

417 Auto Sales

2026 Carp Road Planning Rationale

Site Plan Control

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2026 Carp Road

Ottawa, ON

Planning Rationale

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SECTION 1: INTRODUCTION

1.1 Overview

JD Planning has been retained by 2244434 Ontario Inc. or 417 Auto Sales (the 'Owner') to prepare a Planning Rationale Report for an application for Site Plan Control with respect to the development of a proposed automobile dealership located at 2026 Carp Road, in the City of Ottawa. The site is a rectangular-shaped lot which presently contains a one-storey single-detached building (vacant) with frontage on the east side of Carp Road. The Owner is proposing to convert the existing one-storey single-detached building to a sales dealership and associated office space for the car dealership and introduce a parking and display area for used cars. The following is a Planning Rationale in support of the proposed development and Site Plan Application.

1.2 Subject Site

The site is a rectangularly-shaped lot located on the east side of Carp Road, municipally known as 2026 Carp Road. There is presently a small single-detached dwelling (formerly residential) located on the west side of the lot towards the Carp Road frontage, with a gravel driveway located on the south side towards the southern interior side lot line. The intent is to retain and renovate the existing single-detached dwelling to convert the building into a sales office for used vehicles. Figure 1 below shows the site and approximate property lines per 2021 GeoOttawa mapping.



Figure 1. Subject site showing property lines and surrounding context (GeoOttawa, 2021)

1.2.1 Site Statistics

The respective site statistics and legal description are set out below with the extract of the survey presented in Figure 2:

Area	1,808.09 m ² (0.45 acres)
Frontage	39.62 m (Carp Road)
Depth	45.67 m
Legal Description	Part of Lot 1, Concession 2, Geographic Township of Huntley, City of Ottawa
PIN	04487-0301

