

GENERAL CONTRACTOR SHALL REFER TO CHICK-FIL-A PARKING LOT STRIPING SPECIFICATIONS, SEE DETAIL 2. CONTRACTOR SHALL USE WHITE REFLECTIVE PAINT ON ASPHALT & YELLOW REFLECTIVE PAINT ON CONCRETE

MULTI-LANE SPLIT MULTI-LANE MERGE

NOTES:

1. GENERAL CONTRACTOR SHALL REFER TO CHICK-FIL-A PARKING LOT STRIPING SPECIFICATIONS, SEE DETAIL 2. CONTRACTOR SHALL USE WHITE REFLECTIVE PAINT ON ASPHALT & YELLOW REFLECTIVE PAINT ON CONCRETE.



GENERAL CONTRACTOR SHALL REFER TO CHICK-FIL-A PARKING LOT STRIPING SPECIFICATIONS, SEE DETAIL 2. CONTRACTOR SHALL USE WHITE REFLECTIVE PAINT ON ASPHALT & YELLOW REFLECTIVE PAINT ON CONCRETE.

PARKING LOT STRIPING SPECIFICATIONS:

1. ALWAYS FOLLOW ALL APPLICABLE GOVERNING AUTHORITY'S STANDARDS. SURFACES SHOULD BE CLEAN, DRY, AND FREE FROM LOOSE OR PEELING PAINT. REMOVE ALL OIL, DUST, GREASE, DIRT, AND OTHER FOREIGN MATERIAL TO ENSURE ADEQUATE ADHESION. DO NOT APPLY WHEN AIR OR SURFACE

TEMPERATURES ARE BELOW 40°F. 3. APPLY SHERWIN-WILLIAMS SETFAST PREMIUM ALKYD ZONE MARKING PAINT A300 WHITE OR A303 YELLOW USING EITHER AIRLESS OR CONVENTIONAL LINE STRIPING EQUIPMENT. USE THE FOLLOWING SETTINGS AS A GUIDE-ACTUAL SETTINGS DEPEND ON ATMOSPHERIC CONDITIONS AT THE TIME OF APPLICATION:

HOSETIP FILTER

REDUCTION

1800-2700 PSI 1/4"-8" ID 0.015"-0.017" ONLY IF NECESSARY, UP TO 1PT/GAL

VM&P NAPTHA R1K3 BLINKS 21 (BLEEDER) OR EQUIVALENT

 FLUID NOZZLE AIR NOZZLE INTERNAL MIX, #709 ATOMIZATION PRESSURE FLUID PRESSURE REDUCTION

45-80 PSI 40-70 PSI ONLY IF NECESSARY, UP TO 1PT/GAL VM&P NAPTHA R1K3

 SHERWIN WILLIAMS, H&C SHARK GRIP SLIP RESISTANT ADDITIVE TO BE MEASURED. AND ADDED TO ALL PAINT PER MANUFACTURER'S WRITTEN SPECIFICATIONS. MIX THOROUGHLY PER MANUFACTURER'S RECOMMENDATIONS SO THAT NO CLUMPING IS APPARENT AND UNTIL EVEN DISTRIBUTION IS ACHIEVED. MAINTAIN EVEN DISTRIBUTION OF ADDITIVE IN PAINT THROUGHOUT THE APPLICATION PROCESS. MIX PAINT THOROUGHLY BY BOXING, STIRRING, OR POWER AGITATION BEFORE USE. APPLY AT 15 MILS WET TO ACHIEVE A SPREAD RATE OF 400-500 LINEAL FEET OF STANDARD 100mm STRIPE PER GALLON. APPLIED AT THIS RATE AT 70 DEGREES F AND 50% RELATIVE HUMIDITY, PAINT WILL DRY WITH NO TRAFFIC PICKUP AFTER

GENERAL CONTRACTOR TO RE-STRIPE THE LOT 45 DAYS AFTER OPENING.

REFER TO PARKING LOT STRIPING SPECIFICATION . CROSSWALK ALONG AN ACCESSIBLE ROUTE SHALL MAINTAIN A CROSS SLOPE OF 1.5% MAX AND A RUNNING SLOPE OF 5% MAX.

4 CROSSWALK DETAIL

M-GREASE

FRAMING:

-CEDAR

FRAMING

-ORANGE -CEDAR BEIGE

-GREEN

-BLUE -GREY

-BLACK

-SILVER

-BLACK

ALUMINUM FRAMING

-MAHOGANY RECYCLED PLASTIC

CAPACITY: 800 L / 1.0 YD 3

EON PLASTIC FRAMING

MOLOK WASTE MANAGEMENTS SYSTEMS

7 MOLOK DETAIL (GREASE) 1:50

A102 N.T.S.

TURNER FLEISCHER

> 67 Lesmill Road Toronto, ON, M3B 2T8 T 416 425 2222 turnerfleischer.com

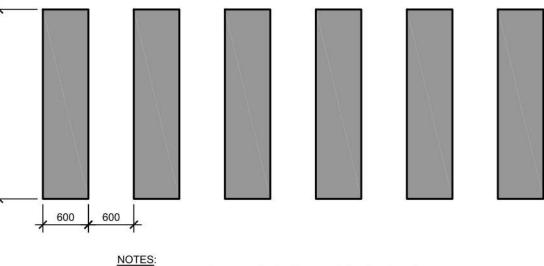
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ALLISON HAMLIN

MANAGER (A), DEVELOPMENT REVIEW WEST PLANNING, REAL ESTATE & ECONOMIC DEVELOPMENT DEPARTMENT, CITY OF OTTAWA

APPROVED

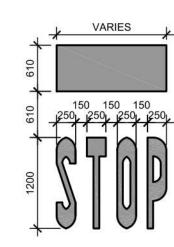
By Allison Hamlin at 1:54 pm, Sep 19, 2023



DIRECTIONAL ARROWS A102 N.T.S.

2 MULTI-LANE DIRECTIONAL ARROWS A102 N.T.S.

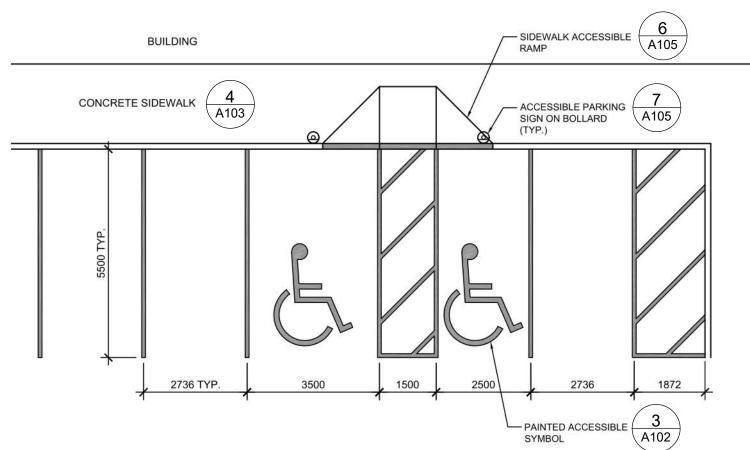
3 ACCESSIBLE SYMBOL A102 N.T.S.



REQUIRED.

GENERAL CONTRACTOR SHALL REFER TO CHICK-FIL-A PARKING LOT STRIPING SPECIFICATIONS, SEE DETAIL CONTRACTOR SHALL USE WHITE REFLECTIVE PAINT ON ASPHALT & YELLOW REFLECTIVE PAINT ON CONCRETE.

. IF STOP SIGN IS PROPOSED, STOP LINE GRAPHIC NOT



1. ACCESSIBLE PARKING AND ACCESSIBLE AISLES SHALL NOT EXCEED 1.5% IN SLOPE IN ANY DIRECTION. IF ONLY ONE ACCESS AISLE IS INSTALLED, IT IS TO BE A VAN SIZE.

PARKING STALL DIMENSIONING SHALL BE IN ACCORDANCE WITH APPLICABLE GOVERNING AUTHORITY'S AND ADA STANDARDS AND IF DIFFERENT THAN THIS DETAIL SHALL BE THE DIMENSIONING SHOWN ON THE SITE LAYOUT PLAN.

GENERAL CONTRACTOR SHALL REFER TO CHICK-FIL-A PARKING LOT STRIPING SPECIFICATIONS.

4. CONTRACTOR SHALL USE 100mm WIDE WHITE REFLECTIVE PAINT FOR STRIPING ON ASPHALT PARKING LOTS.

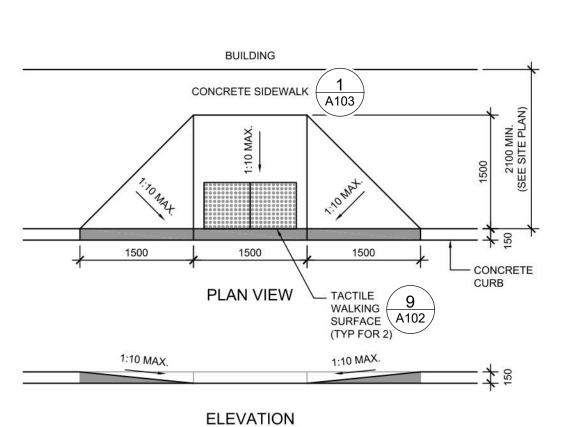
5. CONTRACTOR SHALL USE 100mm WIDE YELLOW REFLECTIVE PAINT FOR STRIPING ON CONCRETE PARKING LOTS.

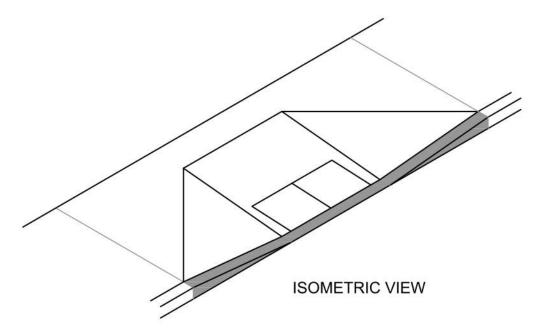
NO WHEEL STOPS TO BE INSTALLED WHEN PARKING IS ADJACENT TO SIDEWALK.

