

PLANNING RATIONALE REPORT

**1545 WOODROFFE AVENUE - IMPERIAL OIL
CITY OF OTTAWA**

**PREPARED BY: P H ROBINSON CONSULTING
APRIL 2015**

Introduction

This report has been prepared on behalf of Imperial Oil in support of a Site Plan Control application for their property at 1545 Woodroffe Avenue. The legal description of the property is Part of Lot 30 and Part of the Road Allowance between Lots 30 and 31, Concession 1 (Rideau Front), formerly Geographic Township of Nepean, now in the City of Ottawa

The property is located on the north east corner of Woodroffe Avenue and Medhurst Avenue (**see Figure 1 on following page**) in the Hillside community. Lands immediately to the south (south side of Medhurst Avenue) are used as a utility corridor. Further to the south the lands are used for residential purposes. Lands immediately on the west side of Woodroffe Avenue are used as a utility corridor and then further to the west they are used for residential purposes. Lands to the north and east consist of condominium townhouse uses that are separated from the subject property by fencing and vegetation.

The subject property has approximately 103 metres of frontage (338') on the east side of Woodroffe Avenue and a depth of approximately 70 metres (230') along the Medhurst Drive (these measurements include lands at the corner where it is expected that a daylighting triangle dedication will be required). There is also a triangular shaped parcel of land at the north edge of the property which does not have road frontage that is a leftover from an old closed road allowance. As a result, the east property line is approximately 122 metres long (400') compared to the 103 metres of frontage on Woodroffe. The overall legal lot area is 8214 m² (88415 ft² 2.03 ac) for the property. It is likely that there will be a requirement for some small land dedications for road widening purposes along Woodroffe Avenue and Medhurst Drive and a small dedication at the corner of the two roads for a daylighting triangle.

Currently there is a small gas bar kiosk building and 10 pumping locations (8 regular fuel and 2 diesel fuel locations) with an overhead canopy on the west side of the property, a rollover car wash building near the southeast corner of the property and a large stand alone Tim Horton's restaurant near the north east corner of the site. (**see Figure 2 - Existing Survey Plan**) .

Through the applications being filed all of the existing buildings on site will be removed. In their place will be a new larger convenience store with a drive-thru facility located parallel to the east property line, a car wash facility in the north central area of the site and 10 fuel pump locations with an overhead canopy between the convenience store and Woodroffe Avenue. Refer to **Figure 3 (Site Plan)** for details.

The convenience store drive-thru lineup will be a double stacked facility which is now being utilized by Imperial Oil at many of their new facilities for the food

partners that they team up within the convenience store building. The stacking area for the rollover car wash facility will be located between the north side of the convenience store and the north property line. Fencing along the north and east property line will be in accordance with the recommendations in the Acoustical Report being filed with this application.

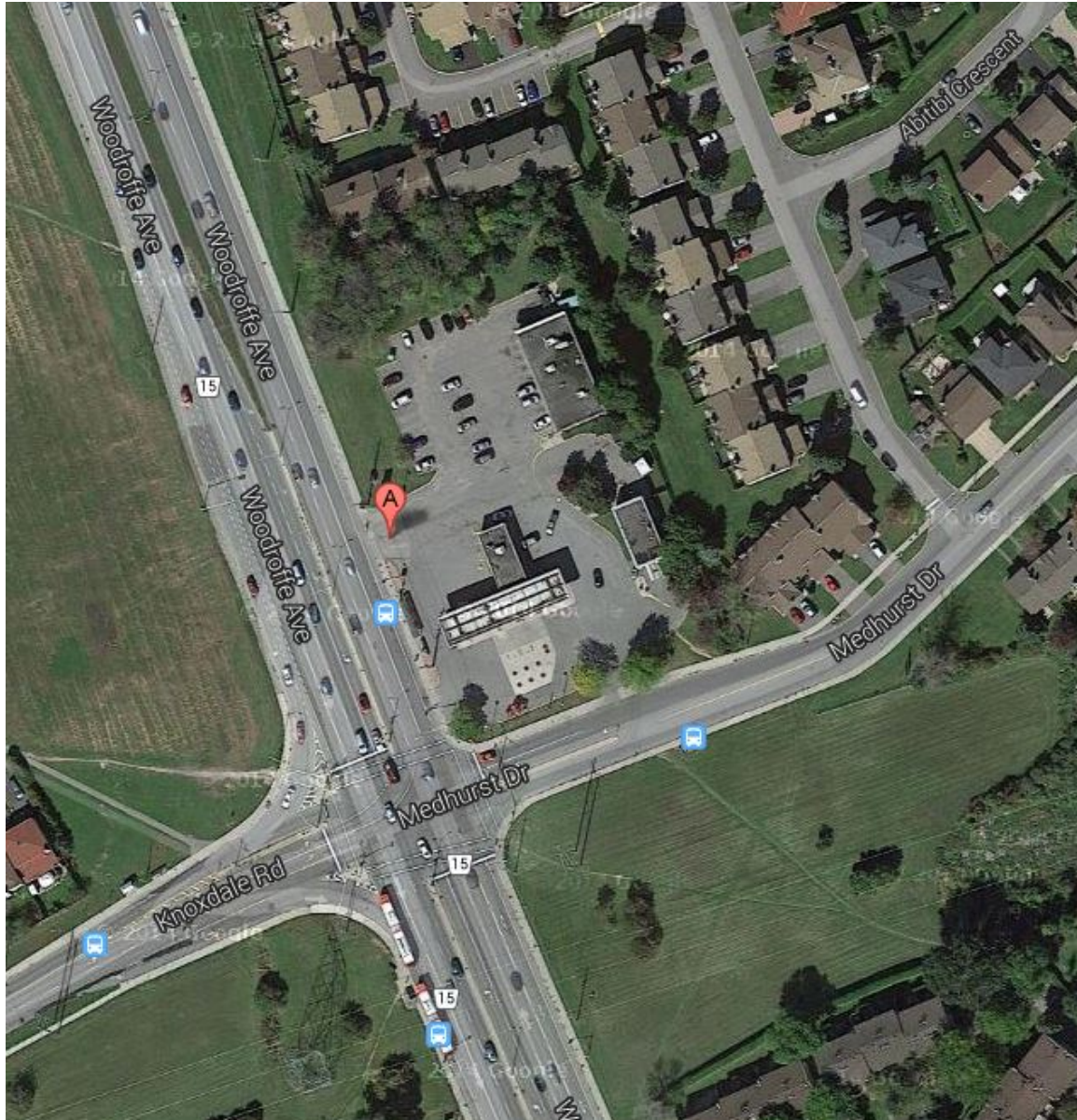


Figure 1 - Location Plan

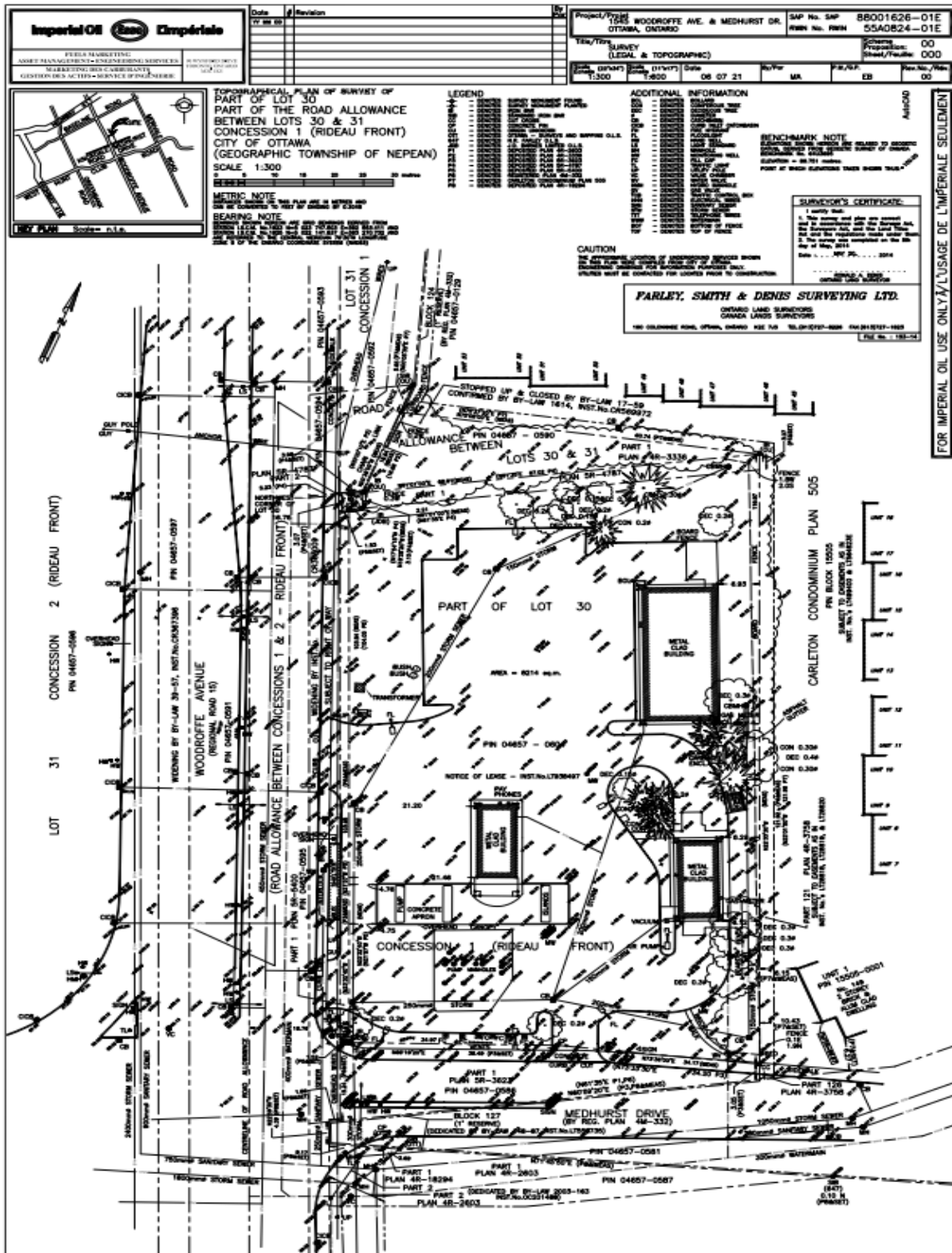


Figure 2 Survey Plan

Development Proposal

The proposal calls for the removal of the small gas bar kiosk and existing car wash building with a new 344 m² (3700 ft²) convenience store and 111 m² rollover car wash (1195 ft²) building to be developed on the site (**see Site Plan drawing - Figure 3**) Architectural elevation drawings of the convenience store, car wash and overhead canopy buildings, the site plan, engineering and landscaping plans are included as full size figures at the end of this Report.

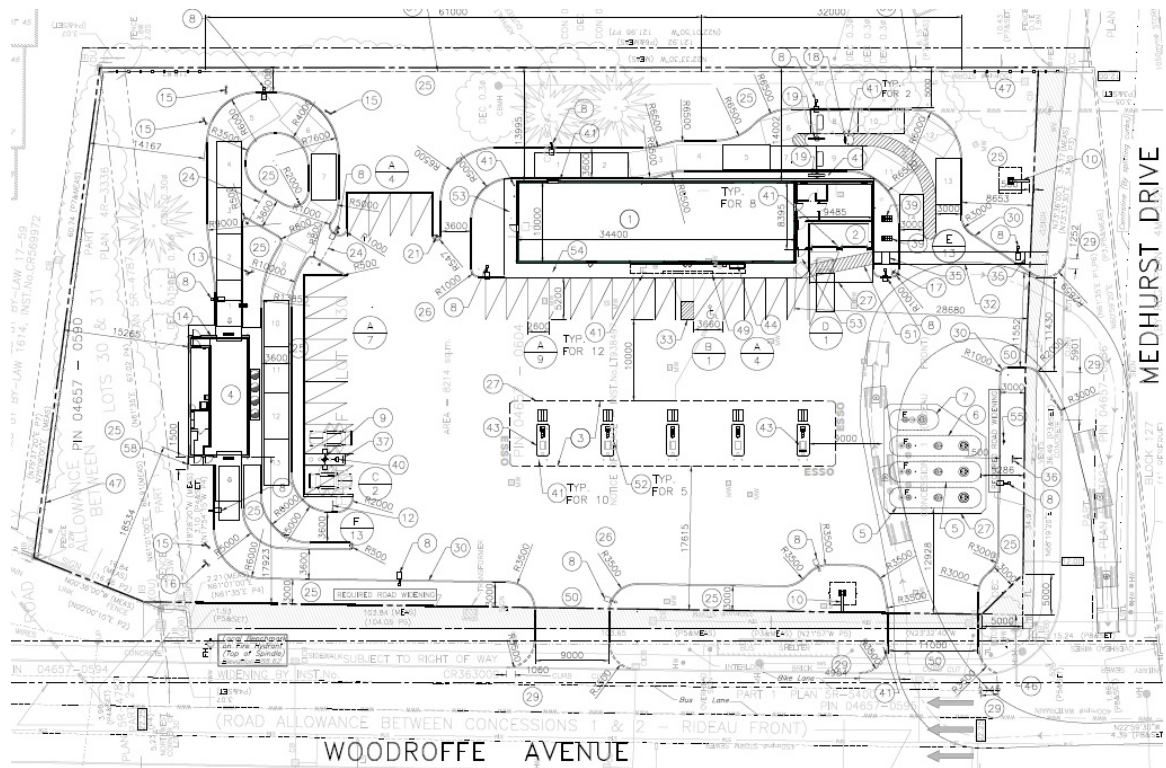


Figure 3 SITE PLAN

The car wash facility will be able to store 13 cars without impeding any vehicular movement on the rest of the site.

The drive-thru lineup for the food partner within the convenience store will be able to accommodate 14 cars in the lineup. The City zoning requirements require a minimum of 11 cars in the drive-thru lineup.

The existing southerly entrance on Woodroffe Avenue and the existing entrance on Medhurst will be maintained. The existing northerly entrance on Woodroffe Avenue will be closed off and a new entrance will be located further to the north. The existing fuel tanks are to remain and the tanker truck will continue to enter the site from the southerly entrance on Woodroffe and exit the site westbound from the Medhurst access.

The existing identification sign along the Woodroffe Avenue frontage will be replaced by a new identification sign located to the south along the Woodroffe frontage.

Garbage facilities for the site will be located on the south side of the proposed convenience store. This will be in a fully enclosed climate controlled extension of the convenience store building and this represents the standard way that Imperial Oil build garbage facilities for new sites in Ottawa.

In total there will be 25 dedicated parking spaces **(24 standard size spaces and 1 handicapped parking space)** provided for the entire site. One of the spaces in front of the convenience store will be for handicapped patrons. The parking requirements for this type of development as per City By-law regulations are as follows:

- Convenience Store = 3.4 spaces per 100 m² of gfa. The total convenience store building is 344 m². Within the convenience store building approximately 151 m² is comprised of the food partner area (10 spaces/100 m² of gfa) and approximately 132 m² is comprised of the convenience store. The areas that can be deducted for floor area calculations (washrooms, mechanical/electrical rooms or common storage)= 61 m². The maximum parking requirement would be 19.58 spaces which would be rounded up to 20 required parking spaces.
- Car Wash = no parking requirement.

The total requirement is 20 spaces and 25 spaces are being provided. The majority of these spaces will be in front of or in very close proximity to the food partner/convenience store building.

The food counter within the convenience store will have a seating capacity of 30 . In addition, customers who are in the store already (already parked in front of the store or at a pump) will be able to get food/drink products to take out.

Background studies addressing civil engineering, acoustical noise, landscaping, environmental and traffic aspects are being filed as part of this Site Plan Application application.

The existing infrastructure in the adjacent Right of Ways (Woodroffe Avenue and Medhurst Drive) will be utilized. Through this redevelopment proposal, there will be acoustical fencing installed along the east and north property lines in order to ensure that there is no negative impact from a noise point of view to the adjacent townhouse lands.

New landscaping will be planted through this development to screen the car wash and the rear of the convenience store/drive-thru operation from adjacent lands.

There will be three access points (one on Medhurst and two on Woodroffe) for this site after the redevelopment with 2 of the existing access points being retained and the northerly access point on Woodroffe will be in close proximity to the current access point. The proposed site plan allows for parking totals in excess of city zoning requirements and the stacking spaces for the car wash and drive-thru facility also exceed zoning requirements. The net impact in terms of traffic for the site is not expected to create undue impacts as the overall seating capacity of restaurant space on the property is going from 70 seats to 30 seats while the re-development of the site will now have a drive-thru facility for the restaurant

Zoning

The subject lands are in a General Mixed Use Zone (GM15 H (9.5)). An automobile service station, car wash and gas bar are all permitted uses that this zoning category permits.

The proposed uses of the site are in conformity with the existing zoning and the majority of the setback requirements that apply to this site.

The following are the General Mixed Use Zone zoning requirements and how the proposed development meets these requirements.