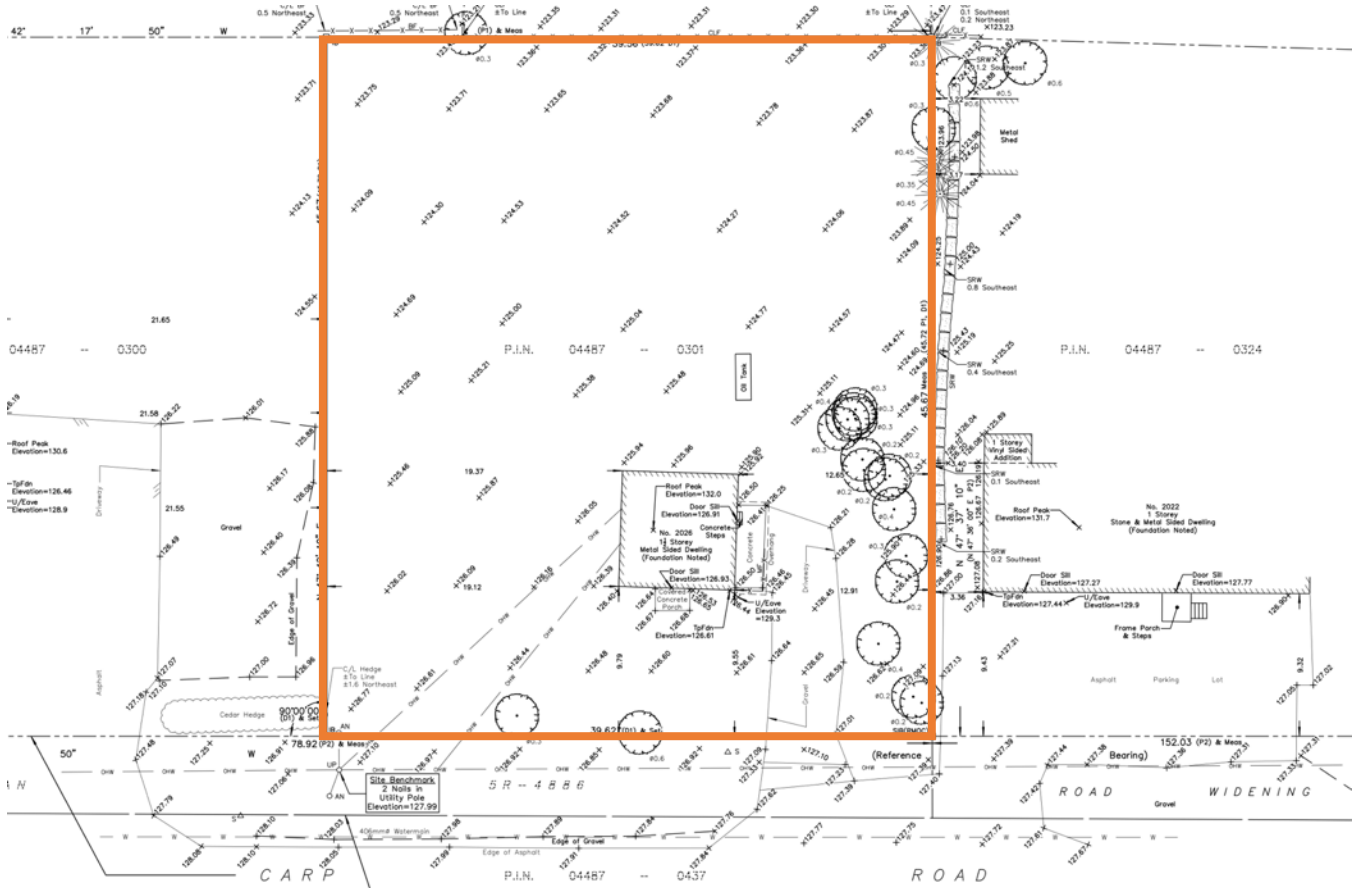


Figure 2. Extract of survey prepared by Farley Smith & Denis Surveying Ltd.

Images 1, 2 and 3 below represent the existing site conditions (Google Streetview, July 2022) from Carp Road. The existing building is to be retained and converted to a sales office for the automobile dealership use. Images 4 through 7 present the immediately surrounding context (Google Streetview, 2019).



Image 1. Subject site looking north from Carp Road (Google Streetview, July 2022)



Image 2. Subject site looking east from Carp Road (Google Streetview, July 2022)



Image 3. Subject site and adjacent commercial use looking north from Carp Road (Google Streetview, July 2022)



Image 4. Car dealership across from subject site looking southwest from Carp Road (Google Streetview, July 2022)



Image 5. Property immediately to the north of site looking northeast from Carp Road (Google Streetview, July 2022)



Image 6. Surrounding conditions looking southwest from Carp Road (Google Streetview, July 2022)



Image 7. Surrounding commercial uses north of site looking north from Carp Road (Google Streetview, July 2022)



Figure 3. Aerial imagery of subject site and immediately surrounding land uses (GeoOttawa, 2021)

1.3 Surrounding Context

The site is situated in Ottawa’s rural area within the Stittsville neighbourhood in Ward 6 – Stittsville which is generally located north of Richmond, west of Kanata Lakes – Arcadia and Katimavik – Hazeldean, south of Carp and east of Corkery and Munster-Ashton. The neighbourhood is bordered by Highway 417 to the north, west of Terry Fox Drive, north of Fernbank Road, and east of the rural areas of Richmond, bounded by low-rise residential subdivisions at the most westerly edge of Stittsville. Stittsville is largely characterized by low-rise, low-density residential subdivisions areas of low-rise commercial uses. More specifically, the site is located on the east side of Carp Road, south of the intersection of Westbrook Road and north of the intersection of Rothbourne Road. To the east, Lloydalex Crescent wraps behind the property, connecting Carp Road to the north, traveling south to a cul-de-sac past Echowoods Avenue. Existing uses along both the west and east sides of Carp Road immediately surrounding the site include predominantly low-rise rural commercial including offices, service, automobile dealership, and others on generally equally sized lots. Immediately east of the site behind the rear lot line is a pocket of residential uses which front onto both the east and west sides of Lloydalex Crescent. More specifically, the site is located on the west side of Carp Road, bordered by low-rise commercial uses to the north and south, low-rise residential to the east, and an automobile dealership and low-rise residential to the west.

1.3.1 Transit & Transportation Network

The site fronts onto the east side of Carp Road, which is a two-lane generally north-south Arterial which connects Stittsville to Fitzroy, travelling from Stittsville Main Street to the southeast, to Galetta Side Road to the northwest. Carp Road provides direct access to Highway 417 which travels east-west across the City of Ottawa and is easily navigable for trucking routes. Given the rural location of the site, there is limited transit accessible to the site, with the closest transit stations being located to the southeast at Tanger Outlets, located Palladium Drive and Campeau Drive.



