



## **1500 Merivale Road**

Planning Rationale Addendum  
Site Plan Control  
December 1, 2023



Prepared for Claridge Homes

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<b>1.0</b>	<b>Introduction</b>	<b>2</b>
1.1	Application History	2
1.2	Required Applications	2
1.3	Public Consultation Strategy	3
<b>2.0</b>	<b>Site Context and Surrounding Area</b>	<b>4</b>
2.1	Subject Property	4
2.2	Surrounding Context	4
2.3	Road Network	6
2.4	Neighbourhood Amenities	6
<b>3.0</b>	<b>Proposed Development and Design Brief</b>	<b>8</b>
3.1	Project Overview	8
3.2	Parkland Dedication	9
3.3	Integration within Overall Master Plan Concept	9
<b>4.0</b>	<b>Policy and Regulatory Review</b>	<b>11</b>
4.1	Provincial Policy Statement	11
4.2	City of Ottawa Official Plan (2022)	12
4.3	Comprehensive Zoning By-law 2008-250	14
<b>5.0</b>	<b>Supporting Studies</b>	<b>18</b>
5.1	Servicing & Stormwater Management Report	18
5.2	Phase I & II – Environmental Site Assessment (ESA)	18
5.3	Roadway Traffic Noise Assessment	18
5.4	Pedestrian Level Wind Study	18
5.5	Transportation Impact Assessment (TIA)	19
<b>6.0</b>	<b>Conclusion</b>	<b>20</b>

# 1.0 Introduction

Fotenn Planning + Design (“Fotenn”) has been retained by Claridge Homes (“Claridge”) to prepare this Planning Rationale Addendum in support of a Site Plan Control application to facilitate the proposed development on the property known as 1500 Merivale Road (municipally known as 1366 Baseline Road and 1490 Merivale Road) in the City of Ottawa.

This Planning Rationale Addendum will supplement the previous Planning Rationale as part of the existing Site Plan Control application on the site but will act as a stand-alone report in support of this proposal. The previous submission package for the Site Plan Control application encompassed the entire master plan area, including all 10 phases. **This letter will hone the focus of the application on Phase 1 of the proposal, with considerations for the greater development concept as a whole.** This Site Plan Control application should be considered as an independent request separate from the overall Master Plan.

## 1.1 Application History

The proposed development on the subject lands was the subject of an initial Site Plan Control application, submitted September 27<sup>th</sup>, 2021. The application and comprehensive plan featured the phased development of five (5) mid- to high-rise buildings between nine (9) and 11 storeys. The master plan was initially proposed to contain a total of 1,967 residential units, including a range of unit types from studio to three-bedroom units. Additionally, one (1) 129m<sup>2</sup> retail space was to be provided as part of the proposal. Parking on the site was to be accommodated in a consolidated two (2) level underground parking garage with approximately 2,008 vehicular spaces.

Following the initial submission, technical review comments were received from staff which focused commentary on the process, layout, overall design, and phasing strategy in order to help guide subsequent submissions. Following a review of the City’s comments, a response letter and resubmission package was submitted to the City of Ottawa on December 5<sup>th</sup>, 2022. The second submission began to focus the process on the development lands as part of the overall concept but placing greater emphasis on the site plan-specific details as it related to the developments on the Phase 1 lands. A schematic plan was provided as part of the comment response letter which illustrated the preliminary concept for the Phase 1 lands.

Revisions to the overall concept were also included as part of the resubmission package in response to City comments. The revisions included changes to the internal street network, such as the inclusion of a public thoroughfare connecting Merivale Road and Baseline Road through the site. Additional revisions related the parkland and POPS spaces as well as revisions to later-phase building design was included as part of the resubmission package. The resubmission additionally included an 11<sup>th</sup> Phase along the southern portion of the site, adjacent to the Phase 1 lands featuring a six (6) storey building (E). The most recent submission, dated March 17<sup>th</sup>, 2023, provided supporting plans and studies related to Transportation, Servicing, Stormwater Management, and Environmental Site Assessments, specifically relating to Phase 1 of the development.

## 1.2 Required Applications

To facilitate the proposed development for Phase 1, a Site Plan Control application is being submitted. This comprehensive Site Plan Control process will be initiated to address the detailed design of the site and buildings, including such aspects as site layout, site servicing, landscaping, and building materiality.

### 1.3 Public Consultation Strategy

The City of Ottawa has developed a Public Notification and Consultation Policy for development applications. The following consultation steps will be undertaken, or have been undertaken, in accordance with the Policy and Planning Act notification requirements.

- / Pre-Application Consultation Meeting
  - A Pre-Application Consultation Meeting was held with City Staff, a member of the local community association, and the applicant team on March 25, 2021.
  
- / Community Information Session #1
  - The Ward Councillor was notified of the proposed development for the subject site prior to the Site Plan Control application being submitted. A community information session was held by the Ward Councillor, Keith Egli, a member of the local community association, and the applicant team on April 27, 2021.
  
- / Community “Heads Up” to local registered Community Associations
  - A ‘heads up’ notification to local registered community associations will be completed by City of Ottawa during the application process.
  
- / Community Information Session #2
  - If requested by the Ward Councillor, the applicant team will participate in a community information and comment session to discuss the proposed development.
  - It is anticipated that the Ward Councillor would provide notice to residents via the ward website and newsletter, Facebook, and Twitter.
  
- / Urban Design Review Panel Consultation
  - Review of the proposed development will be undertaken by the Urban Design Review Panel upon the application being deemed complete.

# 2.0 Site Context and Surrounding Area

## 2.1 Subject Property

The subject property, located in the Knoxdale-Merivale Ward (Ward 9), is an irregular-shaped lot with a total area of approximately 62,066m<sup>2</sup> (6.2 ha). The property is located within the north western portion of Skyline-Fisher Heights neighbourhood, and was the former site of CTV Ottawa television studios, which was demolished by a fire in 2010. The majority of site has since remained vacant and contains sporadic paved areas and vegetation. An auto repair shop is located within the site boundaries at the north of the property along Baseline Road. The property contains frontage along three arterial streets; Merivale Road, Baseline Road and Clyde Avenue and can be accessed from all frontages, including where Kimway Crescent connects into the site from Merivale Road to the south.

The lands subject to the proposed development (the "Phase 1 lands" – although named as such, are separate from the Master Planning process on the subject property) are identified on Figure 1, being located at the western edge of the subject property at the corner of Merivale Road and Kimway Crescent. The Phase 1 lands have a lot frontage of approximately 48.3 metres along Merivale Road and a lot depth of approximately 61.5 metres, resulting in a lot area of approximately 3,562m<sup>2</sup>.



Figure 1: Boundaries of the subject property and Phase 1 lands.