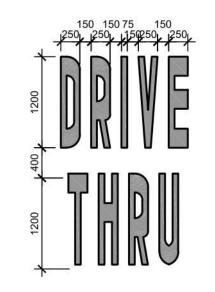
7. ACCCESSIBLE SIGNS IN BOLLARDS AND BOLLARDS SHALL BE INSTALLED WHEN PARKING IS ADJACENT TO FLUSH CURB OR A



6 STANDARD PARKING LAYOUT A102 N.T.S.







GENERAL CONTRACTOR SHALL REFER TO CHICK-FIL-A PARKING LOT STRIPING SPECIFICATIONS, SEE DETAIL 2. CONTRACTOR SHALL USE WHITE REFLECTIVE PAINT ON

ASPHALT & YELLOW REFLECTIVE PAINT ON CONCRETE

TACTILE WALKING SURFACE TO COMPLY WITH O.B.C. 3.8.3.18
AND OPSD 310.039 - "CONCRETE SIDEWALK RAMPS TACTILE WALKING SURFACE INDICATOR COMPONENTS"

610



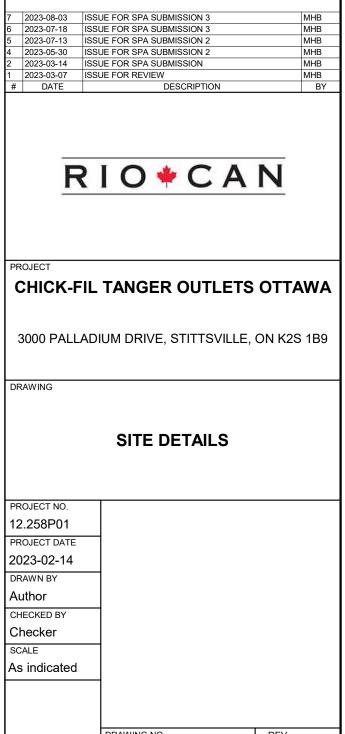
1264

(4' - 2")

- 1. CURB RAMP RUNNING SLOPES SHALL NOT BE STEEPER THAN 1:14 AND CROSS SLOPE SHALL BE 1.5% OR FLATTER.
- 2. DETECTABLE WARNING (IF REQUIRED) SHALL BE INSTALLED AT A DEPTH OF 24" FROM THE BACK OF CURB LINE FOR THE LENGTH
- OF THE RAMP LANDING. 3. DETECTABLE WARNING SURFACES SHALL BE CONSTRUCTED BY TEXTURING PRODUCTS CONFORMING TO PROWAG R304. TRANSITION SLOPES ARE NOT TO HAVE DETECTABLE WARNINGS.
- CONTRACTOR SHALL CONFIRM LOCAL CODES ARE MET. 4. WHERE A CURB RAMP IS CONSTRUCTED WITHIN AN EXISTING CURB, CURB & GUTTER AND/OR SIDEWALK, THE EXISTING CURB & GUTTER SHALL BE REMOVED TO THE NEAREST JOINT BEYOND THE CURB TRANSITIONS OR TO THE EXTENT THAT NO REMAINING SECTION OF CURB OR CURB & GUTTER IS LESS THAN 5' LONG. THE EXISTING SIDEWALK SHALL BE REMOVED TO THE NEAREST JOINT BEYOND THE TRANSITION SLOPE WALK AROUND OR TO THE
- 5. THE PLAN MUST PROVIDE FOR DETECTABLE WARNING SURFACE COLORS OR MATERIALS THAT PROVIDE THE NECESSARY CONTRAST, EITHER DARK-ON-LIGHT, OR LIGHT-ON-DARK. STANDARD DOME COLOR IS BRICK RED.

EXTENT THAT NO REMAINING SECTION OF SIDEWALK IS LESS

6. TRUNCATED DOMES TO BE INSTALLED USING ARMOR TILE CAST IN PLACE DOME TACTILE TILE. PART NUMBER ADA-2424 OR OTHER EQUIVALENT APPROVED MATERIAL. PREFERRED MANUFACTURER ARMOR TILE TACTILE SYSTEMS LANCE MITCHELL(919)622-4615 UNLESS PAVERS ARE REQUIRED, CONTRACTOR TO VERIFY THAT CURB RAMPS MEET LOCAL CODES AND ADA REQUIREMENTS.

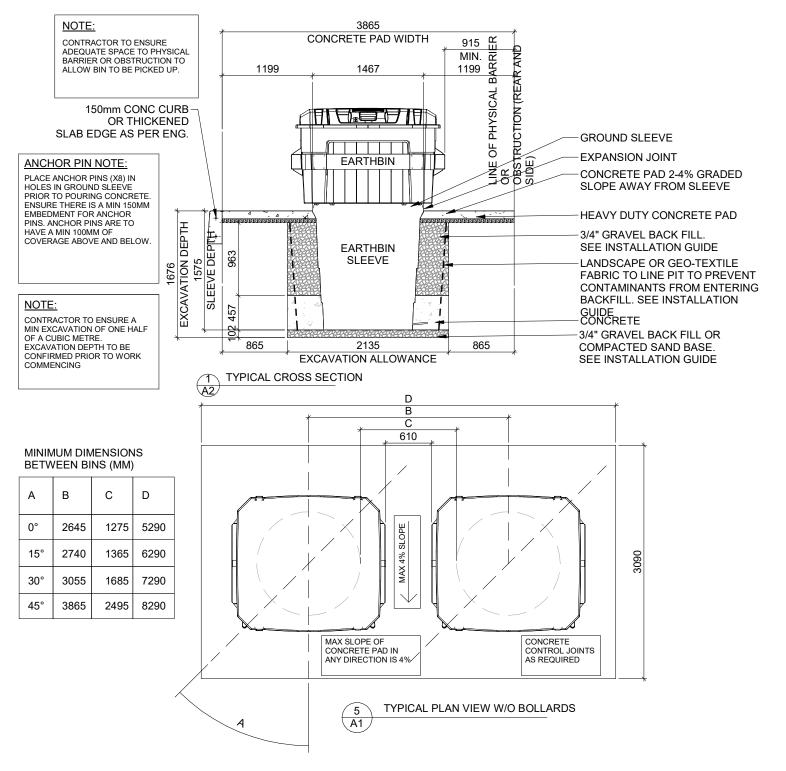


√8 \ CURB RAMP DETAIL \A102/ N.T.S.

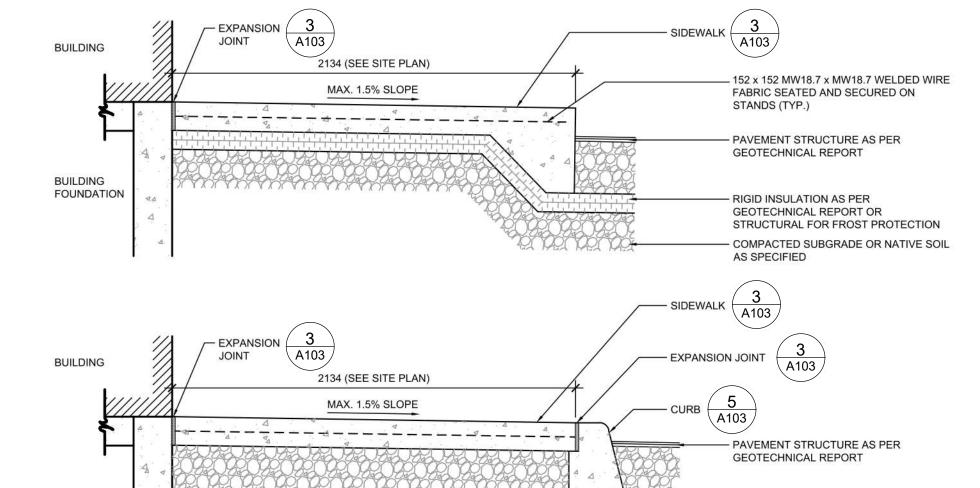
10 DRIVE-THRU GRAPHICS A102 N.T.S.

9 TACTILE WALKING SURFACE A102 N.T.S.

A102 CITY PLAN No. #18996



EARTH BIN DETAIL A103 1:50



- RIGID INSULATION AS PER

GEOTECHNICAL REPORT OR

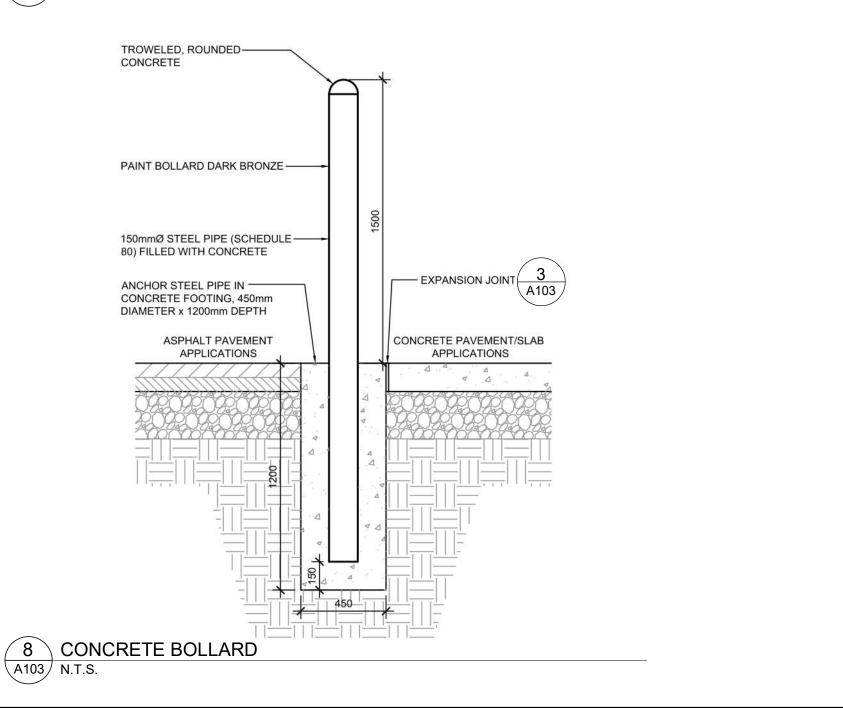
STRUCTURAL FOR FROST PROTECTION

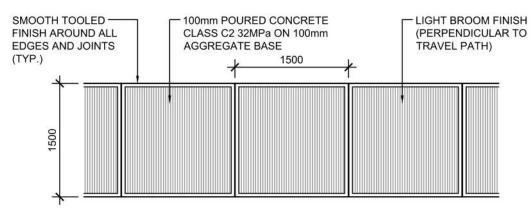
- COMPACTED SUBGRADE OR NATIVE SOIL

4 SIDEWALK AT BUILDING DETAILS A103 N.T.S.

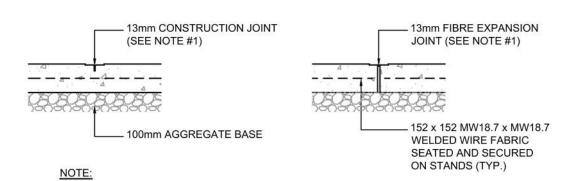
BUILDING

FOUNDATION





PLAN VIEW



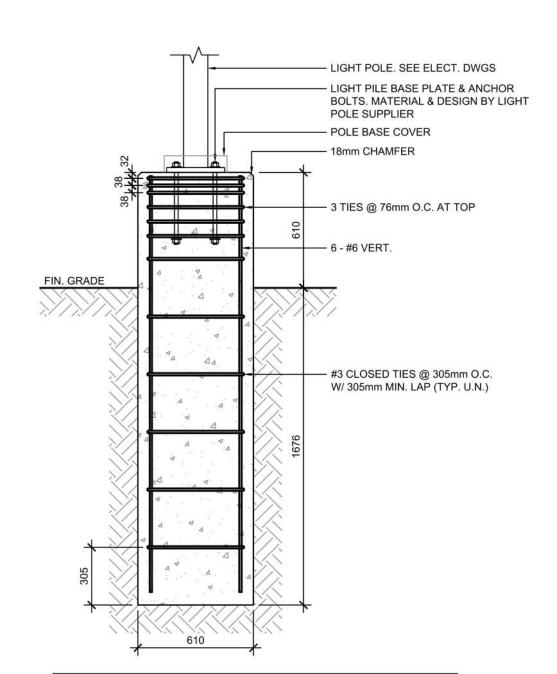
1. JOINTS AT 1500mm O.C. TOOLED 13mm WIDE, 25mm DEEP OR MAX. D DEEP WHICHEVER IS GREATER. EXPANSION JOINTS AT 6m MAX. & ALL P.C.s, UNLESS APPROVED OR INDICATED OTHERWISE ON PLAN VIEW JOINT PATTERN.

