

This **ARCHITECTURAL DESIGN PACKAGE** has been prepared in support to a Site Plan Control Application regarding the construction of two buildings containing a total of 44 stacked townhomes along Innes Road in the City of Ottawa

Design Summary:

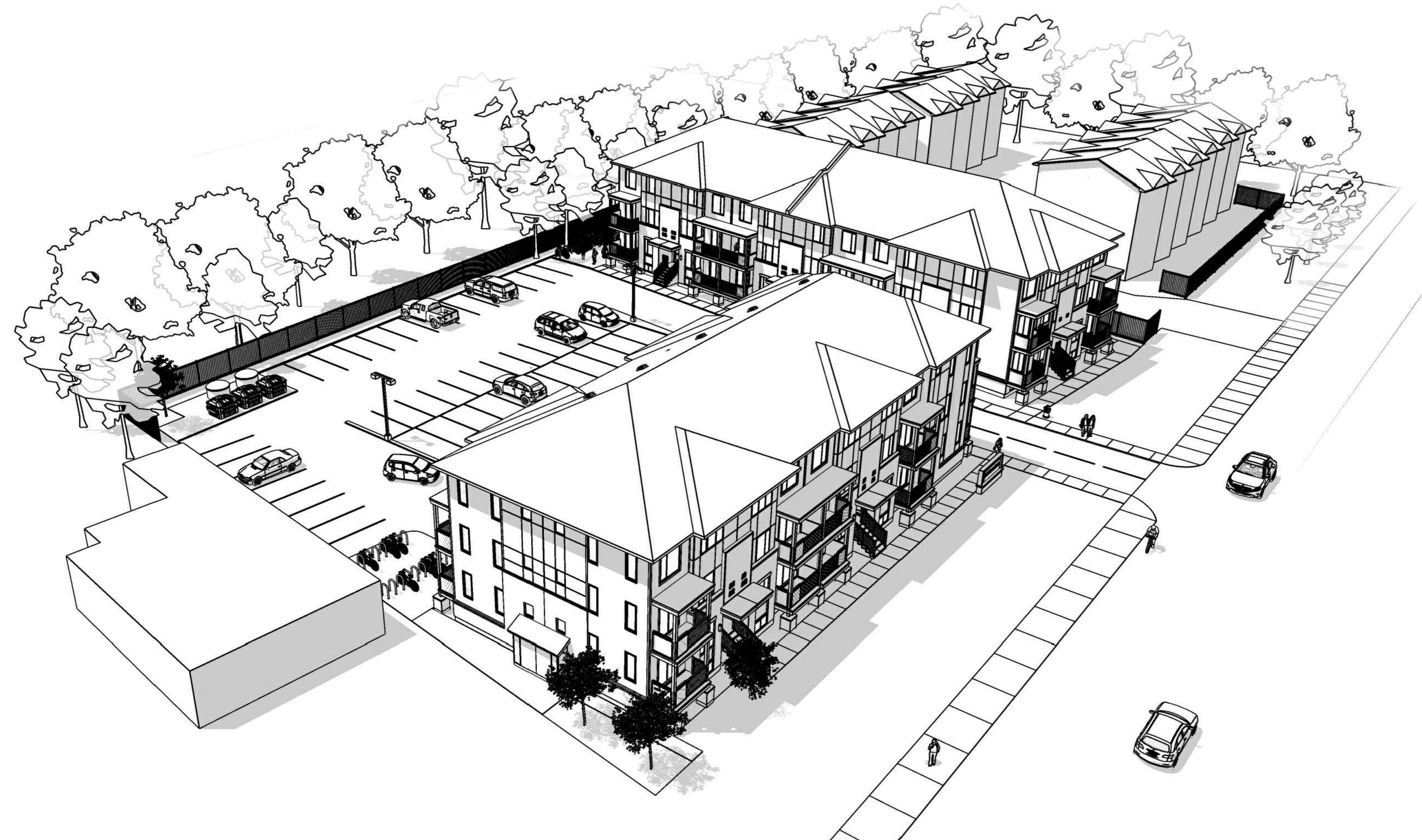
The proposed design features two three-story buildings with an elevated basement level, reaching a height of approximately 12.5 meters. Embracing a low-rise design principle with pitched rooflines, the buildings seamlessly integrate with the established neighborhood character, particularly in areas with two- and three-story residential dwellings. Setbacks are strategically planned to ensure connection and smooth transitions with adjacent properties. The design aligns with the prevalent low-rise architectural pattern in the neighborhood, aiming to enhance the community's visual appeal.