Figure 4. Aerial imagery of subject site and immediately surrounding land uses (Google Maps, 2023)

SECTION 2: PROPOSED DEVELOPMENT

2.1 Project Overview

The proposed development is a simple conversion of the existing residential building into a new office and dealership for automobile sales. There is no proposed new construction of buildings on site, with the existing building footprint remaining as is, and updates restricted to an interior retrofit. The proposed site changes will be to accommodate parking on site for both employees and customers, as well as vehicle display and storage.

2.2 Access & Layout

The existing access is along the south side of the property on the east side of Carp Road, and this location will be maintained and slightly widened. The drive aisle will be asphalt and leads to 4 parking spaces for customers and employees, located towards the most southerly interior lot line.

2.3 Landscaping

The proposed dedicated parking area will be asphalt, with a gravel parking area wrapping around the existing building for vehicle storage and display. The remaining lands will be softly landscaped, with sod along the interior side and rear yards, rose and spirea bushes along the front yard, as well as serviceberry trees along the front yard. A cedar shrub privacy screen is proposed in line with the front wall of the building to screen the vehicle storage/display area from the street.

There are presently Manitoba maple and white cedar trees that are either shared or privately owned on site to be protected along the front lot line and interior side yard. Trees conflicting with the proposed parking area are to be removed, per the enclosed Tree Conservation Report.

2.4 Development Statistics

As noted, there is no new construction on site save for site alteration associated with introducing a new asphalt parking area for the building, and a new gravel parking area for vehicle storage, along with associated. The proposed site and development statistics are indicated in Table 1 below, with the proposed site plan and landscape plan presented in Figures 5 and 6.

Table 1. Development details

Lot Area	1,808.09 m ²	Building Height	~4.5 m
Front Yard Setback	9.55 m	Building Footprint Area	~57 m ²
Interior Side Yard Setback	12.65 m & 19.12 m	Vehicular Parking	4 spaces
Rear Yard Setback	28.32 m	Drive Aisle Width	6.01 m