SECTION VIEW

. CONTRACTOR HAS THE OPTION TO DRILL, SET AND EPOXY GROUT DOWLS, IF NOT INSTALLED WHEN THE BUILDING SLAB WAS POURED. 2. REFER TO ARCHITECTURAL DRAWINGS FOR DOOR LOCATIONS. DOOR STOOP SHALL EXTEND 300mm PAS EACH SIDE OF DOOR MIN. STOOPS TO BE SEPARATED ON ALL SIDES WITH 13mm EXPANSION JOINT MATERIAL, TOPPED WITH 3. USE A 2400X2100mm SLAD MINIMUM AT A DOUBLE DOOR ENTRY. 4. USE A 2100X1500mm SLAD MINIMUM AT A DOUBLE DOOR ENTRY.

5. DOWELS TO BE INSTALLED INTO THE BUILDING FOUNDATION ONLY. THE REMAINING THREE SIDES OF THE SLAB TO NOT GET TIED OT THE SURROUNDING SIDEWALK.

3 TYPICAL SIDEWALK DETAIL A103 N.T.S.



MAX. HEIGHT OF LIGHT POLE IS 7.62m ABOVE GRADE. MAX HEIGHT OF CONCRETE BASE IS .61m ABOVE FINISHED GRADE. MAX. EXPOSED AREA OF LIGHT FIXTURES IS 0.2 sq.m @ EACH FIXTURE. MAX. 3 FIXTURES PER POLE. TOTAL EXPOSED AREA OF POLE = 1 sq.m.

\ LIGHT STANDARD BASE

POURED CONCRETE -APRON CLASS C2 32MPa FOR RADIUS PROTECTION A. 4. A ___| | | ___| | | ___| | | ___| | | ___ | SOIL AS SPECIFIED

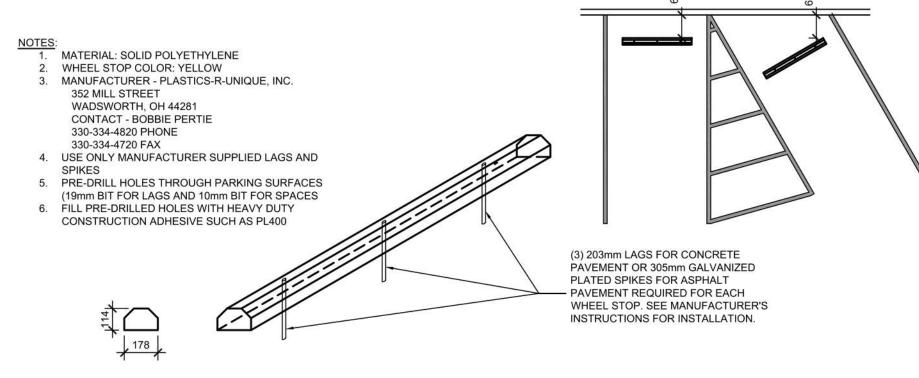
PAVEMENT STRUCTURE AS PER GEOTECHNICAL REPORT

 SEE GEOTECHNICAL REPORT FOR THICKNESS OF AGGREGATE UNDERNEATH PAVEMENT AND CURB COMPACTED SUBGRADE OR NATIVE

5 LANDSCAPE & IRRIGATION PROTECTION DETAIL N.T.S.

RADIUS PROTECTION APRON SHALL MEET THE MINIMUM LENGTH AND WIDTH DIMENSIONS AS PROVIDED ON THE SITE PLAN. √13mm CONSTRUCTION JOINTS AT 1500mm MAX. SPACING & EXPANSION JOINTS AT 6.0m MAX. LIGHT BROOM FINISH

7 LANDSCAPE & IRRIGATION PROTECTION PLAN N.T.S.



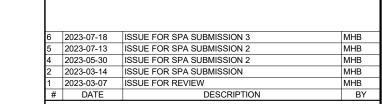
WHEEL STOPS ONLY TO BE USED WHEN NO OTHER PRACTICAL OPTION EXISTS

oxedown 6 igwedge SOLID PLASTIC WHEEL STOP DETAIL A103 N.T.S.

ALLISON HAMLIN

MANAGER (A), DEVELOPMENT REVIEW WEST PLANNING, REAL ESTATE & ECONOMIC DEVELOPMENT **DEPARTMENT, CITY OF OTTAWA**

APPROVED By Allison Hamlin at 1:54 pm, Sep 19, 2023



RIO+CAN

67 Lesmill Road

T 416 425 2222

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Toronto, ON, M3B 2T8

CHICK-FIL TANGER OUTLETS OTTAWA

3000 PALLADIUM DRIVE, STITTSVILLE, ON K2S 1B9

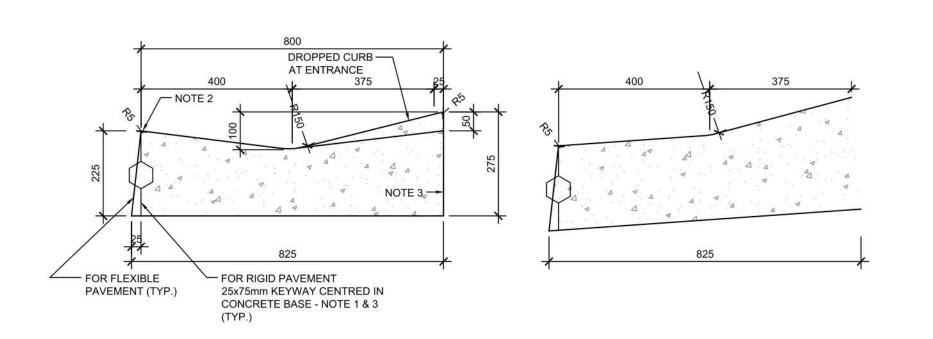
SITE DETAILS

PROJECT NO. 12.258P01

PROJECT DATE 2023-02-14 DRAWN BY CHECKED BY Checker

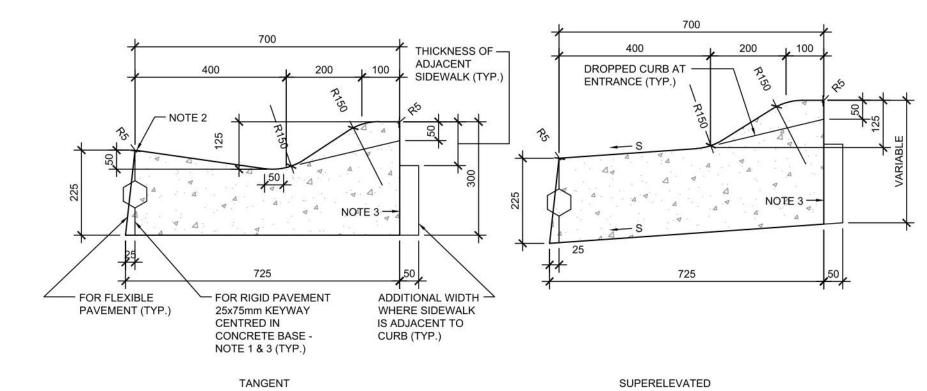
As indicated

6 CITY PLAN No. #18996



SUPERELEVATED

CONCRETE MOUNTABLE CURB WITH WIDE GUTTER (AS PER OPSD 600.030)



CONCRETE SEMI-MOUNTABLE CURB WITH WIDE GUTTER (AS PER OPSD 600.020)

1. WHEN CURB AND GUTTER IS ADJACENT TO CONCRETE PAVEMENT OR BASE, THIS DRAWING IS TO BE USED IN CONJUNCTION WITH OPSD-552.010 AND 552.020.

2. FLEXIBLE AND COMPOSITE PAVEMENT SHALL BE PLACED 5mm ABOVE THE ADJACENT EDGE OF GUTTER. 3. FOR SLIPFORMING PROCEDURE, A 5% BATTER IS ACCEPTABLE.

A. TREATMENT AT ENTRANCES SHALL CONFORM WITH OPSD-351.010. B. OUTLET TREATMENT SHALL CONFORM WITH OPSD-610 SERIES.

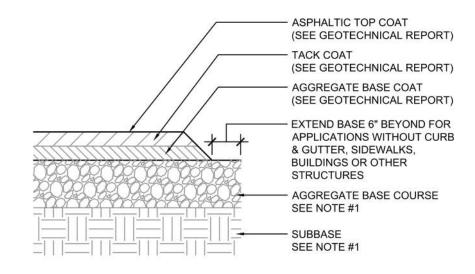
C. THE TRANSITION FROM ONE CURB TYPE TO ANOTHER SHALL BE A MINIMUN LENGTH OF 3.0m, EXCEPT IN CONJUNCTION WITH GUIDE RAIL WHERE IT SHALL BE ACCORDING TO THE OPSD-900 SERIES.

D. ALL DIMENSIONS ARE IN MILLIMETRES UNLESS OTHERWISE SHOWN.

LEGEND: S - RATE OF PAVEMENT SUPERELEVATION IN PERCENT, %

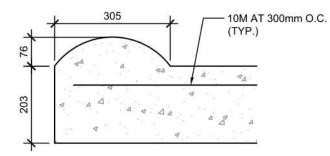
6 CONCRETE MOUNTABLE CURB DETAILS 1

TANGENT



IF PRESENT ON-SITE, UNSUITABLE SUBGRADE MATERIAL SHALL BE REPLACED WITH SUITABLE MATERIAL AS SPECIFIED IN THE GEOTECHNICAL REPORT.

√8 \ TYPICAL ASPHALT SECTION (A104) N.T.S.



3-12 MOUNTABLE DOME CURB

PRE-CUT EXPANSION JOINT MATERIAL SHALL BE USED IN ALL EXPANSION JOINTS.

DUMMY JOINTS SHALL BE INSTALLED AT 10 ft. INTERVALS, MAXIMUM. INSTALL EXPANSION JOINTS EVERY 40 ft., MAXIMUM, AT ENDS OF RADIUS, AND A MINIMUM OF 5 ft. FROM INLET STRUCTURES.

BREAK AS NEEDED TO ENSURE POSITIVE DRAINAGE CONCRETE TO BE 3,000 P.S.I.

6. MOUNTABLE CURB TO BE PAINTED YELLOW WITH 6" BLACK STRIPE PAINTED EVERY 24" O.C.

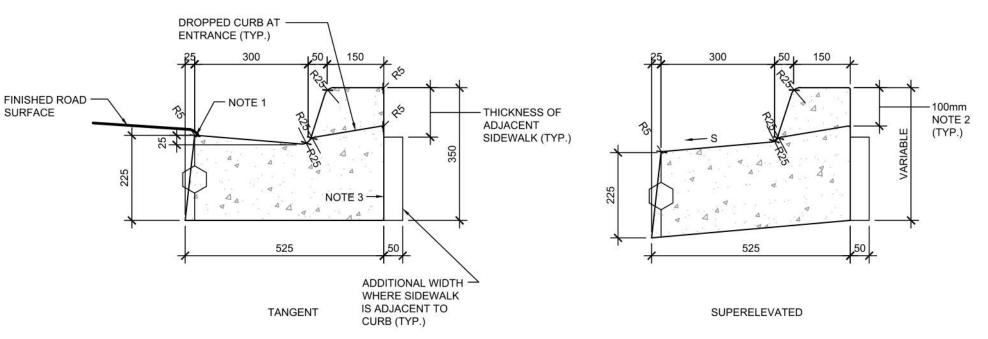
A104 N.T.S.

