interaction of residents and eyes on the street.

Building functionality

Inside the building, at each one of the three floors above grade, the apartments are set along a central corridor. The apartment size is varying from studios to large 2 bedroom units. The uppermost units have two levels, the mezzanines having access to large private terraces. All units benefit of good day lighting coming through large windows.

The main entrance is endowed with a lifting platform that provides access at the first floor to people using wheelchairs. Three of the apartments on the first floor are fully accessible per the Ontario Building Code.

Six parking spaces are provided in the basement. The technical rooms are located in the basement and first floor.

The waste collection area is located at the rear of the garage ramp. Along with the temporary storage pad, the waste management of the development meets the requirements of the Solid Waste Collection Design Guidelines for Multi-Unit Residential Development.

Construction

The building structure is mainly wood framing combined with cast-in-place concrete for the basement level and steel members for the upper floors.

Conclusion

The project responds to its neighborhood by proposing a good quality development, in terms of functionality and architecture, that is highly compatible with the surrounding properties. Moreover, the development enhances the urban side of Rivedale Avenue with a clean and contemporary design.

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project

Apartment Building

559 Riverdale Ave
Ottawa ON K1S 1S3

reference

14-10

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Project Description

scale

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Contextual Analysis

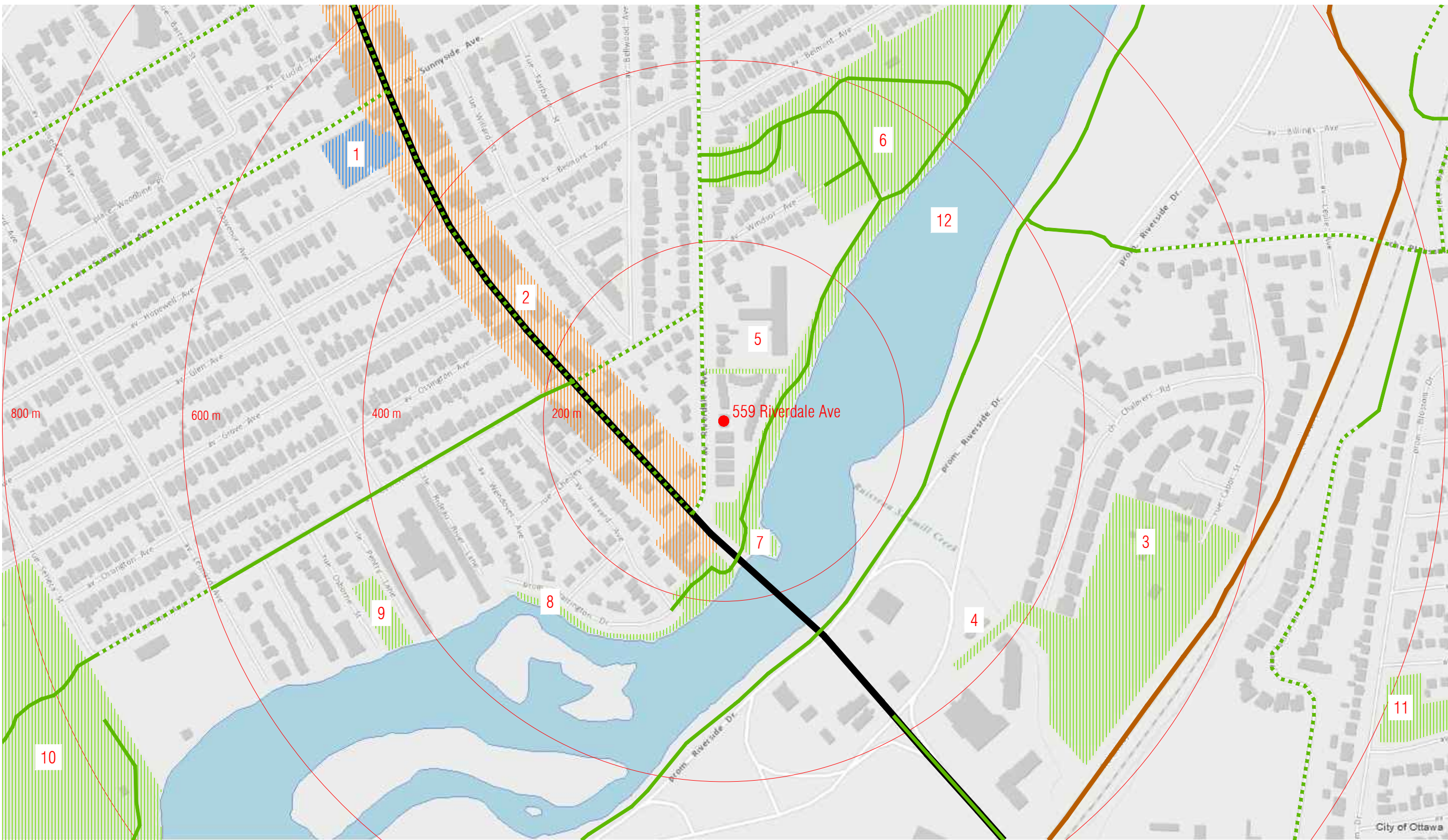
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The project is located in Old Ottawa South neighbourhood, which is nestled between the Rideau Canal and the Rideau River, as north-south limits, and Avenue Road and Carleton University in east-west direction. Mainly residential, the neighbourhood displays a mature appearance due to its buildings architecture and full-grown trees. Beside an exceptional natural setting, the neighbourhood is crossed by Bank Street, with its particular flavor, and benefits of the proximity to Carleton University. The project site is close to the southern end of Riverdale Avenue, where this one meets Bank Street before crossing the Rideau River. The site is closely surrounded by the main features of the neighbourhood: nature, residential and commercial. In addition, gravitates around a fourth one, the Carleton University Campus.

Riverdale Avenue has access to Ottawa downtown and Highway 417 by Main Street at the northern end and Bank Street at the southern end. Passed the Bank Street bridge over the Rideau River, the site can access further destinations either in south, east or west directions. Within walking distance the project site has access to a large number of commercial destinations (stores, restaurants, services, etc) located on Bank Street. Equally close, the parks along Rideau River offer both walking and biking opportunities to the future building occupants. The larger parks (Brewer, Windsor Park Ottawa, etc.) provide several other activities and sports facilities. Located at less than 2km from the site, the Carleton University has a dynamic influence over the entire area.

Legend

- | | |
|--------------------------------------|---------------------------|
| 1 Hopewell Avenue Public School | 10 Brewer Park |
| 2 Bank Street Commercial Area | 11 Applewood Park |
| 3 Billings Estate Museum | 12 Rideau River |
| 4 Dr. Ernest Couture Day Care Centre | |
| 5 Lexington Green Life Garden | — Bank Street |
| 6 Windsor Park Ottawa | — OC Transitway |
| 7 Linda Thom Park | — Bicycle Path / Lane |
| 8 Parkland | — Suggested Cycling Route |
| 9 Osborne Park | |



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Project Area

The project benefits of an enviable location. Three distinct elements converge in the site's vicinity: Rideau River, Bank Street and the residential pattern of Old Ottawa South.

Riverdale Avenue, which is part of Old Ottawa South, has a strong residential character. Most of the buildings are one or two storeys high. They range from multi-unit apartment buildings to single family cottages.

Not far from there, Bank Street displays a lively and colourful looking streetscape due to its various commercial functions, which include stores, restaurants and services, as well as the mix of building styles.

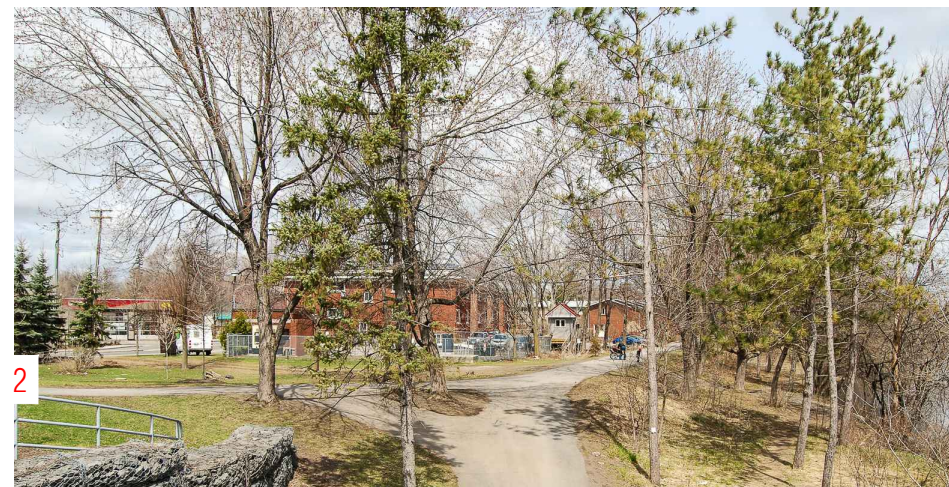
South of the site flows the Rideau River which not only stands for a great natural feature, visually speaking, but also invites to interesting leisure activities through the parks network that range on both shores.

The three features are completed by old trees, which emblematic of mature neighbourhoods.

All these elements create a unique urban setting that is well connected to the rest of the City by the means of public transportation and routes network.



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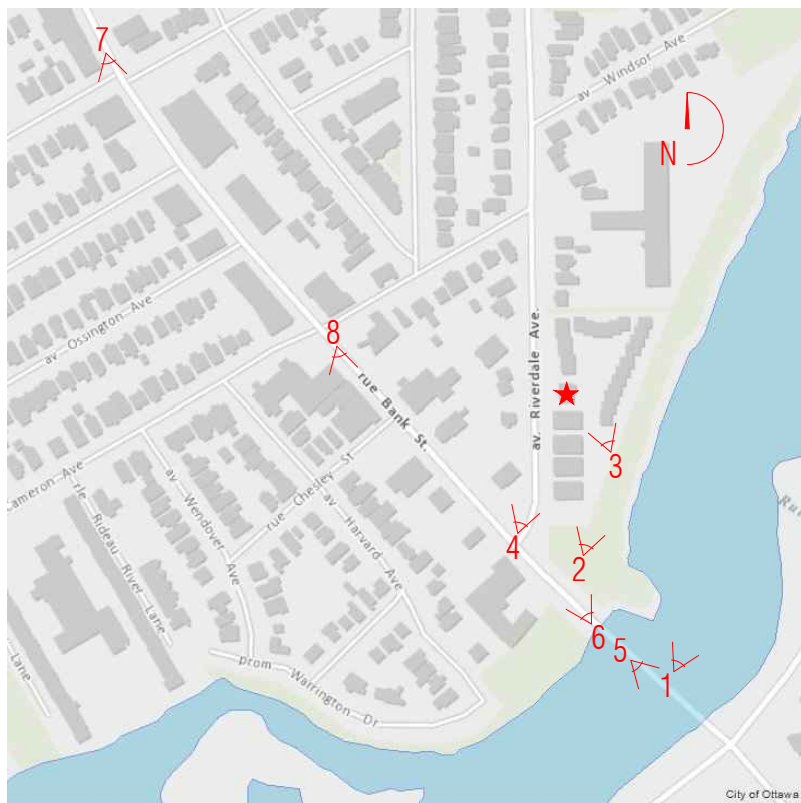


photo key plan



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Apartment Building
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Projet Site & Adjoining Bldgs

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Projet Site & Adjoining Buildings

- A 563 / 565 / 571 / 573 Riverdale Ave.
- B 559 Riverdale Ave. (actual project site)
- C 551 Riverdale Ave.
- D 517 / 515 Riverdale Ave.
- E 522 Riverdale Ave.
- F 530 / 532 Riverdale Ave.
- G 556 Riverdale Ave.
- H 1305 Bank St.