## 2.2 Surrounding Context

*\* It should be noted that Section 2.2, 2.3 and 2.4 have been completed in reference to the larger master plan area.*

**North:** Along the southern edge of Baseline Road are stand alone commercial buildings containing restaurants on both sides of the auto repair shop. Immediately north across Baseline Road are commercial buildings containing commercial

and retail along the northern edge Baseline Road, and a large format retail store surrounded by surface parking. Additional commercial buildings containing offices can be found on the northern side of Baseline Road to the east of this. Further north is a residential neighbourhood which contains low-rise detached, semi-detached and townhome buildings, a condition which continues through to Clyde Woods Park.

**East:** Immediately east of the subject property is a large format commercial building containing retail uses, surrounded by surface parking. Stand-alone commercial buildings containing additional retail and commercial uses with surface parking are found further east between Baseline Road and Merivale Road. Further east are additional commercial buildings at the intersection of Merivale Road and Baseline Road, and a residential neighbourhood which contains low-rise detached, semi-detached, and townhouse buildings. North of Baseline Road are lands which contain the Central Experimental Farm, an agricultural facility, farmlands, research centre, and National Historic site operated by the research branch of Agriculture and Agri-Food Canada (AAFC).

**South:** Immediately south of the subject property are single and two-storey commercial and retail buildings along the southern and eastern edges of Merivale Road, behind which are low-rise residential buildings. Low-rise commercial, mid-rise office buildings and large format retail buildings with surrounding surface parking are located south along either side of Merivale Road through to Viewmount Drive. Residential neighbourhoods about the Merivale Road commercial strip, containing low-rise residential buildings

**West:** Immediately west of the subject property are low-rise commercial and retail buildings along Merivale Road, and a gas station at the southeast corner of Merivale Road and Baseline Road. Further west beyond the commercial buildings along Merivale Road is a residential neighbourhood which contains low-rise detached, semi-detached and townhome buildings, a condition which continues through to Algonquin College and large format buildings located along Baseline Road at Woodroffe Avenue.

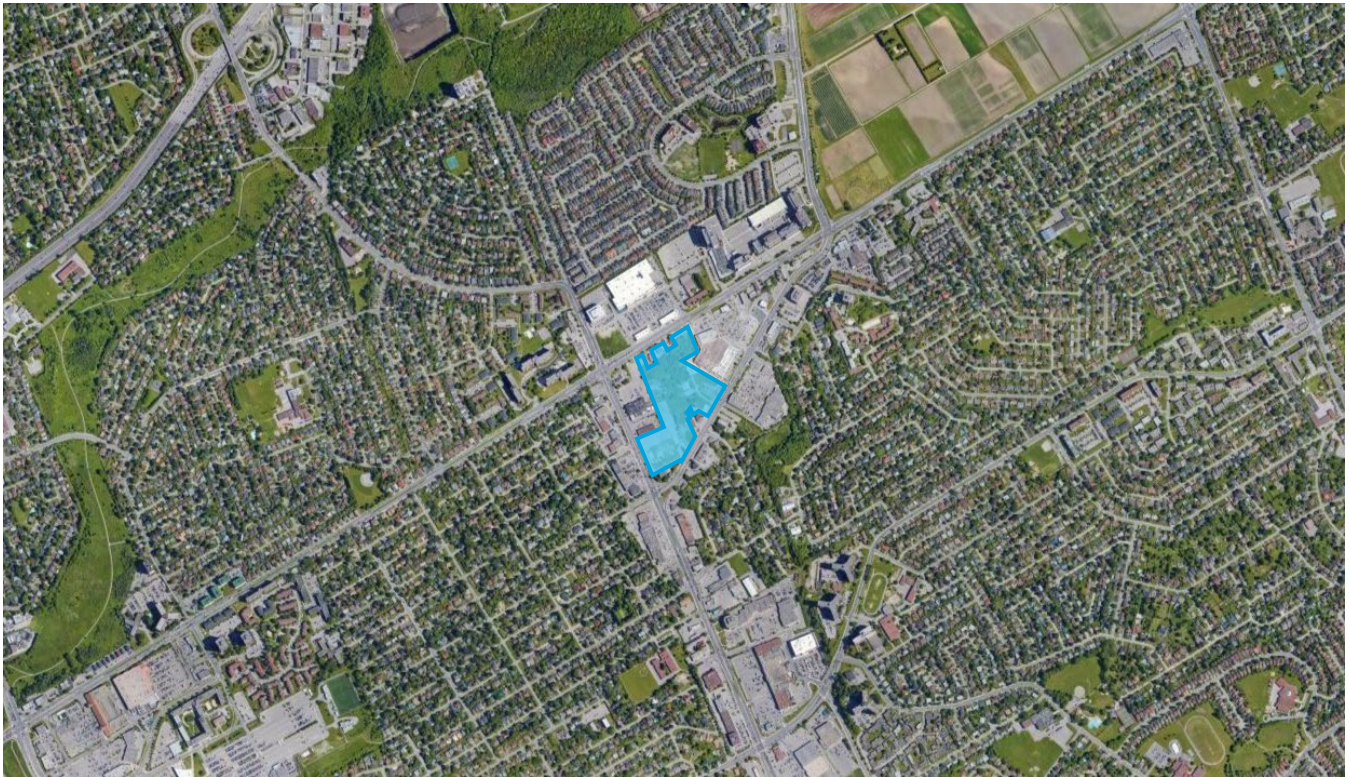


Figure 2: Surrounding context of 1500 Merivale Road

## 2.3 Road Network

The subject property is generally bounded between Baseline Road, Merivale Road, and Clyde Avenue, and contains frontage along each of these roads. Each of these roads are designated as an Arterial Road on Schedule C4 of the Official Plan. Arterial roads primarily function as the major corridors of the urban communities and are intended to accommodate multiple modes of transit including vehicles, pedestrians, bicycles, and public transportation. Arterial roads are designed to meet the specific needs of these users through the provision of, where appropriate, sidewalks, cycling lanes, and transit stops. Lotta Avenue, which is located where Merivale Road intersects with Clyde Avenue, is designated a Collector Road and is one of several Collector Roads near the site (Figure 3).