The buildings are designed to maintain a sensible low-rise form tailored to the site's characteristics and transition seamlessly into neighboring properties. The strategic placement of parking and communal amenity areas, along with existing mature treelines, helps buffer the shift from traditional single residential dwellings to townhomes and commercial buildings. This design strategy aims to mitigate disruptions and ensure cohesive integration within the community's architectural fabric.

Along Innes Road, the buildings are visually prominent and distinctive, featuring walkways, staircases, and marked entrances with cohesive motifs. Abundant fenestration and glazing create an open and inviting atmosphere, with carefully arranged trees framing the buildings aesthetically. The orientation of units provides panoramic views in all directions, optimizing the visual experience within and around the property.

To minimize the impact of service areas and parking, a careful screening approach is employed by placing buildings close to the front property line. Landscaping plays a crucial role in softening the visual impact at street level. Pedestrian walkways are well-designed, connecting the street edge to residential unit entrances, enhancing connectivity between the public realm and private development. The parking entrance is positioned between the buildings to integrate seamlessly with pedestrian flow.

The outdoor amenity spaces, both communal and private, are thoughtfully designed. The communal amenity area is envisioned as a human-scaled environment, surrounded by trees to create a comforting ambiance and minimize traffic sounds. This space is strategically placed for outdoor socialization, gatherings, and meals. Private balconies are designed for functionality, carefully balanced to harmonize with the surrounding environment. Overall, the proposed building design aims to create a distinctive and impactful presence along Innes Road while prioritizing aesthetics, functionality, and community integration.



2506 Innes Road - ARCHITECTURAL DESIGN PACKAGE

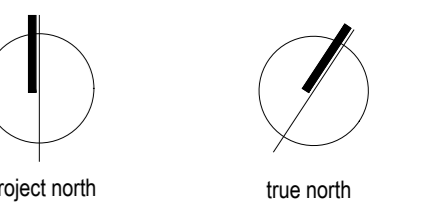
DRAWING LIST:

- A0.0 - COVER PAGE & ARCHITECTURAL DESIGN BRIEF
- A1.0 - SITE PLAN & KEY PLAN
- A2.1 - ELEVATIONS
- A2.2 - ELEVATIONS
- A3.1 - CONCEPTUAL SECTION
- A4.1 - CONTEXTUAL VIEW
- A4.2 - ILLUSTRATIONS
- A4.3 - ILLUSTRATIONS