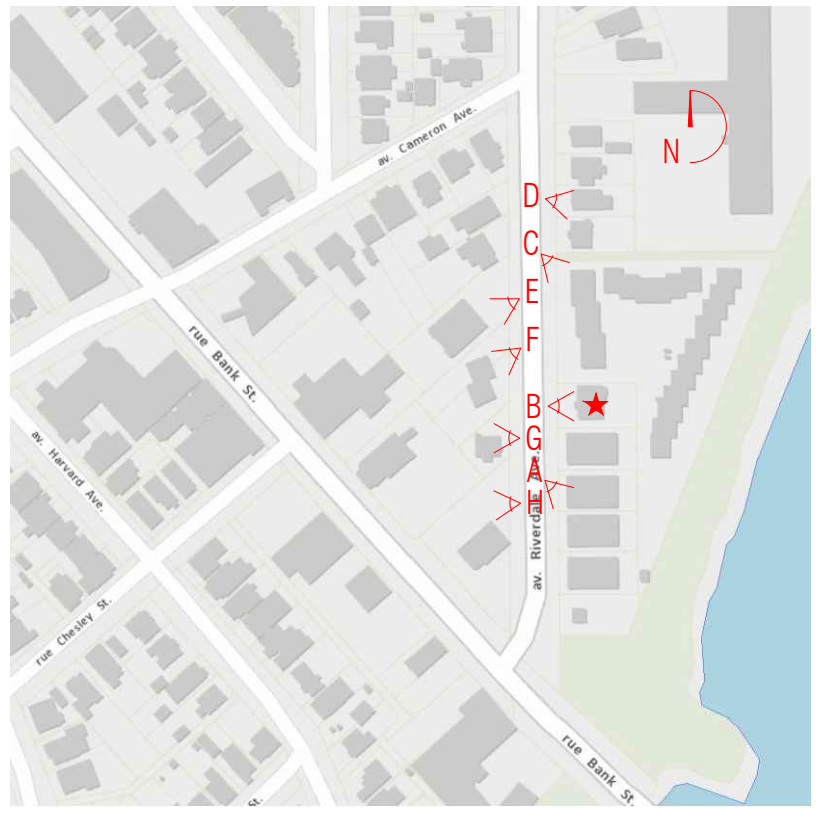
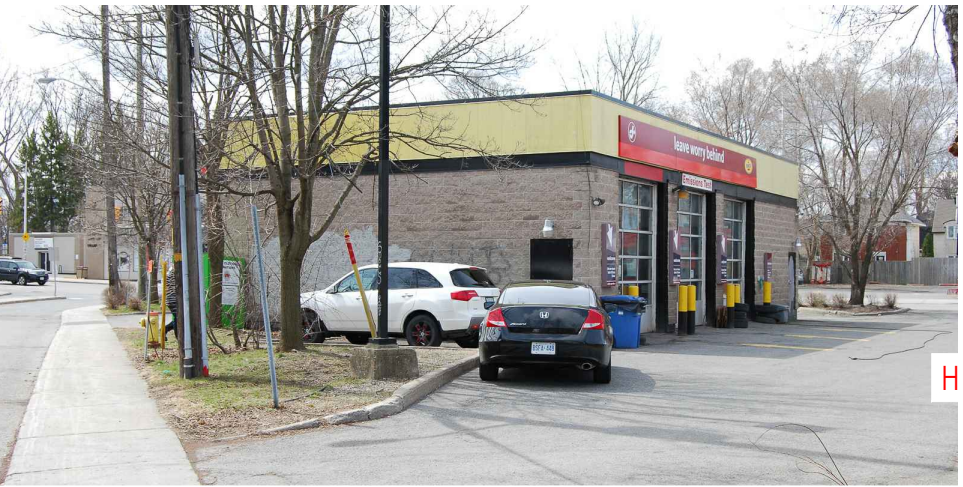


photo key plan





Street view from N-W



Bird-eye view from S-W



West elevation / main entrance

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**Proposed
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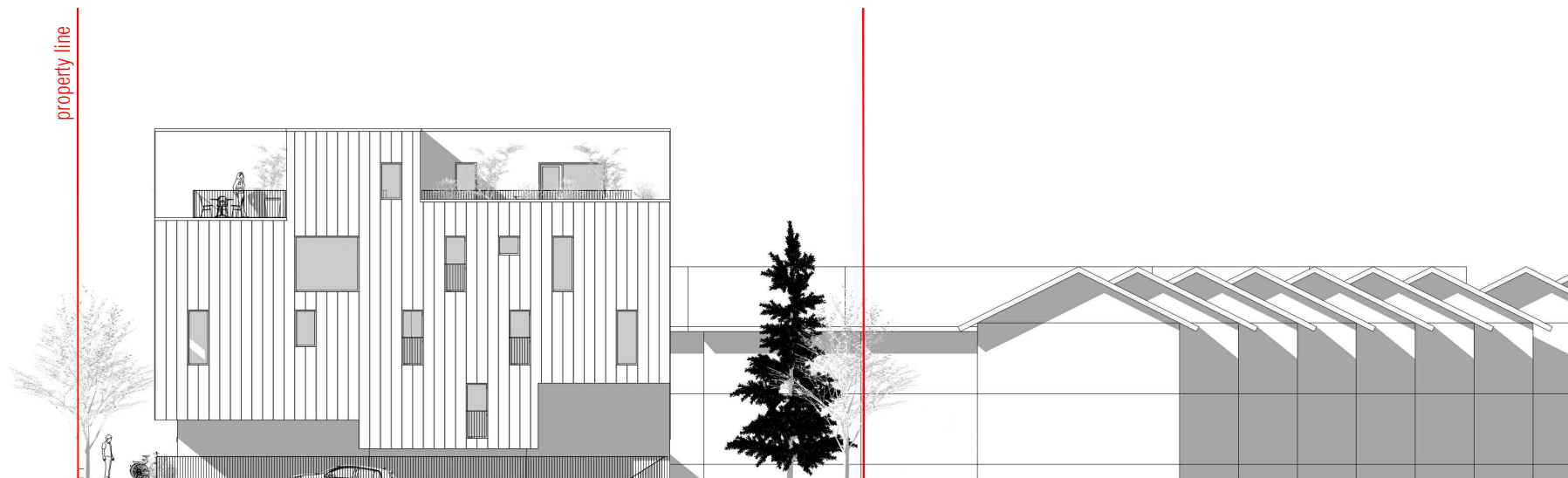
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West Elevation
1:250



South Elevation
1:250

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General Elevations

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CITY OF OTTAWA ZONING BY-LAW NO 2008-250				
SECTION	ZONE PROVISION	REQUIRED	PROPOSED	EXCEPTION
161-162 (R4P)	Principal dwelling type	Apartment dwelling, low rise, Stacked	Idem	No
161-162 (R4P)	Minimum Lot Width (m)	15.0	16.7	No
161-162 (R4P)	Minimum Lot Area (m2)	450.0	511.0	No
161-162 (R4P)	Maximum Building Height (m)	14.5	13.5	No
161-162 (R4P)	Minimum Front Yard Setback	3.0	3.0	No
161-162 (R4P)	Minimum Rear Yard Setback (m)	Endnote 3: The minimum required rear yard setback is equal to 25% of the lot depth but need not exceed 7.5 metres. Notwithstanding the foregoing, where the rear lot line abuts the interior side lot line of an abutting lot, the minimum required rear yard setback is equal to the minimum required interior side yard setback of the abutting lot along each point of the shared lot line (7.5m in this instance)	7.5	No
161-162 (R4P)	Minimum Interior Side Yard Setback (m)	Endnote 3: For any part of a building located within 21 metres of a front lot line the minimum required interior side yard setback is as follows: (a) Where the building wall is equal to or less than 11m in height: 1.5m; (b) Where the building wall is greater than 11 m in height: 2.5m In all other circumstances the minimum required interior side yard setback is 6m.	Interior Side Yard (North): 1.5m (first 18m), 1.5m for remaining 2m Interior Side Yard (South): 1.5m (first 18m), 1.5m for remaining 2m	4.5 metres on both North and South Interior Side Yard beyond 21m (at rear) 1m on both North and South Interior Side Yard above 11m

GENERAL PROVISIONS

SECTION	ZONE PROVISION	REQUIRED	PROPOSED	EXCEPTION
100 (6)	General Parking Provisions	All motor vehicle parking spaces, queuing and loading spaces and aisles and driveways leading to those spaces must have a surface which is: (a) hard, stable and dust preventative	Concrete	No
101 (1)	Minimum Parking Spaces Rates	Off-street motor vehicle parking must be provided for any land use at the rate set out in Table 101. In addition, visitor parking must be provided in accordance with Section 102.	Underground parking	No
101(6)		Despite subsection (1), where all parking spaces provided or required for a permitted land use are located below grade in the same building as that land use, the parking required by Table 101 for that land use may be reduced by 10% to a maximum of 20 spaces. (reduction of 1 space permitted)	None	No
Table 101(b) III (ii)	Residential - Apartment building, low rise	0.5 per dwelling unit = 6 spaces	6	No
Table 102 (a) III (ii)	Minimum visitor parking spaces for Area B on schedule 1 none for first 12 units; 0.2 per dwelling unit for next 300 dwellings;	0 - 12 units: 0 spaces	None	No
106 (1)	Parking space for motor vehicle	2.6m (min) x 5.2m (min)	2 spaces of 2.7x5.2m, 1 space of 2.6x5.2m, 2 spaces of 2.7x4.6m and 1 space of 2.6x4.6m	2 spaces of 2.7x4.6m and 1 space of 2.6x4.6m
107 (1)(a) i / Table 107 I (d)	Aisle and Driveway provisions	A driveway providing access to a parking lot or parking garage must have a minimum width of three metres for a single traffic lane; Angle of Parking = 71-90 degrees / Minimum Required Aisle Width (6.7m);	3.6m for driveway; 6.7m for aisle of parking garage	No
107 (1)(b) ii	Minimum vertical clearance for parking garage	All driveways and aisles providing access to or located within () parking garage must have a minimum vertical clearance clear of obstructions such as signs and other structures () in accordance with the Building Code, as amended	Minimum 2m as per 3.3.5.4 (5) Repair and storage garages in the Building Code	No
108 (1)(b)	Gradient of Driveway leading into garage	Must not exceed that specified in City of Ottawa Private Approach By-law	11%	No
Table 111A	Bicycle Parking Space for Apartment building, low rise	0.5 spaces per dwelling = 6	6	No
Table 111B	Minimum Bicycle Parking Space Dimensions - horizontal	0.6m (min) x 1.8m (min)	0.6m x 1.8m	No
111 (9)	Minimum Bicycle Access Aisle	1.5m	1.5m	No
137(3)	Amenity Area	Outdoor amenity space area must not be located in a required front or corner side yard	Rear Yard and mezzanine level	No
By-law 2014-189	Minimum Amenity Space Area - Apartment Building, low rise of more than four dwelling units	15m ² per dwelling unit up to 8 units, plus 6m ² per unit in excess of 8. A 100% of the amenity area required for the first 8 units is to be: communal, located at grade and in the rear yard be landscaped, consist of at least 80% soft landscaping; and abut the rear lot line, unless the lot has access to a rear lane	Communal amenity area 125.5m ² in the Rear Yard (for the first 8 units) Private amenity area 57.37m ² on four terraces at the mezzanine level Interior amenity area - NIL	No