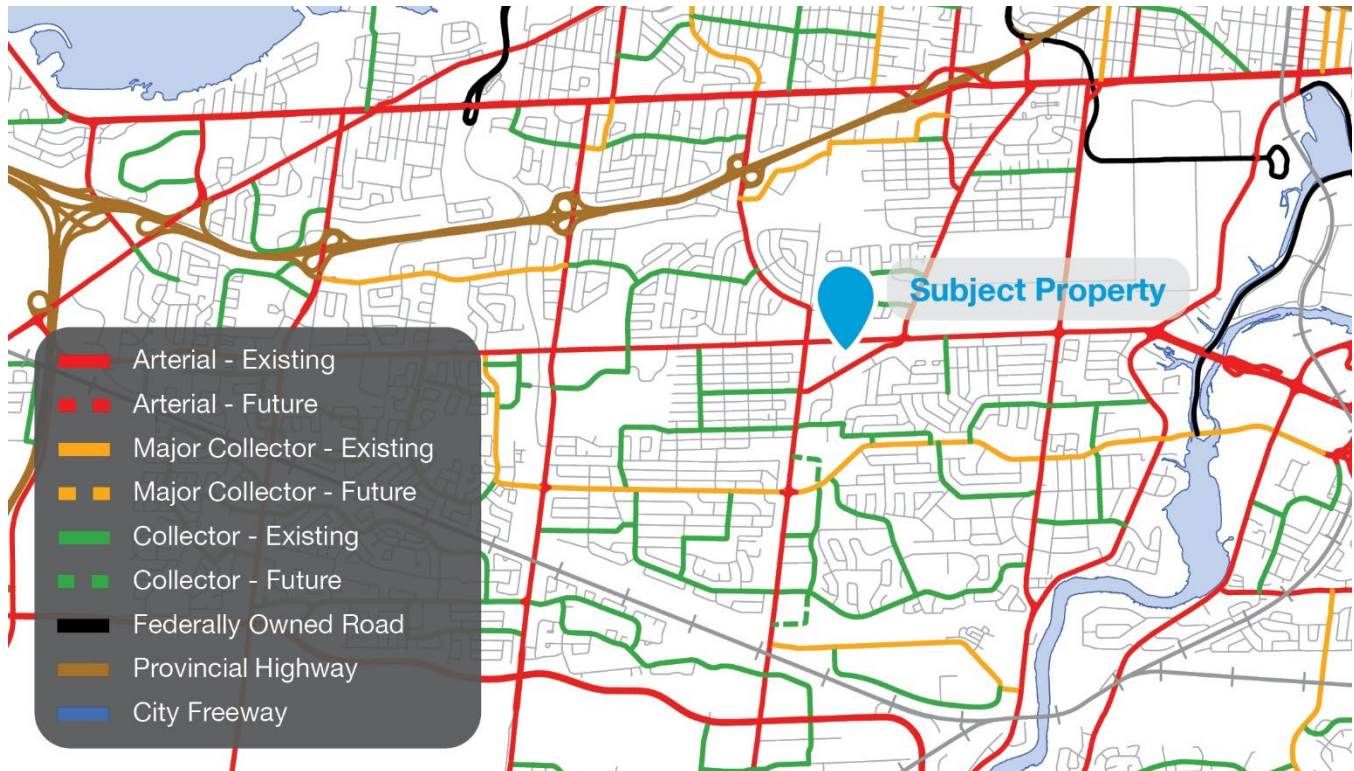


Figure 3: Schedule C4, *Urban Road Network* of the City of Ottawa Official Plan, subject property indicated.

## 2.4 Neighbourhood Amenities

The subject property enjoys close proximity to many neighbourhood amenities including a variety of commercial, retail, recreational, and institutional uses. The surrounding neighbourhood benefits from many nearby park spaces and is well-served with respect to community facilities and neighbourhood amenities.

A non-exhaustive list of nearby neighbourhood amenities including a wide range of uses is listed below (Figure 4):

- / Parks and public greenspaces, which contain public recreation facilities, including Gilby Park, Celebration Park, the Central Experimental Farm, Agincourt Park, Carlington Community Bike Park, Fisher Heights Park, Steve MacLean Park, and the General Burns Park. Note that this list does not include many nearby parks of a smaller scale which serve the local community;



- / Community centres, including the Fisher Heights Community Centre, Nepean Rideau Osgoode Community Resource Centre, Howard Darwin Centennial Arena, Alexander Community Centre, and the Carleton Heights Community Centre;
- / Commercial and retail services located along Merivale Road and Baseline Road, including RioCan Merivale Place, Meadowlands Mall, Merivale Mall, and the College Square Mall, and grocery stores including Loblaws, Walmart, Food Basics, and FreshCo Merivale & Meadowlands;
- / Libraries, including the Emerald Plaza Ottawa Public Library and the Canadian Agriculture Library;
- / Institutional uses and schools including Algonquin College, Sir Winston Churchill Public School, Agincourt Road Public School, Century Public School, Meadowlands Public School, Ottawa Islamic School, St Augustine School, Laurier-Carriere Catholic School and School Secondary Public Omer Deslauriers.



Figure 4: Nearby neighbourhood amenities within the context area of 1500 Merivale Road.

## Proposed Development and Design Brief

### 3.1 Project Overview

The entirety of the subject property is located on a large triangular block, known as the “Triangle Lands”, bounded by Merivale Road to the southeast, Baseline Road to the north and Clyde Avenue to the west. The master plan site is approximately 6.2 hectares in area and is highly irregular with discontinuous frontages on each of the three thoroughfares, which has greatly informed the planning and architectural approach to this project. The areas immediately adjacent to the site are not developed in a manner that creates any notable or relevant architectural or urban context. The Phase 1 lands are located at the south-eastern edge of the subject property along Merivale Road, south of the future public right-of-way extending through the lands (Figure 1).