	REQUIRED	PROPOSED
Minimum Lot Area	No min	8213 m ²
Minimum Lot Width	No min	70m (along Medhurst Drive)
Minimum front yard (Medhurst)	3 m	25 m to canopy
Minimum corner side yard (Woodroffe)	3 m	19 m
Minimum interior side yard	5 m	14 m
Minimum Rear yard	7.5 m	15.5 m
Maximum Building Height	18 m	5.9 m

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Landscape Strip	3 m	5 m
Queuing spaces	11 spaces	14 spaces

Provincial Policy Statement

The current Provincial Policy Statement (PPS) came into effect in 2014 and it outlines the key matters of Provincial planning interest with respect to land use planning decisions made by municipal approval authorities. Any decisions that are made by municipal approval authorities must be consistent with the policies of the PPS.

The following are sections of the PPS that are applicable to additional development at the subject property.

- 1.1.1 Healthy, liveable and safe communities are sustained by:
 - a) promoting efficient development and land use patterns which sustain the financial well-being of the Province and municipalities over the long term; *This re-development on the site represent an efficient intensification of the site and will not create inefficient land use patterns in the area.*
 - e) promoting cost-effective development standards to minimize land consumption and servicing costs. *The intensification of the site will be achieved through a larger convenience store and a relocated car wash facility on the property. In addition, this redevelopment of the site will allow for noise attenuation features that were previously not in place that will ensure that adjacent residential properties are not subject to adverse noise impacts.*
 - f) improving accessibility for persons with disabilities and the elderly by removing and/or preventing land use barriers which restrict their full participation in society. *The new building will be completely accessible for persons with disabilities and will have a dedicated handicapped parking space in front of the building.*
- 1.3 Employment Areas
 - 1.3.1 Planning authorities shall promote economic development and competitiveness by:
 - a) providing for an appropriate mix and range of employment (including industrial, commercial and institutional uses) to meet long-term needs; *The introduction of a larger convenience store and a more modern car wash facility will allow for a greater range and mix of employment opportunities for the property*
 - d) ensuring the necessary infrastructure is provided to support current and projected needs. *The redevelopment of the site is based on the new convenience store and car wash hooking up to the existing municipal services which are located within the Rights of Way of Woodroffe Avenue and Medhurst Drive and are of sufficient size and capacity to*

accommodate this re- development. On site stormwater retention will be employed to ensure that there are no adverse effects on the existing storm water sewers along Medhurst Avenue.

- 1.6 Infrastructure and Public Service Facilities
- 1.6.2 The use of existing infrastructure and public service facilities should be optimized, wherever feasible, before consideration is given to developing new infrastructure and public service facilities. *The existing municipal infrastructure will be utilized for this re-development of the site.*
- 1.6.4.2 Municipal sewage services and municipal water services are the preferred form of servicing for settlement areas. Intensification and redevelopment within settlement areas on existing municipal sewage services and municipal water services should be promoted, wherever possible. *The redevelopment of the site is within a settlement area and will be utilizing municipal infrastructure that is available at the property line.*

Official Plan Designation – City of Ottawa Official Plan

The entirety of the property is located within the General Urban Area designation of the City of Ottawa Official Plan

The General Urban Area designation permits the development of a full range and choice of housing types to meet the needs of all ages, incomes and life circumstances, in combination with conveniently located employment, retail, service, cultural, leisure, entertainment and institutional uses. This will facilitate the development of complete and sustainable communities. A broad scale of uses is found within this designation, from ground-orientated single-purpose to multi-storey mixed use; from corner store to shopping centre.

The proposed redevelopment of the property is in conformity with the General Urban Area designation of the Official Plan.

Section 4.11 in the City of Ottawa Official Plan deals with compatibility. The following are the applicable compatibility criteria that need to be evaluated for this site plan application which represents a redevelopment of the property.

- a. Traffic: Roads should adequately serve the development, with sufficient capacity to accommodate the anticipated traffic generated. Generally development that has the potential to generate significant amounts of vehicular traffic should be located on arterial or major collector roadways so as to minimize the potential for traffic infiltration on minor collector roadways and local streets;

Woodroffe Avenue is an Arterial Road in the City Official Plan and it has sufficient capacity to handle the expected traffic flow for this

development. The majority of traffic will be entering and exiting the site via Woodroffe Avenue and it is expected that local traffic from the residential neighbourhood to the east will be using the Medhurst Drive access. Any additional traffic generated as a result of the addition of a drive-thru facility for the branded food partner will be reduced by the fact that the restaurant seating capacity is being reduced from about 70 seats to about 30 seats.

b. Vehicular Access: The location and orientation of vehicle access and egress should address matters such as the impact of noise, headlight glare and loss of privacy on development adjacent or immediately opposite. Vehicular access and egress for development that has the potential to generate a significant amount of vehicular traffic should be oriented on streets other than local streets, wherever the opportunity exists, considering traffic safety and other transportation objectives of this Plan;

The location and sizes of the proposed access points are close in location and width to the existing situation. The location and width of these access points also takes into account the route that the fuel delivery truck will need to make in order to safely access the site, deliver fuel to the station and exit the site all in a forward motion. The proposed access points to the site do not impact adjacent residential properties in terms of headlight glare or noise.

c. Parking Requirements: The development should have adequate on-site parking to minimize the potential for spillover parking on adjacent areas. A range of parking forms, including surface, decked, and underground, should be considered taking in account the area context and character. Opportunities to reduce parking requirements and promote increased usage of walking, cycling and transit should be considered, where appropriate, particularly in the vicinity of transit stations or major transit stops in accordance with the provisions of Section 4.3;

The number of parking spaces being proposed for the redevelopment is in conformity with City zoning standards for this type of development with a convenience store, sit down restaurant and car wash facility. There are parking spaces in front of the convenience store/restaurant for patrons and it is expected that the spaces to the north of the convenience store will be used by staff. These parking spaces locations do not interfere with the turning movements of vehicles entering or exiting the fuel pumping locations or the car wash facility. Bicycle racks are to be provided on the property for the use of patrons.

d. Building Height and Massing: New buildings should have regard to the area context - the massing and height of adjacent buildings, and planned function for the area. Application of design principles that contribute to a sense

of human scale will improve and enhance user comfort and the perception of new development within its existing context. Where variation in building height or massing is appropriate, a transition in building heights is desirable. The desire for a transition in building heights can be offset where natural buffers and setbacks exist and/or through the use of appropriate design measures to create a more pedestrian-friendly at-grade environment;

The proposed building and the extension of the overhead canopy on the property will be generally the same height as what is currently there. The car wash facade facing Woodroffe Avenue will be constructed of brick and will have a flat roof. (see architectural elevation drawings on the following pages). The exterior facade of the convenience store/restaurant building and the car wash will consist of brick and stucco finishes that have been utilized by Imperial Oil in their new facilities for the last few years. The building heating and cooling equipment will be hidden from view. The existing overhead canopy for the fuelling stations will partially screen the convenience store from Woodroffe Avenue. There will be planting and new fencing and sizeable setbacks between the proposed new buildings and existing residential buildings on adjacent properties.

e. Pattern of the Surrounding Community: Where the height, building mass, proportion, street setback and distance between buildings for the proposed development varies from the pattern for the area, the proposed design may compensate for this variation through its treatment of other characteristics common to the surrounding community;

The exterior elevations, scale and massing of the proposed building will be compatible with residential and commercial buildings in the immediate vicinity. The proposed street setback is also compatible with surrounding properties.

f. Outdoor Amenity Areas: The development should respect the privacy of outdoor amenity areas of adjacent residential units and minimize any undesirable impacts through the siting and design of the buildings and the use of screening, lighting, landscaping or other mitigative design measures;

The proposed redevelopment will add noise attenuation fencing along the north and east property lines to provide additional screening for the outdoor amenity areas of adjacent properties. The current site fencing does not provide any noise attenuation. Along these property lines there will be additional landscaping and any site lighting will be designed to ensure that there is no spillage on to adjacent properties.

g. Loading Areas, Service Areas, and Outdoor Storage: The operational characteristics and visual appearance of loading facilities, service areas (including garbage), parking and areas for the outdoor storage of goods or

materials should be mitigated using a variety of methods (e.g., location, containment, screening, berms, and/or landscaping). These uses and activities should be located away from residences where possible;

The redevelopment of the property does not require a loading area based on the zoning by-law. However, there will be a loading space that will also serve as the parking space for the garbage truck when there is a pickup required. The parking areas are screened from adjacent residential properties by the convenience store and the new fencing and landscaping.

h. Lighting: The potential for light spill over or glare from any lighting source onto adjacent light-sensitive areas should be avoided or mitigated;

The proposed lighting has been designed with cut off fixtures to ensure that there is no spillover to adjacent properties. This applies to both the site lighting and also the lighting on the underside of the canopy above the gas pumps. The lighting plan provides further details.

Noise and Air Quality: The development should be located and designed to minimize the potential for significant adverse effects on adjacent sensitive uses related to noise, odours, and other emissions.

The mechanical, electrical, heating and cooling systems for the convenience store are all screened and hidden and their noise levels minimized. There will be an acoustical fence built along the north and east property lines to bring any ambient noise levels from the equipment and car wash lineup to the accepted levels. At the present time, the site fencing does not provide any acoustical attenuation from the existing car wash facility.

j. Sunlight: The development should minimize shadowing on adjacent properties, to the extent practicable, particularly on outdoor amenity areas, through the siting of buildings or other design measures;

The proposed convenience store/restaurant building is one storey in height (maximum height = 5.9 metres) and is located approximately 14 metres from the east property line. The proposed car wash is located a minimum of 15 metres from the north property line. It will not cause any undue shadowing on adjacent properties. Acoustical fencing will be located along the east and north property lines. Currently, the site fencing does not provide any acoustical function.

k. Microclimate: The development should be designed to minimize adverse effects related to wind, snow drifting, and temperature on adjacent properties;

The low profile of the proposed development plus existing and proposed landscaping and new fencing will minimize any possible micro-climate impacts on adjacent properties.

I. Supporting Neighbourhood Services: The development should contribute to or be adequately served by existing or proposed services and amenities such as health facilities, schools, parks and leisure areas. Where the proposed development itself is to contribute such services and amenities, they should be of a scale appropriate to the needs and character of the area.

The proposed development will be of an appropriate scale for the adjacent neighbourhood and customers travelling along Woodroffe Avenue.

In summary, the proposed development recognizes and addresses all of the required computability criteria. Based on these policies, the development minimizes its impact on the surrounding community and with its location on Woodroffe Avenue, a major arterial road, it is an ideal location for a commercial redevelopment that is compatible with adjacent properties.

The other important planning documents that must be reviewed are the Urban Design Guidelines for Gas Stations and Urban Design Guidelines for Drive-Through Facilities. These two sets of guidelines were approved by City Council on May 24, 2006.

City of Ottawa Urban Design Guidelines for Gas Stations

The City of Ottawa's Urban Design Guidelines provide urban design guidance that are to be applied throughout the city for all gas station development in order to assess, promote and achieve appropriate development of gas stations. Accordingly, when gas stations are located together with drive-through facilities, the guidelines for drive through facilities also apply.

Current design of Imperial Oil (Esso) at 1545 Woodroffe Avenue

Within the Urban Design Guidelines for Gas Stations, there are guidelines to minimize impacts on adjacent land uses that could be caused by on-site activities. The current Imperial Oil station does not comply with these guidelines due to a lack of sound barriers adjacent to neighbouring residential properties and also the current car wash location at the south side of the property does not allow for sufficient stacking spaces for the car wash lineup.

The current station does not sufficiently provide safe access for pedestrian and cyclist traffic and there is no dedicated bicycle parking being provided.

Proposed design of Imperial Oil (Esso) at 1545 Woodroffe Avenue

The proposed design incorporates landscaping to provide a separation of the on site land uses from the residential properties to the north and east. In order to facilitate the safe movement of the tanker truck used for fuel deliveries and to ensure that the appropriate number of vehicles can be safely located on the property as part of the car wash line-up and the drive-thru for the fast food facility, it is necessary to locate the convenience store near the rear of the site. In addition, it is advantageous for the pumps and convenience store to be located so that they are most prominent to the main frontage street (Woodroffe Avenue) for the travelling public.

The proposed new development meets Guidelines 26 and 27 by providing a minimum “2.5 metre wide landscape area along the site’s side and rear yards” and a “minimum 3.0 metre wide landscape area, which may include a solid wall or fence in addition to planting, at the edges of sites that are adjacent to residential or institutional properties.” The actual landscape areas shown on the site plan are a minimum of 5 metres along the side yard (east property line) and approximately 15 metres along the rear yard (north property line). In addition to the landscape strip with planting that is in excess of the Guidelines, most of the east and north property lines will also contain an acoustical fence that will allow for privacy and acoustical screening to the adjacent residential properties. A significant portion of the north property line has a landscaped strip almost 15 metres in depth. Large areas of the east property line adjacent to the rear of the convenience store building actually have a landscaped strip approximately 10 metres in depth.

Portions of the waiting lanes for the car wash and the fast food facility are located adjacent to the residential lands to the east and north however acoustic fencing will lower the impact on the residential community. The parking for the development is screened from the residential properties to the north and east by the fencing along the property line, landscaped areas and the car wash/convenience store building. None of the parking spots on the property will be visible from the adjacent residential properties.

City of Ottawa Urban Design Guidelines for Drive Through Facilities

Many of the Urban Design Guidelines for Drive Through Facilities are similar to those discussed above for Gas Stations. Below is a discussion of the guidelines that are unique for Drive Through Facilities.

In compliance with the guidelines, the proposed car wash and fast food drive through facilities are located at the rear of the building and the vehicular access points are located sufficiently away from the intersection.

Guideline 7 suggests that ‘the majority of the pedestrian level façade facing the street highly transparent with clear glass windows and doors that animate public streets and maximize views in and out of the building’. The proposed convenience store front façade will face Woodroffe Avenue and will consist of access doors and a high proportion of clear glass windows for views in and out of the building.

According to Guidelines 21 and 22, it is important to “locate stacking lanes away from adjacent sensitive uses, such as residential and outdoor amenity areas”, and “avoid locating the stacking lane between the building and the public street”. Guideline 24 also suggests separating stacking lanes from parking areas. The stacking lane for the car wash has been screened from the nearby residential areas through landscaping (3 metres wide in one isolated location but generally between 10 and 15 metres in depth) coupled with new cedar sound attenuation walls along the north and east property lines. The stacking lanes are separate from the parking areas and do not interfere with parking movements. The stacking lane is not between the building and the street and will be screened from the adjacent properties through fencing and landscaping.

Conclusions

The proposed redevelopment of the Imperial Oil (Esso) property at 1545 Woodroffe Avenue is consistent with current zoning and Official Plan policies for these lands.

The site represents a sensitive redevelopment that will allow for:

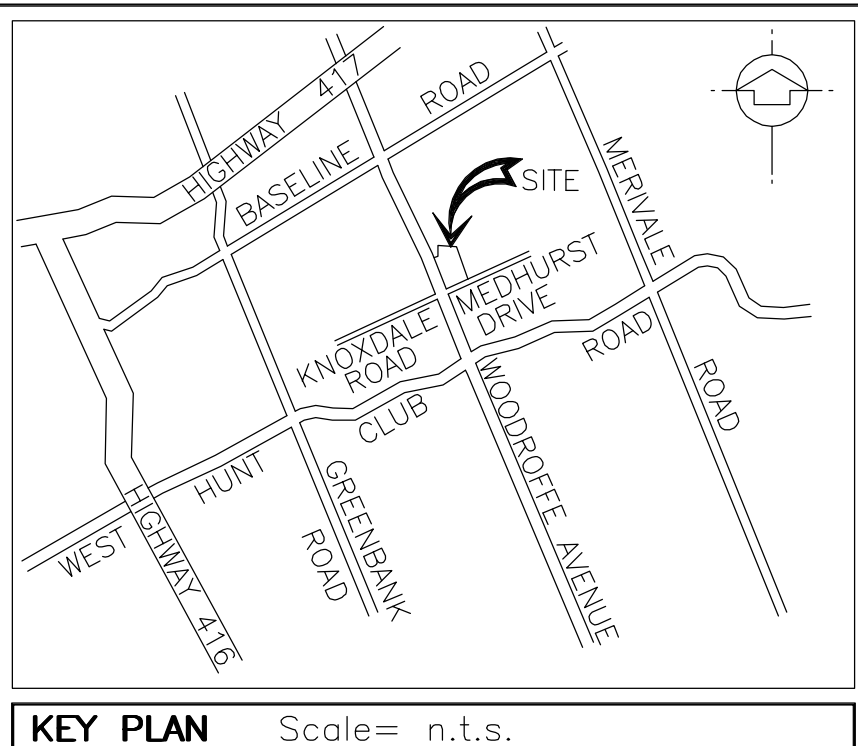
- a well designed contemporary building to be located on the property
- adequate parking for the development
- a well designed site that will allow for customer movement and safe pedestrian movement within the site
- a connection to existing municipal infrastructure that is currently located beneath adjacent roads or on the property itself.

Through the site plan process there are a number of key engineering studies and drawings required in support of the development. These studies show that the site can be serviced through existing municipal infrastructure. The overall site drainage and grading has been designed based on current City and Conservation Authority guidelines and will have a peak discharge rate to match current peak discharge rates. The majority of the site landscaping will be provided along the north and east edges of the property.