SITE STATISTICS	
Lot Area	511m ²
Building Footprint Area	210.5m ²
Total Gross Floor Area	995m ²
Basement Gross Floor Area	185m ²
1st level Gross Floor Area	220m ²
2nd level Gross Floor Area	255.5m ²
3rd level Gross Floor Area	255.5m ²
Mezzanine level	79m ²
Paving & hard landscaping	195m ²
Soft landscaping	105.5m ²
Building footprint % lot	41%
Paving & hard % lot	38%
Soft landscaping % lot	21%

NOTES:

The entrance elevation 98.40 in the architectural drawings corresponds to 59.35 geodetic elevation per the site survey by Annis O'Sullivan Vollebakk Ltd. (i.e. the 1st floor elevation is 100.00)

Property boundary information was derived from a Annis O'Sullivan Vollebakk Ltd. field survey, completed December 18th, 2014

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Site Data

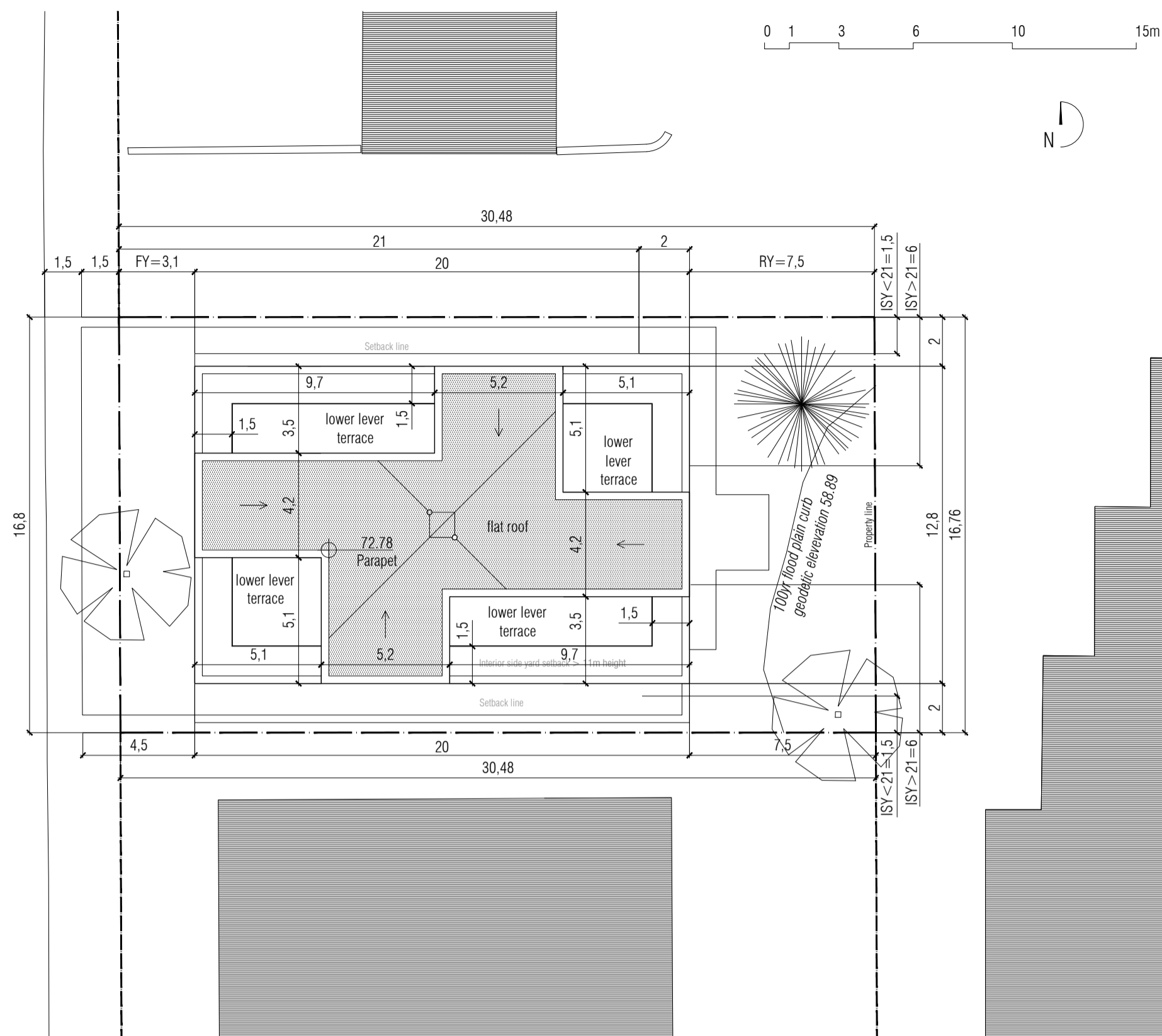
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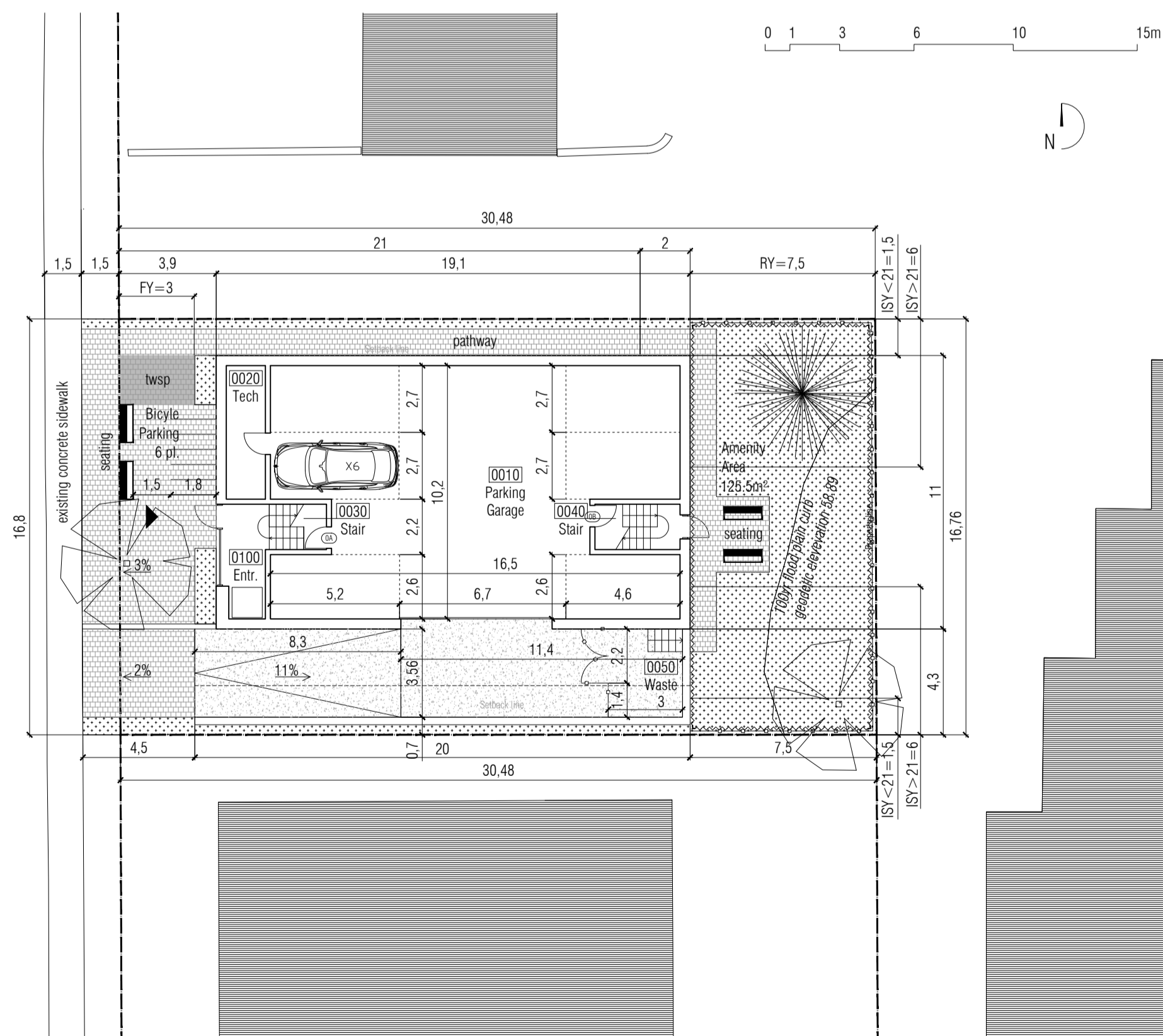
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4 Site Plan (rooftop view)
1:200