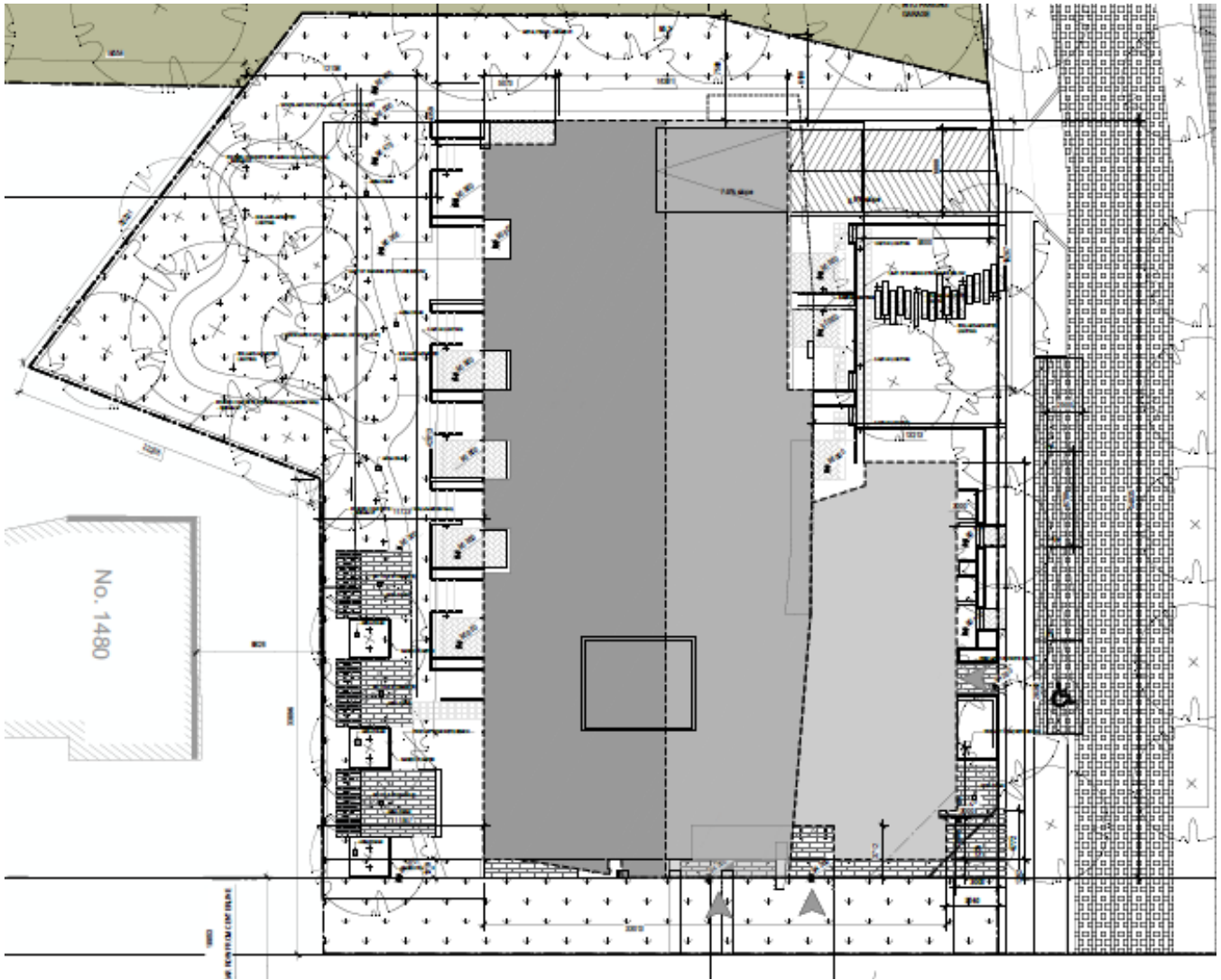


Figure 5: Proposed Site Plan for the Phase 1 lands.

The proposed development, to take place on the Phase 1 lands, is for a 10-storey mixed-use building with 121 residential units and retail space at grade (Figure 5). This high-rise development provides a range of residential units, ranging from studio units to ground-oriented townhomes integrated into the eastern frontage of the building, allowing for greater street

activation in conjunction with the ground-floor retail at the south-eastern corner of the building. The residential units are also provided with ample amenity space, through private balconies for some of the units, common amenity spaces across the building, and outdoor, landscaped amenity spaces at the rear of the building, adjacent to the planned parkland to the north.

The parking on site, for the purposes of the residential units, is located in a three (3) storey underground parking garage which provides 122 spaces – inclusive of visitor spaces. Given the building's proximity to the future Baseline Road BRT as well as the other transit connections in proximity of the lands, a parking rate of less than 1:1 is desirable and supported through the applicable policies. Additionally, 107 bicycle parking spaces are provided as part of the proposed development, 83 of which are located indoors across the three (3) levels of below-grade parking.

### 3.2 Parkland Dedication

During previous discussions on the master plan, it was determined that for the entirety of the Triangle Lands, 6,572m<sup>2</sup> of dedicated parkland was to be provided as part of the first phase of development. The Parkland Dedication By-law, as well as changes resultant from the Province of Ontario's Bil 23 (More Homes Built Faster), limit the maximum amount of parkland dedication at 10% of the gross developable area. As a result, the maximum land to be dedicated to parkland as part of the entire development is approximately 650m<sup>2</sup>. This parkland is proposed to be located directly to the north of the proposed development. While the current proposal has since become separate from the master planning process, the amount of parkland dedication required for the full build out of the project has not changed. Parkland will be dedicated to the City, who will be responsible for its development. At the time of Site Plan Approval, it is proposed that conditions be utilized in order to secure parkland dedication. These conditions would confirm, among other things, the timing of the dedication.

### 3.3 Integration within Overall Master Plan Concept

The Phase 1 lands are planned to be integrated within the overall concept for the Triangle Lands as the first parcel to be developed, independently. It is worth noting that although this proposal seeks to develop the lands independently from the Master Planning process of the overall concept, the development will continue to be integrated within the overall plan for the Triangle Lands as previously envisioned.

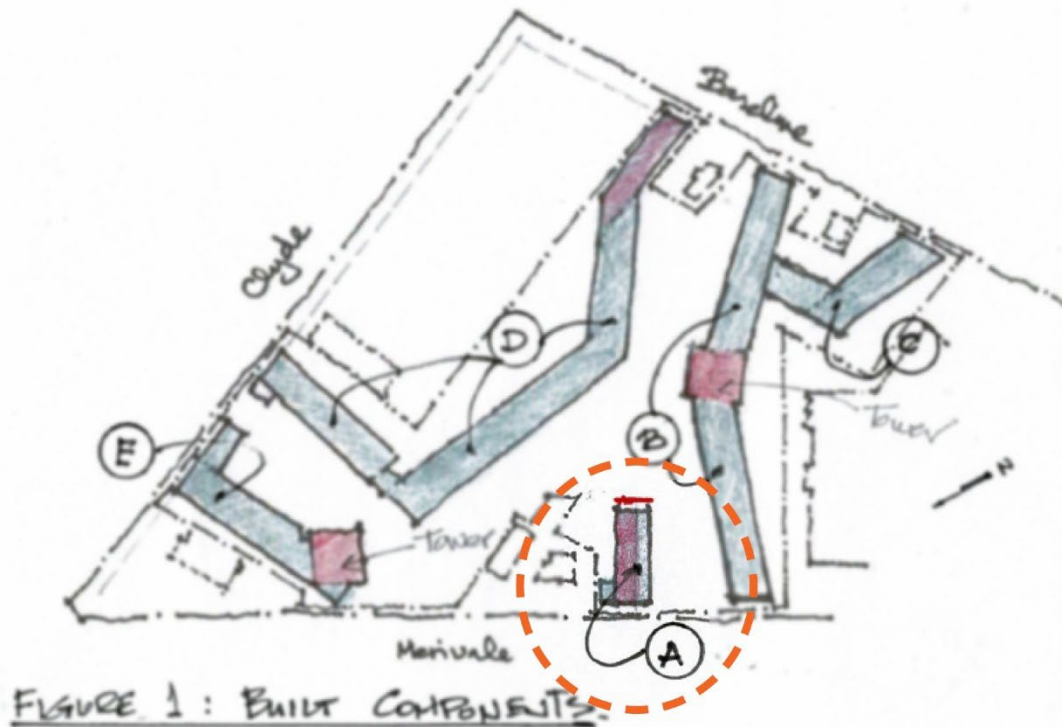


Figure 6: Built Components sketch provided as part of the previous Master Plan application on the lands, highlighting the portion of the lands to be developed as part of this application.

The proposed development is an outward-facing block of the master plan, acting as a gateway feature to the internal block structure on the remainder of the lands. The proposed development and intended independent parcel abut the proposed parkland to the north. The outdoor amenity space proposed through this application features a path network and array of landscaping features which run adjacent to and in tandem with the overall vision for the lands. Through both the landscaping, outdoor amenity space, and streetscape retail presented in this proposal, this development will contribute to the envisioned character for the Triangle Lands as presented in previous iterations of the Master Plan.