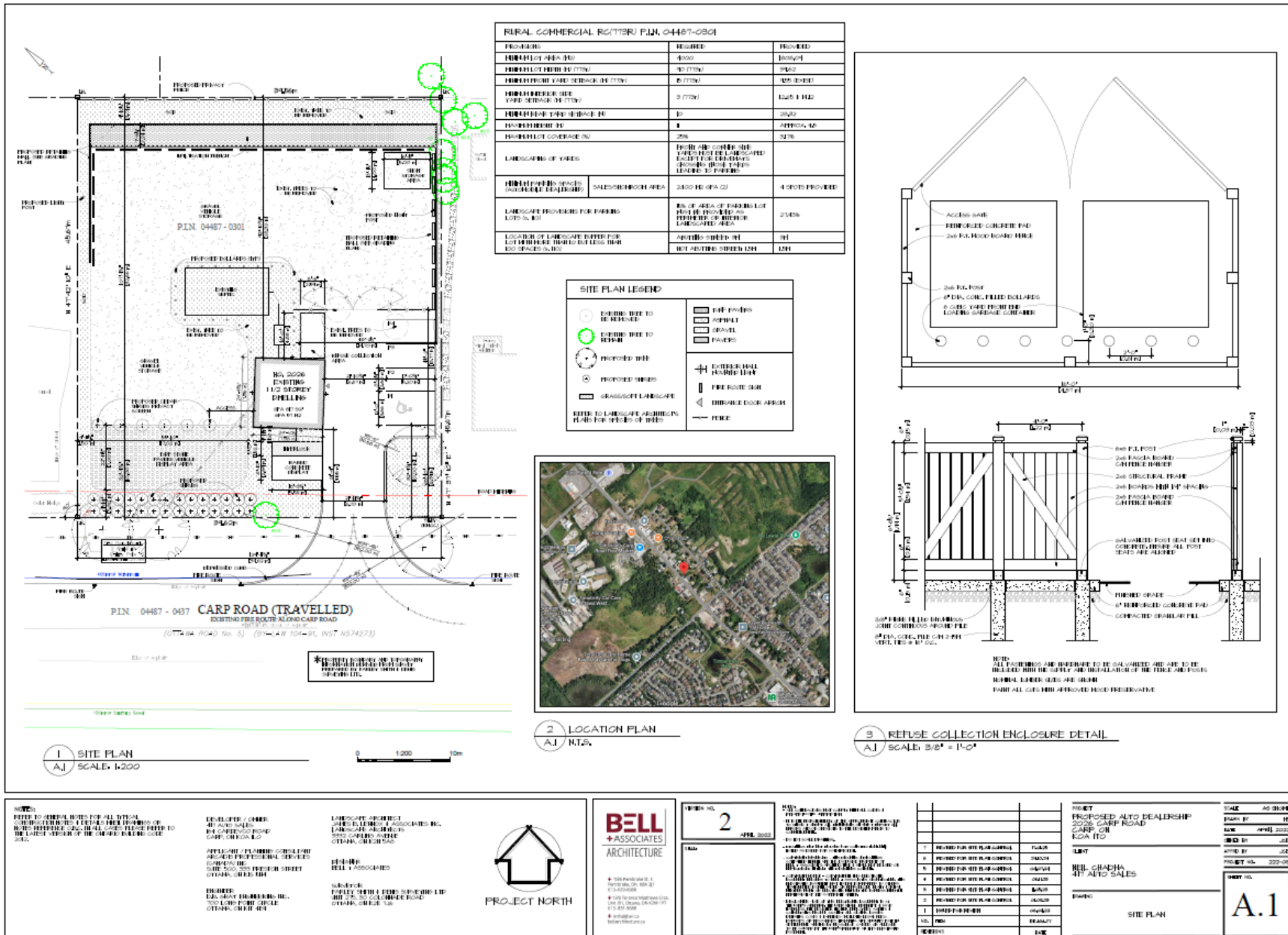


Figure 4. Proposed Site Plan prepared by Bell + Associates Architecture

SECTION 3: POLICY CONTEXT

3.1 Provincial Policy Statement

The Provincial Policy Statement 2020 (PPS) provides policy direction on planning matters for the Province of Ontario. Decisions affecting all planning matters shall be consistent with the Provincial Planning Policies. The proposed development complies with the applicable policies of the PPS, as demonstrated below.

Section 1.1.1 of the PPS states that healthy, livable, and safe communities are sustained by:

- a) “promoting efficient development and land use patterns which sustain the financial well-being of the Province and municipalities over the long term;
- b) accommodating an appropriate affordable and market-based range and mix of ...employment (including industrial and commercial)...and other uses to meet long-term needs;
- c) avoiding development and land use patterns which may cause environmental or public health and safety concerns;
- d) avoiding development and land use patterns that would prevent the efficient expansion of settlement areas in those areas which are adjacent or close to settlement areas;
- f) improving accessibility for persons with disabilities and older persons by addressing land use barriers which restrict their full participation in society;
- e) ensuring that necessary infrastructure and public service facilities are or will be available to meet current and projected needs”...

Section 1.1.4 of the PPS states that “[r]ural areas are important to the economic success of the Province and quality of life”. Per the PPS, “rural areas and urban areas are interdependent in terms of markets, resources and amenities. It is important to leverage rural assets and amenities and protect the environment as a foundation for a sustainable economy”. **Section 1.1.4.1** notes that healthy, integrated, and viable rural areas should be supported by:

- a) “building upon rural character, and leveraging rural amenities and assets;
- e) using rural infrastructure and public service facilities efficiently;
- f) promoting diversification of the economic base and employment opportunities through goods and services, including value-added products and the sustainable management or use of resources...”

Section 1.1.5.4 of the PPS states that “development that is compatible with the rural landscape and can be sustained by rural service levels should be promoted”. **Section 1.1.5.5** states that “development shall be appropriate to the infrastructure, which is planned or available, and avoid the need for the unjustified and/or uneconomical expansion of this infrastructure”. Finally, **Section 1.1.5.6** states that “opportunities should be retained to locate new or expanding land uses that require separation from other uses”.

Section 1.3.1 of the PPS sets out policies for employment and states that “planning authorities shall promote economic development and competitiveness by:

- a) providing for an appropriate mix and range of employment, institutional, and broader mixed uses to meet long-term needs;

b) providing for a diversified economic base, including maintaining a range and choice of suitable sites for employment uses which support a wide range of economic activities and ancillary uses, and take into account the needs of existing and future businesses.”

→ In the above PPS policies, there is a significant emphasis on efficient development that is safe, respects the natural environment, uses available infrastructure, and provides for the needs of the community in both the short- and long-term. The PPS also emphasizes the need to respect existing rural character as well as retention and diversification of rural employment opportunities. The proposed conversion of the former residential building into an automobile dealership office and associated vehicular storage is a permitted use of this land and meets the intent of the PPS. The introduction of an automobile dealership to this site, which is zoned and designated to permit this type of use, supports the rural employment area policies of the PPS. The change of use to an automobile dealership supports diversification of the rural economic base. There are several other examples of similar uses including other automobile dealerships, service, retail, office, and light industrial uses along Carp Road, which has direct access to Highway 417. The proposed dealership is appropriate for and compatible with these uses and is adequately separated between the nearby residential uses to the east. Given the existing building will be reused and repurposed rather than demolished, this is an efficient and sustainable use of infrastructure. Overall, the proposed development meets the intent and purpose of the Provincial Policy Statement, as exemplified by policies set out above.