1 Site Plan (parking view)
1:200

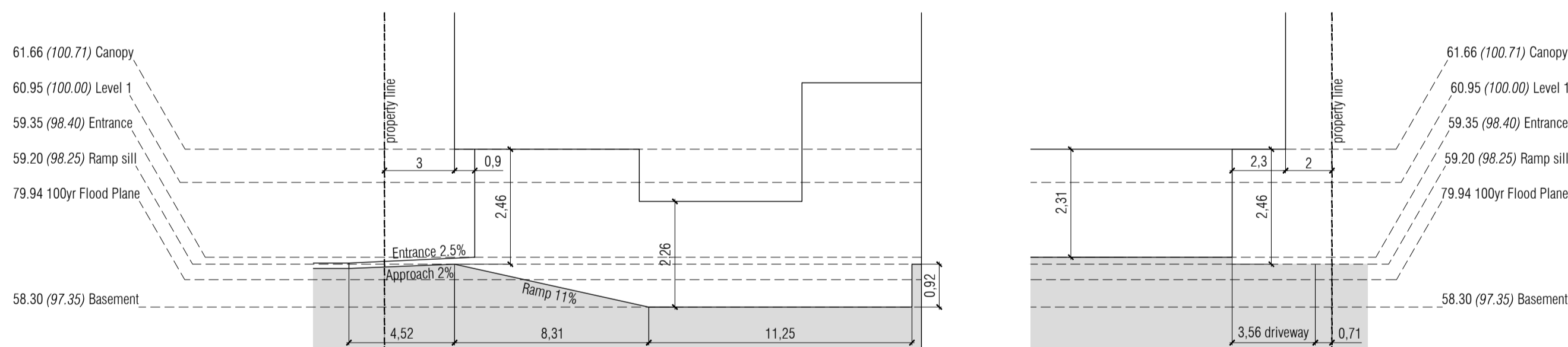
CITY OF OTTAWA ZONING BY-LAW NO 2008-250				
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GENERAL PROVISIONS				
100 (6)	General Parking Provisions	All motor vehicle parking spaces, queuing and loading spaces and aisles and driveways leading to those spaces must have a surface which is: (a) hard, stable and dust preventative	Concrete	No
101 (1)	Minimum Parking Spaces Rates	Off-street motor vehicle parking must be provided for any land use at the rate set out in Table 101. In addition, visitor parking must be provided in accordance with Section 102.	Underground parking	No
101(6)		Despite subsection (1), where all parking spaces provided or required for a permitted land use are located below grade in the same building as that land use, the parking required by Table 101 for that land use may be reduced by 10% to a maximum of 20 spaces. (reduction of 1 space permitted)	None	No
Table 101(b) iii (ii)	Residential - Apartment building, low rise	0.5 per dwelling unit = 6 spaces	6	No
Table 102 (a) iii (ii)	Minimum visitor parking spaces for Area B on schedule 1 note for first 12 units: 0.2 per dwelling unit for next 300 dwellings:	0 - 12 units: 0 spaces	None	No
106 (1)	Parking space for motor vehicle	2.6m (min) x 5.2m (min)	2 spaces of 2.7x5.2m, 1 space of 2.6x5.2m, 2 spaces of 2.7x4.6m and 1 space of 2.6x4.6m	2 spaces of 2.7x4.6m and 1 space of 2.6x4.6m
107 (1)(a) / Table 107 (d)	Asile and Driveway provisions	A driveway providing access to a parking lot or parking garage must have a minimum width of three metres for a single traffic lane: Angle of Parking = 71-90 degrees / Minimum Required Asile Width (6.7m);	3.6m for driveway, 6.7m for aisle of parking garage	No
107 (1)(b)ii	Minimum vertical clearance for parking garage	All driveways and aisles providing access to or located within (i) parking garage must have a minimum vertical clearance clear of obstructions such as signs and other structures (i) in accordance with the Building Code, as amended	Minimum 2m as per 3.3.5.4 (5) Repair and storage garages in the Building Code	No
108 (1)(b)	Gradient of Driveway leading into garage	Must not exceed that specified in City of Ottawa Private Approach By-law	11%	No
Table 111A	Bicycle Parking Space for Apartment building, low rise	0.5 spaces per dwelling = 6	6	No
Table 111B	Minimum Bicycle Parking Space Dimensions - horizontal	0.6m (min) x 1.8m (min)	0.6m x 1.8m	No
111 (9)	Minimum Bicycle Access Aisle	1.5m	1.5m	No
137(3)	Amenity Area	Outdoor amenity space area must not be located in a required front or corner side yard	Rear Yard and mezzanine level	No
By-law 2014-189	Minimum Amenity Space Area - Apartment Building, low rise of more than four dwelling units	15m ² per dwelling unit up to 8 units, plus 6m ² per unit in excess of 8. A 100% of the amenity area required for the first 8 units is to be: communal, located at grade and in the rear yard for landscaping, consist of at least 80% soft landscaping; and about the rear lot line, unless the lot has access to a rear lane	Communal amenity area 125.5m ² in the Rear Yard (for the first 8 units) Private amenity area 57.37m ² on four terraces at the mezzanine level Interior amenity area - NIL	No

- LEGEND**
- Property line
 - Setback line
 - Surrounding buildings
 - Pavers (see Landscape Drawings)
 - Concrete (see Landscape Drawings)
 - Soft landscaping (see Landscape Drawings)
 - Privacy fence offset 0.1m inside property line
 - Building overhang / recess line
 - Temporary Waste Storage Pad
 - Amenity area
 - Building access / Egress
 - Bench w/ double orientation

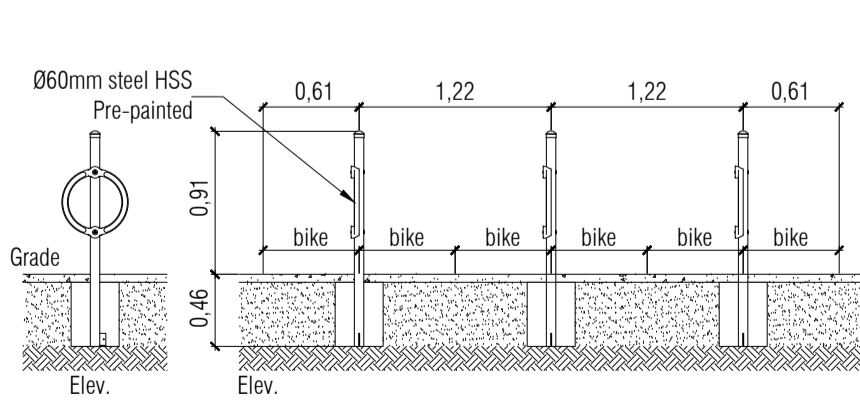
- ABBREVIATIONS**
- FY Min. Front Yard Setback (m)
 - RY Min. Rear Yard Setback (m)
 - ISY<21 Interior Side Yard Setback within 21m of front line
 - ISY>21 Interior Side Yard Setback beyond 21m of front line
 - ROW Right of way
 - AA# Amenity area
 - DC Depressed curb 5m radius
 - TWSP Temporary Waste Storage Pad

NOTES:

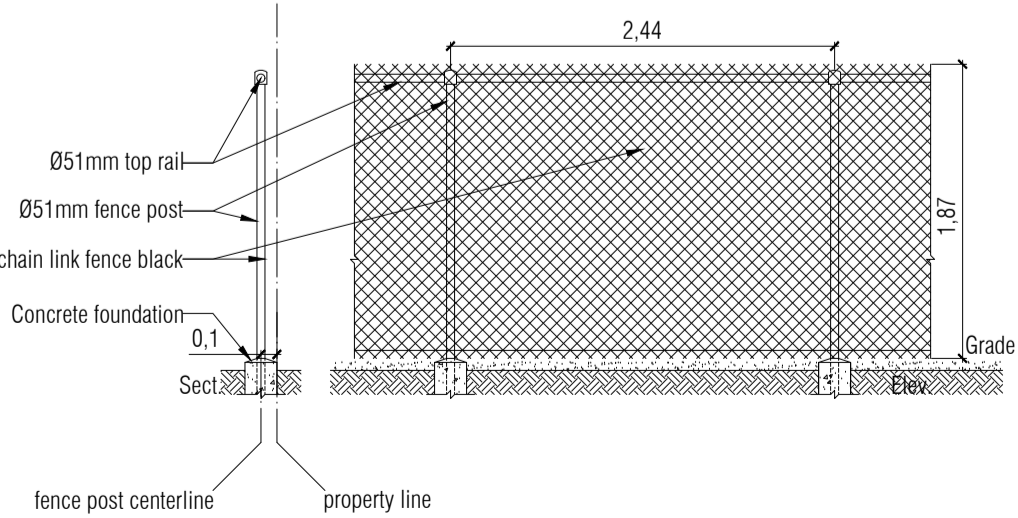
The entrance elevation 98.40 in the architectural drawings corresponds to 59.35 geodetic elevation per the site survey by Amis O'Sullivan Vollebek Ltd. (i.e. the 1st floor elevation is 100.00)
Property boundary information was derived from a Amis O'Sullivan Vollebek Ltd. field survey, completed December 18th, 2014



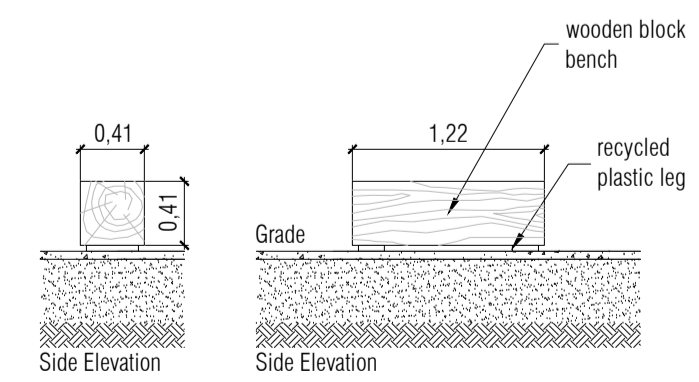
5 Schematic sections
Anisometric drawing



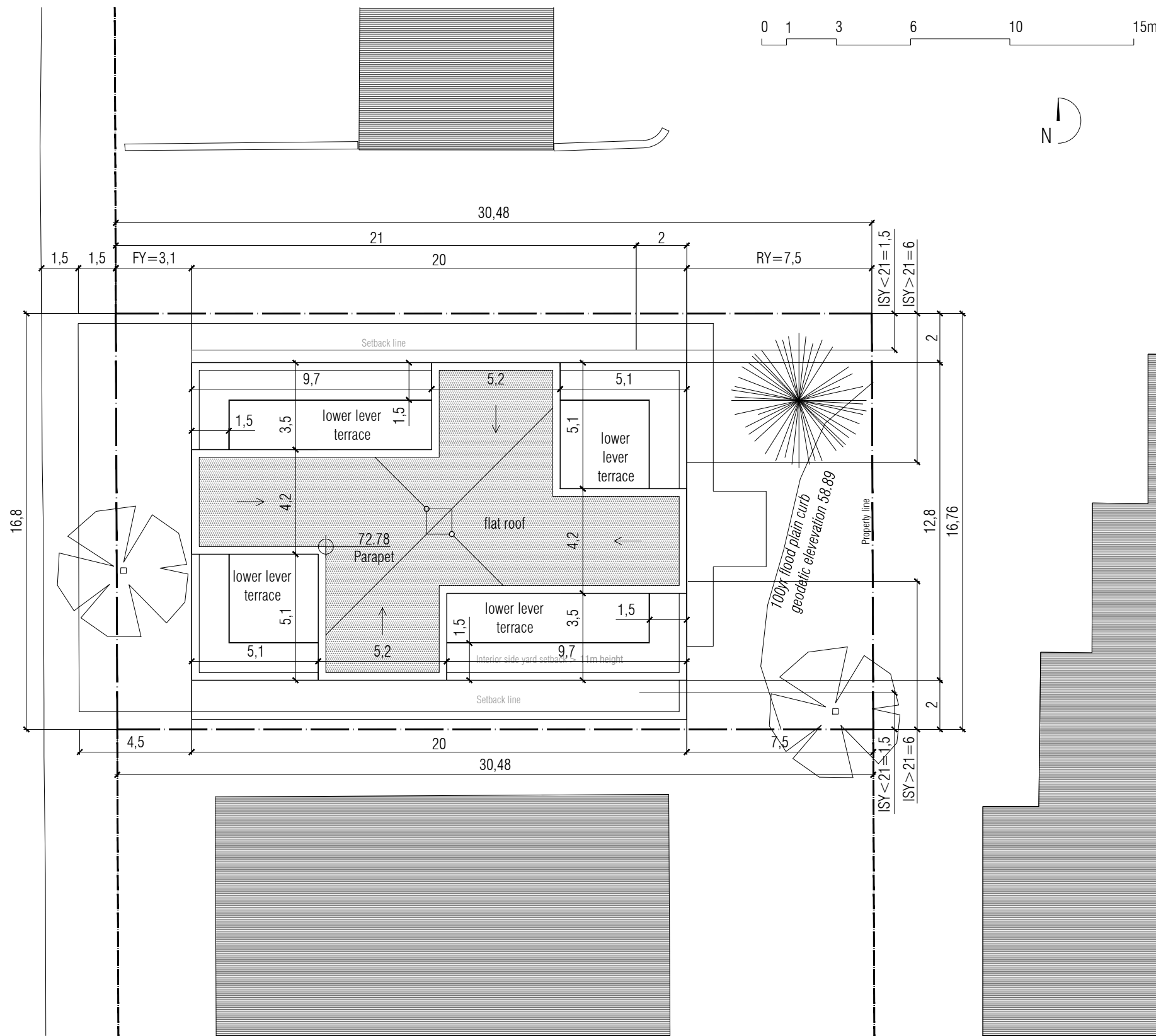
3 Bicycle Rack
1:50




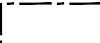

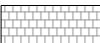
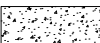
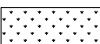