## Policy and Regulatory Review

### 4.1 Provincial Policy Statement

The Provincial Planning Statement (PPS) is a policy document issued under the Planning Act which provides direction on matters of provincial interest related to land use planning and development. All decisions on planning matters shall be consistent with this document. Generally, the PPS recognizes that “land use must be carefully managed to accommodate appropriate development to meet the full range of current and future needs, while achieving efficient development patterns”. As part of this response, a range of housing options is encouraged through new development and intensification.

The PPS contains policies which support the development and intensification of the subject property, including:

- / 1.1.1: Healthy, liveable and safe communities are sustained by:
  - o Promoting efficient development and land use patterns which sustain the financial well-being of the Province and municipalities over the long term (1.1.1.a);
  - o Accommodating an appropriate affordable and market-based range and mix of residential types, including multi-unit housing (1.1.1.b);
  - o Promoting the integration of land use planning, growth management, transit-supportive development, intensification and infrastructure planning to achieve cost-effective development patterns, optimization of transit investments, and standards to minimize land consumption and servicing costs (1.1.1.e);
  
- / 1.1.3: Settlement areas shall be the focus of growth and development, with land use patterns in settlement areas to be based on densities and a mix of land uses which efficiently use land and resources; are appropriate for, and efficiently use, the infrastructure and public service facilities which are planned or available, and avoid the need for their unjustified and/or uneconomical expansion (1.1.3.2).
  - a. Policy 1.1.3.6 states that new development taking place in designated growth areas should occur adjacent to the existing built-up area and shall have a compact form, mix of uses and densities that allow for the efficient use of land, infrastructure and public service facilities;
  
- / 1.4.3: Planning authorities shall provide for an appropriate range and mix of housing options and densities to meet projected market-based and affordable housing needs or current and future residents of the regional market area by:
  - a. Permitting and facilitating:
    - a. All housing options required to meet the social, health, economic and well-being requirements of current and future residents, including special needs requirements and needs arising from demographic changes and employment opportunities (1.4.3.b.1); and
    - b. All types of residential intensification, including second additional residential units, and redevelopment (1.4.3.b.2);
  - b. Promoting densities for new housing which efficiently use land, resources, infrastructure and public service facilities, and support the use of active transportation and transit in areas where it exists or is to be developed (1.4.3.d);
  
- / Section 1.6 of the PPS provides policies for infrastructure and public service facilities. Policies require that growth be directed in a manner that optimizes the use of existing infrastructure and public service facilities, including municipal sewage and water services;
  
- / 1.7.1: Long-term economic prosperity should be supported by:
  - a. Encouraging residential uses to respond to dynamic market-based needs and provide necessary housing supply and range of housing options for a diverse workforce (1.7.1.b).

- / 1.8.1: Planning authorities shall support energy conservation and efficiency, improved air quality, reduced greenhouse gas emissions, and preparing for the impacts of a changing climate through land use and development patterns which:
  - a. promote compact form and a structure of nodes and corridors (1.8.1.a);
  - b. promote the use of active transportation and transit in and between residential, employment (including commercial and industrial) and institutional uses and other areas (1.8.1.b); and,
  - c. encourage transit-supportive development and intensification to improve the mix of employment and housing uses to shorten commute journeys and decrease transportation congestion (1.8.1.e).

Policies for achieving the vision of the PPS address efficient development and land use patterns; accommodating an appropriate range and mix of residential types to meet long-term needs; promoting cost-effective development patterns; and supporting transit and active transportation. Furthermore, the policies direct development to locations that have been identified for intensification and redevelopment by the municipality.

**The proposed development is consistent with the Provincial Policy Statement, 2020. The underutilized subject property is located within the City of Ottawa Urban Area and abuts a Mainstreet defined as a Transit-Priority Corridor and is located in close proximity to the future Baseline Road Rapid Transit Corridor. Redevelopment of the subject property advances provincial goals of healthy, liveable and safe communities that efficiently utilizes existing infrastructure, improves the range and mix of housing types, and supports transit use.**

## 4.2 City of Ottawa Official Plan (2022)

The Official Plan for the City of Ottawa was approved November 4, 2022. The Plan provides a framework for the way that the City will develop until 2046 when it is expected that the City's population will surpass 1.4 million people. The Official Plan directs how the city will accommodate this growth over time and set out the policies to guide the development and growth of the City.

Schedule A divides the City into six (6) concentric policy areas called Transects. Each Transect represents a different gradation in the type and evolution of built environment and planned function of the lands within it, from most urban (the Downtown Core) to least urban (Rural).

The subject property is located within the Outer Urban Transect area as defined on Schedule A of the Official Plan (Figure 7). The Outer Urban Transect consists of neighbourhoods within the Greenbelt, built in the latter part of the twentieth century. The dominant urban form is that of the classic suburban model with the distinct separation of uses and car-oriented infrastructure. The Plan sets out strategies to transform these spaces into versatile areas capable of supporting multi-modal transportation. The policies of the Outer Urban Transect outline growth strategies for existing established neighbourhoods as well as areas with greater potential for intensification, including hubs and corridors.

- / 5.3.1(2) The Outer Urban Transect is generally characterized by low- to mid-density development. Development shall be:
  - b. Generally Mid- or High-rise along Mainstreets, except where the lot is too small to provide a suitable transition to abutting low-rise areas, in which case only low-rise development shall be permitted;
- / 5.3.1(3) b) Targeting Hubs and selected segments of Mainstreets for mid-density and mixed-use development to reinforce or establish an urban pattern as described in Table 6.
- / 5.3.1(4) In the Outer Urban Transect, the Zoning By-law shall provide for a range of dwelling unit sizes in:
  - a. Multi-unit dwellings in Hubs and on Corridors;
- / 5.3.3(1) Within Hubs, except where a secondary plan or area-specific policy specifies different heights, permitted building heights are as follows:
  - a. Up to 300 metre radius or 400 metres walking distance of an existing or planned rapid transit station, whichever is greatest, at least 3 storeys and up to High-rise;

- / 5.3.3(3) Along Mainstreets, permitted building heights are as follows, subject to appropriate height transitions, setbacks and angular planes:
  - a. On sites that front on segments of streets whose right-of-way (after widening requirements have been exercised) is 30 metres or greater as identified in Schedule C16 for the planned street context, and where the parcel is of sufficient size to allow for a transition in built form massing, not less than 2 storeys and up to High-rise.

Since this site is located along a Mainstreet Corridor, per Schedule B3, as well as located in a Hub, the policies strictly speaking to building heights and densities found in Section 5.3.3 prevail over those related to the Mainstreet Corridor designation.