NOTES:

1. GENERAL CONTRACTOR SHALL REFERENCE GEOTECHNICAL REPORT FOR PAVEMENT

EXPANSION JOINT FILLER SHALL BE FLEXIBLE, LIGHTWEIGHT, NON-STAINING, POLYETHYLENE, CLOSED-CELL EXPANSION JOINT

5 CONCRETE MOUNTABLE CURB DETAILS

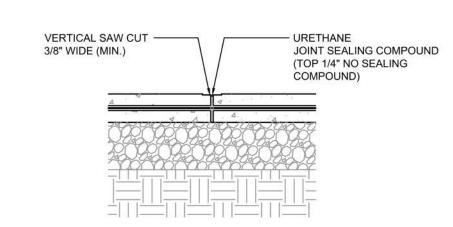


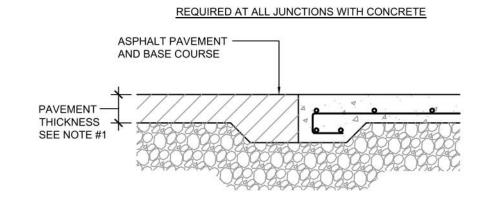
CONCRETE BARRIER CURB WITH STANDARD GUTTER (AS PER OPSD 600.040)

- 1. FLEXIBLE AND COMPOSITE PAVEMENT SHALL BE PLACED 5mm ABOVE THE ADJACENT EDGE OF GUTTER. WHERE SIDEWALK IS CONTINUOUSLY ADJACENT, REDUCE THE DROPPED CURB AT ENTRANCES TO 75mm.
- FOR SLIPFORMING PROCEDURE, A 5% BATTER IS ACCEPTABLE. FOR COMPOSITE PAVEMENT THE DEPTH OF CONCRETE CURB SHALL BE ADJUSTED TO DEPTH OF CONCRETE PAVEMENT.
- WHEN TIE BARS ARE SPECIFIED, REFER TO OPSD 552.010 AND 552.020 FOR DETAILS. TREATMENT AT ENTRANCES SHALL CONFORM WITH OPSD-351.010.
- OUTLET TREATMENT SHALL CONFORM WITH OPSD-610 SERIES. THE TRANSITION FROM ONE CURB TYPE TO ANOTHER SHALL BE A MINIMUN LENGTH OF 3.0m, EXCEPT IN
- CONJUNCTION WITH GUIDE RAIL WHERE IT SHALL BE ACCORDING TO THE OPSD-900 SERIES. D. ALL DIMENSIONS ARE IN MILLIMETRES UNLESS OTHERWISE SHOWN.

S - RATE OF PAVEMENT SUPERELEVATION IN PERCENT, %.

CONCRETE CURB WITH GUTTER DETAILS A104 N.T.S.





GENERAL CONTRACTOR SHALL REFERENCE GEOTECHNICAL REPORT FOR PAVEMENT SECTION REQUIREMENTS.

WELDED WIRE FABRIC

STANDS

SEE NOTE #1

SEE NOTE #1

SEATED AND SECURED ON

AGGREGATE BASE COURSE

- #3 @ 12" ON CENTER EACH

- AGGREGATE BASE COURSE

SEE NOTE #1

SEE NOTE #1

- SUBBASE

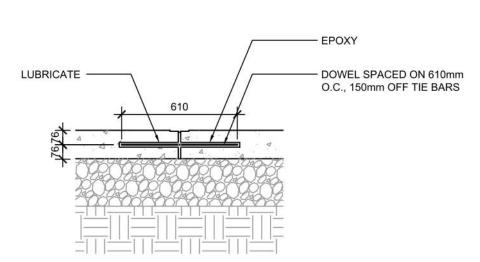
10 PAVEMENT EDGE DETAIL

CONCRETE AS PER CSA-

STANDARDS

REQUIREMENTS.

CONCRETE PAVING DRIVE-THRU LANE 1



NOTES:

1. NO. 5 SMOOTH DOWEL BAR MAY BE USED IN 125mm AND 150mm PAVEMENT

- THICKNESS. 2. LONGITUDINAL BUTT CONSTRUCTION MAY BE UTILIZED IN PLACE OF
- LONGITUDINAL HINGED (KEYWAY) JOINT AT CONTRACTORS OPTION. 3. DOWEL BARS SHALL BE DRILLED & EPOXIED INTO PAVEMENT HORIZONTALLY
- BY USE OF MECHANICAL EQUIP. 4. PUSHING DOWEL BARS INTO WET CONCRETE NOT ACCEPTABLE.

NOTES:

1. NO. 5 SMOOTH DOWEL BAR MAY BE USED IN 125mm AND 150mm PAVEMENT

LONGITUDINAL HINGED (KEYWAY) JOINT AT CONTRACTORS OPTION.

3. DOWEL BARS SHALL BE DRILLED INTO PAVEMENT HORIZONTALLY BY USE

4. DRILLING BY HAND IS NOT ACCEPTABLE, PUSHING DOWEL BARS INTO WET

2. LONGITUDINAL BUTT CONSTRUCTION MAY BE UTILIZED IN PLACE OF

OF A MECHANICAL EQUIPMENT.

CONCRETE NOT ACCEPTABLE.

✓ 2 XPANSION JOINT

A104 N.T.S.

5. JOINT SPACING TO BE 24'X24' (EVERY OTHER JOINT)

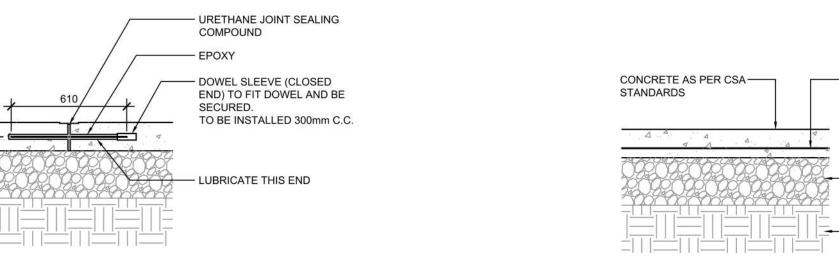
12 LONGITUDINAL BUTT JOINT N.T.S.

13 CONCRETE PAVING DRIVE-THRU LANE 2 (A104) N.T.S.

GENERAL CONTRACTOR SHALL REFERENCE GEOTECHNICAL REPORT FOR PAVEMENT SECTION

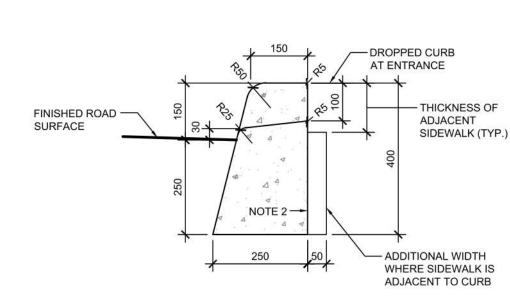
2. MINIMUM PAVEMENT THICKNESS SHALL BE 6"

CONCRETE, 4" AGGREGATE BASE



GENERAL CONTRACTOR SHALL REFERENCE GEOTECHNICAL REPORT FOR PAVEMENT SECTION REQUIREMENTS. 2. MINIMUM PAVEMENT THICKNESS SHALL BE 6" CONCRETE, 4" AGGREGATE BASE

CONCRETE PAVING PARKING LOT A104 N.T.S.

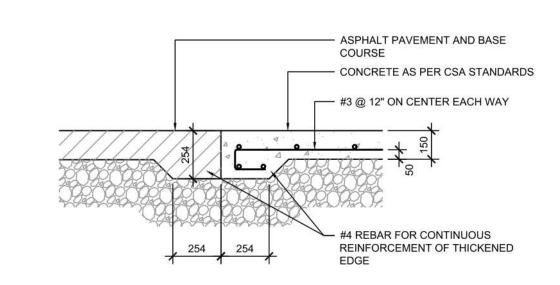


CONCRETE BARRIER CURB (AS PER OPSD 600.110)

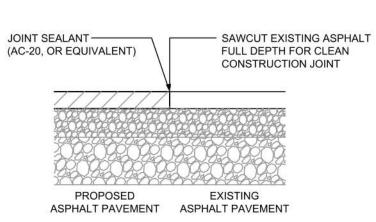
1. WHERE SIDEWALK IS CONTINUOUSLY ADJACENT, REDUCE THE DROPPED CURB AT

- ENTRANCES TO 75mm. 2. FOR SLIPFORMING PROCEDURE, A 5% BATTER IS ACCEPTABLE TREATMENT AT ENTRANCES SHALL CONFORM WITH OPSD-351.010.
- OUTLET TREATMENT SHALL CONFORM WITH OPSD-610 SERIES. THE LENGTH OF TRANSITION FROM ONE CURB TYPE TO ANOTHER SHALL BE 3.0M, EXCEPT IN CONJUNCTION WITH GUIDE RAIL, IT SHALL CONFORM TO OPSD-900
- D. ALL DIMENSIONS ARE IN MILLIMETRES UNLESS OTHERWISE SHOWN.

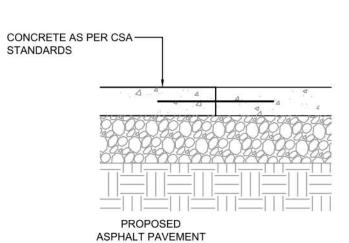




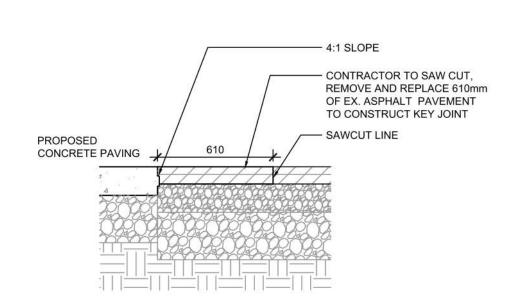




14 BUTT JOINT A104 N.T.S.



TRANSVERSE & LONGITUDINAL DOWELLED CONSTRUCTION JOINT



4 KEYED CONSTRUCTION JOINT N.T.S.



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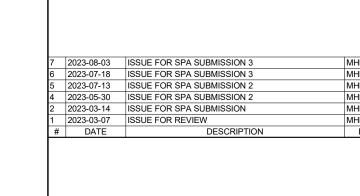
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ALLISON HAMLIN MANAGER (A), DEVELOPMENT REVIEW WEST