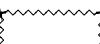


2 Chainlink fence
1:50



6 Double orientation bench
1:50



LEGEND

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ABBREVIATIONS

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reference
14-10

title
Site Plan
w/ roof top view

scale
1:200

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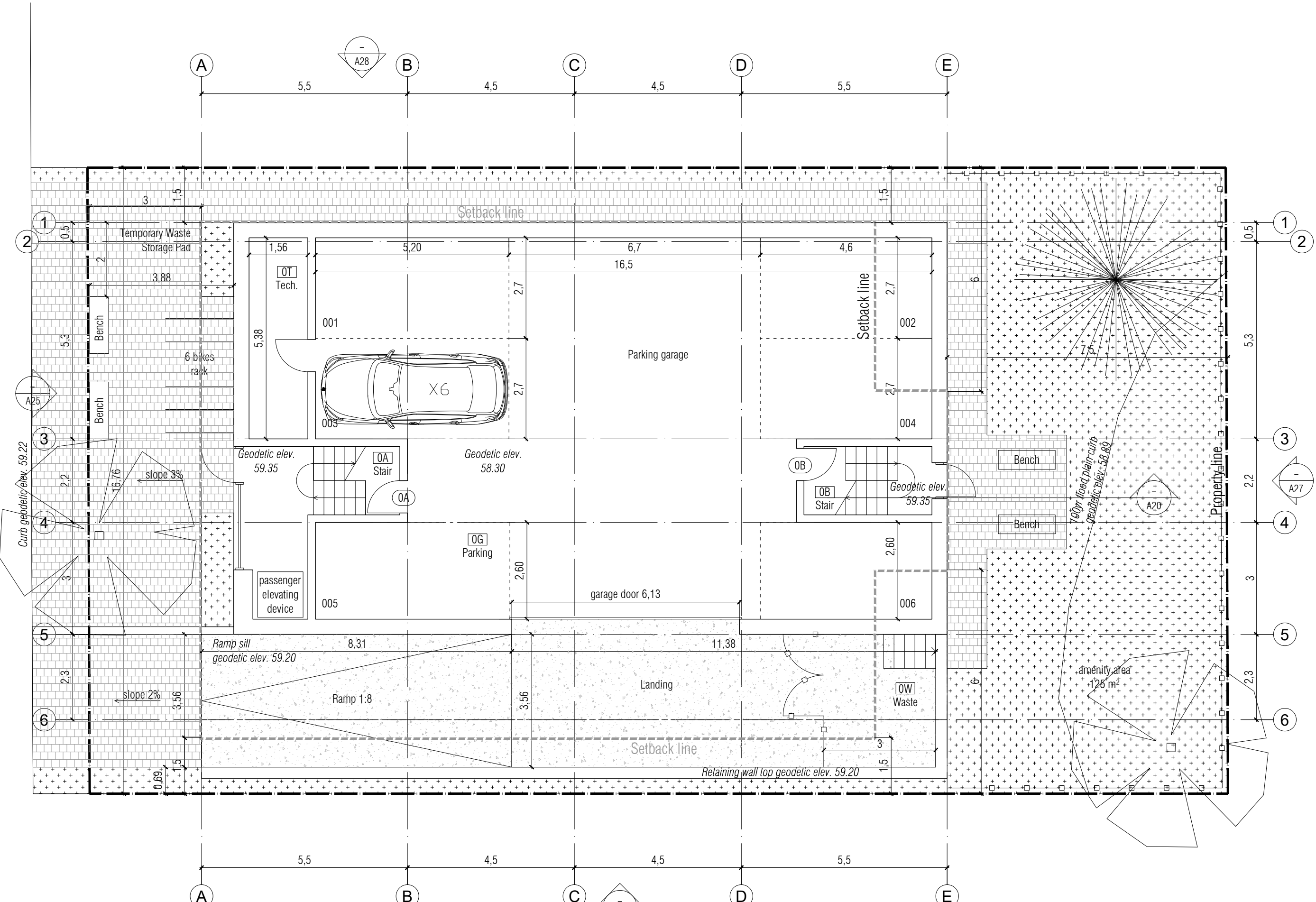
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NB: The entrance elevation 98.40 in the architectural drawings corresponds to ± 59.35 geodetic elevation per the site survey by Annis O'Sullivan Vollebek Ltd.

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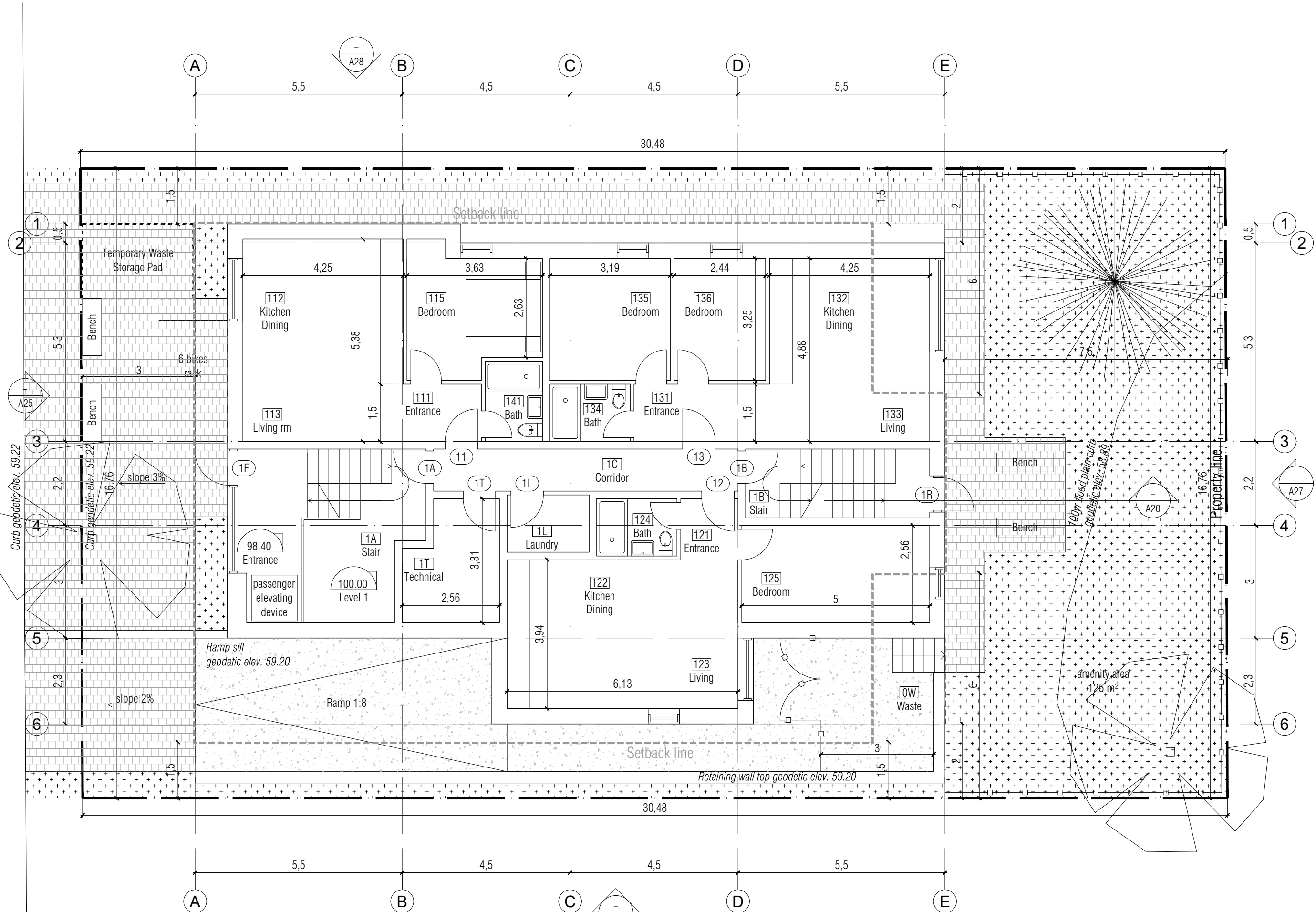
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scale
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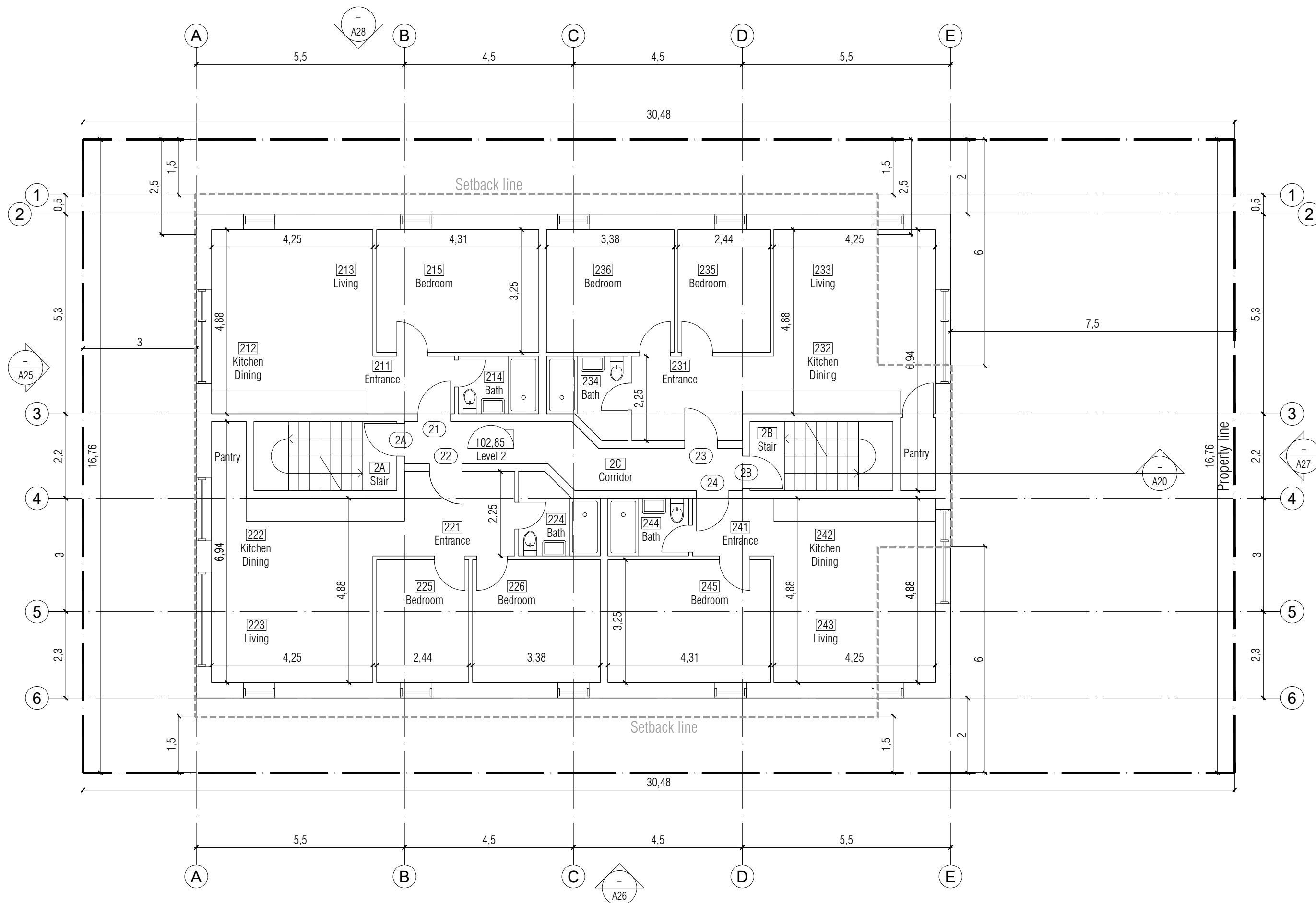
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reference
14-10