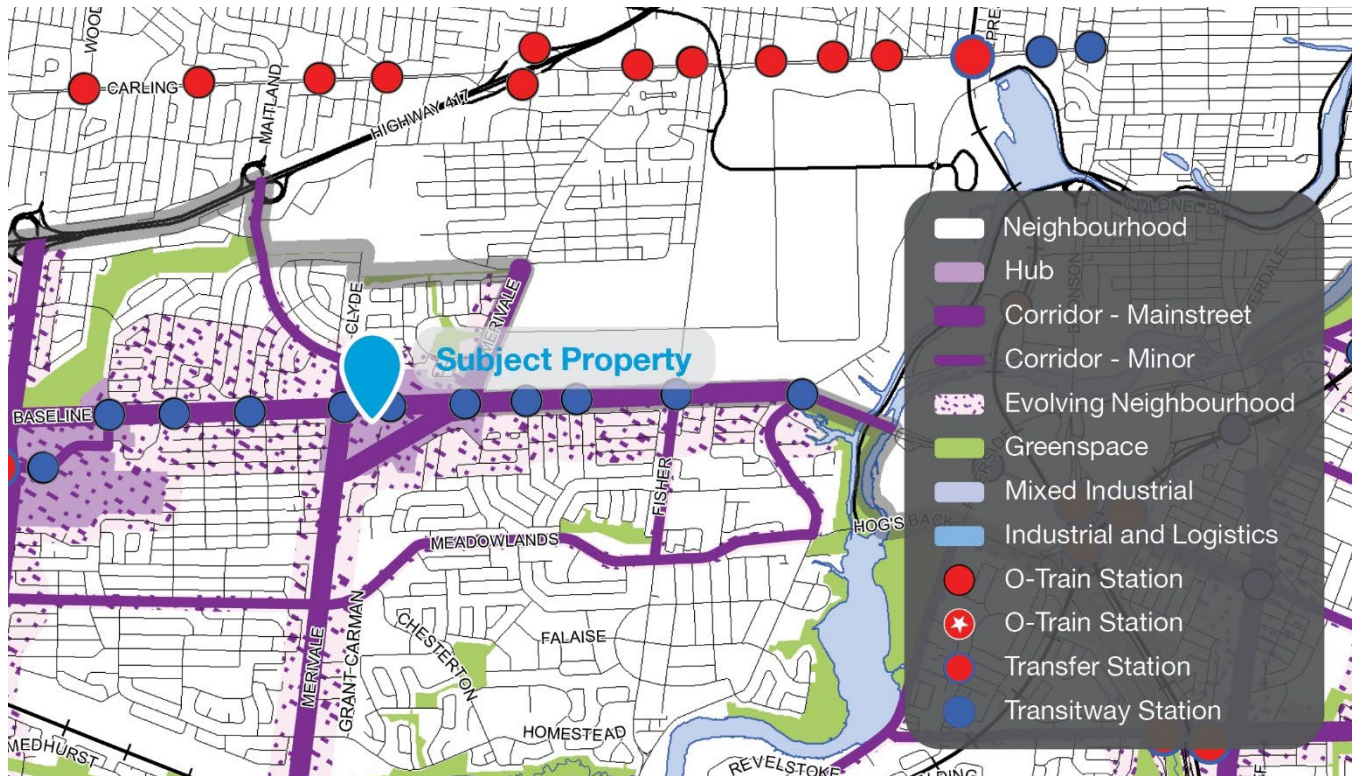


Figure 7: Schedule B3 - Outer Urban Transect (subject property identified)

The subject property is designated “Corridor – Mainstreet” on Schedule B3 of the Official Plan. The Corridor designation applies to bands of land along specified streets whose planned function combines a higher density of development, a greater degree of mixed uses and a higher level of street transit service than abutting Neighbourhoods. The property’s location within a Hub allows for greater heights and densities as prescribed through the policies of Section 6.1.1(6).

The applicable policies as they relate to the following development proposal are as follows:

- / 6.2.1(2) Development within the Corridor designation shall establish buildings that locate the maximum permitted building heights and highest densities close to the Corridor, subject to building setbacks where appropriate. Further, development:
  - a. Shall ensure appropriate transitions in height, use of land, site design and development character through the site, to where the Corridor designation meets abutting designations;

- / 6.2.1(3) Corridors will generally permit residential uses and such non-residential uses that integrate with a dense, mixed-use urban environment. The City may require through the Zoning By-law and/or development applications to amend the Zoning By-law:
  - a. Commercial and service uses on the ground floor of otherwise residential, office and institutional buildings with a strong emphasis on uses needed to contribute to 15-minute neighbourhoods;
  - b. Residential and/or office uses on the upper floors of otherwise commercial buildings; and/or
  - c. Minimum building heights in terms of number of storeys to ensure multi-storey structures where uses can be mixed vertically within the building.
- / 6.2.2(1) In the Mainstreet Corridor designation, this Plan shall permit a mix of uses including offices. These uses are permitted throughout the building, however the Zoning By-law may require active commercial or service uses on the ground floor, which include those that support cultural development in order to maintain, extend, or create a continuous stretch of active frontages along a Mainstreet.

The subject property is located in the Outer Urban Transect and is designated Mainstreet Corridor within a Hub in the City of Ottawa's Official Plan. The proposal seeks to develop a high-rise, residential tower with commercial uses at grade, conforming to the applicable Transect and Designation policies which seek to vertically mix uses and activate building frontages along Merivale Road. The relevant policies permit for the proposed development as it relates to height and density as well as the proposed unit mix, design features, and relationship to the existing context.