PLANNING, REAL ESTATE & ECONOMIC DEVELOPMENT **DEPARTMENT, CITY OF OTTAWA**

APPROVED

By Allison Hamlin at 1:54 pm, Sep 19, 2023



RIO+CAN

CHICK-FIL TANGER OUTLETS OTTAWA

3000 PALLADIUM DRIVE, STITTSVILLE, ON K2S 1B9

SITE DETAILS

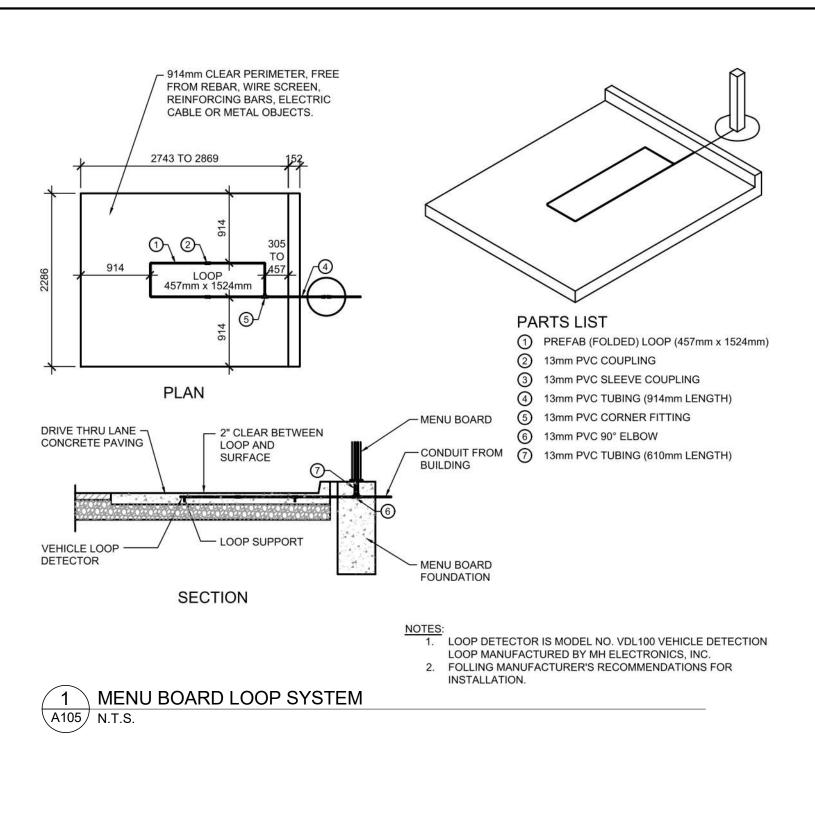
DRAWING

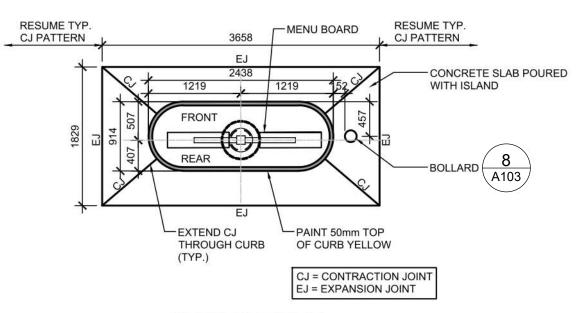
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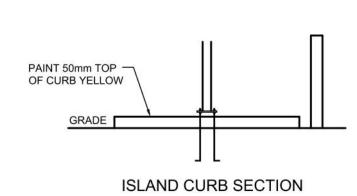
12.258P01 PROJECT DATE 2023-02-14 DRAWN BY Author CHECKED BY Checker SCALE

N.T.S.

CITY PLAN No. #18996

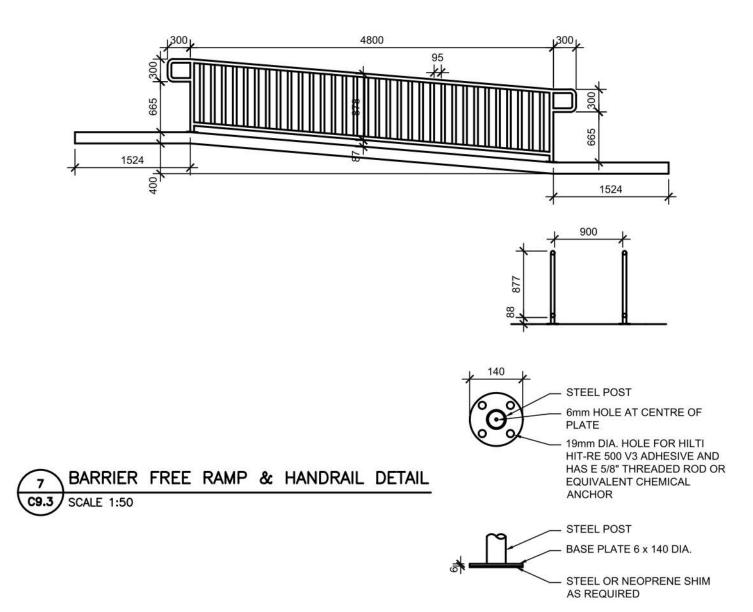


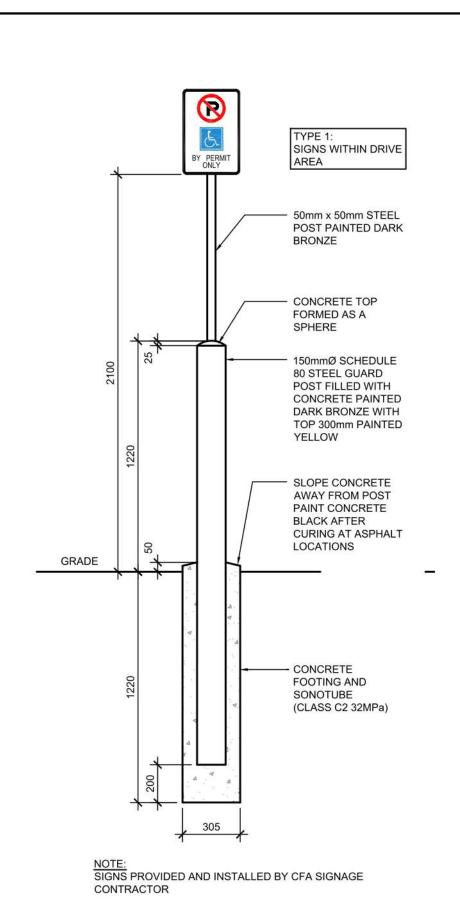




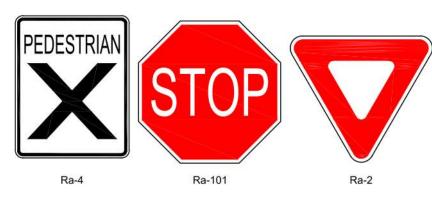
ISLAND CURB PLAN

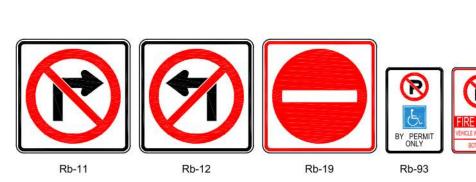
DRIVE-THRU ORDER POINT ISLAND CURB N.T.S.





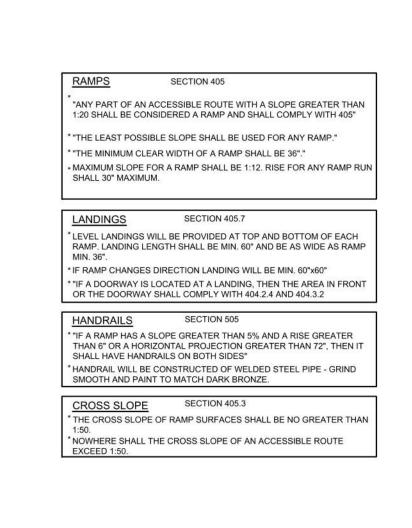


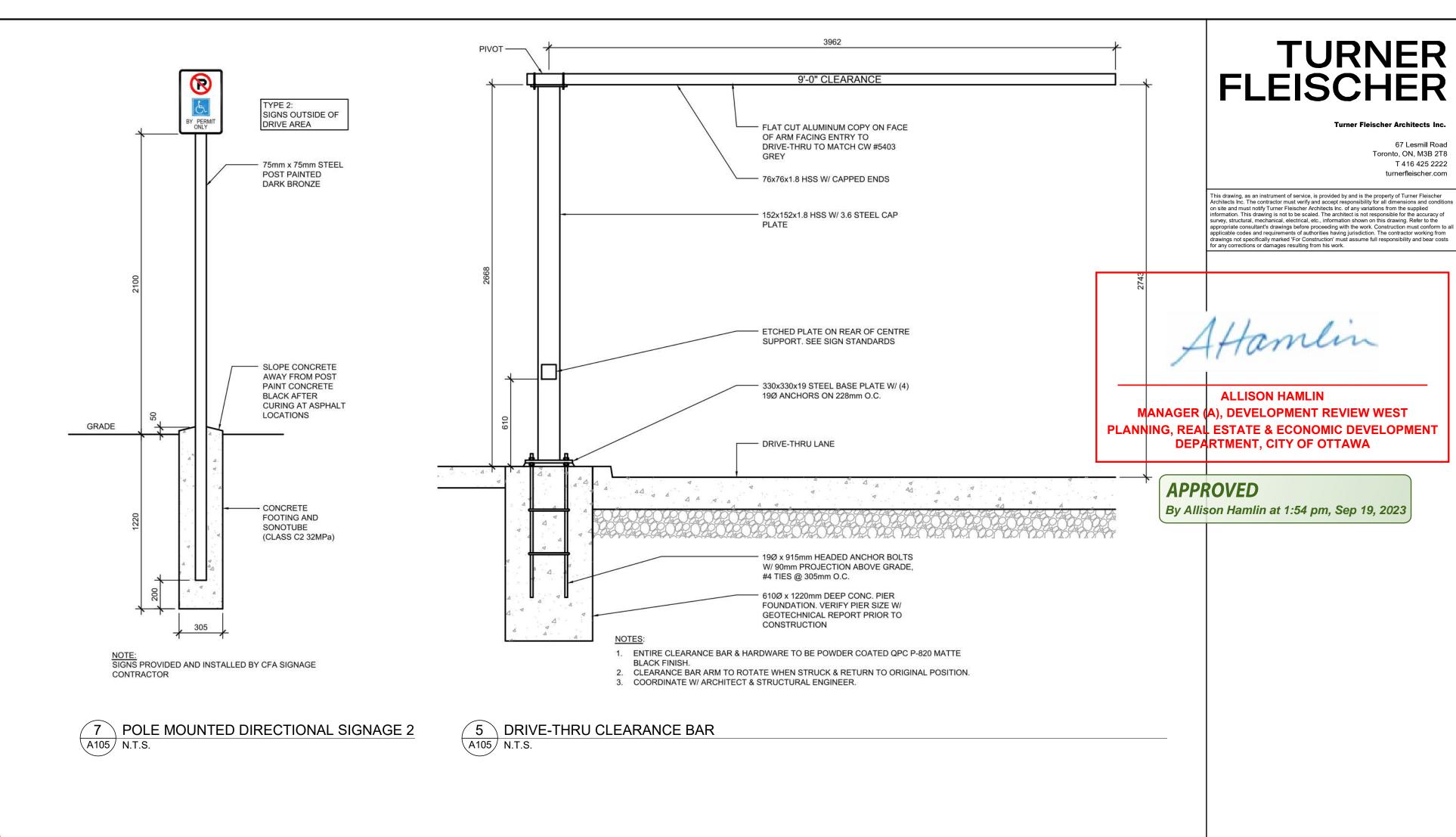


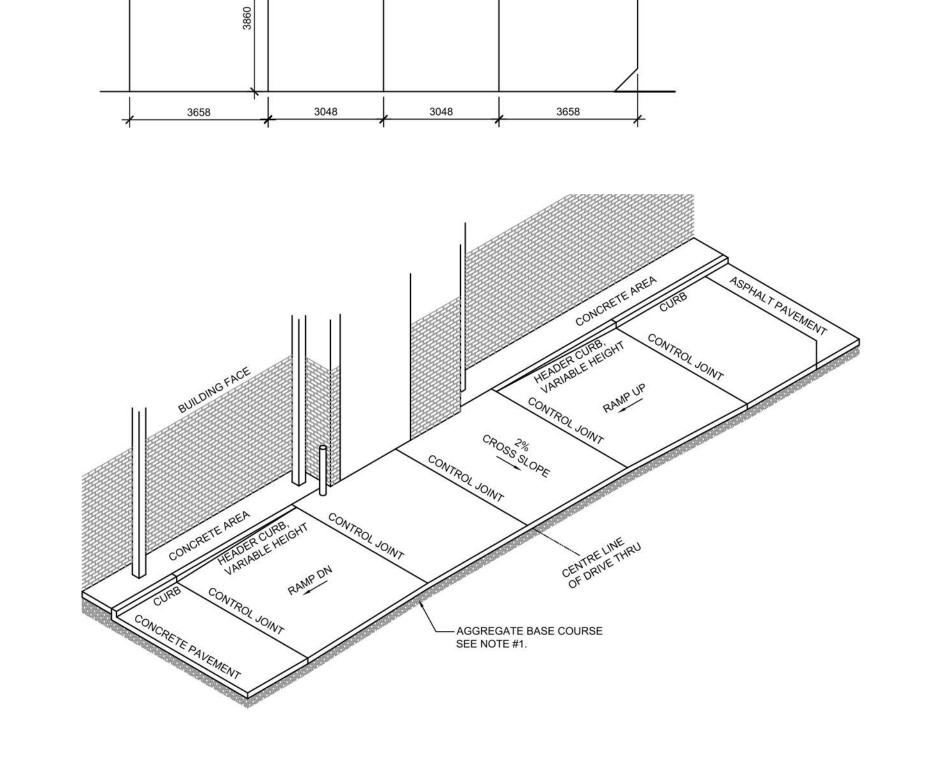


NOTE:
ALL DIRECTIONAL SIGNAGE TO COMPLY WITH THE ONTARIO TRAFFIC MANUAL AND LOCAL STANDARDS.