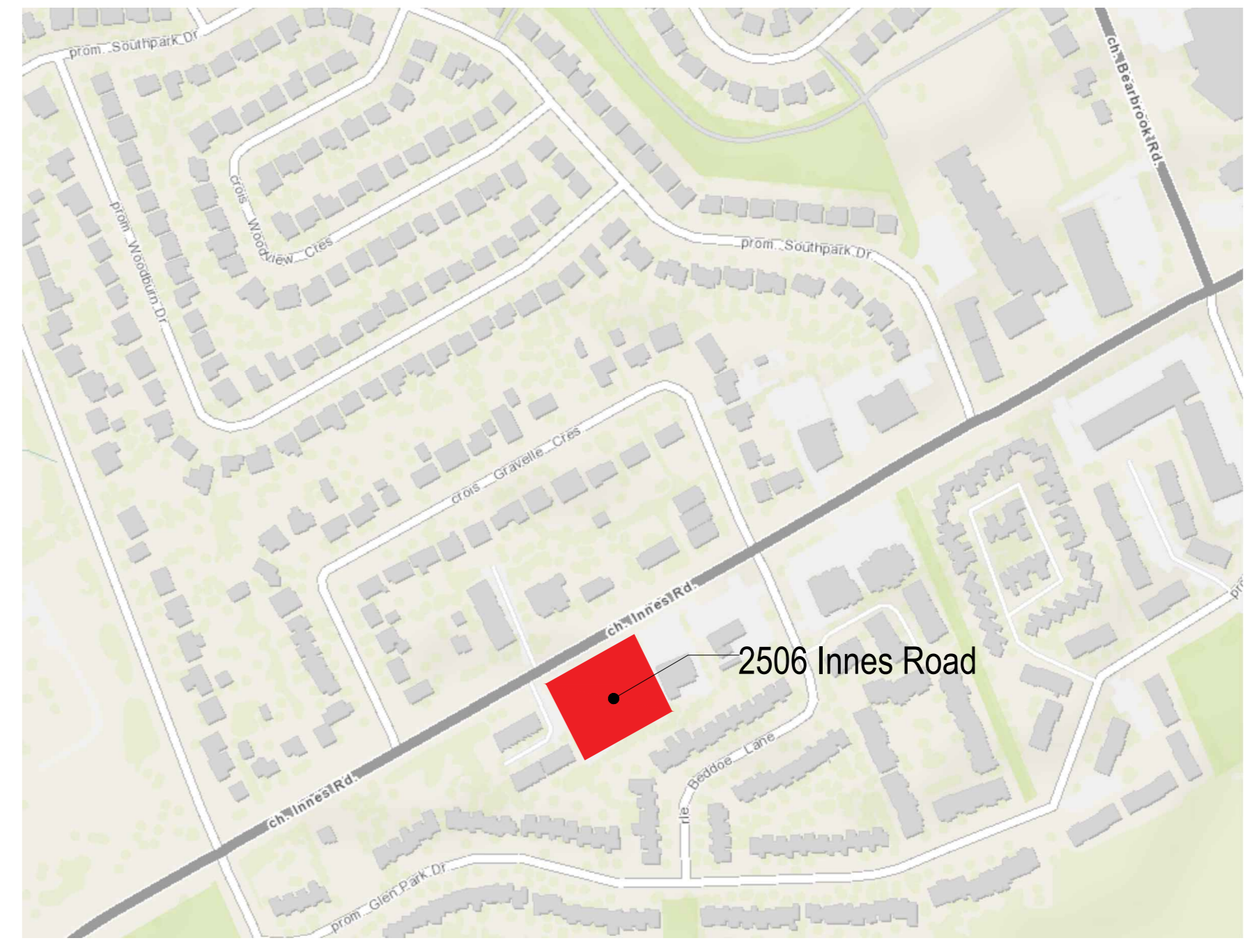