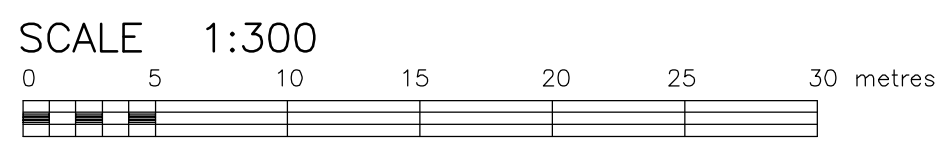
It is being proposed at an appropriate scale of development and will be compatible with surrounding land uses.

Date	#	Revision	By
YY MM DD			Par

Project/Projet 1545 WOODROFFE AVE. & MEDHURST DR. OTTAWA, ONTARIO	SAP No. SAP RWIN No. RWIN	88001626-01E 55A0824-01E
Title/Titre SURVEY (LEGAL & TOPOGRAPHIC)	Scheme Proposition:	00 000
Scale Echelle 1:300	Scale Echelle 1:600	Date 06 07 21
	By/Par MA	P.M./G.P. EB
		Rev.No./Rév. 00



TOPOGRAPHICAL PLAN OF SURVEY OF
PART OF LOT 30
PART OF THE ROAD ALLOWANCE
BETWEEN LOTS 30 & 31
CONCESSION 1 (RIDEAU FRONT)
CITY OF OTTAWA
(GEOGRAPHIC TOWNSHIP OF NEPEAN)



METRIC NOTE
DISTANCES SHOWN ON THIS PLAN ARE IN METRES AND
CAN BE CONVERTED TO FEET BY DIVIDING BY 0.3048

BEARING NOTE
BEARINGS SHOWN HEREON ARE GRID BEARINGS DERIVED FROM
STATION I.S.C.M. No.1623 N=5 023 747.503 E=362 553.411 AND
STATION I.S.C.M. No.1625 N=5 022 141.837 E=363 270.732 AND
ARE REFERRED TO THE CENTRAL MERIDIAN 76°30'W LONGITUDE
ZONE 9 OF THE ONTARIO COORDINATE SYSTEM (NAD83)

LEGEND

+	DENOTES SURVEY MONUMENT FOUND
⊕	DENOTES SURVEY MONUMENT PLANTED
IB	DENOTES IRON BAR
SIB	DENOTES STANDARD IRON BAR
CC	DENOTES CUT CROSS
CP	DENOTES CONCRETE PIN
OU	DENOTES ORIGIN UNKNOWN
OTT	DENOTES OTTAWA - SURVEYS AND MAPPING O.L.S.
647	DENOTES H.R. FARLEY O.L.S.
JDB	DENOTES J.D. BARNES LIMITED O.L.S.
P1	DENOTES DEPOSITED PLAN 4R-2603
P2	DENOTES DEPOSITED PLAN 4R-3336
P3	DENOTES DEPOSITED PLAN 5R-3623
P4	DENOTES DEPOSITED PLAN 5R-4787
P5	DENOTES DEPOSITED PLAN 5R-5400
P6	DENOTES REGISTERED PLAN 4M-332
P7	DENOTES CARLETON CONDOMINIUM PLAN 505
P8	DENOTES DEPOSITED PLAN 4R-18294

ADDITIONAL INFORMATION

BOL	DENOTES BOLLARD
CON	DENOTES CONIFEROUS TREE
DEC	DENOTES DECIDUOUS TREE
Ø	DENOTES DIAMETER
CB	DENOTES CATCHBASIN
CICB	DENOTES CURB INLET CATCHBASIN
FL	DENOTES FLOODLIGHT
FH	DENOTES FIRE HYDRANT
LS	DENOTES LIGHT STANDARD
MH	DENOTES MANHOLE
MW	DENOTES MONITORING WELL
FC	DENOTES FILL CAP
TL	DENOTES TRAFFIC LIGHT
UP	DENOTES UTILITY POLE
VC	DENOTES VALVE CHAMBER
WV	DENOTES WATER VALVE
HMH	DENOTES HYDRO MANHOLE
GV	DENOTES GAS VALVE
TCB	DENOTES TRAFFIC CONTROL BOX
HWH	DENOTES ELECTRICAL WIRES
SAN	DENOTES SANITARY SEWER
STM	DENOTES STORM SEWER
TTT	DENOTES TELEPHONE WIRES
WWW	DENOTES WATERMAIN
BOF	DENOTES BOTTOM OF FENCE
TOF	DENOTES TOP OF FENCE

BENCHMARK NOTE

ELEVATIONS SHOWN HEREON ARE RELATED TO GEODETIC DATUM, DERIVED FROM GEODETIC SURVEY OF CANADA BENCHMARK No. 64U3695
ELEVATION = 86.751 metres.
POINT AT WHICH ELEVATIONS TAKEN SHOWN THUS: + 100.00

SURVEYOR'S CERTIFICATE:

I certify that:
1. This survey and plan are correct and in accordance with the Surveys Act, the Surveyors Act, and the Land Titles Act and the regulations made under them.
2. The survey was completed on the 5th day of May, 2014.
Date: . . . MAY 20, . . . 2014

RONALD A. DENIS
ONTARIO LAND SURVEYOR

CAUTION

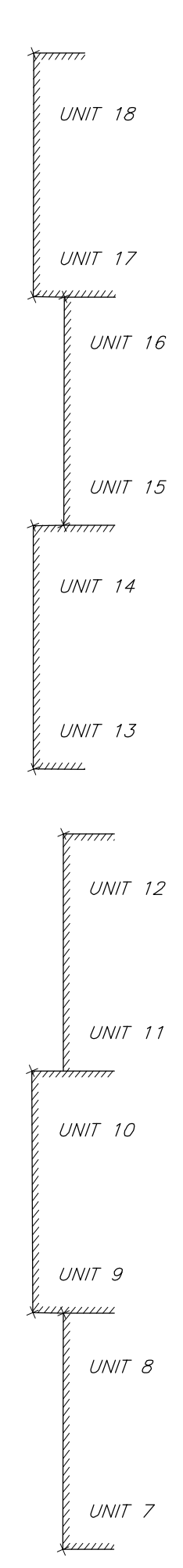
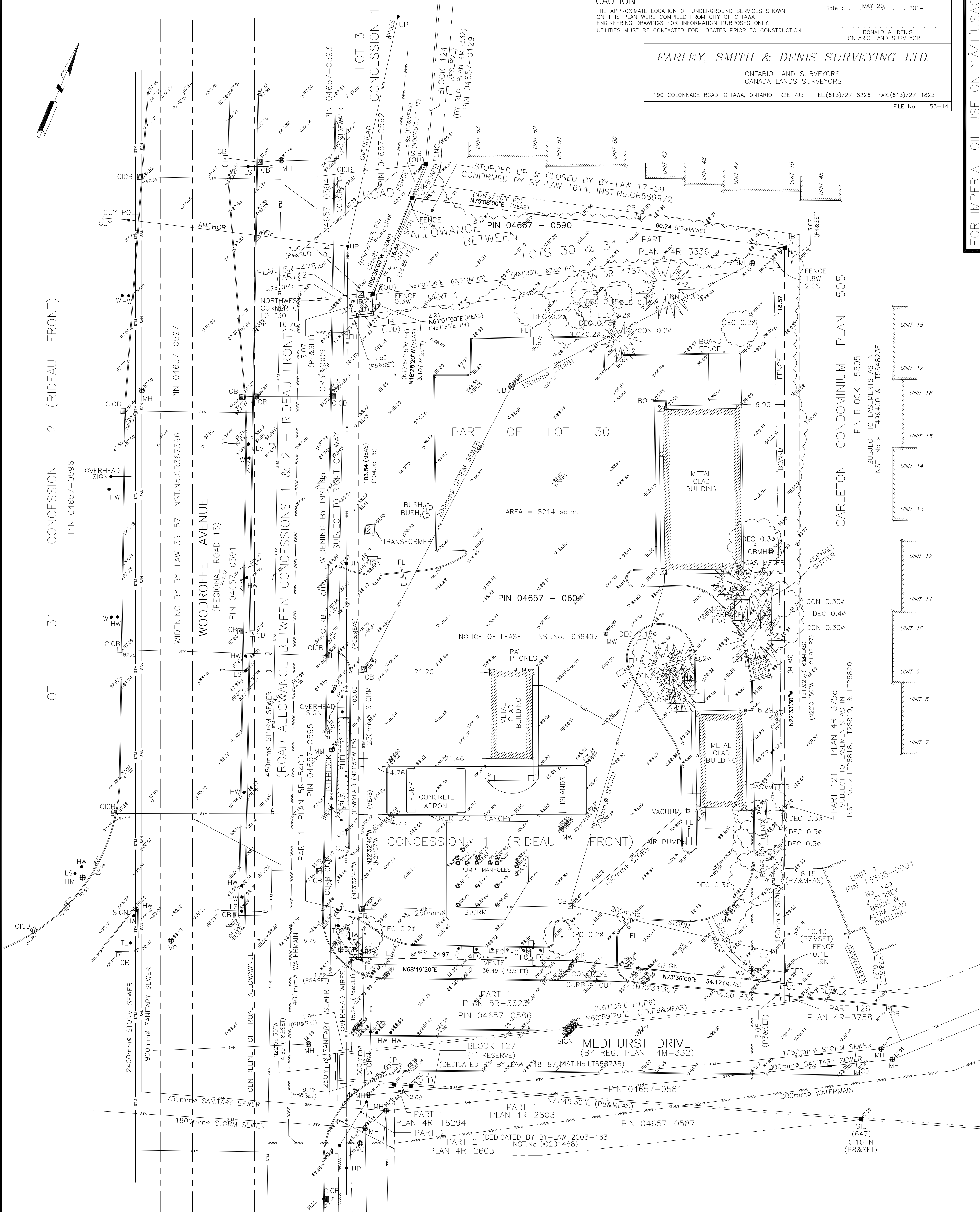
THE APPROXIMATE LOCATION OF UNDERGROUND SERVICES SHOWN ON THIS PLAN WERE COMPILED FROM CITY OF OTTAWA ENGINEERING DRAWINGS FOR INFORMATION PURPOSES ONLY. UTILITIES MUST BE CONTACTED FOR LOCATES PRIOR TO CONSTRUCTION.

FARLEY, SMITH & DENIS SURVEYING LTD.

ONTARIO LAND SURVEYORS
CANADA LANDS SURVEYORS

190 COLONNADE ROAD, OTTAWA, ONTARIO K2E 7J5 TEL:(613)727-8226 FAX:(613)727-1823

FILE No. : 153-14



FOR IMPERIAL OIL USE ONLY À L'USAGE DE L'IMPERIALE SEULEMENT

Do not explode, alter or rename titleblock

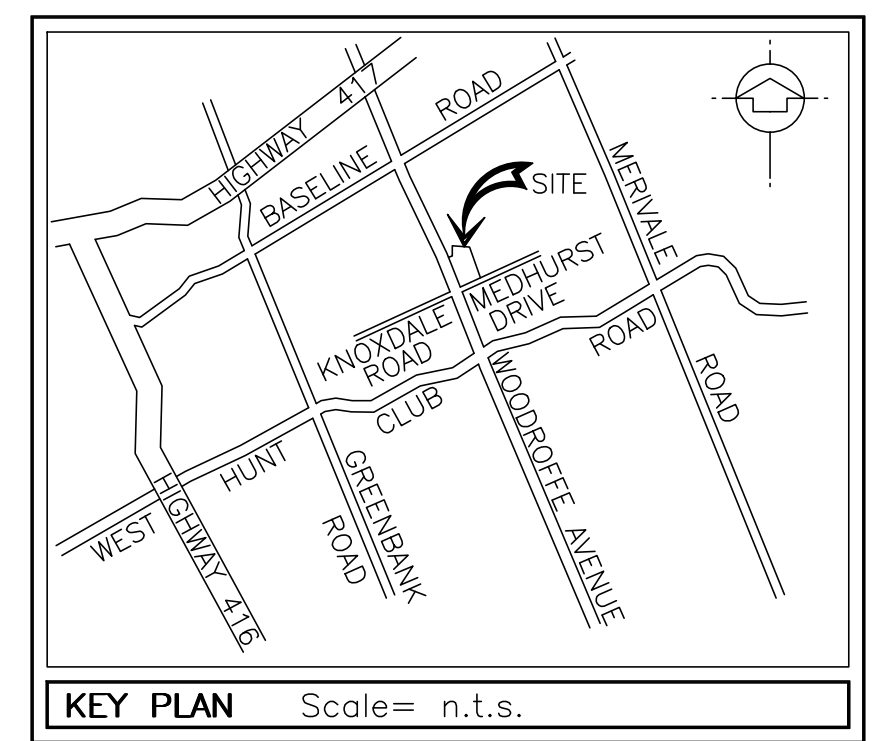
MAY 20, 2014 14:49:56 CAD FILE= U:\Jijima\2014\TOPO\153-14-ES50\LJ153-14-1545 WOODROFFE AVE & MEDHURST DR.dwg

Date	#	Revision
14 06 12	08	INCREASE RIGHT WOODROFFE ENTRANCE TO 11 METRES
14 06 12	09	ADD TDL STORAGE, ADD ROLLUP LEFT OF RIGHT WOODROFFE ENTRANCE
14 10 27	10	ADDED 5+1 DISPENSER
15 02 12	11	UPDATED MID CURB REMOVED PAY PHONE
15 02 20	12	UPDATED MID CURB REMOVED PAY PHONE
15 03 30	13	ADDED SOUND BARRIERS, UPDATED TDL STACKING, PARKING AND SITE STATISTICS
14 06 11	07	SIGNS ADDED (#21, 49 AND 55)

By Design	Checked	Verified	Approved
VAH	VAH	EM	VAH
VAH	FMR	FMR	FMR
VAH	FMR	FMR	FMR

Project/Proposed	Title/Type	Date
1445 WOODROFFE AVE. & MEDHURST DR OTTAWA, ONTARIO	PROPOSED SITE PLAN	14 03 17

Scale	Scale	Date	By/For	Scale	Scale
1:300	1:300	14 03 17	AMEC (176071)	1:300	1:300
1:300	1:600				



LEGAL DESCRIPTION
PART OF LOT 30
PART OF THE ROAD ALLOWANCE
BETWEEN LOTS 30 & 31
CONCESSION 1 (RIDEAU FRONT)
CITY OF OTTAWA
(GEOGRAPHIC TOWNSHIP OF NEPEAN)

SITE STATISTICS

EXISTING SITE AREA	8213.9 sq.m
NEW LOT AREA AFTER PROPOSED ROAD WIDENINGS	7818.8 sq.m
C-STORE BUILDING AREA	424.0 sq.m
CANOPY BUILDING AREA	111.0 sq.m
CANOPY COVERAGE	320.0 sq.m
LANDSCAPE AREA	2741.8 sq.m
ASPHALT AREA	4222.0 sq.m

PROPOSED PARKING SPACES	REQUIREMENTS
24 + 1 ACCESSIBLE	+ 1 LOADING + 2 VACUUM
20 + 1 ACCESSIBLE	

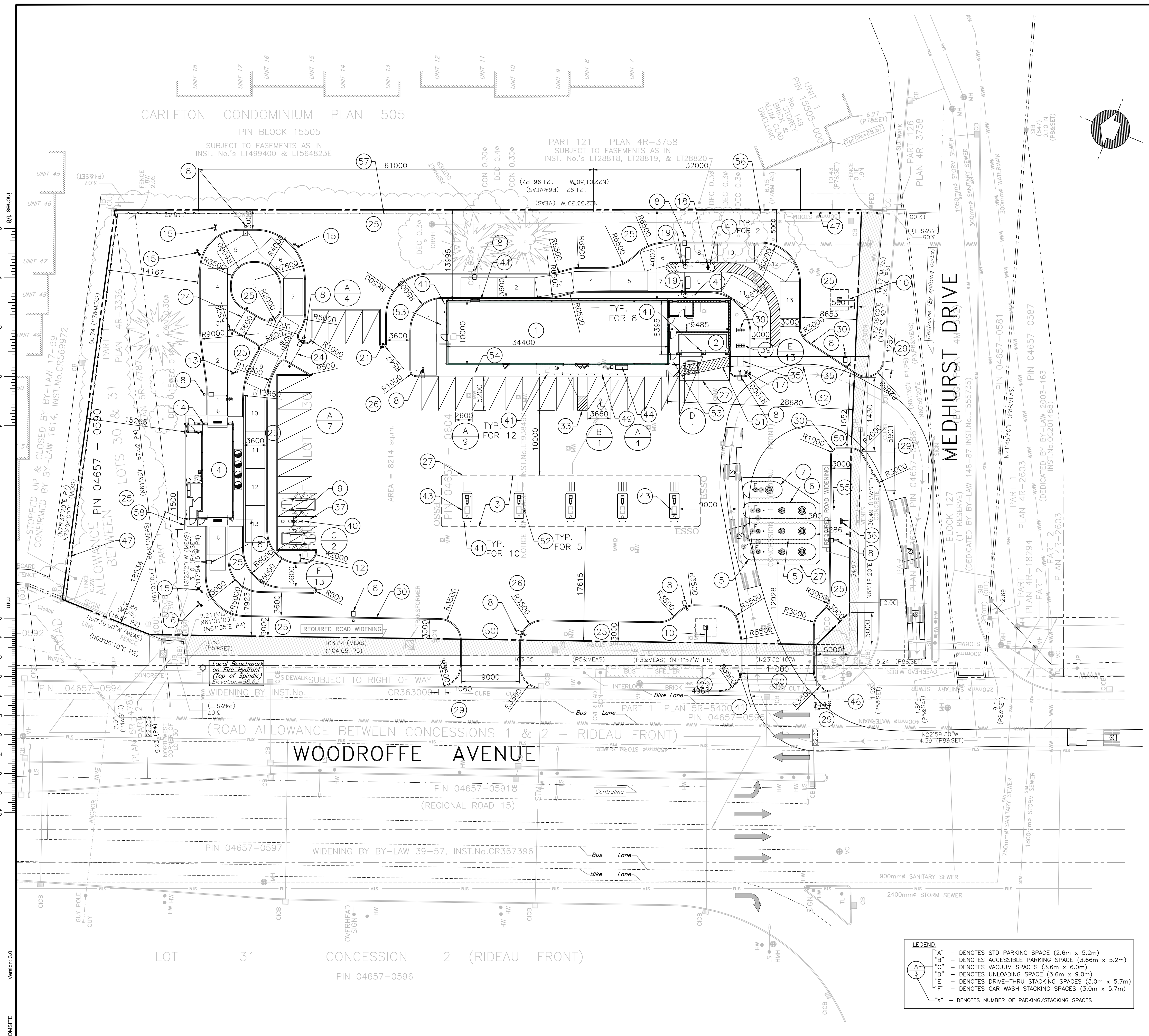
C-STORE	178m ² x 3.4/100m ² = 6.05
DRIVE-THRU RESTAURANT	141m ² x 10/100m ² = 14.1
C-STORE STACKING SPACES REQUIRED	11
C-STORE STACKING SPACES PROPOSED	14