3.2 Official Plan

The subject site is Designated Rural Countryside in Schedule B9 – Rural Transect in the Official Plan, per Figure 7 below. The intent of the Rural Transect Area is to “provide for a range of social and economic activity” with policies that are “intended to ensure the responsible use of resources for the protection of public health and the environment while supporting economic development for rural businesses in recognition of their valuable contributions to the regional economy” (Section 5.5). The site is also designated as “Future Neighbourhood Overlay” in the Urban Expansion Area per Schedule C17 of the Official Plan.

Policy 5.5.1(b) and (c) state that, “[o]utside Villages, where development is permitted, built form and site design shall be premised on maintaining the rural character, image and identity; and... “sites shall be designed to locate surface parking, storage and paved areas far from the road frontage, and access to such areas shall be designed to maintain rural character. The frontage along the road shall be landscaped and treed in a way that respects the rural landscape and enhances the green edge of rural roads. Elements such as low fences, hedges or landscape-based ornaments may be used to enhance the site frontage”.

→ The existing one-storey building will remain unchanged and is in keeping with the character of the surrounding context. The paved surface parking located towards the south side of the lot, and the proposed storage area for vehicles will be gravel materiality to minimize the asphalt surfacing on site. The site will be bordered by soft landscaping and trees to provide landscape buffering and visual screening from the proposed use and the street.