8 DRIVE-THRU DETAILS N.T.S.



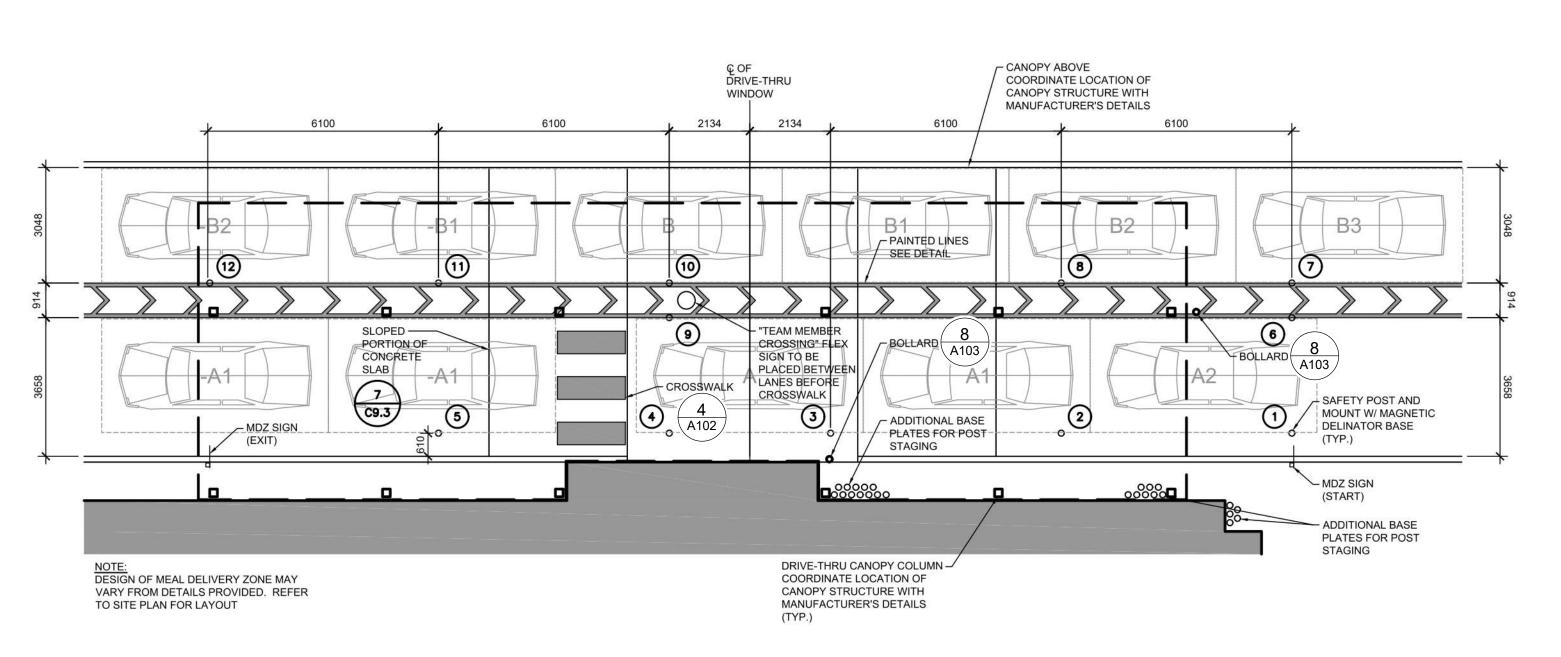


CITY PLAN No. #18996

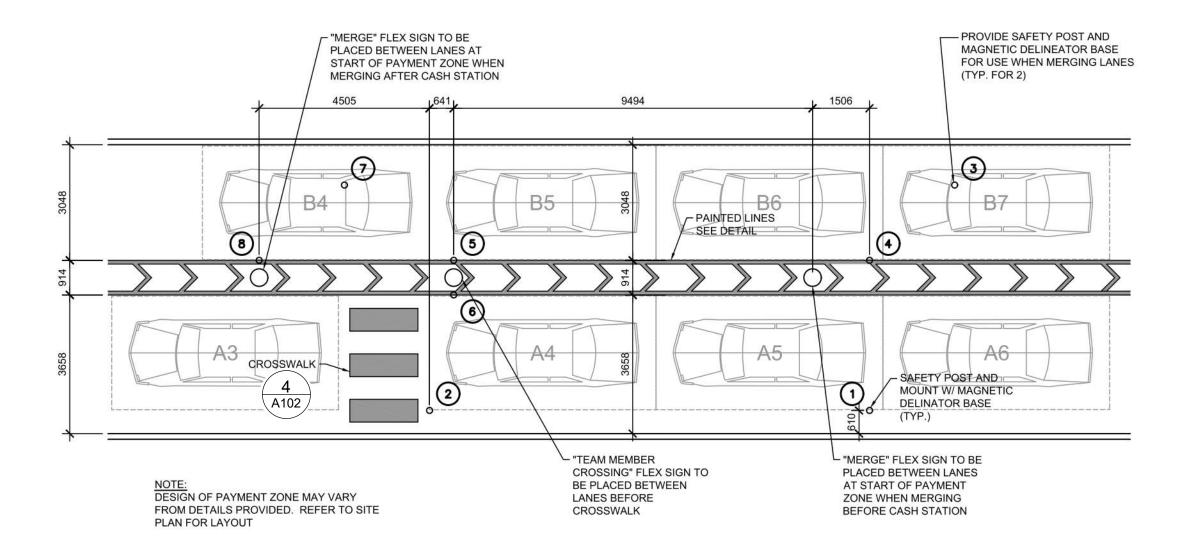
67 Lesmill Road

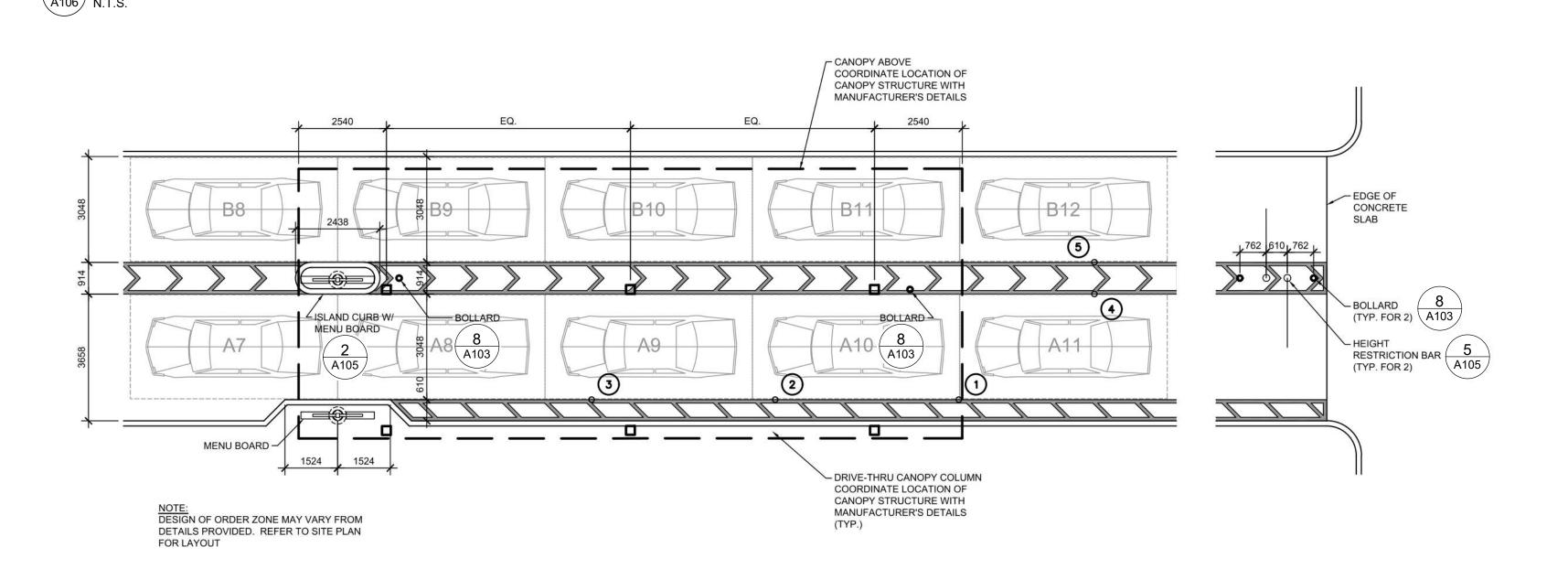
Toronto, ON, M3B 2T8 T 416 425 2222

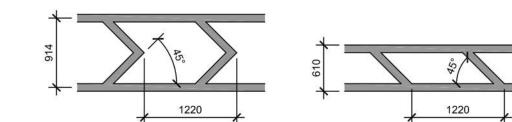
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4 MEAL DELIVERY ZONE - DUAL LANE CANOPY LAYOUT N.T.S.