LEGEND: No=Item Number T=Total N=New E=Existing R=Relocate

No	T	N	E	R	Description
1	1	1			C-Store (10m x 34.4m)
2	1	1			Garbage Enclosure
3	1	1			Canopy (8m x 40m)
4	1	1			Car Wash (7.4m x 15.0m)
5	2	2			Regular Tank (Double Wall Fiberglass, 50kl)
6	1	1			Premium Tank (Double Wall Fiberglass, 50kl)
7	1	1			Diesel Tank (Double Wall Fiberglass, 25kl)
8	13	13			Single LED Light Standard (TYP)
9	1	1			Double LED Light Standard (TYP)
10	2	2			MID (2500 series)
11	-	-	-	-	CAR WASH SIGNS
11	1	1			Overhead Boom Sign
12	1	1			Hours of Operation
13	1	1			Instructional Sign
14	1	1			Disclaimer Sign
15	4	4			Promotional Sign
16	1	1			Exit Sign
17	1	1			Entrance Sign
18	1	1			Pre-menu board
19	2	2			Integrated Speaker and Menu Board
20	1	1			Caution Pedestrian Crossing Sign
21	1	1			Exit Sign
22	1	1			Stop Sign
23	1	1			Painted Stop Line
24	2	2			Chain across Escape Lane
25	1	1			Landscape
26	1	1			Asphalt
27	3	3			Concrete Apron/Pad
28	1	1			Curb Fill/Close Existing Access
29	5	5			Curb Cut (New Access)
30	1	1			Concrete Curbing (Typical)
31	1	1			Existing Curb to be removed (typical)
32	1	1			1500 Wide Concrete Pedestrian walkway connecting store to sidewalk
33	1	1			Barrier-Free Access Ramp
34	1	1			New Concrete Sidewalk
35	1	1			Depressed curb at sidewalk
36	1	1			Vent Rack
37	1	1			Vacuum Island c/w 2 vacuums
38	1	1			Payphone
39	2	2			Bike Rack
40	1	1			Tirelator on concrete pad
41	36	36			Bollard
42	1	1			Hydro Transformer (Dimension)
43	2	2			3+1 Combo Gasoline/Diesel Dispenser
44	1	1			Propane Cage
45	1	1			1000mm Safety Hand Rolling
46	1	1			5m Daylight Triangle
47	1	1			Board-on-board Privacy Fence
48	1	1			Retaining Wall
49	1	1			Rb-93 'BY PERMIT ONLY' Sign
50	2	2			Access
51	1	1			Painted Line
52	5	5			Concrete Pump Island
53	2	2			1500 wide Sidewalk
54	1	1			1800 wide Sidewalk
55	1	1			Rb-51 'NO PARKING' Sign
56	1	1			3.2m high Sound Barrier
57	1	1			2.0m high Sound Barrier
58	1	1			3.0m high Sound Barrier

LEGEND:

- - DENOTES STD PARKING SPACE (2.6m x 5.2m)
- - DENOTES ACCESSIBLE PARKING SPACE (3.66m x 5.2m)
- - DENOTES VACUUM SPACES (3.6m x 6.0m)
- - DENOTES UNLOADING SPACE (3.6m x 9.0m)
- - DENOTES DRIVE-THRU STACKING SPACES (3.0m x 5.7m)
- - DENOTES CAR WASH STACKING SPACES (3.0m x 5.7m)
- * - DENOTES NUMBER OF PARKING/STACKING SPACES



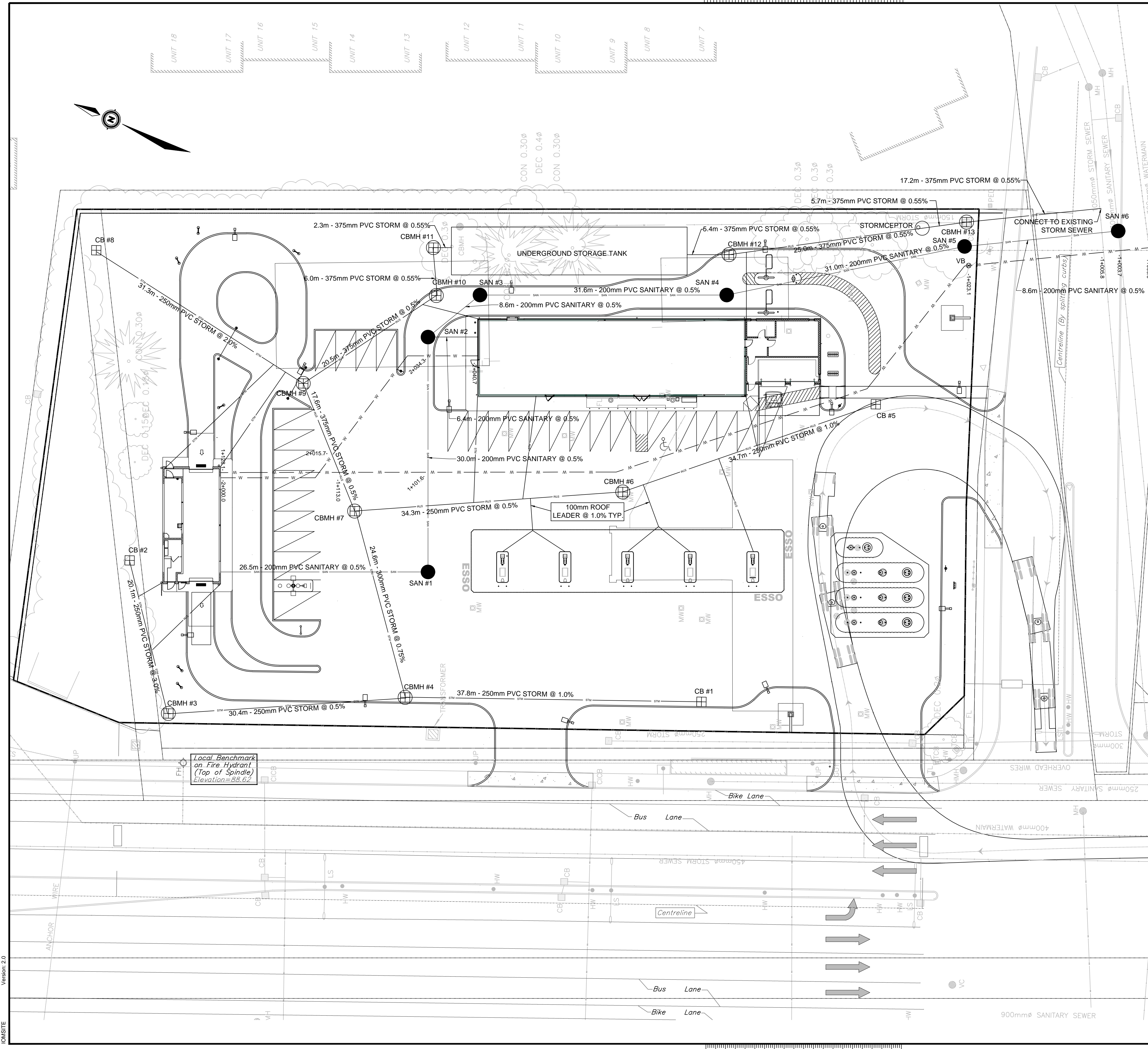
CARLETON CONDOMINIUM PLAN 505
PIN BLOCK 15505
SUBJECT TO EASEMENTS AS IN
INST. No.'s LT499400 & LT564823E

PART 121 PLAN 4R-3758
SUBJECT TO EASEMENTS AS IN
INST. No.'s LT28818, LT28819, & LT28820-7
32000

Version: 3.0
Date: 14 03 17
Scale: 1:300

LOT 31 CONCESSION 2 (RIDEAU FRONT)
PIN 04657-0596

inches 1/8 1 2 3 4



STORM INVERT SCHEDULE

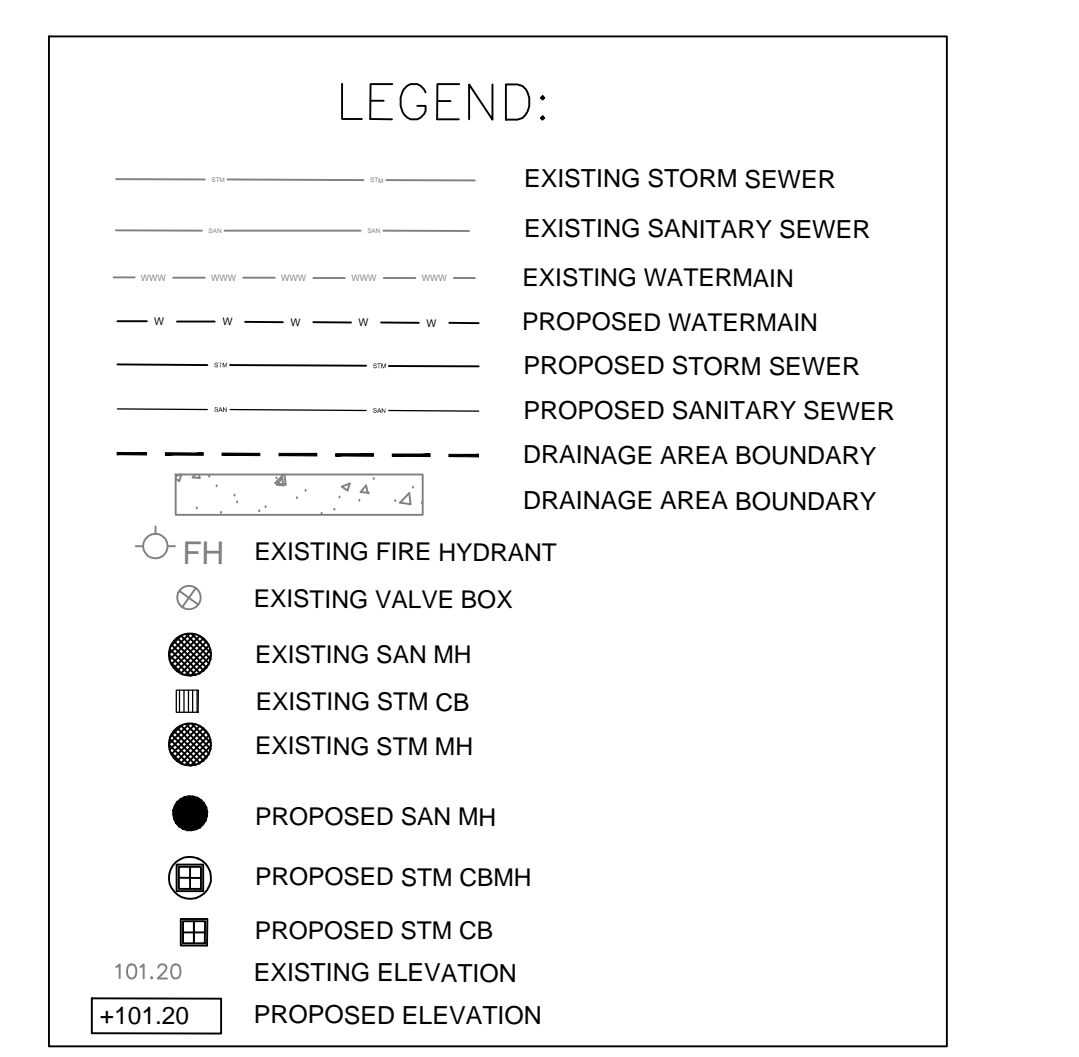
STRUCTURE	TOP	INVERT	COMMENTS
CB#1	88.05	N = 86.30	NEW 600mm x 600mm CATCHBASIN AS PER OPSD 705.010
CB#2	88.45	W = 86.70	NEW 600mm x 600mm CATCHBASIN AS PER OPSD 705.010
CBM#3	87.85	E = 86.10 S = 86.04	NEW 1200mm DIAMETER MANHOLE WITH 300mm SUMP AS PER OPSD 701.010
CBM#4	88.05	S = 85.92 N = 85.89 E = 85.83	NEW 1200mm DIAMETER MANHOLE WITH 300mm SUMP AS PER OPSD 701.010
CB#5	88.30	N = 86.55	NEW 600mm x 600mm CATCHBASIN AS PER OPSD 705.010
CBM#6	88.53	S = 86.20 N = 86.14	NEW 1200mm DIAMETER MANHOLE WITH 300mm SUMP AS PER OPSD 701.010
CBM#7	88.30	S = 85.97 W = 85.65 E = 85.57	NEW 1200mm DIAMETER MANHOLE WITH 300mm SUMP AS PER OPSD 701.010
CB#8	88.40	S = 86.65	NEW 600mm x 600mm CATCHBASIN AS PER OPSD 705.010
CBM#9	88.50	N = 85.48 N = 86.02 E = 85.42	NEW 1200mm DIAMETER MANHOLE WITH 300mm SUMP AS PER OPSD 701.010
CBM#10	88.63	NW = 85.32 S = 85.26	NEW 1200mm DIAMETER MANHOLE WITH 300mm SUMP AS PER OPSD 701.010
CBM#11	88.40	W = 85.23	NEW 1200mm DIAMETER MANHOLE WITH 300mm SUMP AS PER OPSD 701.010
UNDERGROUND STORAGE	VARIABLES	N = 85.21 S = 84.91	UNDERGROUND STORAGE TANK USING ATLANTIS MATRIX PENTA TANK MODULES. CAPACITY = 380m3
CBM#12	88.65	N = 85.88 E = 84.82	NEW 1200mm DIAMETER MANHOLE WITH 300mm SUMP AS PER OPSD 701.010
STORMCEPTOR	88.45	N = 84.68 S = 84.68	NEW STORMCEPTOR - MODEL STC 750
CBM#13	88.15	N = 84.65 S = 84.58	NEW 1200mm DIAMETER MANHOLE WITH 300mm SUMP AS PER OPSD 701.010
EXISTING SEWER	87.96	N = 84.48 E = EXISTING	TIE INTO EXISTING SEWER AS PER S11

SANITARY INVERT SCHEDULE

STRUCTURE	TOP	INVERT	COMMENTS
CONNECTION TO CAR WASH	88.90	SEE MECHANICAL	CONNECT TO BUILDING SERVICES
SAN MH#1	88.50	N = 86.57 E = 86.51	NEW 1200mm DIAMETER MANHOLE AS PER OPSD 701.010
CONNECTION TO C-STORE	89.03	SEE MECHANICAL	CONNECT TO BUILDING SERVICES
SAN MH#2	88.75	W = 86.36 S = 86.80 SE = 86.30	NEW 1200mm DIAMETER MANHOLE AS PER OPSD 701.010
SAN MH#3	88.70	NW = 86.26 S = 86.20	NEW 1200mm DIAMETER MANHOLE AS PER OPSD 701.010
SAN MH#4	88.80	N = 86.04 S = 85.98	NEW 1200mm DIAMETER MANHOLE AS PER OPSD 701.010
SAN MH#5	88.20	N = 85.82 S = 85.76	NEW 1200mm DIAMETER MANHOLE AS PER OPSD 701.010
SAN MH#6	87.92	N = 85.72 E = EXISTING W = EXISTING	NEW 1200mm DIAMETER MANHOLE AS PER OPSD 701.010