### 4.3 Comprehensive Zoning By-law 2008-250

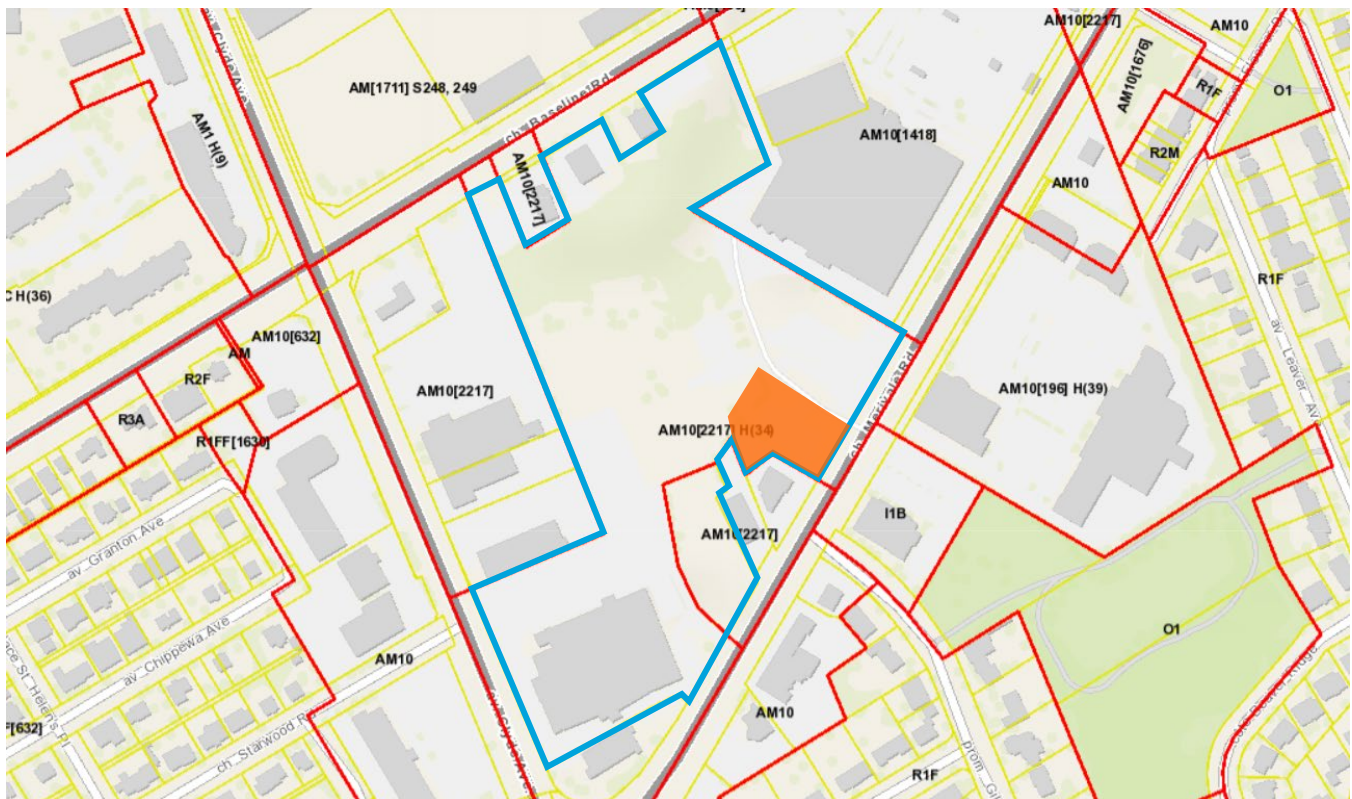


Figure 8: Zoning Map for 1500 Merivale Road, subject property indicated in blue, and the Phase 1 lands indicated in orange.

The master plan area is zoned Arterial Mainstreet, Subzone 10, Urban Exception 2217, Height Limit of 34 metres per the City of Ottawa's Comprehensive Zoning By-law (2008-250), except for a small area along Merivale Road that has a lesser height as shown in Figure 8. The Phase 1 lands subject to this Site Plan approval are zoned AM[2217] H(34).



Table 1 demonstrates some of the permitted uses on the subject property.

Table 1: Permitted Uses

Residential	Non-Residential
apartment dwelling low rise, <b>apartment dwelling</b> , mid rise and <b>high-rise</b> , bed and breakfast, dwelling units, group home, home-based business, home-based day care, retirement home, rooming house	amusement centre, amusement park, animal care establishment, animal hospital, artist studio, automobile dealership, automobile rental establishment, automobile service station, <b>bank</b> , bank machine, bar, broadcasting studio, car wash, catering establishment, cinema, click and collect facility, community centre, community health and resource centre, <b>convenience store</b> , day care, diplomatic mission, drive-through facility, emergency service, funeral home, gas bar, hotel, instructional facility, library, medical facility, municipal service centre, museum, nightclub, office, park, parking garage, payday loan establishment, personal brewing facility, personal service business, place of assembly, place of worship, post office, production studio, recreational and athletic facility, research and development centre, residential care facility, <b>restaurant</b> , <b>retail food store</b> , <b>retail store</b> , school, service and repair shop, sports arena, storefront industry, technology industry, theatre, training center, urban agriculture

As it is the intention to sever the Phase 1 lands from the subject property prior to obtaining full building permit, Table 2 describes the applicable Zoning By-law provisions as they relate strictly to the Phase 1 lands. It is worth noting that Phase 1 is not currently in compliance with the provisions related to the minimum front and corner side yard setbacks. Approval of the future Consent application will eliminate the additional frontages along Baseline Road and Clyde Road thus resolving the compliance issue. It is understood that this Site Plan Control approval would be conditional on the severance or subdivision of these lands.

Table 2: Zoning Requirements for AM10(2217) H(34), and Compliance for 1500 Merivale Road Phase 1

Provision	Required	Provided	Compliance
<b>Minimum Lot Width (m)</b>	No minimum	Varies	Yes
<b>Minimum Lot Area (m<sup>2</sup>)</b>	No minimum	3,562m <sup>2</sup>	Yes
<b>Minimum Front<sup>1</sup> and Corner Side Yard Setback (m)</b>	<b>0 metres</b> , at least 50% of the frontage along the front lot line and corner side lot line must be occupied by building walls located within 3 metres of the frontage for a mixed-use building	0 metres 60% of frontage within 4.5 metres of Merivale Road front lot line	<b>No</b> <b>*see above</b>
<b>Minimum Interior Side Yard Setback (m)</b>	No minimum	Varies	Yes
<b>Minimum Rear Yard Setback</b>	3.0 metres for any building wall within 20 metres of a lot line abutting a public street;  7.5 metres in all other cases	Within 20 metres of public street: 6.1 to 7.5 metres  Beyond 20 metres: 7.5 metres	Yes
<b>Minimum Building Height (m)</b>	If located within 10 metres of the front or corner lot lines: A mixed-use building	Ground floor height: 4.5m	Yes

<sup>1</sup> The lot line abutting the designated "Arterial Mainstreet", as per Schedule B of the Official Plan, is the front lot line (Section 186, 10 a))

	must have a minimum ground floor height of 4.5 metres The minimum building height required is 7.5 metres (at least 2-storeys)	Building Height within 10 metres of Front and Corner Lot Lines: 7.69m	Yes
<b>Maximum Building Height (m)</b>	34 metres	31.49m	Yes
<b>Maximum Floor Space Index</b>	No Maximum	2.39	Yes
<b>Transparent glazing</b>	A minimum of 50% of the surface area of the ground floor façade, measured from the average grade up to a height of 4.5 metres, facing a public street must be comprised of transparent glazing	59% ground floor façade fenestration	Yes
<b>Active entrances</b>	The ground floor façade facing a public street of a building located within 4.5 metres of the front lot line or corner side lot line must include: (i) a minimum of one active entrance from each individual occupancy located immediately adjacent to the front lot line or corner side lot line in the case of non-residential uses; and (ii) a minimum of one active entrance in the case of a residential use building;	Active entrances for both residential and non-residential uses are provided along the front and corner side lot lines.	Yes
<b>Amenity Space Requirements</b>	6m <sup>2</sup> per unit, 50% required to be communal space. <i>121 units x 6.0 = 726m<sup>2</sup> amenity space to be provided, 363m<sup>2</sup> to be communal</i>  <b>Exception 2117:</b> 2% must be provided as outdoor communal amenity space located at-grade.  / <b>Lot size:</b> 2,990.75m <sup>2</sup> x 0.02 = <b>59.82m<sup>2</sup></b>	Total: 2,990.75m <sup>2</sup> of amenity space provided, 1,535.2m <sup>2</sup> is communal  663.79m <sup>2</sup> outdoor communal outdoor space located at-grade	Yes  Yes

Table 3 provides a summary of the parking requirements as detailed in Zoning By-law 2008-250 and how the proposed development meets the provisions.