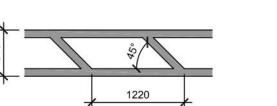






NOTES:

1. CONTRACTOR SHALL USE 100mm WIDE YELLOW REFLECTIVE PAINT FOR STRIPING ON CONCRETE DRIVE-THRU.



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TURNER

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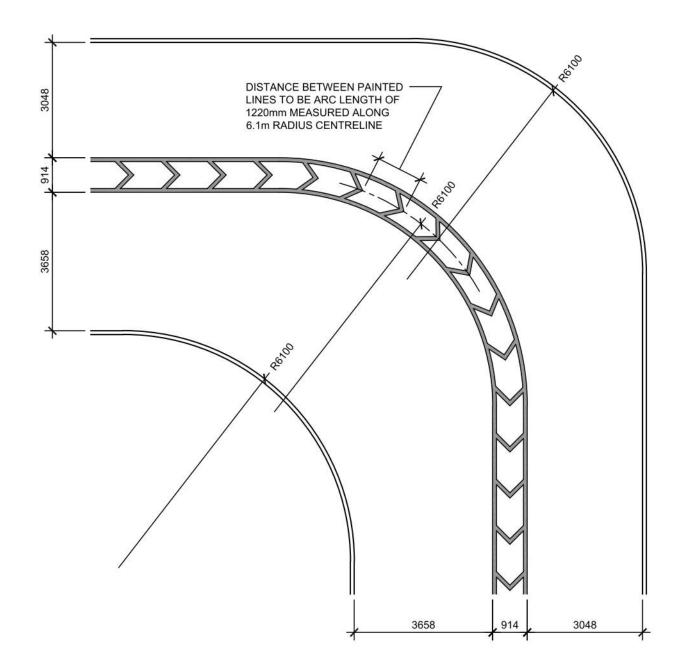
FLEISCHER

ALLISON HAMLIN

MANAGER (A), DEVELOPMENT REVIEW WEST PLANNING, REAL ESTATE & ECONOMIC DEVELOPMENT **DEPARTMENT, CITY OF OTTAWA**

APPROVED

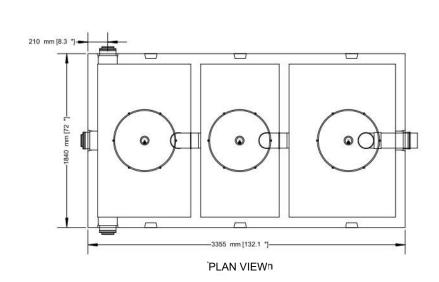
By Allison Hamlin at 1:54 pm, Sep 19, 2023



5 DRIVE-THRU BEND DESIGN N.T.S.

3 DRIVE-THRU PAINTED LINES N.T.S.

6000 LITRES (1319 GAL) OIL AND GREASE INTERCEPTOR 2 PIECE 3 COMPARTMENTS



• PRECAST INTERCEPTOR IS DELIVERED AS A ONE PIECE UNIT • INDIVIDUAL ACCESS LIDS TO EACH COMPARTMENT FOR CLEANOUT • THREE COMPARTMENTS SEPARATE WATER FROM ANY INSOLUBLE LIQUID OF SOLIDS HAVING A SPECIFIC GRAVITY LESS THEN THAT OF WATER

• UNIT MUST BE VENTED IN REGULATION WITH PLUMBING CODES • TANKS ARE CAST WITH 35 MPA STRENGTH CONCRETE AT 7 DAYS WITH 5-7% AIR STEEL REINFORCED WITH 10M REBAR, SPACED AT 400MM IN EACH DIRECTION • REBAR COVER IS 25MM

• INTERCEPTORS ARE USED TO PREVENT OIL WASTE FROM ENTERING THE SANITARY SEWER SYSTEM

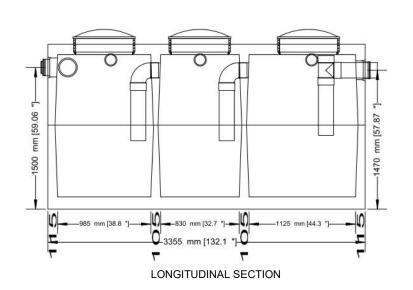
TOP HALF WEIGHT...3157KG (6960LBS) BOTTOM HALF WEIGHT...2794KĠ (6160LB) TOTAL TANK WEIGHT..... ...5951KG (13120LB)

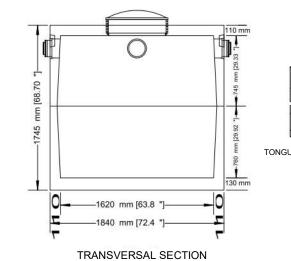
NOTES

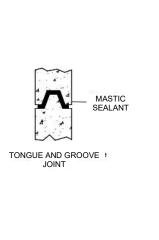
1. 150MM FLOW THROUGH PIPES EXTENDING 600MM DOWN 2. STANDARD THREE 150MM INLETS AS WELL AS ONE OUTLET BAFFLE CONSISTING OF 150MM TEE AND 150 MM PIPE (710MM IN LENGTH) THAT IS READILY ADAPTABLE FOR FILTER 3. CAST IN PLACE 610MM ID PLASTIC RISER WITH PLASTIC LID SECURED WITH STAINLESS STEEL FASTENERS EXTENDING 100MM ABOVE TOP

DESIGNED FOR UP TO ONE METRE BURIAL OVER TOP OF TANK -VEHICLE TRAFFIC AND UP TO TWO METRE BURIAL TANKS ARE AVAILABLE UPON REQUEST.

WWW.NEWMARKETPRECAST.COM | 905-852-6111 | 1-800-263-1297







CHICK-FIL TANGER OUTLETS OTTAWA 3000 PALLADIUM DRIVE, STITTSVILLE, ON K2S 1B9

 7
 2023-08-03
 ISSUE FOR SPA SUBMISSION 3

 6
 2023-07-18
 ISSUE FOR SPA SUBMISSION 3

 5
 2023-07-13
 ISSUE FOR SPA SUBMISSION 2

 4
 2023-05-30
 ISSUE FOR SPA SUBMISSION 2

 2
 2023-03-14
 ISSUE FOR SPA SUBMISSION

 1
 2023-03-07
 ISSUE FOR REVIEW

 #
 DATE
 DESCRIPTION

RIO+CAN

PROJECT NO. 12.258P01 PROJECT DATE 2023-02-14 DRAWN BY Author CHECKED BY Checker SCALE N.T.S.

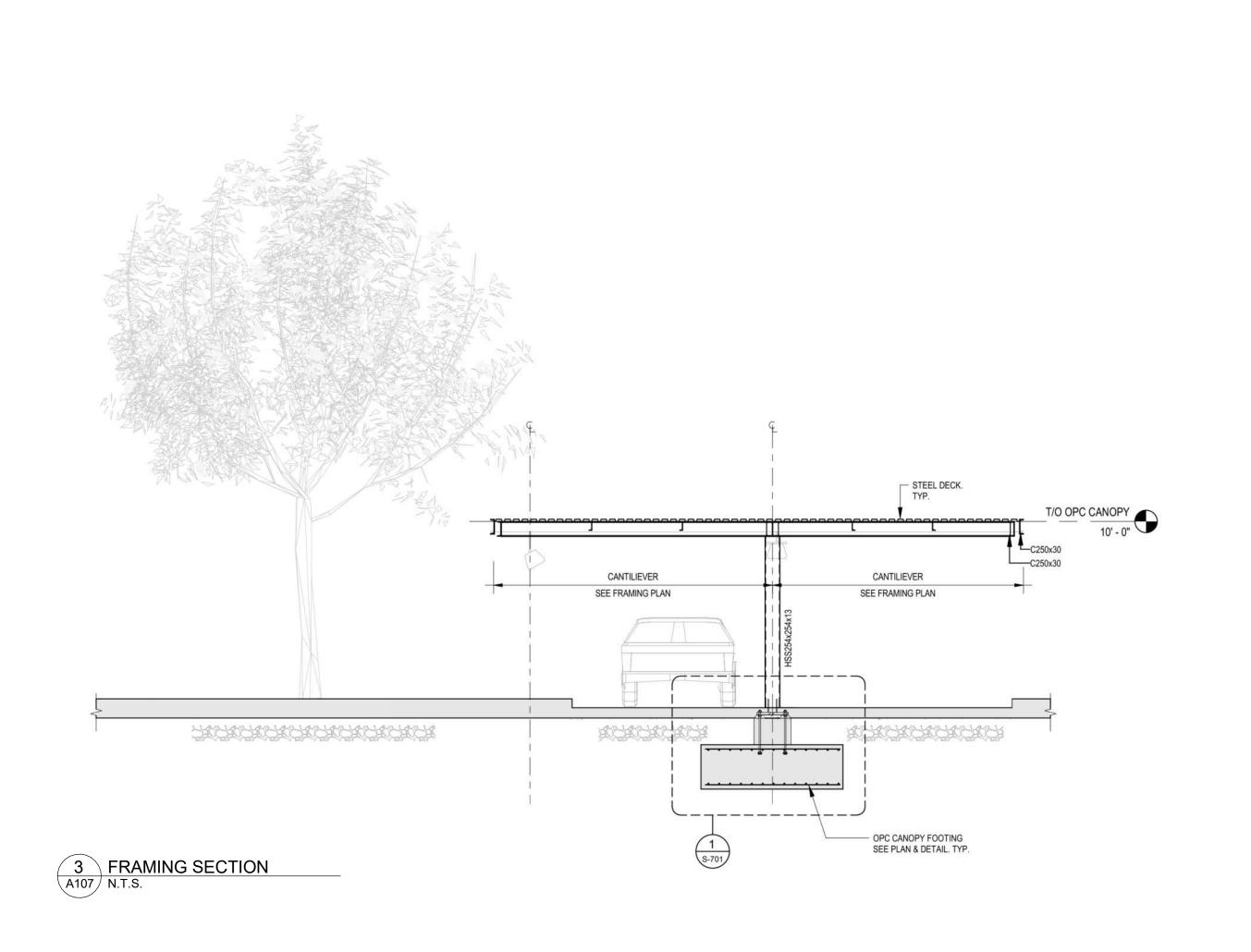
SITE DETAILS

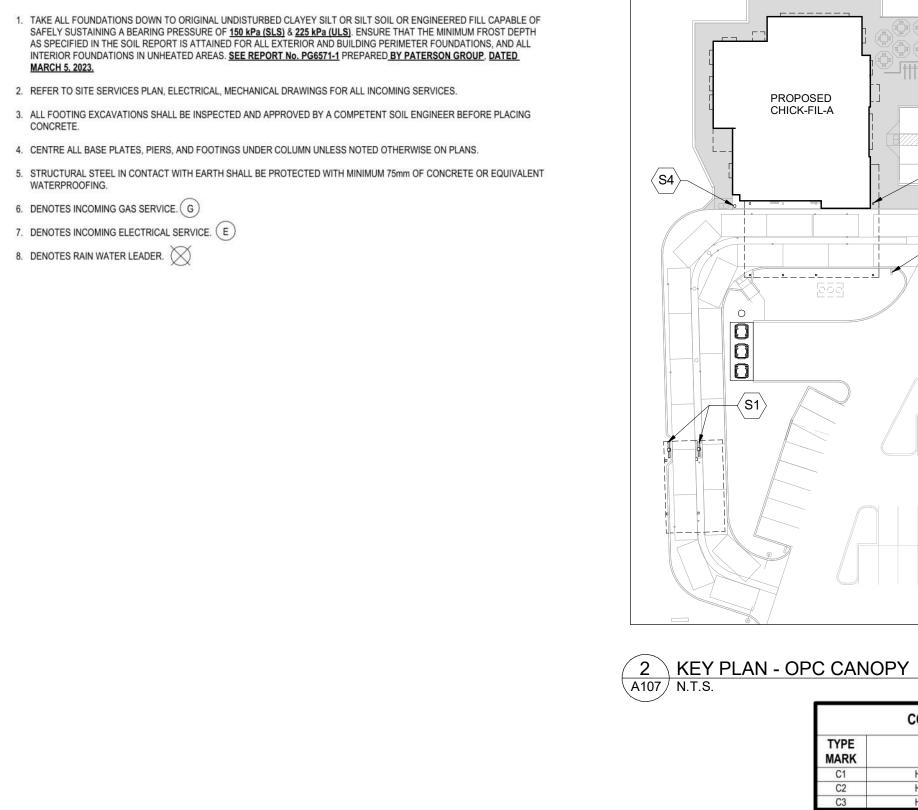


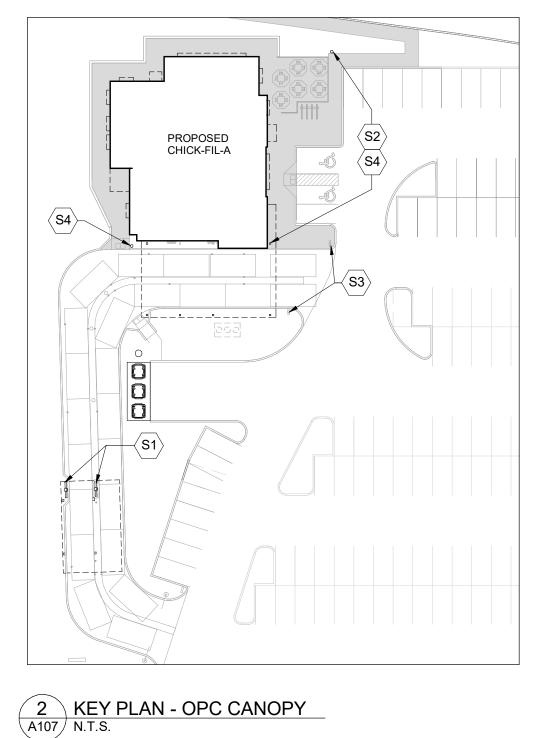
ORDER ZONE LAYOUT
N.T.S.

