

= BOLLARD

= DENOTES TREE TO BE REMOVED

= DENOTES NEW TREE. REFER TO LANDSCAPE

= DENOTES NEW TREE. REFER TO LANDSCAPE

= DENOTES TREE TO REMAIN

ZONING INFORMATION:

Required: 55% (maximum)

Required: 22 m (maximum)

Required: 2 (maximum)

Lot Coverage (Sec 205., Table 205):

Building Height (Sec. 205, Table 205(h)):

Floor Space Index (Sec. 205, Table 205(g)):

Required Yards (Sec. 206, Table 206B):

Min. Rear Side Yard: 12 m (7.5 m required)

Min. Front Yard: Building A - 12 m (12 m required)

Min. Interior Side Yard: Buildings B and E - 7.5 m (7.5 m

Business Park (IP4)

Proposed: 41%

Proposed: 6.6 m

Proposed: 0.5

Business Park Industrial, Subzone 4 – Kanata South

ARCHITECTURE 49

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THIS BAR IS 25mm LONG WHEN PLOTTED A CORRECT SCALE. DO NOT SCALE DRAWINGS

PROJECT NO: 229-00058-11 CONTRACT NO DRAWN BY: NM/AS CHECKED BY: JC/AB APPROVED BY: Approver

1. SITE PLAN AND BUILDING DESIGN IS BASED ON "SECTION 3.10. SELF-SERVICE STORAGE BUILDINGS".

7. ALL EXISTING TREES, SHRUB BEDS, MULCH BEDS, AND

CONSTRUCTION. AREAS DAMAGED DURING CONSTRUCTION

8. USE SPECIFIED BACKFILL IN ALL TRENCHES RUNNING

9. FILL ALL HOLES AND LOW AREAS TO DESIGN SUBGRADE

WITH COMPACTED FILL (SUITABLE TO SURFACE FINISH),

FOR SODDED/PLANTED AREAS USE COMPACTED CLEAN EARTH FILL SUITABLE FOR PLANT GROWTH. FOR PAVED

10. ALL TREES WITHIN OR IMMEDIATELY ADJACENT TO

AREA OF WORK TO BE PROTECTED TO CITY OF OTTAWA

11. REFER TO LANDSCAPE DRAWINGS FOR LOCATIONS OF

ALL EXISTING, REMOVED, AND PROPOSED TREE AND

BELOW ALL STRUCTURES, PAVING, WALKWAYS, ETC.

TO BE REPAIRED TO CONTRACT ADMINISTRATOR'S

SOD TO REMAIN TO BE PROTECTED DURING

APPROVAL AT THE CONTRACTORS OWN COST.

AREAS USE COMPACTED GRANULAR BASE.

TREE PROTECTION STANDARDS.

SHRUB PLANTING

- 2. OCCUPANCY CLASSIFICATION IS F-2, BUILDING IS SPRINKLERED. CONSTRUCTION IS NON-COMBUSTIBLE. OCCUPANT LOAD DOES NOT APPLY.
- 3. SPATIAL SEPARATIONS DOES NOT APPLY BETWEEN BUILDINGS AS PER OBC 3.10.4.3 (4). THE DISTANCE BETWEEN INDIVIDUAL BUILDINGS SHALL NOT BE LESS
- 4. PER OBC 3.10, PROVISIONS FOR FIRE FIGHTING MEETS 9M WIDE ACCESS ROUTE AND UNOBSTRUCTED PATH OF TRAVEL FOR THE FIREFIGHTER FROM THE VEHICLE TO THE FIRE DEPARTMENT CONNECTION (FDC) OF MAX. 45M.
- 5. BUILDING IS EXEMPT PER OBC, MMA SUPPLEMENTARY STANDARD SB-10, DIVISION 3, ENERGY EFFICIENCY DESIGN AFTER DECEMBER 31, 2016, PER SECTION 1.2.1.1 (1)(d) AND 1.2.1.1 (2)(b).

**ACCESS STORAGE** 75 MICHAEL COWPLAND **DRIVE** 

ISSUED FOR SITE PLAN RESUBMISSION

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DRAWING TITLE

**SITE PLAN - GENERAL** 

A101

MANAGER (A), DEVELOPMENT REVIEW WEST

PLANNING, DEVELOPMENT AND BUILDING SERVICES

**DEPARTMENT, CITY OF OTTAWA**