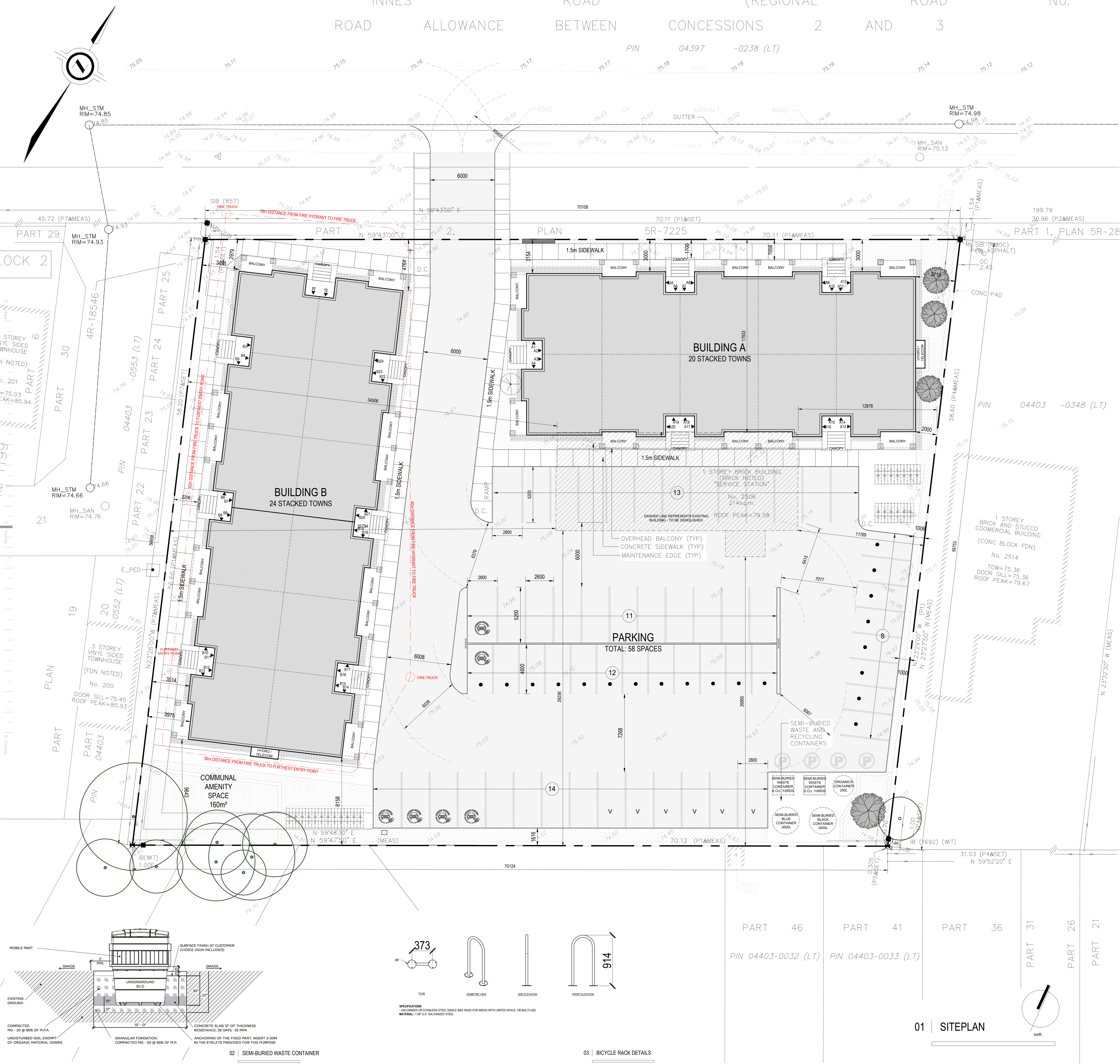
APPLICANT: CONCORDE PROPERTIES



