

SUBJECT

375 Codd's Road – TIA Addendum #1
(formerly 1000-1050 Tawadina Road, Blocks 2, 3 & 5)

DATE

October 3, 2024

DEPARTMENT

Transportation Engineering

COPIES TO

Sameer Gulamani, Bayview Group
Rod Price, Demarco Construction
Kris Benes, Open Plan Architects
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TO

Wally Dubyk, City of Ottawa

OUR REF

https://arcadiso365.sharepoint.com/sites/Projects2/138889/Internal Documents/6.0_Technical/6.23_Traffic/03_Reports/Submissions/6- TechMemo - 375 Cods/

PROJECT NUMBER

138889

NAME

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Background

On behalf of Bayview Group, a Transportation Impact Assessment (TIA) was submitted to the City of Ottawa in support of a Site Plan Control application relating to this proposed mixed-use development consisting of three separate parcels at 1345, 1375 Hemlock Road and 375 Codd's Road. On February 21, 2024, TIA Steps 3 & 4 (final report) were re-submitted to the City of Ottawa, and addressed all technical comments received from the City on the initial submission. It is important to note that the initial TIA submission was referred as '1000-1050 Tawadina Road' and was submitted on July 29, 2022.

Introduction

Arcadis has been retained by Bayview Group to prepare an addendum to the TIA as it relates specifically to refinements to the portion of the development located on the parcel now municipally known as 375 Codd's Road, and previously identified as Building 3 in the TIA.

This portion of the development included a 6-storey apartment building with 108 residential units. Vehicular access was noted as being from Tawadina Road, providing access to 4 surface parking spaces (including two Type 'A' accessible spaces), a ramp to the below-grade parking garage with the balance of the parking spaces, and an on-site waste collection/loading area.

Site Plan Revisions

Overall, the number of residential units within this development block has since increased by 11 units for a total of 119 units. This change in unit count has a nominal impact on the overall person-trip generation of the site and thus does not have any impact on the performance of the adjacent transportation network.

With regards to parking, the site falls within *Area X: Inner Area* of the City of Ottawa Zoning By-law 2008-250 Consolidation. In recognition of the increased unit count, the overall parking supply on-site has increased from 67 to 78 spaces. The surface parking supply has increased to 7 spaces (two being Type 'A' accessible spaces), while the parking garage provides an additional 71 spaces (three being Type 'B' accessible spaces). The

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proposed parking supply continues to satisfy the minimum bylaw requirements of 0.5 per dwelling unit (zero for the first 12 units) for residents and 0.1 per dwelling unit (zero for the first 12 units) for visitors, calculated as 64 vehicular parking spaces required.

A total of 68 bicycle parking spaces are now proposed, including 50 stacked parking positions within the ground floor of the building, as well as 18 outdoor spaces (9 at each building entry). This exceeds the minimum requirement of 0.50 per dwelling unit as specified in the Zoning By-law, calculated as 60 bicycle parking spaces required.

The loading/waste collection area within the site has not changed and therefore swept path analysis undertaken in the TIA remains valid. As the location of the parking garage access has changed, a review of the traffic circulation below-grade was undertaken to ensure that the revised design is functional for vehicle circulation. It should be noted that the configuration of the parking garage is heavily constrained by the structural elements of the building and can not be easily modified, however some refinements to the structure have been made where possible to accommodate vehicular movements. As a result of these constraints, a portion of the parking garage must be limited to one-way flow for safety reasons to eliminate a hairpin turn with insufficient sight lines. Strategic placement of parking spaces within the parking garage has been used to reinforce the one-way configuration. See **Appendix A** for the vehicular swept paths which confirm the functionality of the site.

Conclusion

The net change in unit counts will have a negligible impact on the overall traffic generation of the site and its impacts to the adjacent transportation network. Changes the site's internal configuration have been shown to be functional from a traffic circulation perspective, while loading and waste collection remain unimpacted by this change. The vehicular and bicycle parking supply satisfy minimum zoning requirements with consideration of the increased unit count.

The overall conclusion of this Addendum is that the results of the February 2024 TIA remain valid and that the proposed changes to the site plan are functional with no impact to the adjacent transportation network.