PRIVATE WATERMAIN TABLE

STATION	DESCRIPTION	TOP OF PIPE ELEVATION	GROUND ELEVATION	COMMENTS
1+000.0	CONNECTION TO 300mm DIAMETER WATERMAIN	86.17	88.15	CONNECTION TO CITY SERVICES
1+003.7	SANITARY CROSSING	85.52	87.92	WATERMAIN CROSSES OVER SANITARY PER W25.2. CLEARANCE = 2.52m
1+005.8	STORM CROSSING	85.56	87.96	WATERMAIN CROSSES OVER STORM PER W25.2. CLEARANCE = 2.17m
1+023.1	VALVE BOX	85.88	88.28	NEW VALVE BOX PER W24
1+101.6	SANITARY CROSSING	87.00	88.60	WATERMAIN CROSSES OVER SANITARY PER W25.2. CLEARANCE = 0.25m
1+113.0	STORM CROSSING	86.27	88.34	WATERMAIN CROSSES OVER STORM PER W25.2. CLEARANCE = 0.25m
1+128.1	CONNECTION TO BUILDING	86.50	88.90	CONNECTION TO CARWASH BUILDING
2+000.0	CONNECTION TO BUILDING	86.50	88.90	CONNECTION TO CARWASH BUILDING
2+015.0	STORM CROSSING	86.25	88.35	WATERMAIN CROSSES OVER STORM PER W25.2. CLEARANCE = 0.25m
2+034.3	SANITARY CROSSING	86.87	88.77	WATERMAIN CROSSES OVER SANITARY PER W25.2. CLEARANCE = 0.25m
2+040.7	CONNECTION TO BUILDING	86.63	89.03	CONNECTION TO C-STORE BUILDING



Imperial Oil
Esso
Imperial

FIELD MARKETING
ASSET MANAGEMENT - ENGINEERING SERVICES
MARKETING DES CALBRANTS
GESTION DES ACTIFS - SERVICES D'INGENIERIE

90 WOODROFF DRIVE
TORONTO, ONTARIO
M8C 1K5

Date: 15 03 02
Revision: 01
ISSUED FOR SITE PLAN APPROVAL
15 04 09 02 REVISED FOR COMMENTS

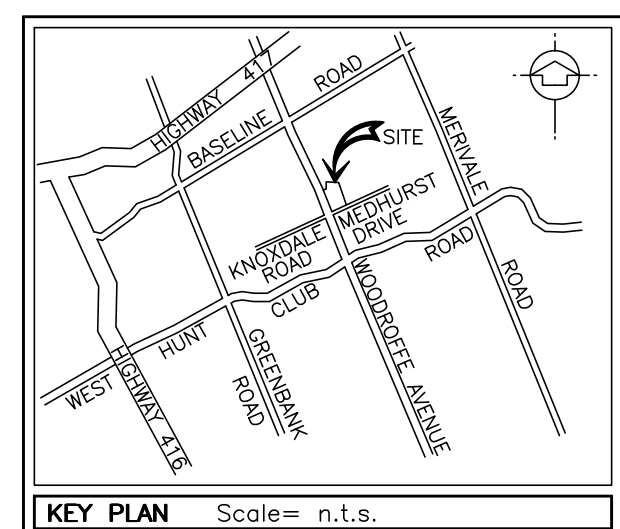
Project/Project: 1345 WOODROFF AVE. & MEDHURST DR
OTTAWA, ONTARIO
Title/Title: PROPOSED SITE SERVICES

Scale: (272x347) 1:250
Scale: (11"x17") 1:500

Date: 15 03 02
By/For: RVA
P.M./G.P.: T.M.K.
Rev. No./Rev.: 01
Sheet/Feuille: 001

PBL No. PBL 0000302287-03P
SAP No. SAP 88001626-03P

inches 1/8 1 2 3 4



KEY PLAN Scale = n.t.s.
 LEGAL DESCRIPTION
 PART OF LOT 30
 PART OF THE ROAD ALLOWANCE
 BETWEEN LOTS 30 & 31
 CONVESSION 1 (RIDEAU FRONT)
 CITY OF OTTAWA
 (GEOGRAPHIC TOWNSHIP OF NEPEAN)

Imperial Oil
Esso
Imperial

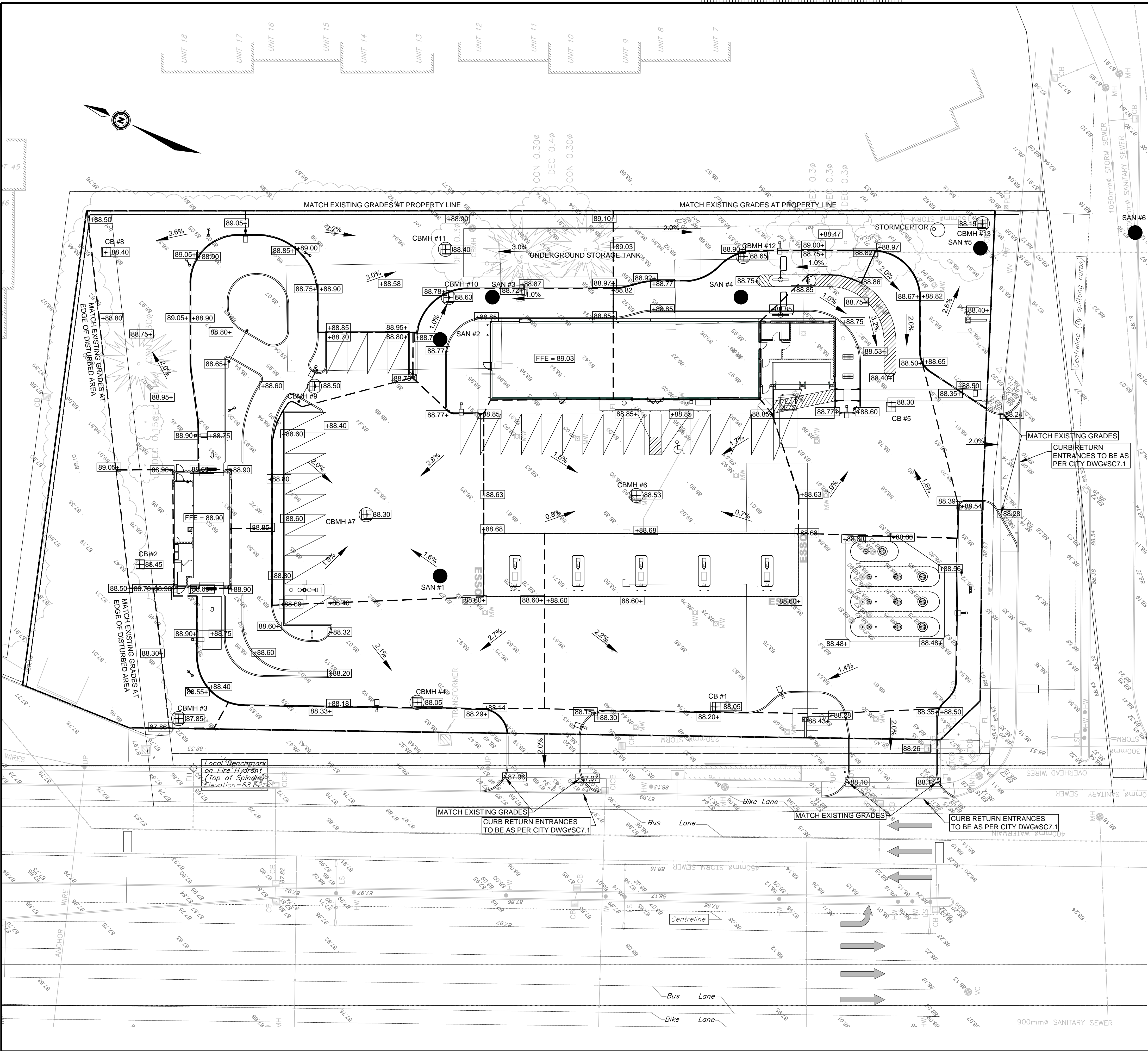
FIELD MARKETING
 ASSET MANAGEMENT - ENGINEERING SERVICES
 MARKETING DISTRICT MANAGERS
 GESTION DES ACTIFS - SERVICES D'INGENIERIE

90 WINDHOOD DRIVE
 TORONTO, ONTARIO
 M2C 1K5

Date	#	Revision
15 02 06	08	ISSUED FOR SITE PLAN APPROVAL
15 04 09	02	REVISED FOR COMMENTS

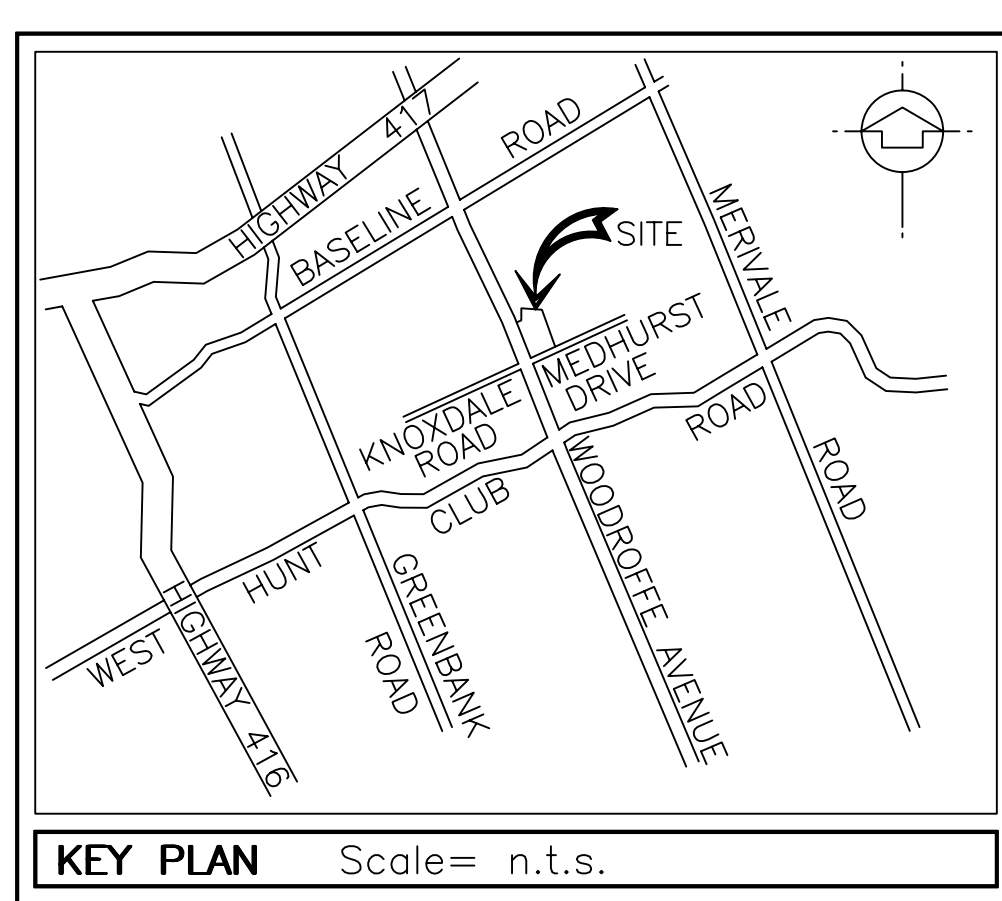
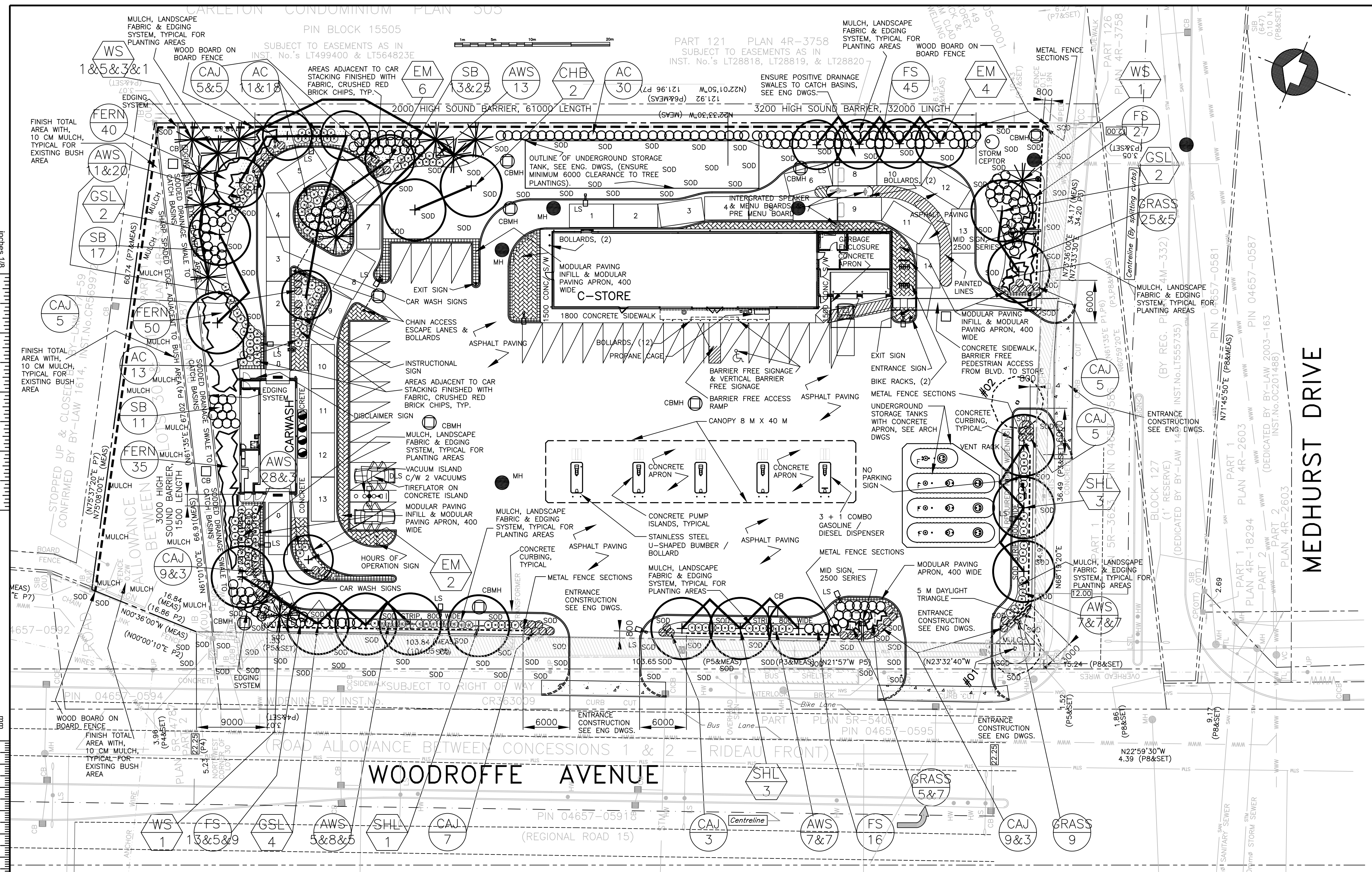
By	TKM	TKM	TKM
Project/Project	1345	WOODROFFE AVE. & MEDHURST DR	PBL No. PBL 0000302287-03P
Title/Title	OTAWA, ONTARIO	PROPOSED SITE GRADING PLAN	SAP No. SAP 88001626-03P
Scale	(227x347)	Scale (1"=17')	Date 15 03 02
Scale	1:250	Enchelle 1:500	By/par
Scale	1:250	Enchelle 1:500	NIR
Scale	1:250	Enchelle 1:500	PM/Grp
Scale	1:250	Enchelle 1:500	TKM
Scale	1:250	Enchelle 1:500	Rev/No./Rev
Scale	1:250	Enchelle 1:500	Sheet/Fauille: 002