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B	Re-Submitted for Site Plan Control	2024.05.16
A	SITE PLAN CONTROL	2023.12.22
Issue	Description	Date



Schematic Design NOT FOR CONSTRUCTION

APPLICANT: CONCORDE PROPERTIES

DEVELOPED FROM
SURVEYOR'S REAL PROPERTY REPORT
 PART OF LOT 15 CONCESSION 3 (OTTAWA FRONT)
 2506 INNES ROAD
 CITY OF OTTAWA

J.D. BARNES 2023

GRAPHIC SCALE FOR REFERENCE ONLY
 ALL MEASUREMENTS ARE METRIC (mm)

ZONING: AM11[2208] - Proposed Use: STACKED RESIDENTIAL DWELLINGS

ZONING MECHANISM	REQUIRED	PROVIDED	COMMENTS
A) MINIMUM LOT AREA		3948m ²	
B) MINIMUM LOT WIDTH		70.11m	along Innes Rd
C) MINIMUM LOT DEPTH		56.78m	
D) MINIMUM FRONT YARD SETBACK	3.0m	3.0m	
E) MINIMUM INTERIOR SIDE YARD SETBACK	ALONG WEST SIDE 3.0m	3.0m	
	ALONG EAST SIDE 3.0m	3.0m	
F) MINIMUM CORNER SETBACK	3.0m	N/A	
H) MAXIMUM BUILDING HEIGHT	varies	12.5m	refer to elevations and siteplan
J) VEHICLE PARKING	12 Spaces per d.u. - 0.2 per d.u. (50/m ²)	53 + 6 visitor	52 + 6 visitor
K) BICYCLE STORAGE	0.5 PER UNIT	22	25

PROPOSED SITE DEVELOPMENT INFO	COMMENTS
LOT AREA	3948m ²
BUILDING HEIGHT	12.5m
PARKING SPACES	58 (6 VISITOR)
NUMBER OF STOREYS	3
NUMBER OF UNITS	44
- TWO-BEDROOM UNITS	44
- THREE-BEDROOM UNITS	0
LOT COVERAGE	30.3%
SOFT LANDSCAPING COVERAGE	1125m ² (28.5%)
PARKING LOT	1625m ²
MINIMUM WIDTH OF PRIVATE WAY ENTRY	6m
SEPARATION BETWEEN BUILDINGS	11.2m
AMENITY SPACE PROVIDED	160m ²
- COMMUNAL	160m ²
- PRIVATE (BALCONIES)	290m ²

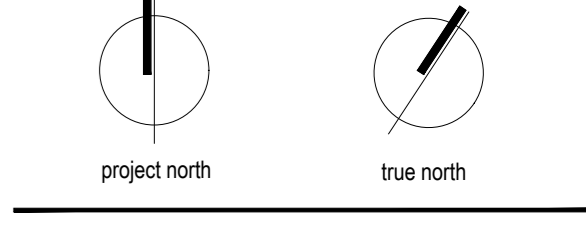
GARBAGE MANAGEMENT REQUIREMENT	REQUIRED	PROVIDED
GARBAGE	0.231 cubic yards per unit: 44x0.231=10.16	2- 6 cubic yard garbage semi-buried container
RECYCLING	for every 6 units provide 1 360L blue cart and 1 360L black cart	1-3000L Blue & 1-3000L Black semi-buried containers
ORGANICS	container for each unit and 1 communal 240L green container per 50 units	container for each unit and 1 communal 250L green container for each unit and 1 communal 250L green container for the winter

BUILDING AREAS (gross areas)	BUILDING A	BUILDING B	TOTAL
BASEMENT	524.5m ²	629.4m ²	1153.9m ²
GROUND FLOOR	524.5m ²	629.4m ²	1153.9m ²
SECOND FLOOR	547.5m ²	657.0m ²	1204.5m ²
THIRD FLOOR	547.5m ²	657.0m ²	1204.5m ²
TOTAL AREAS	2144m ²	2572.8m ²	4716.8m ²

- SITE PLAN LEGEND:**
- ▼ DENOTES UNIT ENTRY POINT
 - DENOTES PROPOSED FIRE HYDRANT LOCATION
 - DENOTES NO PARKING
 - DENOTES EV CHARGING SPACE
 - DENOTES SINGLE BICYCLE RACK
 - DENOTES VISITOR PARKING SPACE
 - DENOTES NUMBER OF PARKING SPACES WITHIN A ROW
 - ▨ REPRESENTS EXISTING BUILDING - TO BE DEMOLISHED
 - DENOTES PROPOSED LOCATION FOR COMMUNITY SIGN
 - D.C. DEPRESSED CURB



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Issue	description	date
H	Re-Submitted for Site Plan Control	2024.05.16
G	REVISED SITE DESIGN Option B	2024.04.25
F	REVISED SITE DESIGN Option B	2024.03.18
E	REVISED SITE DESIGN Option B	2024.02.19
D	SITE PLAN CONTROL	2023.12.22
C	REVISED SITE DESIGN LIMITING DISTANCE ADJUSTMENT	2023.11.13
B	REVISED SITE DESIGN LIMITING DISTANCE ADJUSTMENT	2023.11.13
A	PRELIMINARY SITE DESIGN	2023.11.02