title
2nd level

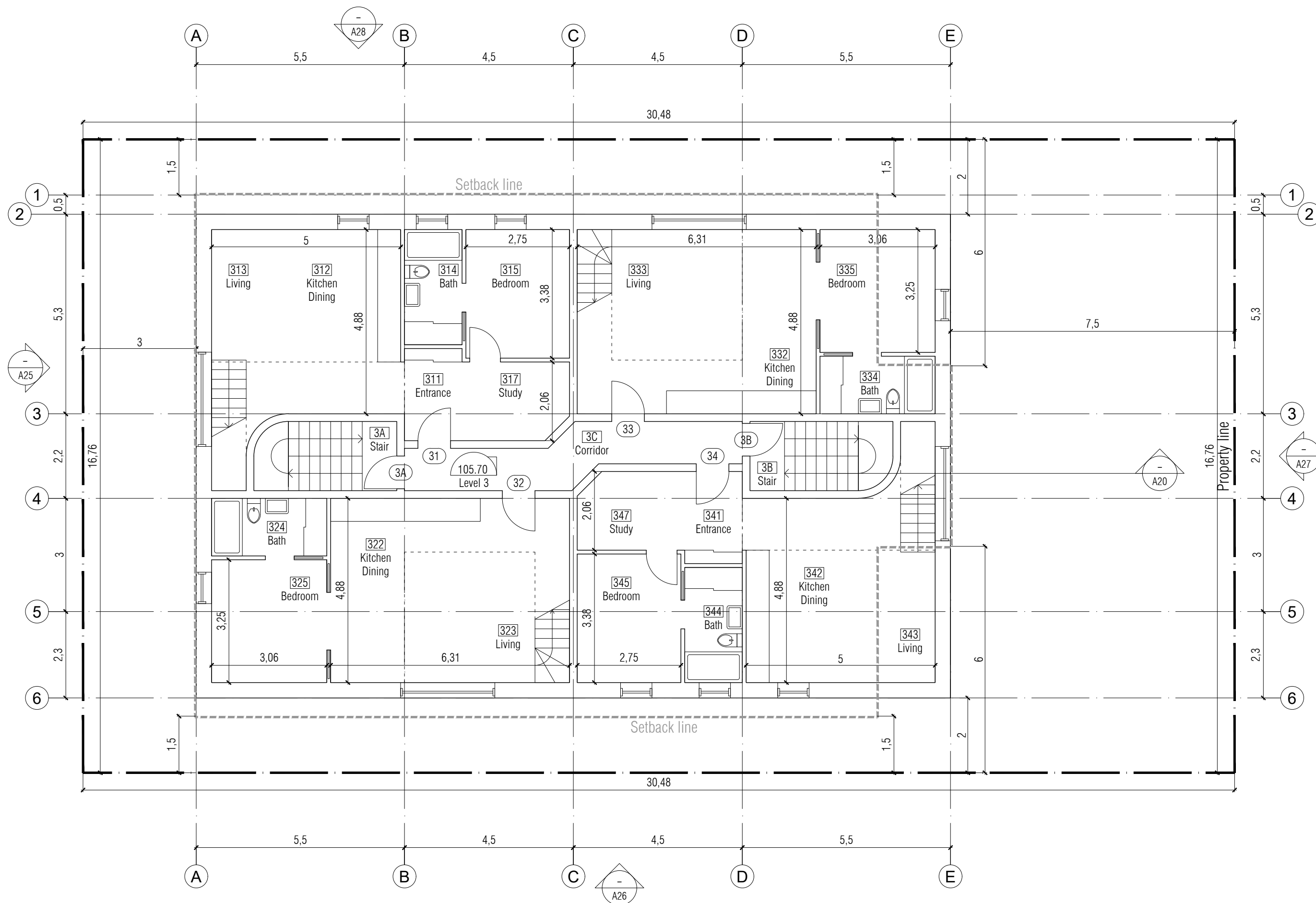
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3rd level

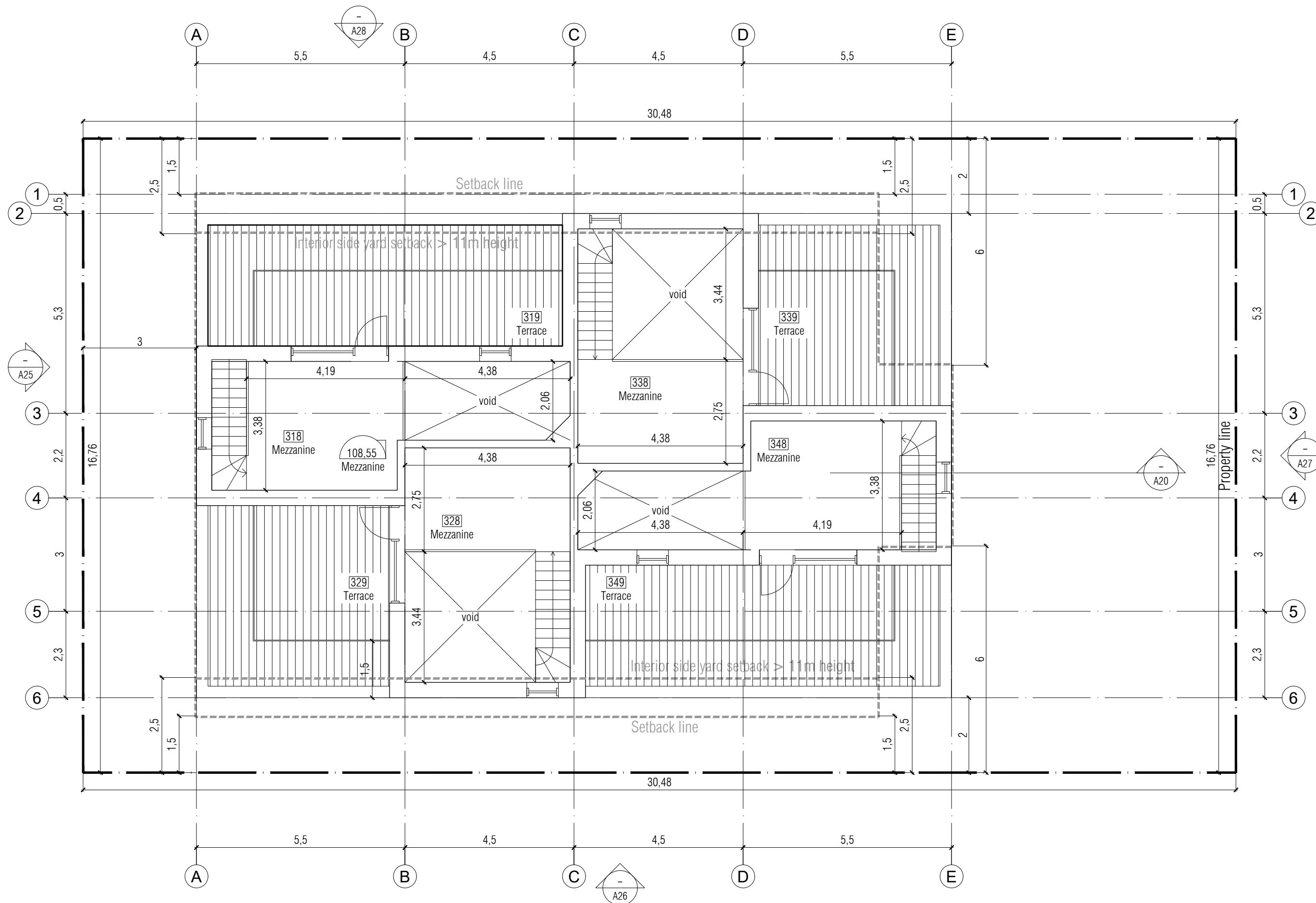
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title
Mezzanine level