- GENERAL NOTES:**
- THIS DRAWING IS FOR SITE SERVICING WORKS ONLY.
 - CONTRACTOR SHALL BE RESPONSIBLE FOR LOCATION OF ALL EXISTING U/G AND OVERHEAD UTILITIES. VARIOUS UTILITIES CONCERNED TO BE GIVEN REQUIRED ADVANCE NOTICE PRIOR TO ANY DIGGING FOR STAKE OUT. THE OWNER AND CONSULTANT ASSUMES NO RESPONSIBILITY FOR THE ACCURACY OF THE LOCATION OF EXISTING UTILITIES AS INDICATED ON THIS DRAWING.
 - UTILITY INFORMATION WAS VERIFIED IN THE FIELD WHERE POSSIBLE. INDIVIDUAL COMPANIES SHOULD BE CONTACTED BY THE CONTRACTOR PRIOR TO COMMENCEMENT FOR CONFIRMATION OF EXISTENCE AND LOCATION OF UTILITIES.
 - DRAWINGS TO BE READ IN CONJUNCTION WITH CONTRACT SPECIFICATIONS.
 - ALL MATERIALS AND CONSTRUCTION METHODS TO BE IN ACCORDANCE WITH CITY OF OTTAWA AND ONTARIO PROVINCIAL STANDARDS AND SPECIFICATIONS. ONTARIO PROVINCIAL STANDARDS AND SPECIFICATIONS WILL APPLY WHERE NO CITY STANDARDS ARE AVAILABLE.
 - ALL WORK SHALL BE COMPLETED IN ACCORDANCE WITH THE CURRENT "OCCUPATIONAL HEALTH AND SAFETY ACT AND REGULATION FOR CONSTRUCTION PROJECTS". THE GENERAL CONTRACTOR SHALL BE DEEMED TO BE THE CONSTRUCTOR AS DEFINED IN THE ACT.
 - ALL TEMPORARY TRAFFIC CONTROL AND SIGNAGE DURING CONSTRUCTION SHALL BE IN ACCORDANCE WITH CURRENT ONTARIO TRAFFIC MANUAL BOOK 7: TEMPORARY CONDITIONS FIELD EDITION.
 - ALL SANITARY SEWERS TO BE INSULATED IF LESS THAN 2.0 METERS COVER. ALL STORM SEWERS TO BE INSULATED IF LESS THAN 2.0 METERS COVER. INSULATE AS PER CITY OF OTTAWA STANDARD DETAIL W22. ALL BUILDING CONNECTIONS TO HAVE SUFFICIENT COVER OR INSULATION IS REQUIRED.
 - CONTRACTOR SHALL CONTACT THE CONSULTANT, R.V. ANDERSON PRIOR TO BACKFILLING OF THE WATER SERVICE CONNECTIONS FOR THE PROPOSED LINES AND TIE-INS TO EXISTING LINES FOR AS-BUILT LOCATION RECORDS AND INSPECTION.
 - ALL WATERMANS TO BE INSULATED IF LESS THAN 2.4 METERS COVER AS PER CITY OF OTTAWA STANDARD DETAIL W22. AT ANY PROXIMITY OF SEWER MANHOLES, INSULATE WATERMAIN AS PER CITY DETAIL W23.
 - SEWERS ARE TO MAINTAIN 500mm BARRELL TO BARRELL CLEARANCE ABOVE AND 250mm BELOW WATERMANS AT ALL CROSSINGS (AS PER CITY DWGS W25 AND W25.2). IF 22' BENDS ARE USED ON THE WATER MAIN, THEY MUST BE ONE METER AWAY FROM THE SEWER.
 - THRUST BLOCKS TO BE AS PER CITY OF OTTAWA STANDARD DRAWINGS W25.3 AND W25.4. RESTRAINING AND RETAINING RINGS TO BE INSTALLED IN ACCORDANCE WITH CITY STANDARD DETAILS W25.5 AND W25.6.
 - TEMPORARY SUPPORT OF EXISTING UNDERGROUND UTILITIES IN ACCORDANCE WITH CITY STANDARD DETAIL W28.
 - WATERMAIN TRENCH AND BEDDING TO BE INSTALLED AS PER CITY DETAIL W17.
 - NEW VALVE BOX ASSEMBLY TO BE INSTALLED AS PER CITY DETAIL W24 AND CURB STOP AS PER STANDARD W26.
 - STORM CB'S AS PER OPSD 705.010 AND FRAME/COVER AS PER CITY STANDARD DRAWINGS S19. STORM CBMHS AS INDICATED IN TABLE WITH SUMP AND FRAME/COVER AS PER OPSD 401.010 TYPE B. SANITARY MH'S AS PER OPSD 701.010 TYPE A BASE WITH BENCHING, AND FRAME/COVER AS PER OPSD 401.010 TYPE A. ADJUSTMENT SECTIONS SHALL BE AS PER OPSD 704.010.
 - ALL STORM BUILDING CONNECTIONS TO BE PROTECTED WITH BACKWATER VALVE AS PER CITY OF OTTAWA STANDARD DETAIL S14 OR S18.
 - ANY ASPHALT CUT SHALL BE SAW CUT ON BOTH SIDES OF THE TRENCH FOR THE ENTIRE LENGTH OF THE EXCAVATION FOR PIPE INSTALLATIONS. REINSTATEMENT OF THE ROADS SHALL MATCH EXISTING OR MEET CITY STANDARD R10.
 - ANY CONCRETE CUT SHALL BE REMOVED AT EXPANSION JOINTS. IF NO JOINTS EXIST, THE CONCRETE SHALL BE SAW CUT ON BOTH SIDES OF THE TRENCH FOR THE ENTIRE LENGTH OF THE EXCAVATION FOR PIPE INSTALLATIONS. REINSTATEMENT SHALL MATCH EXISTING OR MEET CITY REQUIREMENTS.
 - PIPE BEDDING SHALL BE GRANULAR "A" AS PER CITY DETAIL S6, AND SHALL BE COMPACTED TO 95% SPD AND APPROVED SELECT NATIVE BACK FILL COMPACTED TO 95% SPD.
 - GRANULAR LAYERS BENEATH NEW ASPHALT SURFACES ON PROPERTY SHALL BE PLACED AT A THICKNESS NOT EXCEEDING 200mm. THE GRANULAR IS TO BE COMPACTED TO 100% SPMD.
 - THE APPROVAL OF THIS PLAN DOES NOT EXEMPT THE OWNER'S BONDED CONTRACTOR FROM THE REQUIREMENTS TO OBTAIN THE VARIOUS PERMITS/APPROVALS NORMALLY REQUIRED TO COMPLETE A CONSTRUCTION PROJECT, SUCH AS, BUT NOT LIMITED TO THE FOLLOWING: ROAD CUT PERMITS, SEWER PERMITS, APPROACH APPROVAL PERMITS, RELOCATION OF SERVICES, COMMITTEE OF ADJUSTMENT, ENCROACHMENT AGREEMENTS, WATER PERMIT, ETC
 - THE OWNER SHALL REQUIRE THAT THE SITE SERVICING CONTRACTOR PERFORM FIELD TESTS FOR QUALITY CONTROL OF ALL SANITARY SEWERS. SPECIFICALLY, THE LEAKAGE TESTING SHALL BE COMPLETED IN ACCORDANCE WITH OPS 410.07.01.16 AND 407.07.26. THE FIELD TESTS SHALL BE PERFORMED IN THE PRESENCE OF A CERTIFIED PROFESSIONAL ENGINEER WHO SHALL SUBMIT A CERTIFIED COPY OF THE TEST RESULTS.
 - REFER TO LANDSCAPE DRAWINGS FOR DETAILS ON LANDSCAPING AND PLANTINGS.
 - SEWERS TO BE CONSTRUCTED AS PER CITY OF OTTAWA SPECIFICATIONS - SPECIAL PROVISION F-4100, ALL SEWER STRUCTURES AS PER F-4070, ALL WATER MAINS AS PER F-4412 AND ALL ASSOCIATED SPECIFICATIONS.
 - UPON COMPLETION, SEWERS ARE TO BE CLEANED AND HAVE CCTV INSPECTION AS PER OPS 409.
 - PRIVATE ENTRANCES TO BE AS PER CITY OF OTTAWA STANDARD DETAIL DRAWING #SC7.1.
 - CONCRETE CURBS TO BE AS PER CITY DWG. #SC1.1, CONCRETE SIDEWALKS TO BE AS PER CITY DWG. #SC2 OR #SC4
 - THE CONTRACTOR SHALL IMPLEMENT BEST MANAGEMENT PRACTICES TO PROVIDE FOR PROTECTION OF THE AREA DRAINAGE SYSTEM AND THE RECEIVING WATER COURSE DURING CONSTRUCTION. THIS INCLUDES (BUT IS NOT LIMITED TO) LIMITING THE AMOUNT OF EXPOSED SOIL, USING FILTER CLOTH UNDER THE GRATES OF CATCH BASINS AND MANHOLES, AND INSTALLING SILT FENCES AND OTHER EFFECTIVE SEDIMENT TRAPS. EROSION CONTROL MEASURES SHALL ONLY BE REMOVED WHEN THE PROPOSED LANDSCAPING HAS BEEN ESTABLISHED.
 - THE CONTRACTOR ACKNOWLEDGES THAT FAILURE TO IMPLEMENT APPROPRIATE EROSION AND SEDIMENT CONTROL MEASURES MAY BE SUBJECT TO PENALTIES IMPOSED BY ANY APPLICABLE REGULATORY AGENCY.
 - REFER TO THE SITE SERVICING & STORMWATER MANAGEMENT REPORT BY R.V. ANDERSON DATED MARCH 2015 FOR FURTHER DETAILS.
 - EXISTING SEWERS TO BE REMOVED WITHIN THE EXCAVATION. SEWERS OUTSIDE OF THE EXCAVATION TO BE ABANDONED.



mm 0 1 2 3 4 5 6 7 8 9 10

Do not explode, alter or rename titleblock



TOPOGRAPHICAL PLAN OF SURVEY OF
 PART OF LOT 30
 PART OF THE ROAD ALLOWANCE
 BETWEEN LOTS 30 & 31
 CONCESSION 1 (RIDEAU FRONT)
 CITY OF OTTAWA
 (GEOGRAPHIC TOWNSHIP OF NEPEAN)

- LEGEND**
- #00 DENOTES SURVEYED LOCATION OF EXISTING TREES, & # SPECIES / CALIPER / CONDITIONS / REMARKS. REVIEW WITH ARBORIST REPORT, BY GARDINER TREE TRIMMING & REMOVAL FOR TREE PRESERVATION FENCE, SEE DETAIL 1, SHEET 000
 - + DENOTES PROPOSED DECIDUOUS TREE DWG No. 0000302287-06P, SHEET 2, DETAIL 1
 - * DENOTES PROPOSED CONIFEROUS TREE DWG No. 0000302287-06P, SHEET 2, DETAIL 2
 - (x)(x)(x) DENOTES PROPOSED SHRUBBERY & LANDSCAPE FABRIC GROW BARK MULCH & PLASTIC EDGE SYSTEM; DWG No. 0000302287-06P, SHEET 2, DETAIL 3
 - (x)(x)(x) DENOTES AREA OF SHRUBBERY PLANTING FINISHED WITH LANDSCAPE FABRIC & 75 CRUSHED RED BRICK CHIPS; DWG No. 0000302287-06P, SHEET 2, DETAIL 3
 - (x)(x)(x) DENOTES PROPOSED MODULAR PAVING APRON, COLOUR: RED 400 WIDE, 10% SLOPE & EDGE RESTRAINT SYSTEM DWG No. 0000302287-06P, SHEET 2, DETAIL 4
 - (x)(x)(x) DENOTES PROPOSED MODULAR PAVING INFILL, COLOUR: RED 200 SOLDIER COURSE EDGE, WITH HERRINGBONE INFILL DWG No. 0000302287-06P, SHEET 2, DETAIL 4
 - (x)(x)(x) DENOTES PROPOSED CONCRETE SIDEWALK SEE IMPERIAL OIL STANDARD SPECIFICATIONS & DWG No. 0000302287-06P, SHEET 2, DETAIL 5
 - (x)(x)(x) DENOTES PROPOSED PRIVACY FENCE, 1830 HIGH, 140 X 140 POSTS; DWG No. 0000302287-06P, SHEET 3, DETAIL 7
 - (x)(x)(x) DENOTES PROPOSED METAL FENCE SECTION, 1200 HIGH, DWG No. 0000302287-06P, SHEET 3, DETAIL 7
 - (x)(x)(x) DENOTES PROPOSED BIKE RACKS, TOTAL OF 2 UNITS DWG No. 0000302287-06P, SHEET 2, DETAIL 8
 - (x)(x)(x) DENOTES PROPOSED SOUND BARRIER, VARIOUS HEIGHTS, SEE PLAN / REPORT; DWG No. 0000302287-06P, SHEET 3, DETAIL 1
 - SOD DENOTES PROPOSED SODDED AREA, SEE SPECIFICATION SOD TO STREET CURBS & INTERIOR PROPERTY LINES, AND ANY AREAS DAMAGED BY NEW CONSTRUCTION
 - 1 DENOTES PROPOSED BARRIER-FREE SIGNAGE, DWG No. 96408N, SHEET A01A

LIST OF PLANT MATERIAL

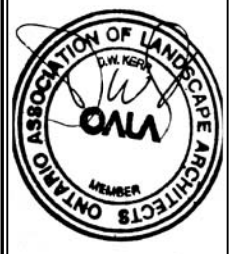
NOTE: QUANTITIES INDICATED ON PLAN TO GOVERN; DO NOT PLANT IN SWALE.

SYMBOL	QUANTITY	BOTANICAL NAME	COMMON NAME	HEIGHT	SPREAD	CALIPER	ROOT SPACING	REMARKS
EM	10	ACER SACCHARUM 'ENDOWMENT'	ENDOWMENT SUGAR MAPLE	400 CM	70 CM	70 MM	WB AS INDICATED	TRUE TO STANDARD FORM
CHB	2	CELTIS OCCIDENTALIS	COMMON HACKBERRY	400 CM	70 CM	70 MM	WB AS INDICATED	TRUE TO STANDARD FORM
SHL	7	GLEDITSIA TRICANTHOS 'SKYLINE'	SKYLINE HONEYLOCUST	400 CM	70 CM	70 MM	WB AS INDICATED	TRUE TO FORM
GSL	8	TILIA CORDATA 'GREENSPIRE'	GREENSPIRE LINDEN	400 CM	70 CM	70 MM	WB AS INDICATED	TRUE TO FORM
WS	12	PICEA GLAUCA	WHITE SPRUCE	200 CM			WB 4.0 M O/C	UNIFORM SPREAD
CAJ	29	JUNIPERUS HORIZONTALIS 'PLUMOSA COMPACTA'	COMPACT ANDORRA JUNIPER		30 CM		POT 0.8 M O/C	UNIFORM SPREAD
AC	72	RIBES ALPINUM	ALPINE CURRANT	50 CM			POT 1.0 M O/C	MIN. 5 CANES
FS	115	SORBARIA SORBIFOLIA	FALSE SPIREA	80 CM			POT 1.0 M O/C	MIN. 5 CANES
AWS	128	SPIREA BUMALDA 'ANTHONY WATERER'	ANTHONY WATERER SPIREA	50 CM			POT 0.8 M O/C	MIN. 4 CANES
SB	66	SYMPHORICARPOS ALBUS	WHITE SNOWBERRY	80 CM			POT 0.8 M O/C	MIN. 4 CANES
GRASS	51	CALAMAGROSTIS ACUTIFLORA 'KARL FOERSTER'	FEATHER REED GRASS				2 GAL POT 0.6 M O/C	2 YEAR PLANTS
FERN	125	MATTEUCCIA STRUTHIOPTERIS	OSTRICH FERN				2 GAL POT 0.6 M O/C	2 YEAR PLANTS

- IT IS THE CONTRACTOR'S RESPONSIBILITY TO:
- DETERMINE LOCATION OF ALL EXISTING SERVICES AND OBTAIN NECESSARY STAKE-OUTS BEFORE ANY EXCAVATION.
 - USE FIGURED DIMENSIONS, OPPOSED TO SCALING, AND VERIFY DIMENSIONS AT THE SITE.
 - REPORT ALL DISCREPANCIES TO THE LANDSCAPE ARCHITECT, AND AGREE IN WRITING, BEFORE PROCEEDING.
 - CONTRACTOR'S SHALL DO THEIR OWN SITE INSPECTIONS FOR ESTIMATED PURPOSES.

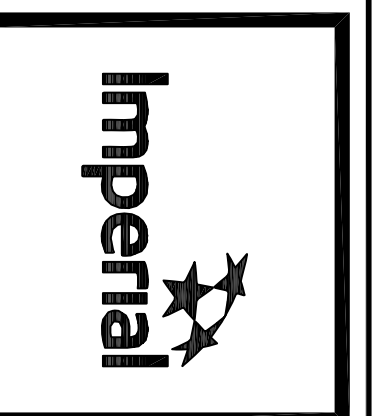
CITY OF OTTAWA SPA # D
 DEVELOPER: IMPERIAL OIL
 1535 WOODROFFE AVENUE, OTTAWA, ONTARIO

DWK Douglas W. Kerr & Associates Ltd.
 LANDSCAPE ARCHITECT
 1595 SIXTEENTH AVENUE, SUITE 301
 RICHMOND HILL, ONTARIO L4B 3N9
 Tel 416-410-5190, Fax 905-709-7443
 email: dwkerr@interlog.com



1520L1R3

APRIL 09, 2015



Date	#	Revision
14.06.04	0	BASED ON SITE PLAN, REV'D #04 DATED 14.05.23 & ARBORIST REPORT DATED 14.06.04
14.11.14	1	REVISE TO SITE PLAN, REV'D #10 DATED 14.11.14
15.03.30	2	REVISE TO SITE PLAN, REV'D #13 DATED 15.03.30
15.04.09	3	REVISE TO ENG. GRADING / SERVING PLANS DATED 15.04.09

By	Design/Check/Verify	Chk'd	Chk'd
DWK	DWK	DWK	DWK
DWK	DWK	DWK	DWK
DWK	DWK	DWK	DWK