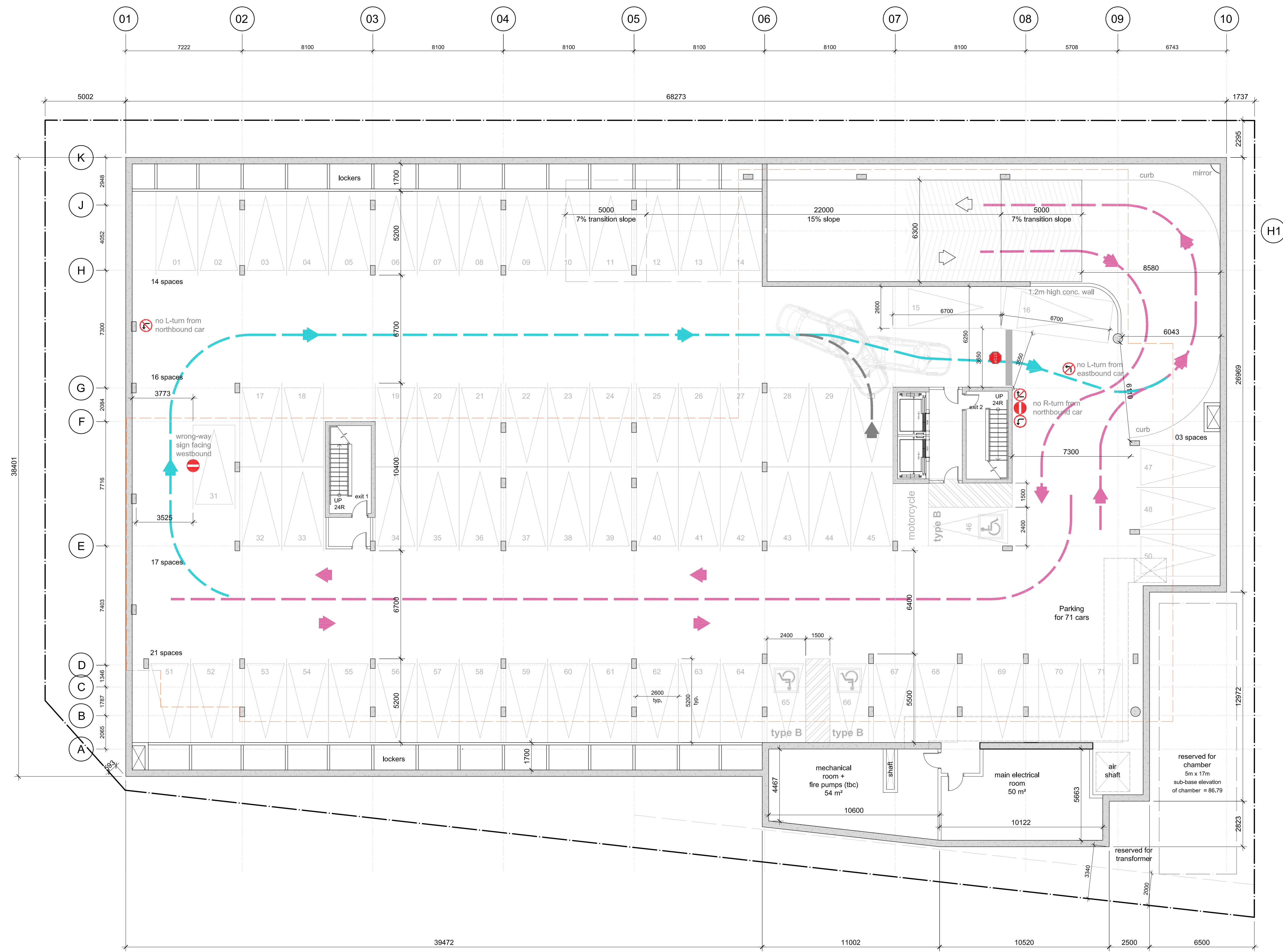
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Appendix A: Swept Path Analysis



Client :
Bayview Group

General Contractor:
Demarco Construction

Structural Engineer:
Elema

Civil Engineer :
WSP

Landscape Architect :
WSP

Mechanical + Electrical Engineer :
Design Works Engineering

06	Issued for coordination meeting	26 Sep. 2024
05	Re-issued for SPA	22 Aug. 2024
04	Issued for coordination	26 July 2024
03	Issued for city comment	04 June 2024
02	Issued for team review	10 May 2024
01	Issued for team review	24 April 2024
rev. / issue	description	date
		MM/DD/YY

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project

Wateridge Apartment Building
375 Codd's Road, Ottawa, ON

drawing

Floor plan
Parking level P1

drawn	KDB	date	April 2024
approved	KDB	revision	0
project no.	2406	scale	1 : 125
drawing no.			

A101