Table 3: Parking Provisions and Compliance

Provision	Required	Provided	Compliance
<b>Minimum Parking Required (Area X of Schedule 1A)</b>	<p>Section 101.5.d: where a residential use building has an active entrance located within 600 metres of a rapid-transit station shown on Schedule 2A, the minimum parking required by Table 101 for the residential use is calculated using the rates for Area X.</p> <p>The entirety of the subject site is in proximity of Baseline Road, which has multiple rapid transit stations within 600m, therefore the following rates apply:</p> <p>0.5 per dwelling unit, less the first 12 units. (55 spaces required)</p> <p>Where a non-residential use located partly or entirely on the ground floor has a gross floor area of 200 square metres or less, no off-street motor vehicle parking is required to be provided.</p>	111 spaces total resident spaces provided	Yes
<b>Maximum Vehicle Parking</b>	<p>1.75 spaces / dwelling unit  <math>1.75 \times 121 \text{ dwelling units} = 212</math></p>	122 total vehicle spaces provided	Yes
<b>Visitor Parking</b>	<p>Visitor Parking (Area X)  0.1 spaces/dwelling unit, less the first 12 units, to a maximum of 30 spaces per building</p> <p><math>109 \times 0.1 = 11 \text{ spaces}</math></p>	11 visitor spaces total	Yes
<b>Bicycle Parking</b>	<p>0.5 spaces / dwelling unit</p> <p><math>121 \text{ units} \times 0.5 \text{ bicycle spaces} = 61 \text{ bicycle spaces}</math></p> <p>1 per 250m<sup>2</sup> Commercial GFA</p> <p>Total spaces required: 1</p> <p>62 spaces required</p>	107 bicycle spaces provided	Yes

## 5.0 Supporting Studies

Plans and reports to support the Master Plan have been prepared and previously submitted in support of the application. They have been appropriately updated reflecting the Phase 1 proposed development and this Planning Rationale Addendum should be read in conjunction with them considering any recommendations and findings.

### 5.1 Servicing & Stormwater Management Report

Novatech was retained to complete a Servicing & Stormwater Management Report for the proposed development. The report, dated October 27, 2023, reviewed the proposed development as it relates to the servicing requirements and capacity as well as the implications for stormwater management. The proposed 200mm watermain under Street 1 (as indicated in the report) to be connected to the existing Merivale Road right-of-way watermain, is found to have adequate capacity to support the proposed development as well as the necessary fire protection services. The existing sanitary sewer connections are found to have adequate capacity to support the proposed development as well. In terms of stormwater management, existing storm sewers in the Merivale right-of-way as well as the on-site controls will provide for adequate stormwater flow and management across the proposed development.

### 5.2 Phase I & II – Environmental Site Assessment (ESA)

Paterson Group was retained to complete an Environmental Site Assessment (ESA) on the lands proposed for development. Following the findings of the Phase I report, dated November 1, 2022, a Phase II was deemed necessary due to the site history and existing environmental site conditions. The Phase II report, dated January 28, 2021, was prepared to address the potentially contaminating activities that were identified during the Phase I study. A total of 7 soil samples and 16 groundwater samples were collected and submitted for laboratory analysis. The analysis identified several occurrences of PAH parameters exceeding the MECP Table 7 residential standards as well as several VOC parameters at concentrations exceeding the applicable MECP standards. The report provides recommendations for remediation measures which includes off-site soil remediation, additional groundwater testing, and the decommissioning of potentially unused or at-risk monitoring wells.

### 5.3 Roadway Traffic Noise Assessment

GradientWind Engineers & Scientists was retained to complete a roadway traffic noise assessment in support of the proposed development. The assessment, dated April 5, 2023, was based on a theoretical noise prediction method which conforms to the MECP and City of Ottawa requirements, noise level criteria as specified by the City of Ottawa, future vehicular traffic volumes, and the architectural drawings provided by EVOQ Architecture in October 2022. The results from the analysis indicated that anticipated noise levels range between 55 and 73 dBA during daytime hours and between 61 to 65 dBA during the nighttime period. Noise levels at the rooftop amenity terrace are expected to not exceed 55 dBA during the daytime period, therefore, no acoustic mitigation is required. Building components with a higher Sound Transmission Class (STC) rating will be required where exterior noise levels exceed 65 dBA, including the southeast façade which is nearest and most exposed to Merivale Road. Results of the calculations also indicate that the development will require central air conditioning, which will allow occupants to keep windows closed and maintain a comfortable living environment. A Type D Warning Clause will also be required in all Lease, Purchase and Sale Agreements, as summarized in Section 6 of the report.

### 5.4 Pedestrian Level Wind Study

GradientWind Engineers & Scientists was retained to prepare a Pedestrian Level Wind Study, dated June 3, 2021. This study was completed in support of the previous Zoning By-law Amendment application on the subject property, based on the architectural set provided as of June 2021. The Study outlined the following findings and recommendations:

- / All grade-level areas within and surrounding the subject site are predicted to be acceptable for the intended pedestrian uses throughout the year. Specifically, wind conditions over the surrounding sidewalks, building access points, transit stops, and nearby parking lots are considered acceptable for the intended pedestrian uses throughout the year. One exception to this finding is over the proposed public park, in which wind conditions are predicted to be suitable for a mix of sitting and sanding during the typical use period, defined as May to October.
- / The rooftop amenity terraces serving Phases 2, 5, 6, and 7 are predicted to experience conditions suitable for the intended uses without mitigation.
- / To extend sitting conditions over the full amenity terrace serving the proposed development, it is recommended that 1.5-m-tall barriers, typically glazed, be installed around the perimeter of the terrace.
- / Within the context of typical weather patterns, which exclude anomalous localized storm events such as tornadoes and downbursts, no pedestrian areas within and surrounding the subject site were found to experience conditions that could be considered dangerous.

As part of the recent updates to the architectural set, an addendum to the original study was prepared, dated May 3, 2023. The conclusions and recommendations regarding wind conditions that were provided as part of the original report remain representative of the current proposed development.

## 5.5 Transportation Impact Assessment (TIA)

Novatech was retained to complete a Transportation Impact Assessment (TIA) in support of the proposed development. The TIA, dated October 27, 2023, reviewed the anticipated traffic generation, existing roadway and infrastructure capacities as well as the design of on-site and off-site traffic controls. The proposed development is anticipated to generate 16 vehicle trips during the AM peak hour and 20 vehicle trips during the PM peak hour. Additionally, the Transportation Demand Measures (TDM) required by the City of Ottawa are met through the inclusion of pick-up/drop-off space, adequate indoor and outdoor bicycle parking, and other design and infrastructure features. Based on the provided analysis, the proposed development is recommended from a transportation perspective.

## 6.0 Conclusion

It is our professional opinion that the proposed development of the Phase 1 lands constitutes good planning and is in the public interest.

Sincerely,



Lisa Dalla Rosa, MCIP RPP  
Associate - Planning



Evan Saunders, M.PL  
Planner