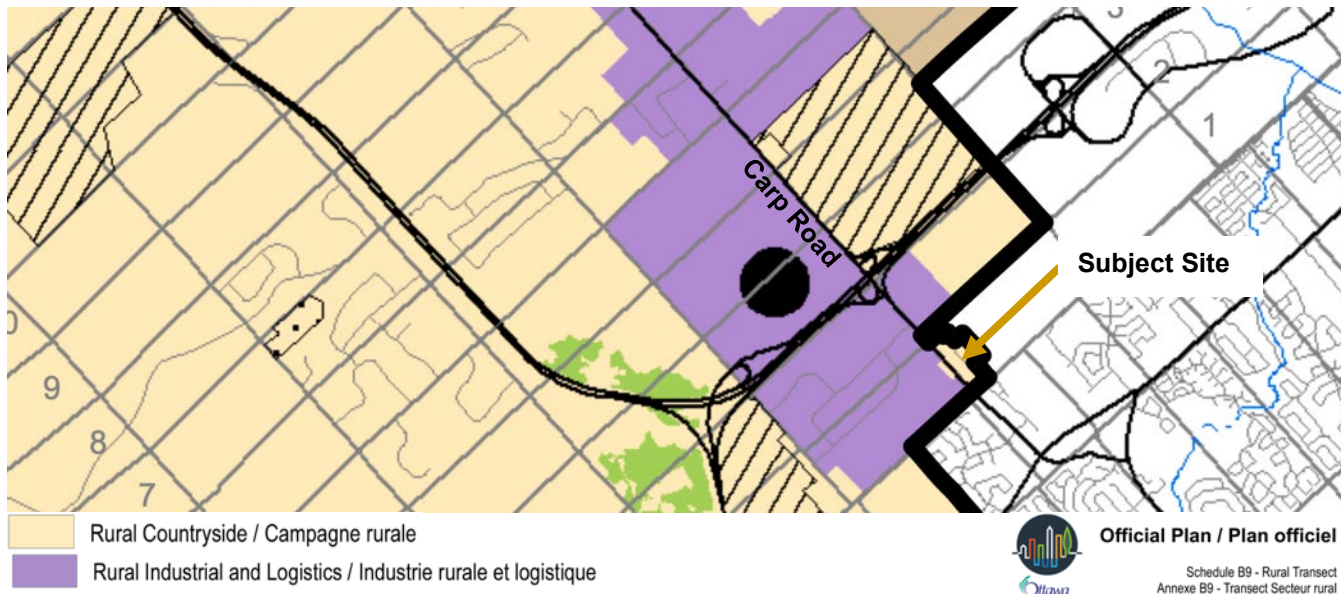


Figure 6. Extract of new Official Plan, Schedule B9 – Rural Transect

The subject site is designated Rural Countryside in the Official Plan. Per **Section 9.2** of the Official Plan, “[t]he intent of this designation is to accommodate a variety of land uses that are appropriate for a rural location, limiting the amount of residential development and support industries that serve local residents and the travelling public, while ensuring that the character of the rural area is preserved”. Per **Section 9.2.2, Subsection 2**, permitted uses, where the appropriate zoning is in place, include small scale commercial where the uses serve the rural community, they are located within 200 metres of an arterial or collector road, are of appropriate scale for the rural context, minimizes incompatibilities with adjacent residential uses, and integrates appropriately with the rural character. **Subsection 3** states that “[w]here the use is proposed within 1 kilometer of a Village or Urban boundary, it will be reviewed to ensure that it shall not impede the expansion of the settlement area and all of the following criteria shall be considered:

- a) The use is a compatible use with a village or urban area;
- b) The development has frontage on an existing public road;
- c) The site shall consider future multi-modal connections and be designed to accommodate these along with street trees in the future; and
- d) The use can be adequately serviced by on-site systems and will not place demand on the extension of public services for any reason, including fire suppression or contaminated groundwater”.

→ The proposed conversion of the existing building to an automobile dealership is a permitted use of the land and appropriate for the context. The proposed dealership is permitted under the Rural Commercial zoning and the use supports the City’s Official Plan policies by providing a context-appropriate low-rise commercial use with access to the arterial Carp Road, which is compatible with the other commercial and dealership uses adjacent to the property and sensitive to the nearby residential uses. The proposed automobile dealership is a permitted and desirable use of this land. The site is fully zoning compliant and conversion of the use can proceed in the Future Neighbourhood Overlay.

3.3 Zoning By-law

The site is zoned RC[773r] – Rural Commercial with Rural Exception in the City of Ottawa’s Zoning By-law 2008-250, as demonstrated in Figure 8 below. The RC zone permits a variety of rural commercial and light industrial uses set out below. The purpose of the RC zone is to:

- (1) permit the development of highway and recreational commercial uses which serve the rural community and visiting public in areas mainly designated as General Rural Area, Village and Carp Road Corridor Rural Employment Area in the Official Plan;
- (2) accommodate a range of commercial uses including services for the traveling public as well as agriculture-related, vehicle-oriented and construction products and services;
- (3) permit research facilities in areas designated Greenbelt Employment and Institutional Area in the Official Plan, and
- (4) regulate development in a manner that has a minimal impact on the surrounding rural area or villages.

The RC[773r] zone permits the following uses:

- amusement centre
- artist studio
- automobile rental establishment
- automobile dealership
- bank [773r]
- bank machine (if located in same building or same lot as a permitted use)
- click and collect facility
- convenience store (same building/lot as permitted use)
- detached dwelling (limited to one, accessory to permitted use)
- warehouse
- dwelling unit (limited to total, accessory to permitted use)
- hotel
- office [773r]
- parking lot
- payday loan establishment [773r]
- personal service business (same building/lot as permitted use)
- research and development centre [773r]
- restaurant
- retail food store, limited to a farmers’ market
- retail store
- storefront industry



Figure 7. Extract of zoning by-law with subject sites shown in orange (GeoOttawa, 2021)

Table 2. RC[773r] zoning provisions

RC[773r] Zoning				
Provisions		Required		Provided
Min. lot area (m ²)		4000		1,808.09*
Min. lot width (m) [773r]		90.0		39.62*
Min. front yard setback (m) [773r]		15.0		9.55*
Min. interior side yard setback (m) [773r]		3.0		12.65 & 19.12
Min. rear yard setback (m)		10.0		28.32
Max. height (m)		11.0		~ 4.5
Max. lot coverage (%)		25%		3.17%
Front yard landscaping		Must be landscaped except for driveways crossing those yards leading to parking		Landscaped
Outdoor storage		Permitted in interior side and rear yard only		Vehicle storage located in interior side yard and rear yard
		Must be screened and concealed and concealed from view abutting streets and from abutting non-commercial or non-industrial zones		Screened from abutting streets and abutting residential
Min. parking spaces	Sales/showroom	2/100 m ² GFA	2	4 spaces
	Service	1/service bay	0 – no service bays	
	Other	1/100 m ² GFA	0 – all sales/showroom	
Min. drive aisle width for double traffic lane to parking lot (m)		6.70		6.70
Landscaping for parking lots		15% of the parking lot area must be perimeter or interior between the lot line and the perimeter of the parking lot, and can include landscape islands, medians, pathways, plazas		28%
Width and location of landscaped buffer for >10 but <100 spaces	Abutting a street (m)	3.0 m		3.0
	Not abutting a street (m)	1.5 m		1.5

* Existing non-complying condition

SECTION 4: SUPPORTING PLANS & STUDIES

A brief overview of the supporting studies prepared by other consultants is included below. Note that this is for ease of reference only and represent a general but incomplete overview of the submitted reports. Full details are found in the referenced reports prepared by others.