scale
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Roof

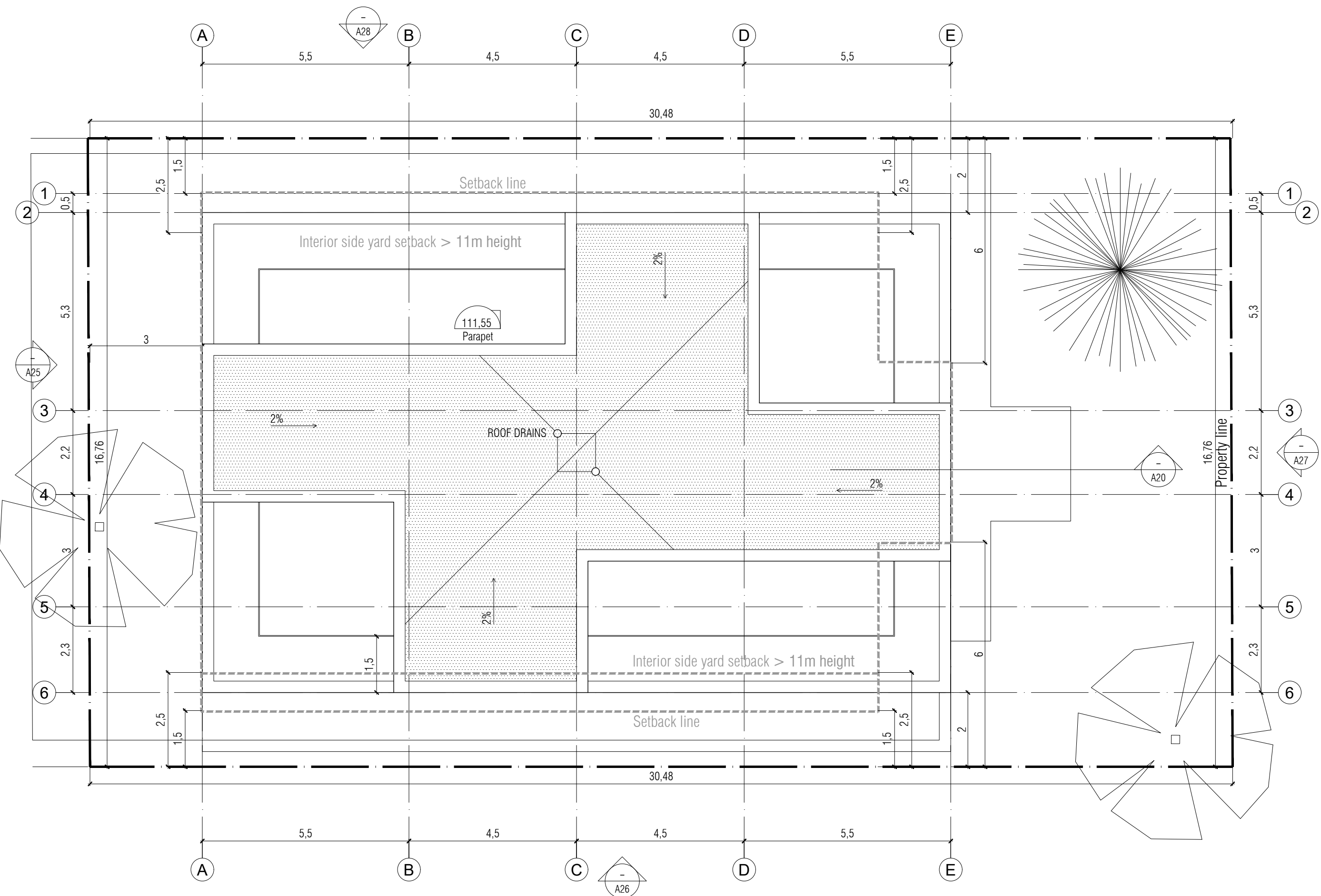
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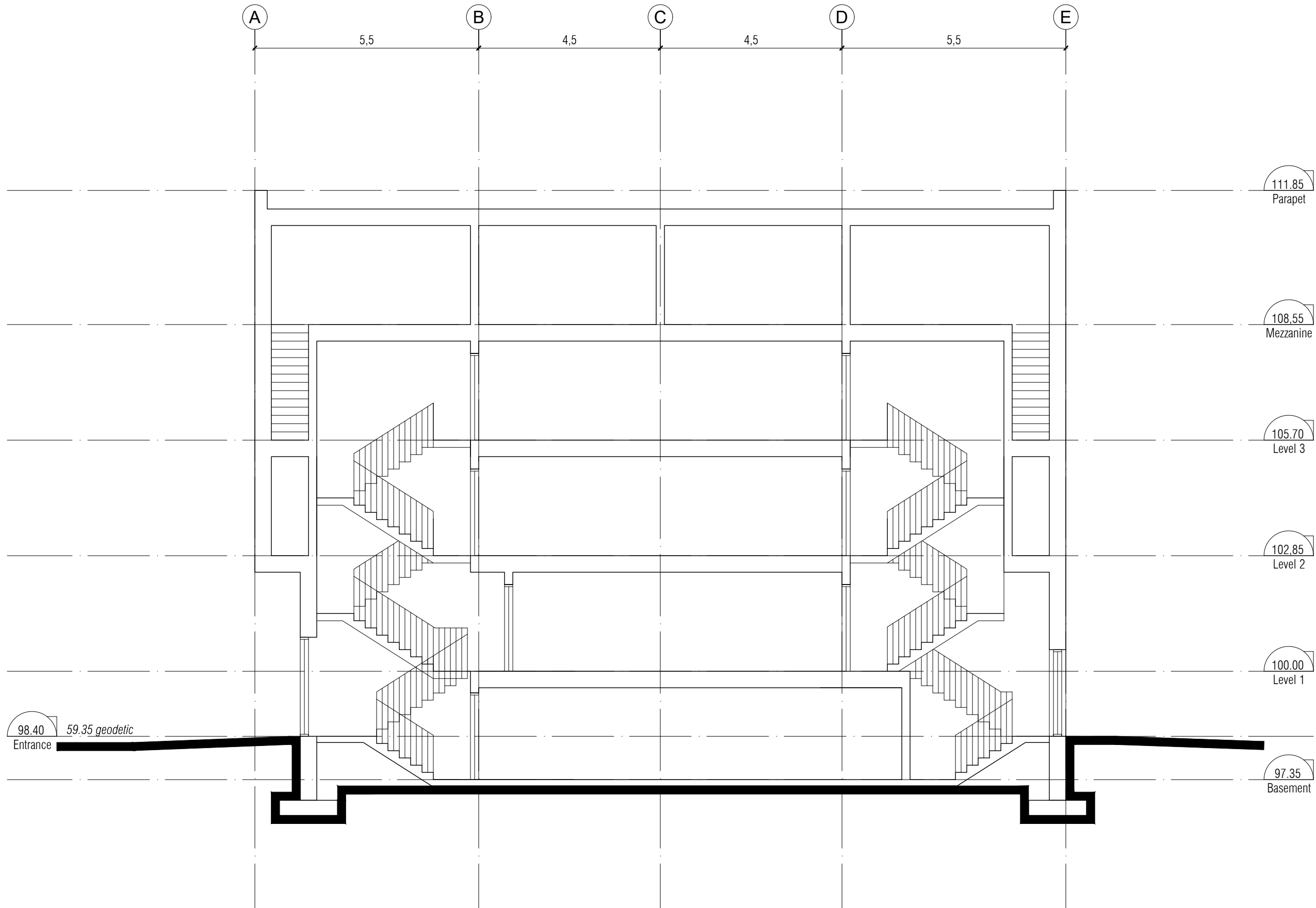
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Cross section

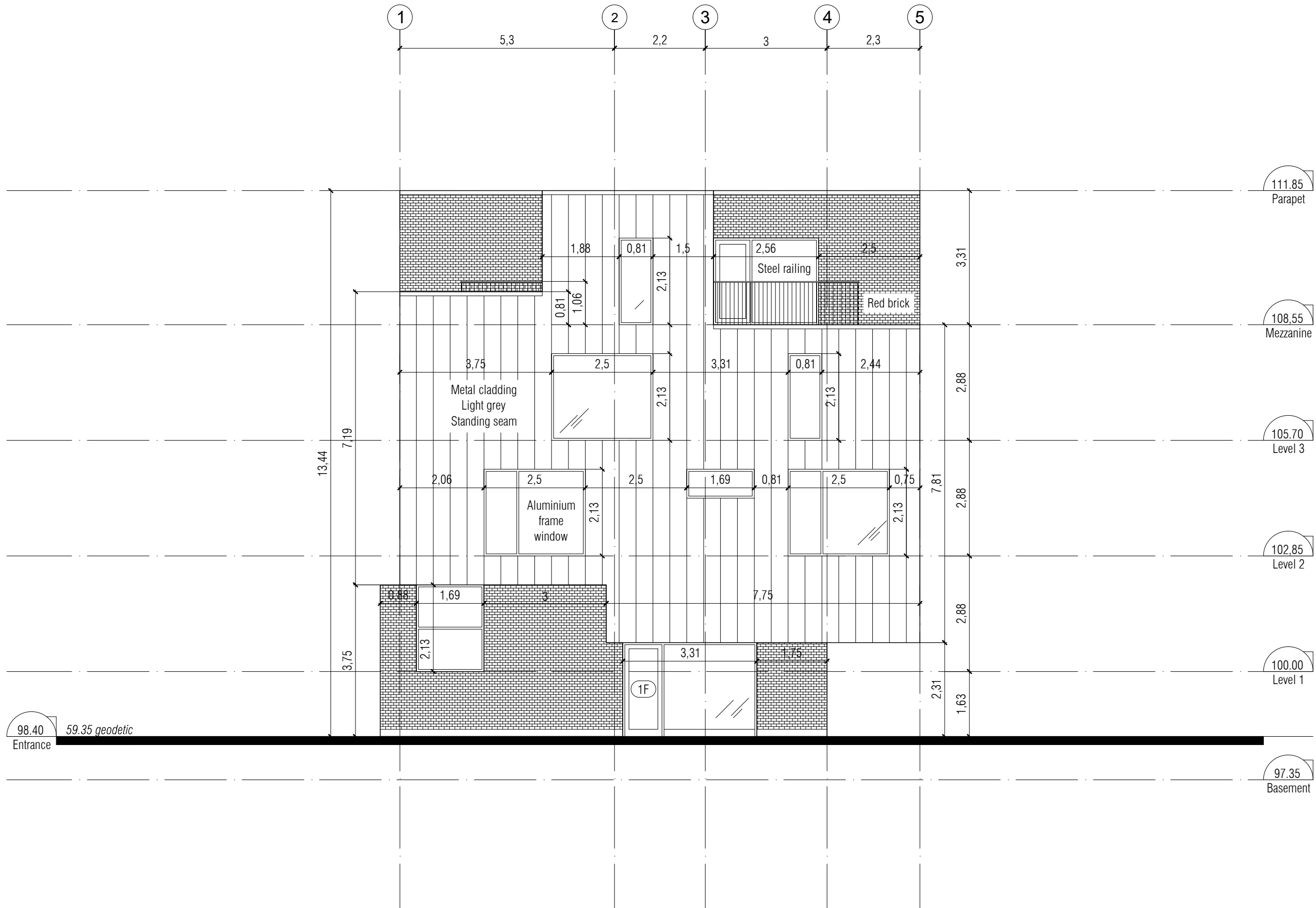
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title
West elevation