1 PAYMENT ZONE LAYOUT

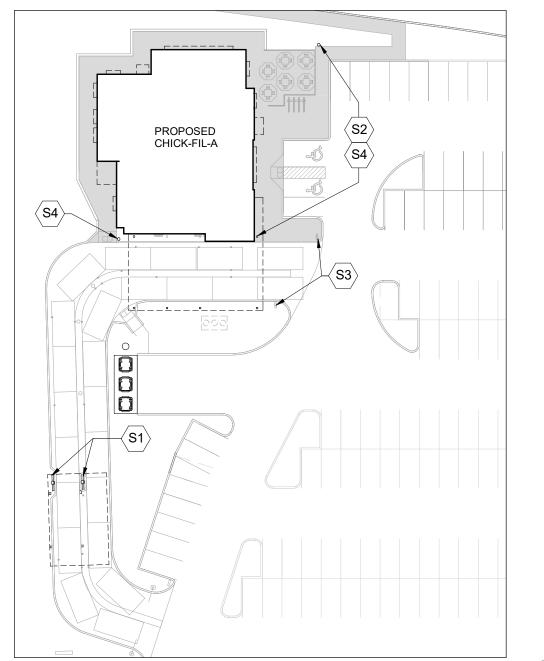
CITY PLAN No. #18996





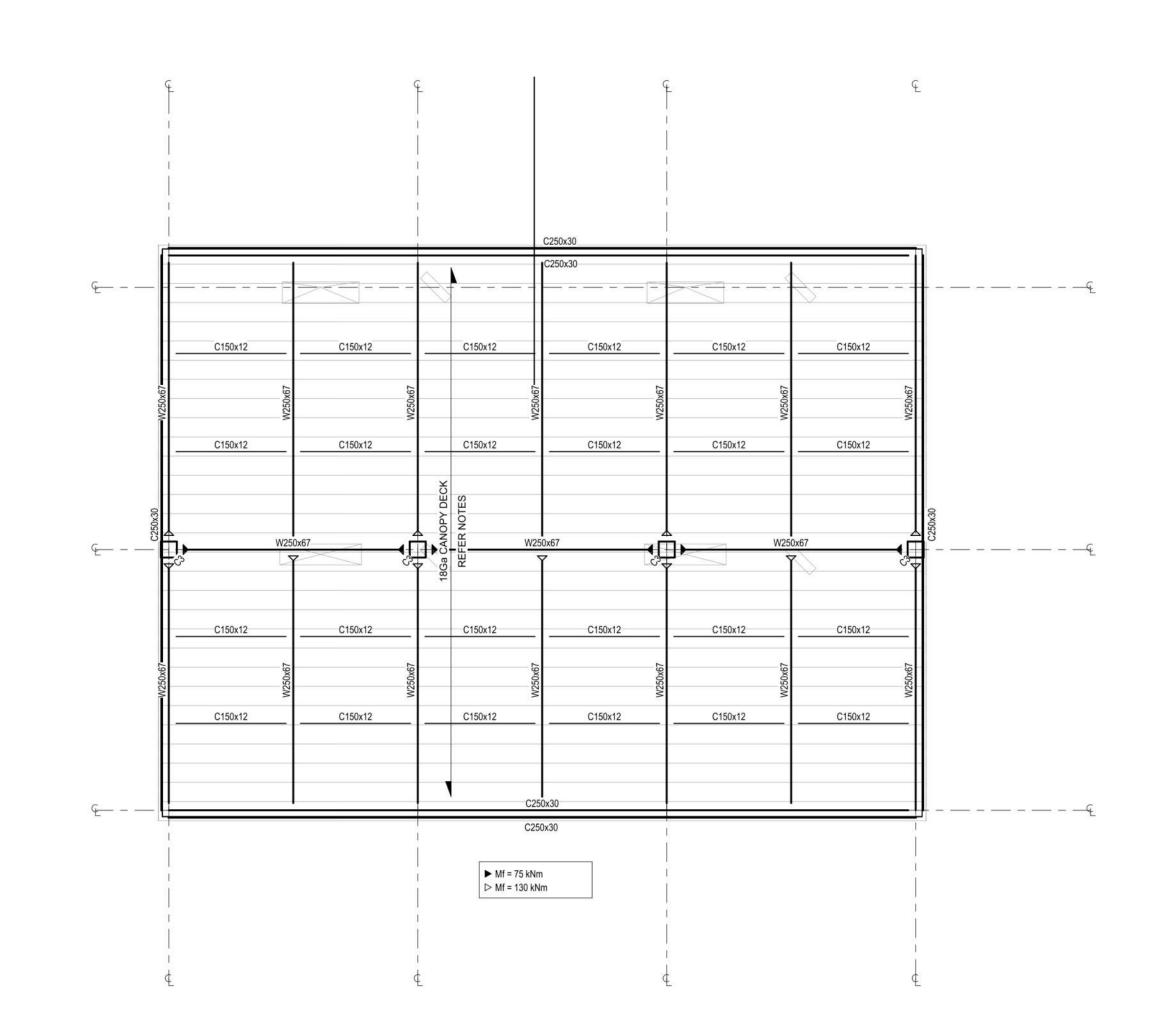


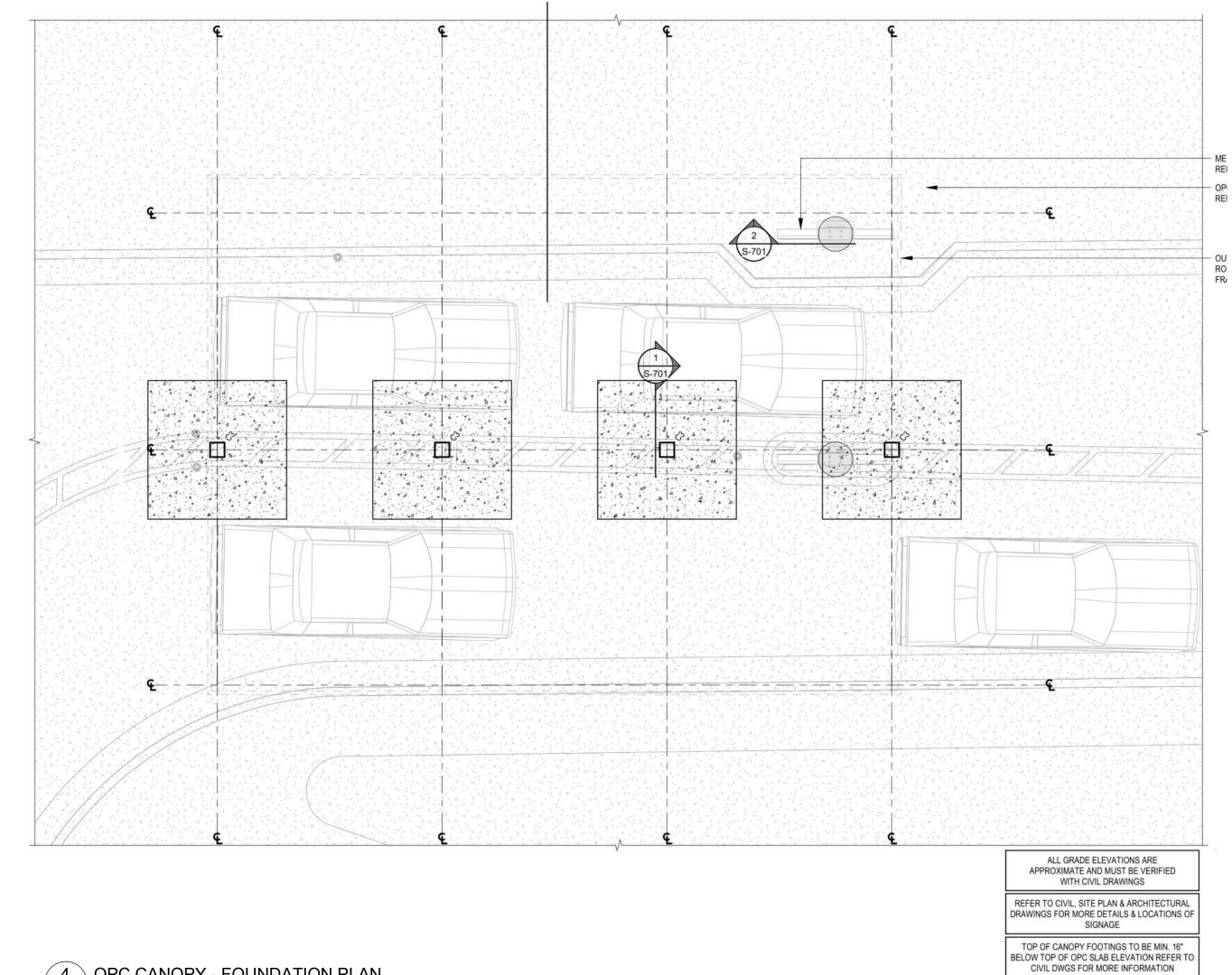
COLUMN SCHEDULE BASEPLATE **THICKNESS** HSS127x127x13 HSS152x152x13 'C' DENOTES FOOTING





- ORDER MENU SIGN.
 REFER TO ARCH. AND CIVIL DWGS
 SEE 2 / S-701
- S2 FLAG POLE, SEE COORD. EXACT LOCATION WITH CIVIL DWGS. SEE 3 / S-701 DRIVE THRU ENTRANCE SIGN. REFER TO ARCH. AND CIVIL DWGS.
 SEE 5 / S-701
- S4 SIGNAGE REFER TO ARCH.
 AND CIVIL DWGS. SEE 4 / S-701







CITY PLAN No. #18996

TURNER FLEISCHER

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ALLISON HAMLIN MANAGER (A), DEVELOPMENT REVIEW WEST

PLANNING, REAL ESTATE & ECONOMIC DEVELOPMENT DEPARTMENT, CITY OF OTTAWA

APPROVED

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4 OPC CANOPY - FOUNDATION PLAN N.T.S.

2. REFER TO SITE SERVICES PLAN, ELECTRICAL, MECHANICAL DRAWINGS FOR ALL INCOMING SERVICES.

4. CENTRE ALL BASE PLATES, PIERS, AND FOOTINGS UNDER COLUMN UNLESS NOTED OTHERWISE ON PLANS.

WATERPROOFING.

6. DENOTES INCOMING GAS SERVICE. (G)

8. DENOTES RAIN WATER LEADER.

7. DENOTES INCOMING ELECTRICAL SERVICE. (E)