2506 INNES ROAD
 Ottawa, ON

SITEPLAN

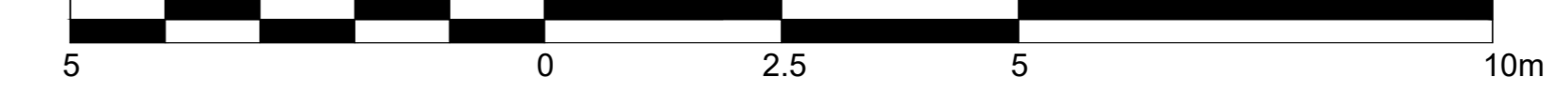
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 scale: 1:125
 date: 2023-10
 project number:
 drawing number:
A0.03 _REV H



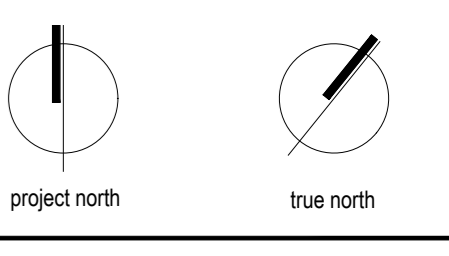
Schematic Design
NOT FOR CONSTRUCTION

APPLICANT: CONCORDE PROPERTIES

01 | NORTH ELEVATIONS - Innes Road



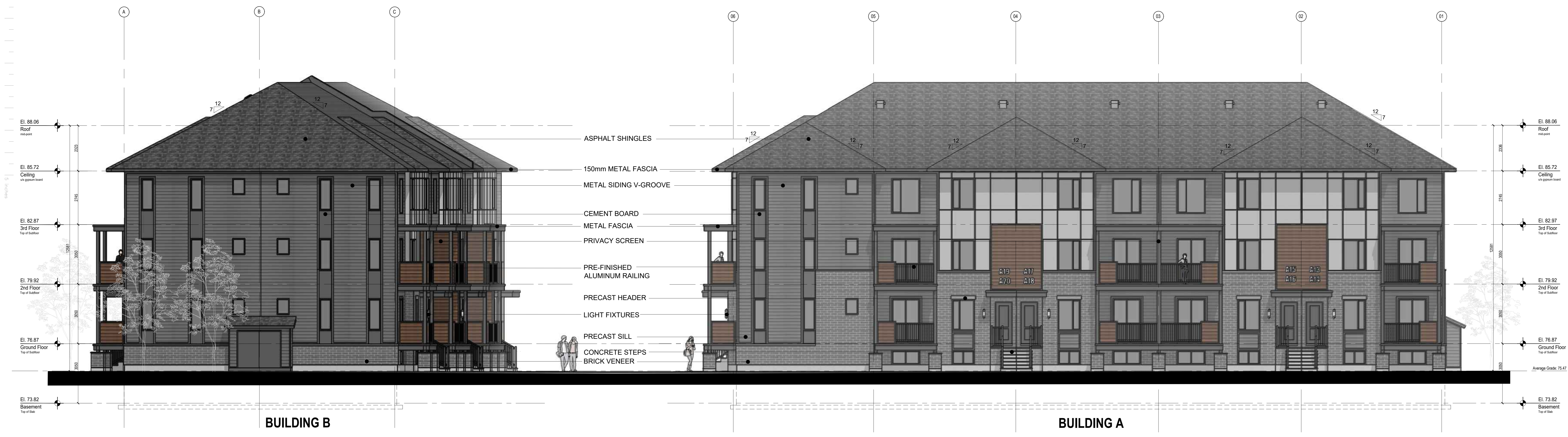
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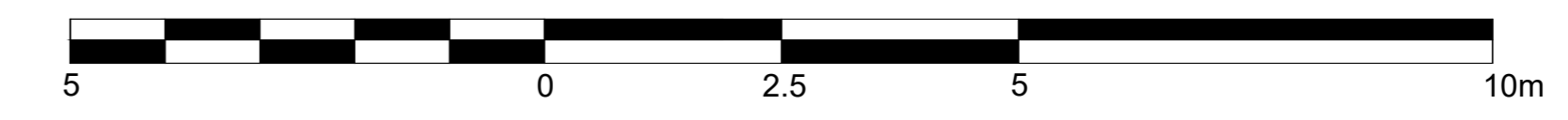
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B	Re-Submitted for Site Plan Control	2024.06.16
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2506 INNES ROAD
Ottawa, ON