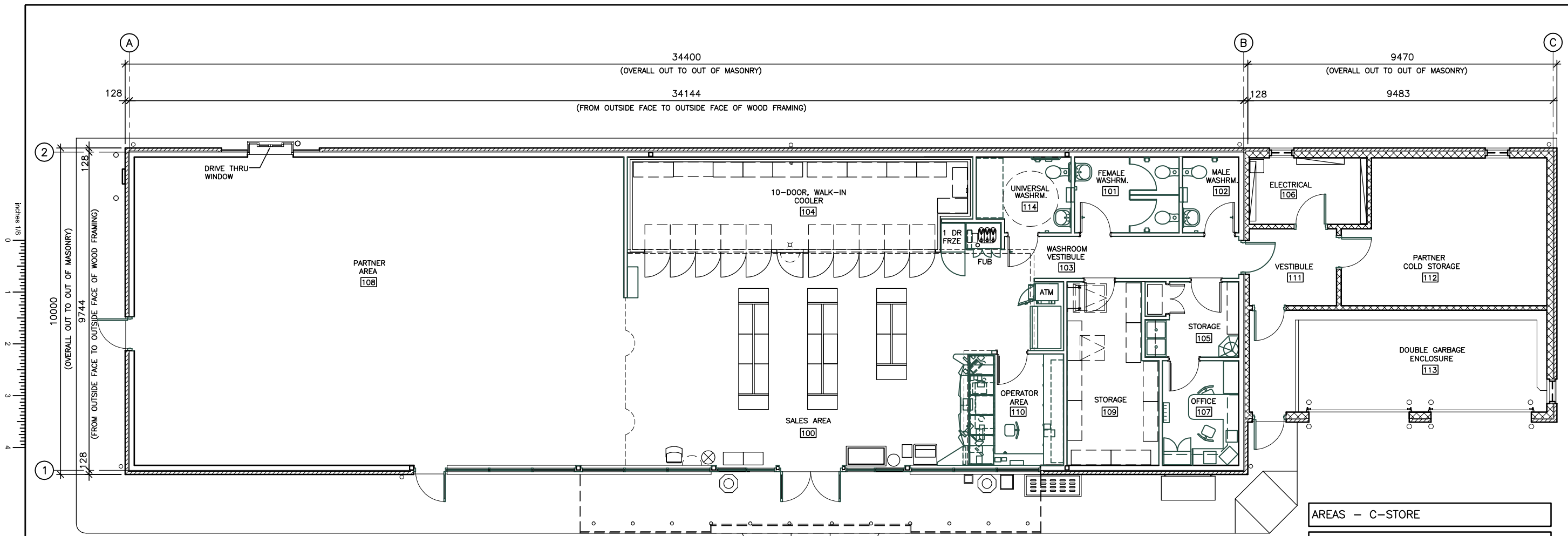
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Scale 1:300	PROPOSED LANDSCAPE PLAN	15.04.09	DWK	DWK	Scale (11"x17")	15.04.09	DWK	DWK
Scale 1:600								

PBL No.: PBL 0000302287-06P
 SAP No.: SAP 88001626-06P

Do not explode, alter or rename titleblock

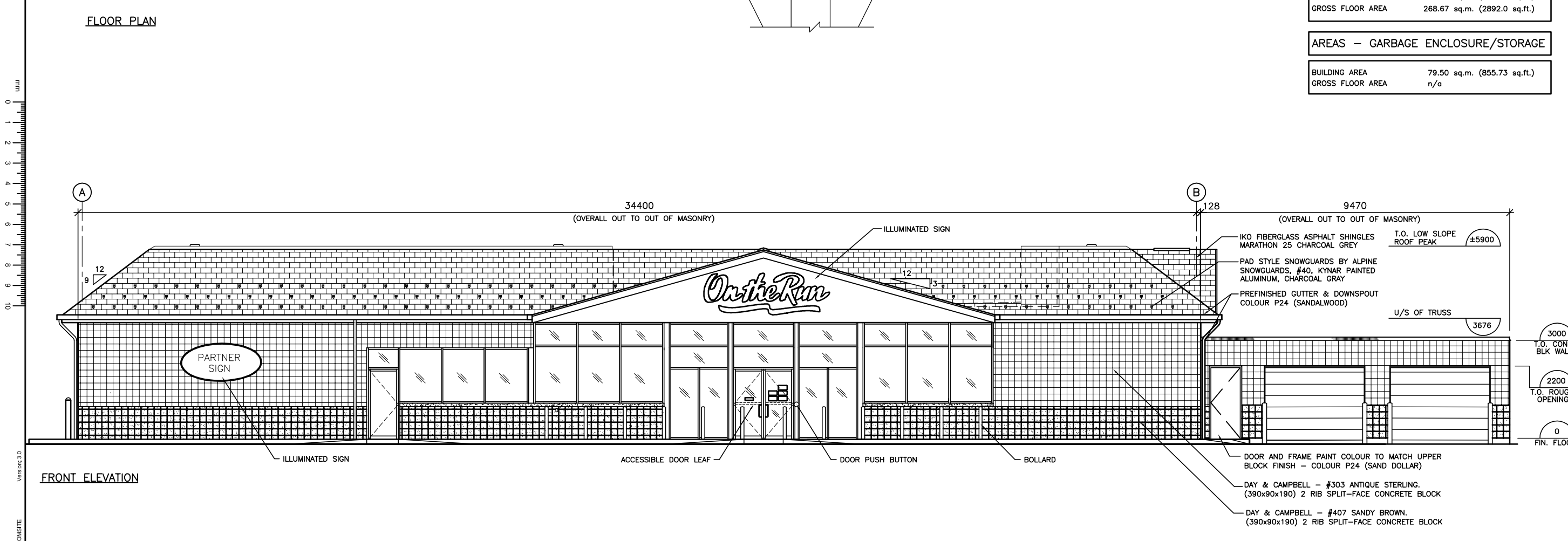
Date	#	Revision	By	Design	Chk'd	Chk'd
14 03 14	0	ISSUED FOR REVIEW	JDM			
15 02 10	1	REVISED WASHROOMS	BMD			
15 02 20	2	REVISED WASHROOMS	BMD			
15 03 04	3	REVISED WASHROOMS	BMD			
15 03 30	4	DOWNSPROUT ADDED TO FRONT ELEVATION	BMD			
15 04 06	5	REVISED AREA TABLES TO CITY INSTRUCTIONS	DCE			

Project/Title	Scale	Date	By/Rev	Proj. No.	Rev. No.
1545 WOODROFFE AVE. & MEDHURST DR OTTAWA, ONTARIO	Scale (1"=1'-7") Scale (1"=1'-25")	14 03 14	AMEC (176071)	0000302287-08P	00
PROPOSED FLOOR PLAN & FRONT ELEVATION				SAP No. SAP	88001626-08P
				P.L. No. P.L.	0000302287-08P
				Scheme	00
				Sheet/feuille:	A0A
				Rev/No./Rev	05



AREAS - C-STORE	
BUILDING AREA	344.00 sq.m. (3702.9 sq.ft.)
GROSS FLOOR AREA	268.67 sq.m. (2892.0 sq.ft.)

AREAS - GARBAGE ENCLOSURE/STORAGE	
BUILDING AREA	79.50 sq.m. (855.73 sq.ft.)
GROSS FLOOR AREA	n/a



INCHES 1/8" 0 1 2 3 4

MM 0 1 2 3 4 5 6 7 8 9 10

Version: 3.0

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Date	#	Revision
14 03 14	0	ISSUED FOR REVIEW
15 02 10	1	ISSUED FOR REVIEW
15 02 19	2	REVISED AS PER P.M. COMMENTS

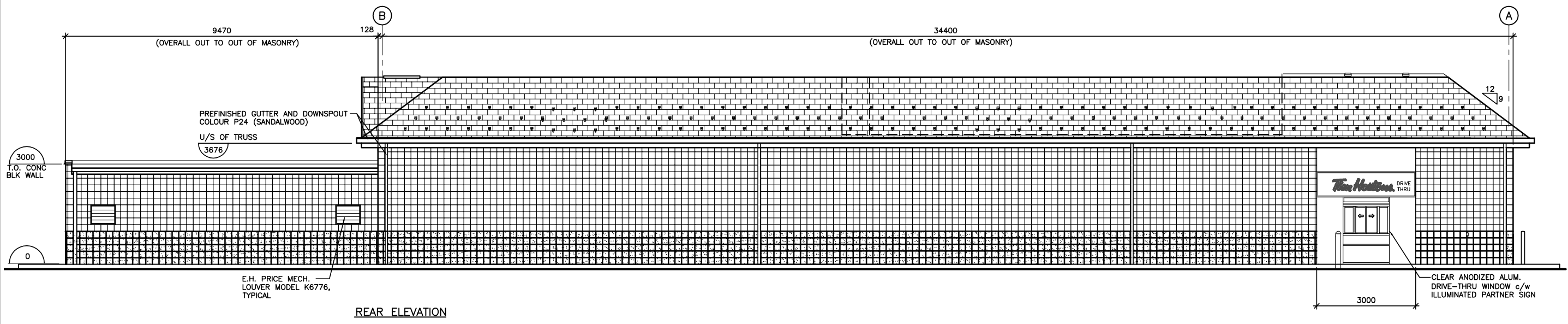
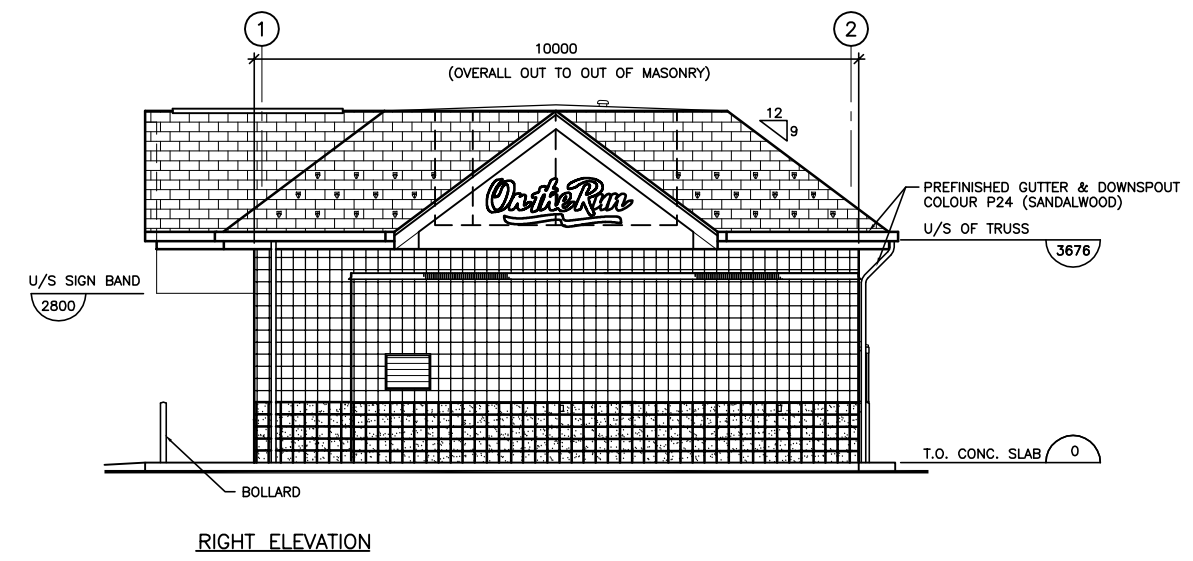
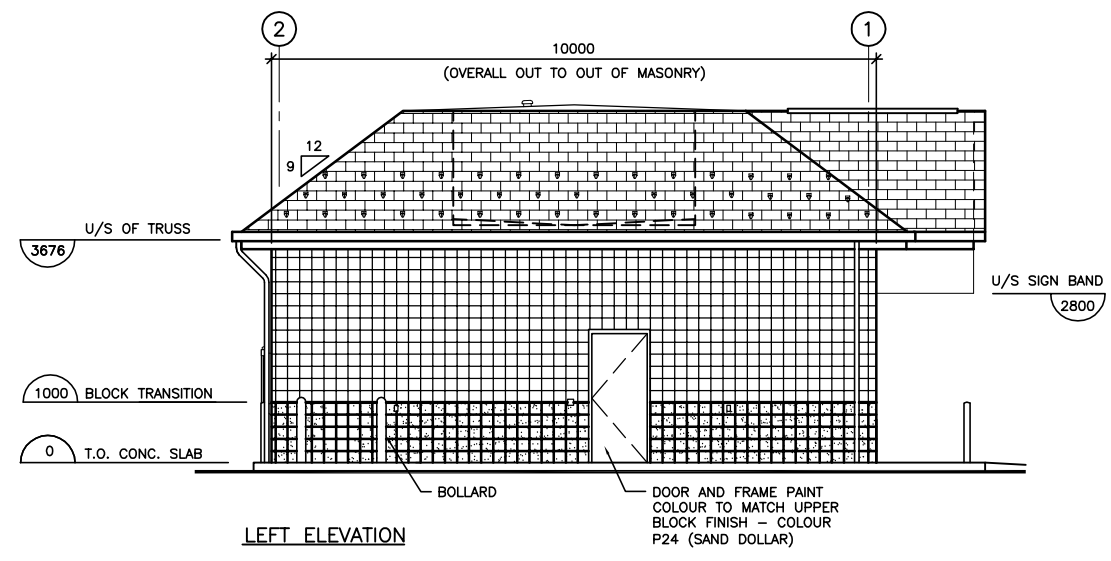
By Design: JDM	Chk'd VÉRIFIÉ	Chk'd VÉRIFIÉ
By Prepare: BMD	Chk'd VÉRIFIÉ	Chk'd VÉRIFIÉ
Title/Title PROPOSED LEFT, RIGHT & REAR ELEVATIONS		
Scale Echelle 1:62.5	Scale Echelle 1:125	Date 14 03 14
By/Par AMEC (176071)	Date 14 03 14	By/Par AMEC (176071)
P.M./G.P.	D.G.	Rev/No./Rev 00