4.1 Site Servicing Study & Stormwater Management Report

A Site Servicing Study & Stormwater Management Report was prepared by D.B. Gray Engineering Inc., dated March 24, 2023 and revised July 2, 2024. The intent of the report is to describe the servicing and

stormwater management design for the proposed automobile sales building with associated display lot and parking. The report makes the following conclusions, as follows for ease of reference:

1. *“A private fire hydrant is not required.*
2. *There is an adequate water supply for firefighting from the existing municipal fire hydrant.*
3. *The existing 19 mm water service is proposed to remain and is adequate for the proposed use.*
4. *The existing on-site septic system is proposed to remain; and as per the Hydrogeological Brief and Assessment it “is considered adequately sized for the proposed change in use”.*
5. *To meet the water quality target of 80% total suspended solids (TSS) removal an infiltration trench with a storage volume of 5.5 m³ is required; however, a 6.7 m³ infiltration trench is proposed.*
6. *The infiltration trench has 148% of the required capacity to capture and infiltrate into the ground to achieve a post development annual infiltration for the property of 243 mm/year; greater than pre-development and existing infiltration; and within 7% of the 262 mm/year target.*
7. *The maximum post-development release during the 100-year event is calculated to be equal to the maximum allowable; and 65% less than the pre-development flow rate. During the 5-year event it is 51% less than the maximum allowable; and 64% less than the pre-development flow rate.*
8. *The post-development reduction in flow rates is expected to have a positive impact on the adjacent properties.”*

For a detailed review of the research and findings, refer to the full report enclosed with this submission.

4.2 Hydrogeological Brief and Assessment of an Existing Sewage System

A Hydrogeological Brief and Assessment of an Existing Sewage System study was prepared by Paterson Group dated December 22, 2022 to determine whether the existing sewage system is presently functioning adequately and to determine if the existing capacity is suitable to support the change of use. The report sets out that there are no new fixtures to be added to the site, with three to give employees occupying the site for a standard 8 hour per day shift, with washroom access limited to employees only. The report confirms that the site is serviced by municipal water and private septic. The report notes that the only property located nearby to be serviced by a private drilled well is 1016 Carp Road, which is considered cross gradient and isolated from potential onsite septic impacts, and a Septic Impact Assessment was not required. The report sets out the following findings and recommendations, for ease of reference:

- *“It is our opinion that the existing sewage system is functioning adequately and showing signs of age with the formation of a light biomat within the clear stone layer. The formation of a biomat is not unexpected for a bed of this age.*
- *The client should be aware that the age of the existing leaching bed is approximately 22 years and the average life expectancy of a bed of this type, when property designed and constructed, is in the order of 30 years. Usage and maintenance will greatly affect the life expectancy.*
- *As a means of prolonging the life of the bed, good water practices (i.e. preventing surge flows) should be undertaken by the occupants.*
- *The location of the existing sewage system components with respect to the existing buildings and drilled wells conform to the OBC regulated separation distances.*
- *Due to the deterioration of the concrete centre wall of the tank, it is recommended that the centre wall be repaired.*
- *The Tank should be inspected and pumped, and the effluent filter be cleaned, on a regular basis (i.e. approx. every three to five years).*

- *Commercial usage of the system would provide a maximum of 1,100 L/day for daily flows (TDDSSF).*
- *The existing system is considered adequate for the proposed commercial use in terms of sizing.”*

Refer to the report enclosed with this submission for a detailed review of the findings and recommendations.