ELEVATIONS



02 | SOUTH ELEVATION - Rear Yard / Parking



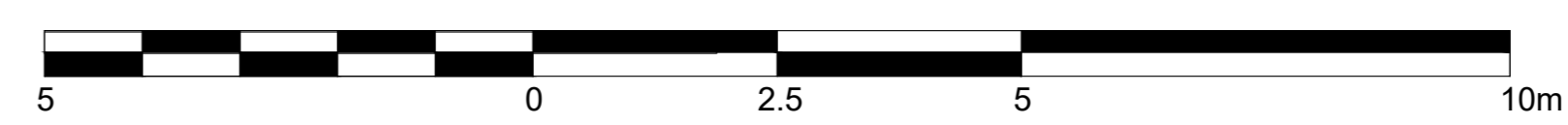
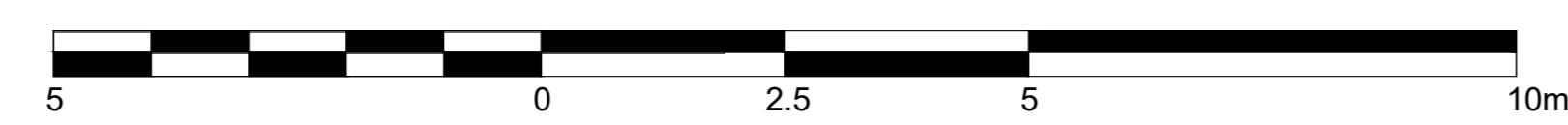
drawn by	P.A.
scale	1:75
date	2024-08
project number	
drawing number	

A2.01 _REV B

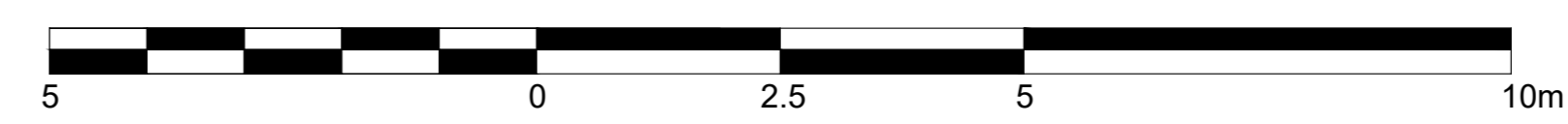


01 | WEST ELEVATION - Building A

02 | EAST ELEVATION - Building A



03 | WEST ELEVATION - Building B

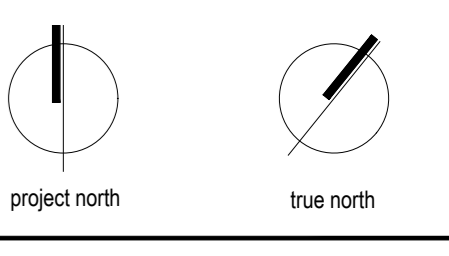


Schematic Design
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APPLICANT: CONCORDE PROPERTIES



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2506 INNES ROAD
Ottawa, ON

Drawing title

ELEVATIONS

drawn by	P.A.
scale	1:75
date	2024-08
project number	
drawing number	

A2.02 _REV B

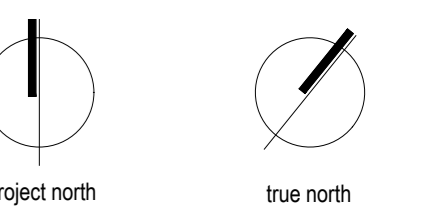
The project responds to its neighborhood by proposing a good quality development, in terms of functionality and architecture, that is highly compatible with the surrounding properties. Moreover, the overall orientation of the proposed buildings contributes to a distinctive and impactful presence along Innes Road, effectively framing the thoroughfare.