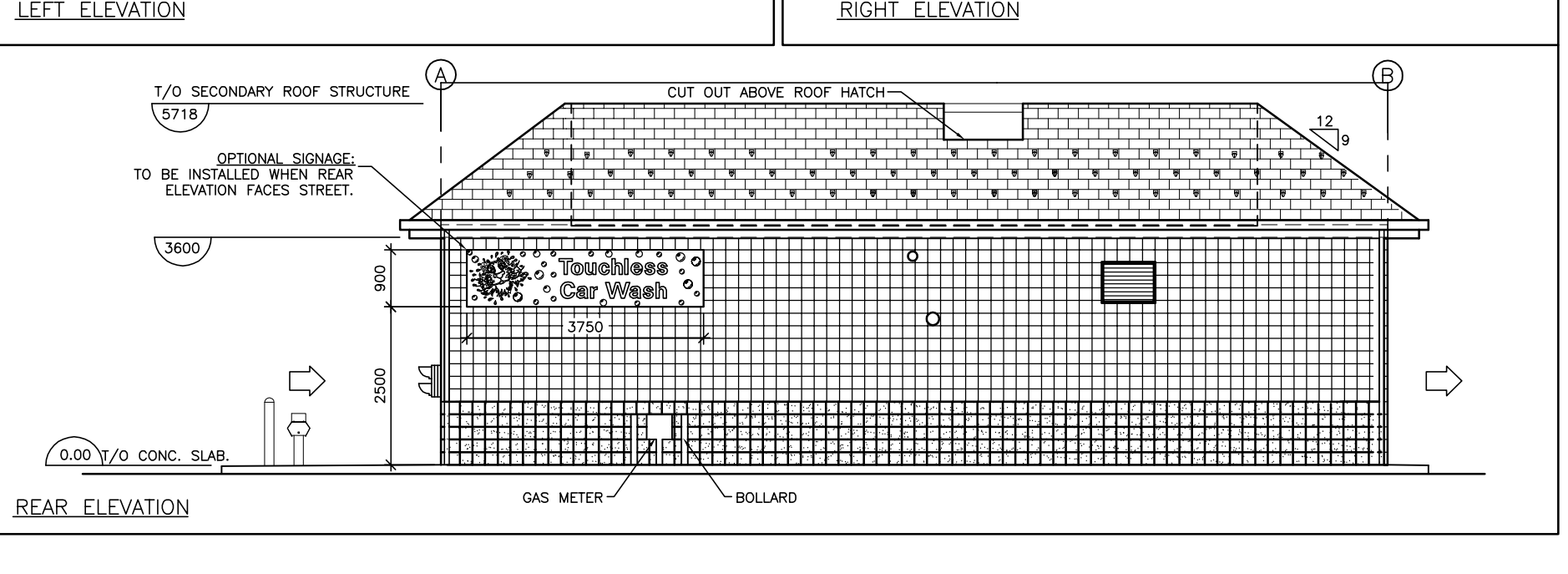
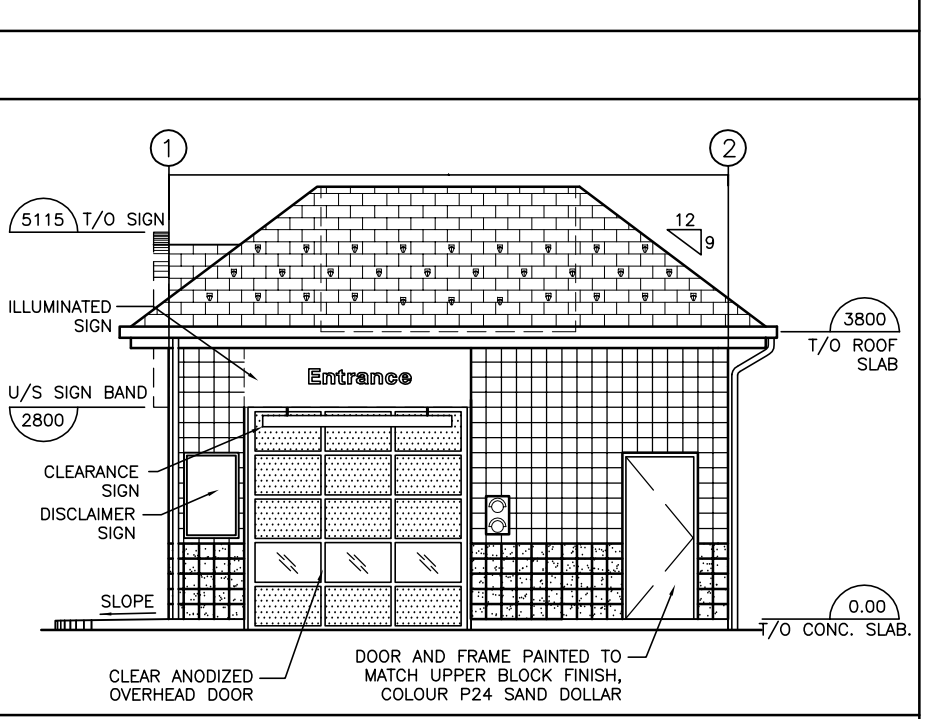
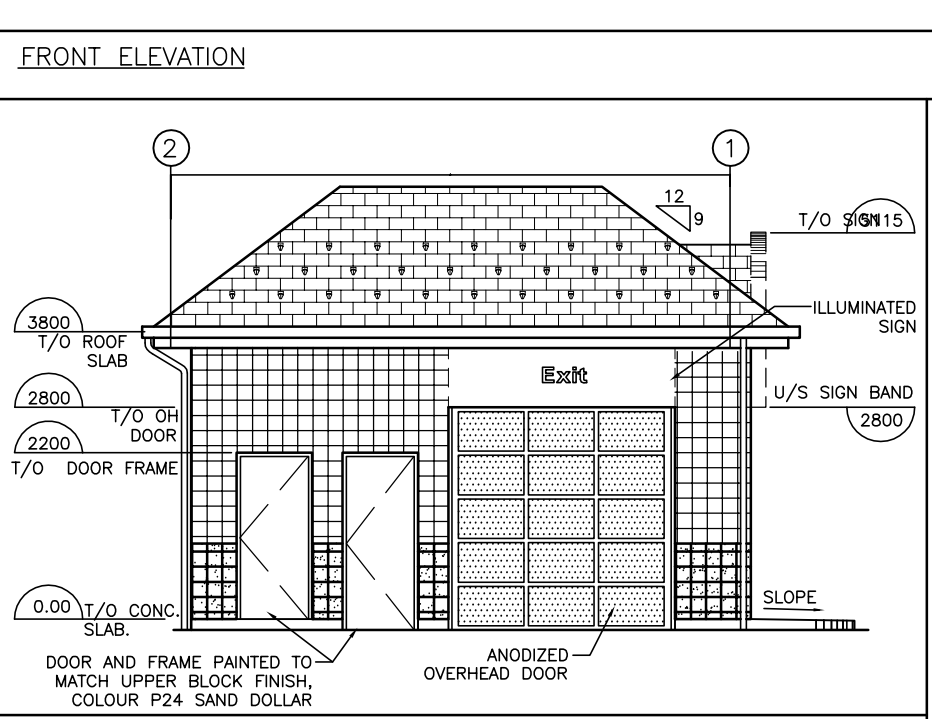
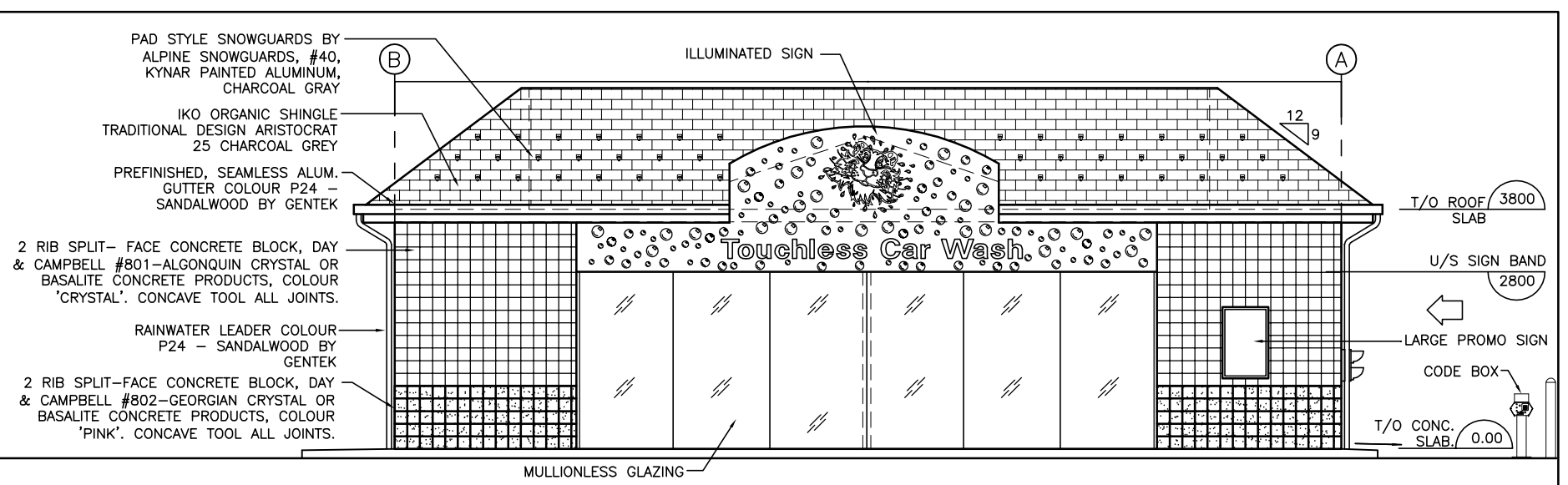
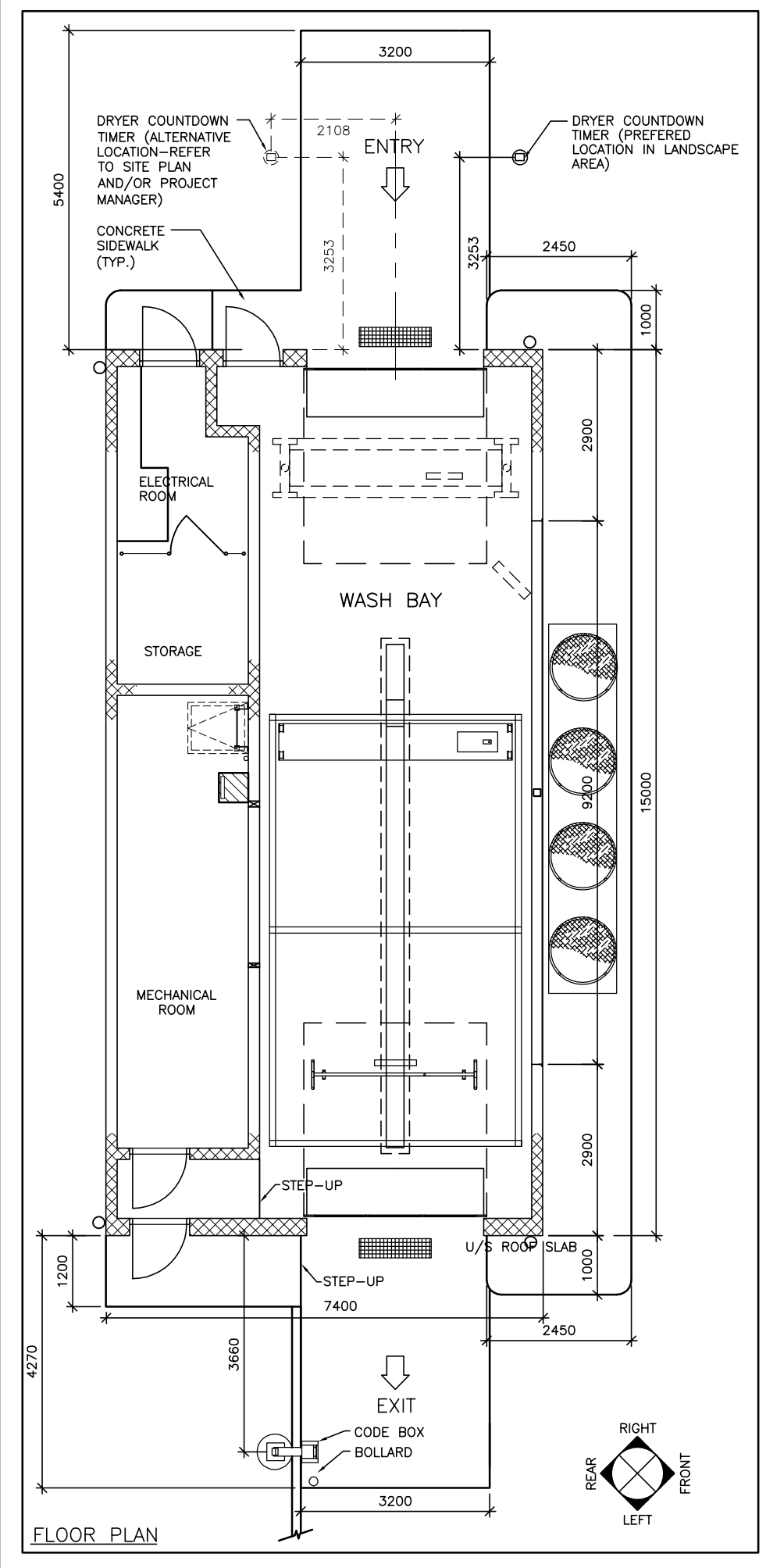
Project/Projet
1545 WOODROFFE AVE. & MEDHURST DR
OTTAWA, ONTARIO
P.L. No. PBL 0000302287-08P
SAP No. SAP 88001626-08P

inches 1/8
0
1
2
3
4

mm
0
1
2
3
4
5
6
7
8
9
10

Version: 3.0
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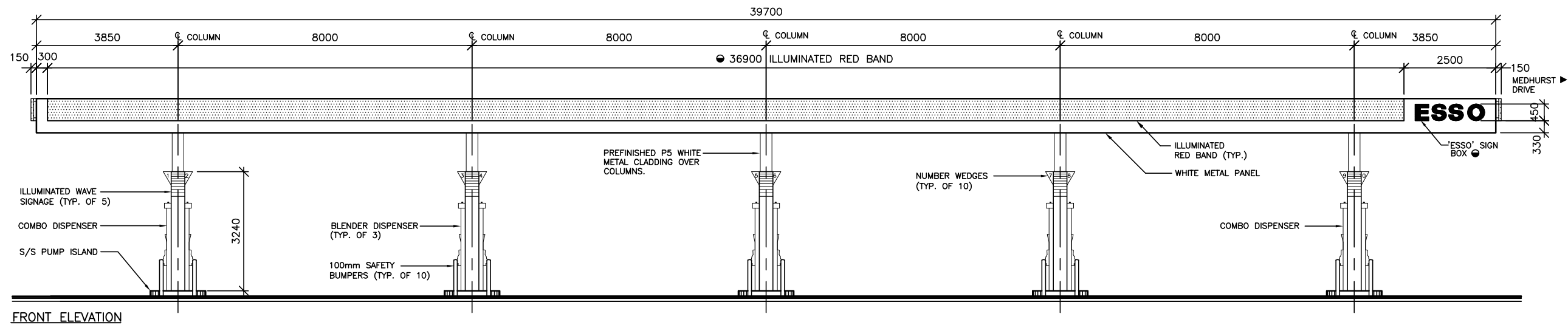




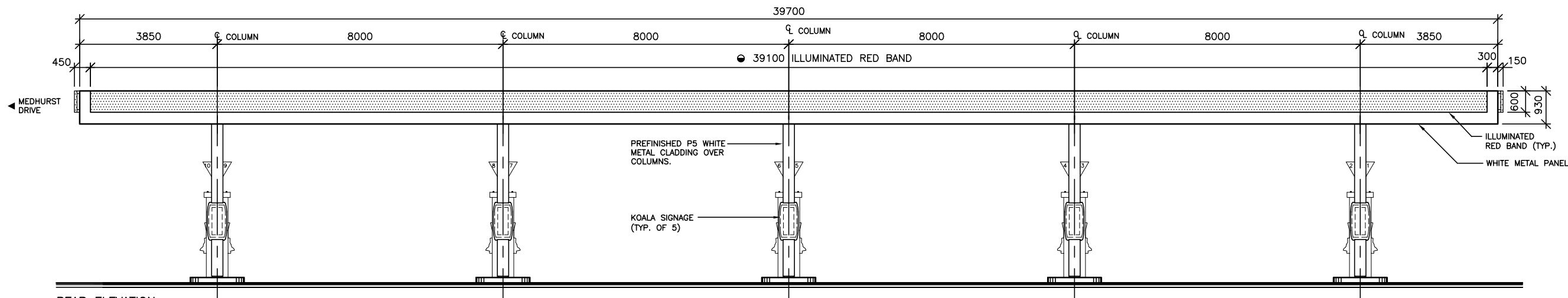
Imperial Oil Esso Imperial		FUEL MARKETING ASSET MANAGEMENT - ENGINEERING SERVICES MARKETING DES. CABRIERANS GESTION DES ACTIFS - SERVICE D'INGENIERIE TORONTO, ONTARIO MFC 1K5
Date 14 03 18	# Revision 0 ISSUED FOR REVIEW	By JMW
Project/Prop 1545 WOODROFFE AVE. & MEDHURST DR OTTAWA, ONTARIO		P.L. No. PBL 0000302287-09P S.A.P. No. SAP 88001626-09P
Title/Title PROPOSED MIRRORRED CARWASH FLOOR PLAN & ELEVATIONS		Scale 1:50 1:100
Date 14 03 14	By/Par AMEC (176071)	P.M./G.P. D.G.
Scheme 00		Propositio: AOA
Sheet/feuille: 00		Rev/No./Rev 00

Date	#	Revision	By	Design	Chk'd	Chk'd
14 03 18	0	ISSUED FOR REVIEW	JDM		Verifié	Verifié
15 02 10	1	ISSUED FOR REVIEW				
15 02 11	2	ESSO SIGN BOX LOCATIONS REVISED				
15 03 08	3	ADDED STENERGY COMPONENTS				

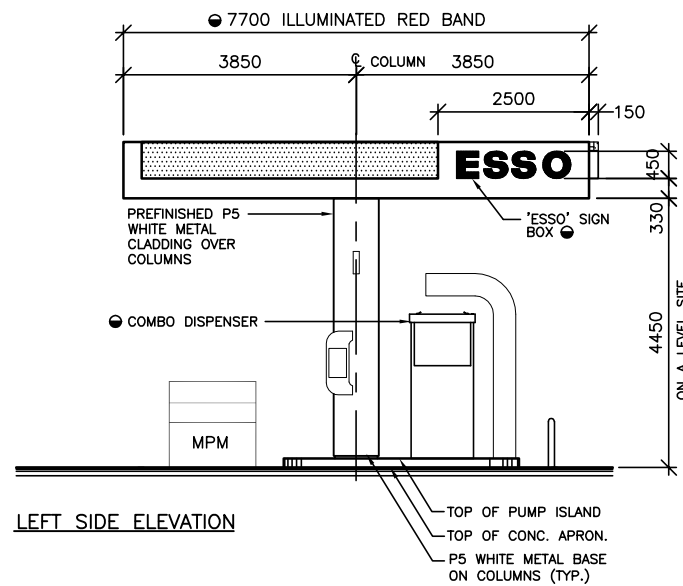
Project/Projet 1345 WOODROFFE AVE. & MEDHURST DR OTTAWA, ONTARIO		Title/Titre PROPOSED CANOPY ELEVATIONS		Scale Echelle 1:62.5	
P.L. No. PBL 0000302287-07P SAP No. SAP 88001626-07P		Date 14 03 20		Scale (1"=1'7") 1:125	
Scheme Proposition: 00		By/Par AMEC (176071)		Date 14 03 20	
Rev/No./Rev 03		D.G.		P.M./G.P.	



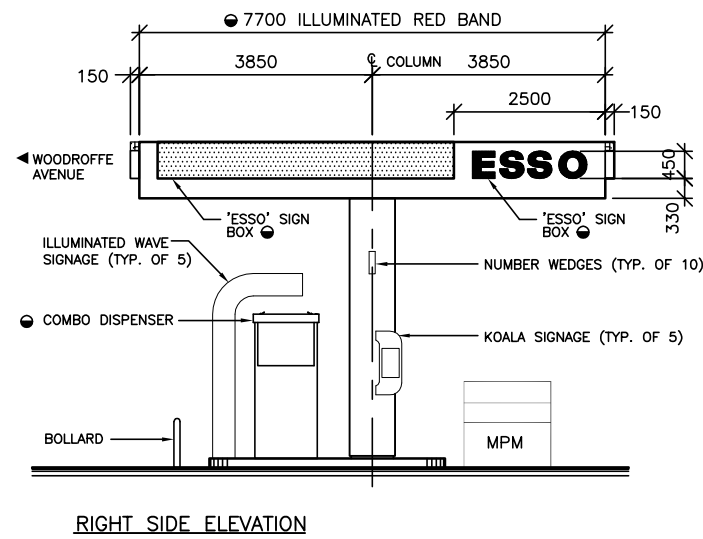
FRONT ELEVATION



REAR ELEVATION



LEFT SIDE ELEVATION



RIGHT SIDE ELEVATION

inches 1/8"

0 1 2 3 4

mm

0 1 2 3 4 5 6 7 8 9 10

Version: 3.0

IONSITE