4.3 Geotechnical Investigation

A Geotechnical Investigation was prepared by Paterson Group, dated August 9, 2022, and revised June 18, 2024 following completion of a groundwater monitoring program. The objective of the investigation was to determine the existing subsoil and groundwater conditions at the site via boreholes, and to provide the geotechnical recommendations for the design of the development. The report confirms that from a geotechnical perspective, the site is suitable for the proposed development. The report notes that “[i]f required, foundation support for proposed buildings could consist of conventional spread footings bearing on undisturbed, compact to very dense glacial till”. Further, the contractor should be prepared for cobbles and boulders removal during the site excavation. The memorandum prepared by Paterson Group (PG6271-MEMO.02) dated June 18, 2024 also provides responses to City comments regarding soil percolation rates and seasonal high-ground water levels (spring). The responses confirm the seasonal high groundwater table is at a depth greater than 5.5 m below the existing ground surface, and the seasonal high water levels will have negligible impact on the functionality of the site. Refer to the enclosed report and memorandum for complete findings and recommendations.

4.4 Noise / Vibration Study

An Environmental Noise Control Study was prepared by Paterson Group, dated August 25, 2022. The objective of the study was to determine the primary noise sources impacting the site and compare the projected sound levels to guidelines set out by the MOECC and City of Ottawa and review the projected noise levels and provide recommendations regarding warning classes, construction materials or sound barriers. The report concludes that there is a single major source of surface transportation noise to the development which is Carp Road. There is no outdoor living area for the development. STAMSON modeling indicates that the southwestern, northwestern, and southeastern elevations are expected to exceed the 65dBA threshold specified by the ENCG, and the noise levels at the building will be below 70dBA. The report indicates that there should be a central AC unit along with a warning clause Type D on those elevations. Standard building materials are acceptable for adequate soundproofing. Refer to the enclosed report for detailed findings, recommendations, and warning clause wording.

4.5 Phase I Environmental Site Assessment

A Phase I Environmental Site Assessment was prepared by Paterson Group, dated June 20, 2022, to research past and current use of the site and area to identify any potential environmental concerns. The report indicates that the site was first developed as residential in the 1960s and has continued with that use since then, and no PCAs were identified based on the site’s historical use. Neighbouring properties have historically been residential, with some commercial uses, as well as a former automotive service garage at 2021 Carp Road and a retail fuel outlet at 1016 Carp Road. Given their locations and cross gradient orientations, the report notes that these former uses do not result in an area of potential environmental concern for the subject site. Current uses surrounding the site are primarily residential, with two contractor yards containing private fuel outlets, located at 1017B Carp Road and 1016 Carp

Road. The contractor yards are not considered APECs for the site. The report concludes that a Phase II Environmental Site Assessment is not required for the site. The recommendations set out in the report conclude that there may be materials with asbestos in the structure including drywall joint compound, vinyl floor tile and stipple plaster; however, the materials are in good condition and do not present an immediate concern. Refer to the enclosed Phase I ESA for complete findings and recommendations.

4.6 Tree Conservation Report

A Tree Conservation Report was prepared by IFS Associates Inc. dated April 13, 2023 to complete an inventory and assessment of individual trees on the site and adjacent private property, as well as City of Ottawa lands, with field work completed in October 2021 and January 2023. A table of all trees recommended for removal and preservation are provided. Notably, two trees on private neighbouring property to the southeast are proposed for removal and permission from the landowner is included in the report. Ten trees were assessed, and four total are proposed for removal (white cedar and Manitoba maple). Refer to the enclosed report for a detailed review of the trees, imagery, declarations, as well as preservation and protection methods for those to be retained.

SECTION 5: CONCLUSION

The subject is a rural lot located on the east side of Carp Road, municipally known as 2026 Carp Road. The property presently contains a small single-detached dwelling which was formerly residential, which will be converted to an automobile dealership. A surface parking lot for employees and customers as well as vehicle display area is proposed, with the existing drive aisle access maintained from Carp Road. The site will be bordered by soft landscaping including some trees and shrubs.

The site is located within an area surrounded by other similar rural commercial uses including sales offices, personal service businesses, auto dealerships, and others. There are existing residential uses further to the east along Lloydalex Crescent. The proposed automobile dealership is a permitted use under the existing Rural Commercial zoning. The site enjoys legal non-complying rights to a smaller than required lot area, width, and front yard setback, and the site is otherwise fully zoning compliant with no required minor variance application.

Overall, the proposed conversion of the existing building to a permitted automobile dealership is consistent with the Provincial Policy Statement 2020, conforms to the policies of the City of Ottawa’s Official Plan and complies with the provisions of the City of Ottawa’s Zoning By-law 2008-250. The development and associated application for Site Plan Control represents good land use planning and is recommended for approval.

Respectfully submitted,



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