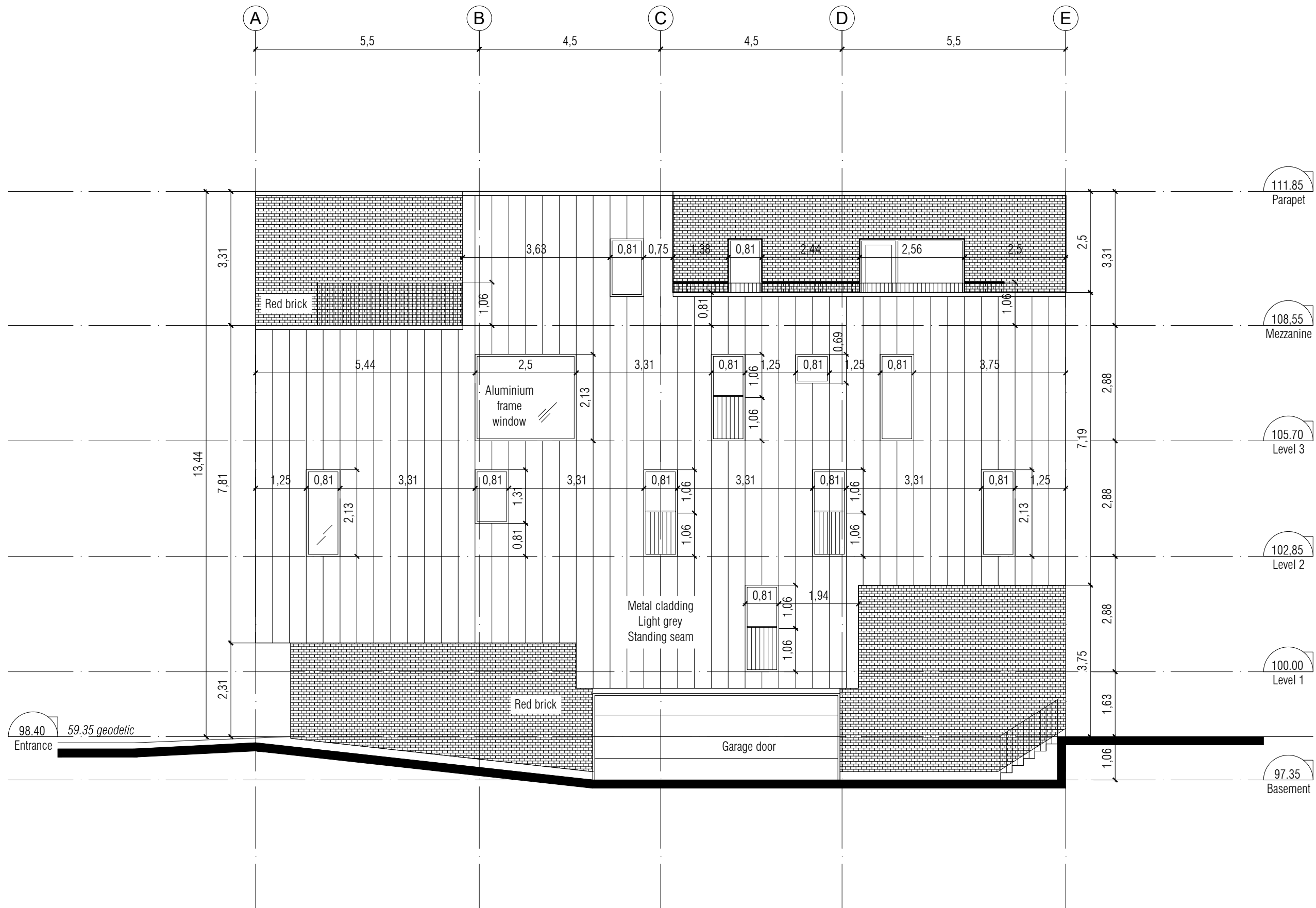
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South elevation

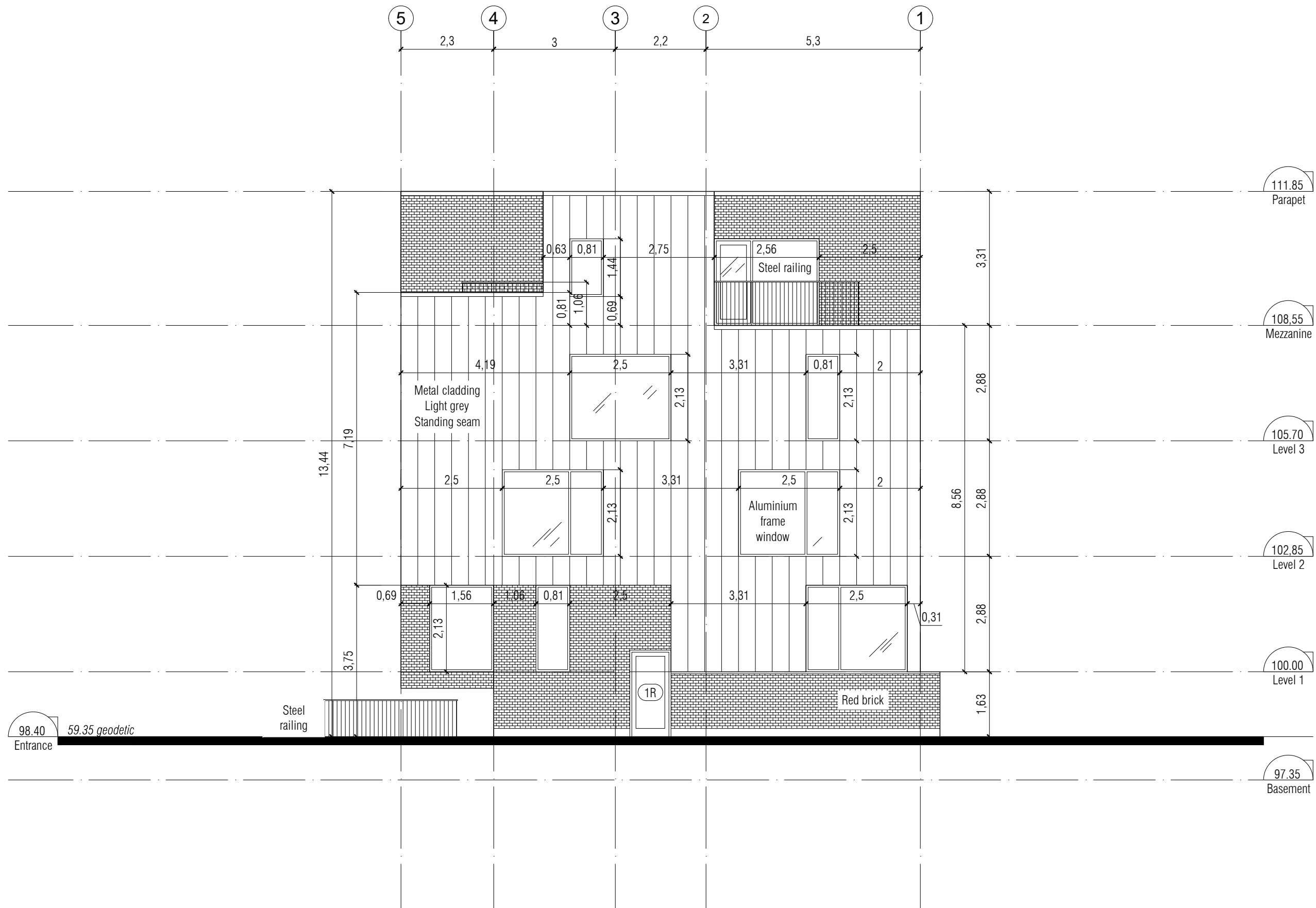
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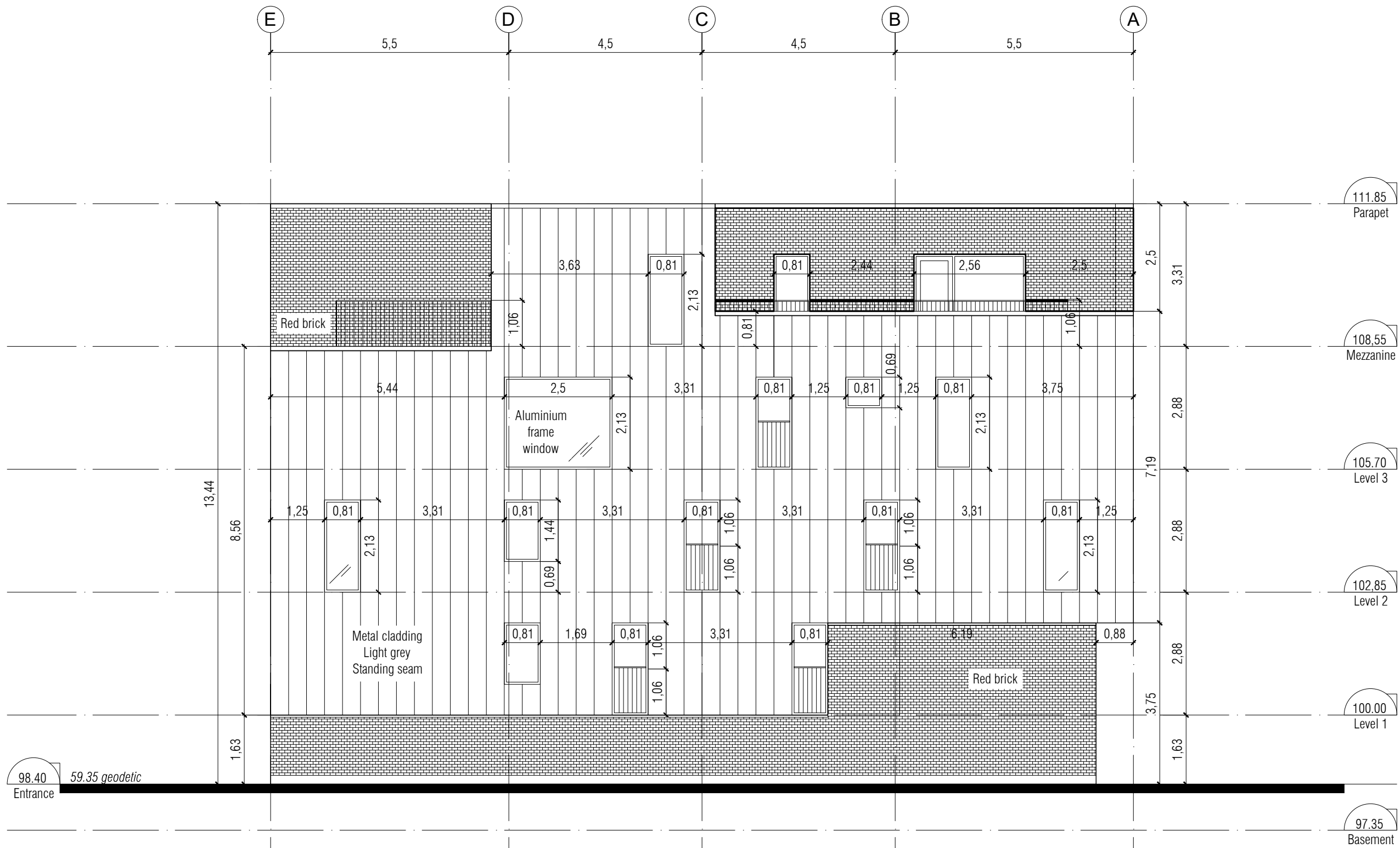
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North elevation

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