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01 | CONTEXTUAL VIEW

2506 INNES ROAD
Ottawa, ON

Drawing title

CONTEXTUAL VIEW

drawn by	P.A.
scale	1:75
date	2024-08
project number	-
drawing number	-

A4.01 _REV B



01 | VIEW ALONG INNES ROAD

Façade and roofline articulation

The frontage and roof design exhibit a resonance with the architectural patterns present in the surrounding neighborhood, echoing the features of adjacent residences. Nevertheless, a deliberate articulation of the facades is introduced through rhythmic compositions. This rhythmic interplay is achieved through the repetitive arrangement of architectural elements, fostering a sense of cohesion across all facets of the proposed structures. The relationship between the vertical planes is accentuated by the careful selection of materials for each volume. The lower sections are clad with brick veneer, not only visually grounding the building but also establishing a robust connection with the site. In contrast, the higher volumes are enveloped in lightweight siding and composite materials, contributing to a harmonious interplay of textures, and elevating the overall aesthetic appeal.

Colours and materials

The depicted rendered perspectives showcases a palette of superior quality materials, comprising brick, aluminum railings, concrete steps, and expansive windows. Embracing light and dark grey tones, these materials instill a pronounced sense of value to the overall aesthetic. Notably, the chosen materials align seamlessly with those employed in recent residential developments along Innes Road, underscoring a harmonious consistency with the prevailing architectural trends in the area.

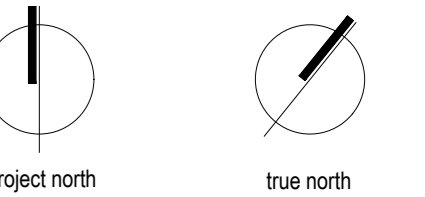


02 | REAR VIEW

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2506 INNES ROAD
Ottawa, ON

ILLUSTRATIONS

drawn by	P.A.
scale	NTS
date	2024-08
project number	-
drawing number	-



01 | SITE ACCESS VIEW

Architectural elements, including windows, doors and projections

Architectural projections incorporate balconies, closely resembling those of townhouse dwellings and residential buildings in the vicinity. Noteworthy are the significant amount of windows, characterized by darker-toned frames and railing systems, revealing a slightly richer quality to the facade, introducing a nuanced departure from neighboring structures. While the majority of building materials and elements align with those employed in nearby residential developments, their deployment showcases a subtle divergence in application, contributing to a distinctive and refined architectural expression.

Incorporating elements and details of common characteristics of the area

Typical features within the vicinity encompass elements such as the townhouse-style building form, hip rooflines, a blend of sash and larger windows, and the use of both brick and siding materials, all clad in light and dark grey tones. However, what distinguishes the proposed design is not only the presence of these common characteristics but also the innovative arrangement and interplay of each element. This distinctive configuration introduces a refreshing architectural expression, contributing a distinctive and modernized aesthetic to the neighborhood.

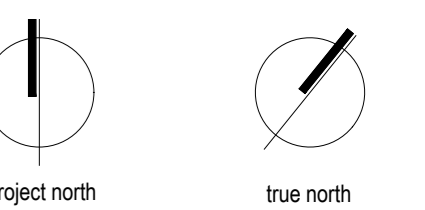


02 | PEDESTRAIN VIEW AT THE FRONT

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2506 INNES ROAD
Ottawa, ON

Drawing title

ILLUSTRATIONS

drawn by	P.A.
scale	NTS
date	2024-08
project number	-
drawing number	-