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**50 LEIKIN DRIVE
CANADA POST OTTAWA PROCESSING CENTRE**

DESIGN BRIEF AND RATIONALE IN SUPPORT OF A
SITE PLAN CONTROL APPLICATION

Submitted By:
Submitted on behalf of:

Peter Hume
Canada Post Corporation
April 2023

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1.1 Background

Canada Post Corporation has commissioned this rationale for a zoning compliant site plan application for the lands legally described as follows:

PART BLOCK 2 PLAN 4M-1354, PARTS 1 AND 3 PLAN 4R-32287. SUBJECT TO AN EASEMENT IN GROSS OVER PART 3 ON PLAN 4R-32287 AS IN OC1093047 SUBJECT TO AN EASEMENT IN GROSS OVER PART 1 PLAN 4R-33446 AS IN OC2325901 CITY OF OTTAWA

The subject site is designated “Mixed Industrial” in the 2020 City of Ottawa Official Plan. The Subject Property is currently zoned IL9, being an Industrial Light Zone, subzone 9 – South Merivale Business Park. In the IL9 zone, a warehouse is only a permitted use if it is associated with an enumerated permitted use, such as an office.

The owner is proposing to develop the subject site as a zoning compliant office and warehouse site plan. An application for Site Plan Control approval is required to facilitate the proposed development.

1.2 Pre-Consultation

A pre consultation meeting was held November 16, 2022, with City of Ottawa staff. The meeting minutes were provided on December 1, 2022, along with the plan and study requirements.

The following City required plans and studies are submitted in support of the applications:

- Site Servicing Plan
- Storm Drainage / Ponding Plan
- Stormwater Management Report and Site Servicing Study
- Grade Control and Drainage Plan
- Geotechnical Study
- Erosion and Sediment Control Plan
- Noise Study (within airport 25 NEF/NEP line)
- Transportation Impact Assessment (TIA)
- Site Plan
- Landscape Plan
- Survey Plan
- Architectural Building Elevations
- Phase 1 ESA
- Environmental Effects Determination
- Tree Conservation Report
- Site Lighting Certificate Letter

Additional Supporting Documents (attached to this rationale):

- National Capital Commission Federal Design Approval

The City of Ottawa required documentation along with the additional supporting documents with this Rationale | Design Brief and the Public Consultation Strategy form a complete application.

1.3 Public Consultation

After the filing of the necessary development applications the following meetings are planned:

- A formal meeting with the Ward Councillor
- An on-site information sign will allow comments from the public.

2.1 Location and Site Description

The municipal address for the subject site is 50 Leikin Drive and has the legal description of:

PART BLOCK 2 PLAN 4M-1354, PARTS 1 AND 3 PLAN 4R-32287. SUBJECT TO AN EASEMENT IN GROSS OVER PART 3 ON PLAN 4R-32287 AS IN OC1093047 SUBJECT TO AN EASEMENT IN GROSS OVER PART 1 PLAN 4R-33446 AS IN OC2325901 CITY OF OTTAWA.

The subject site is 22.371 ac in size.

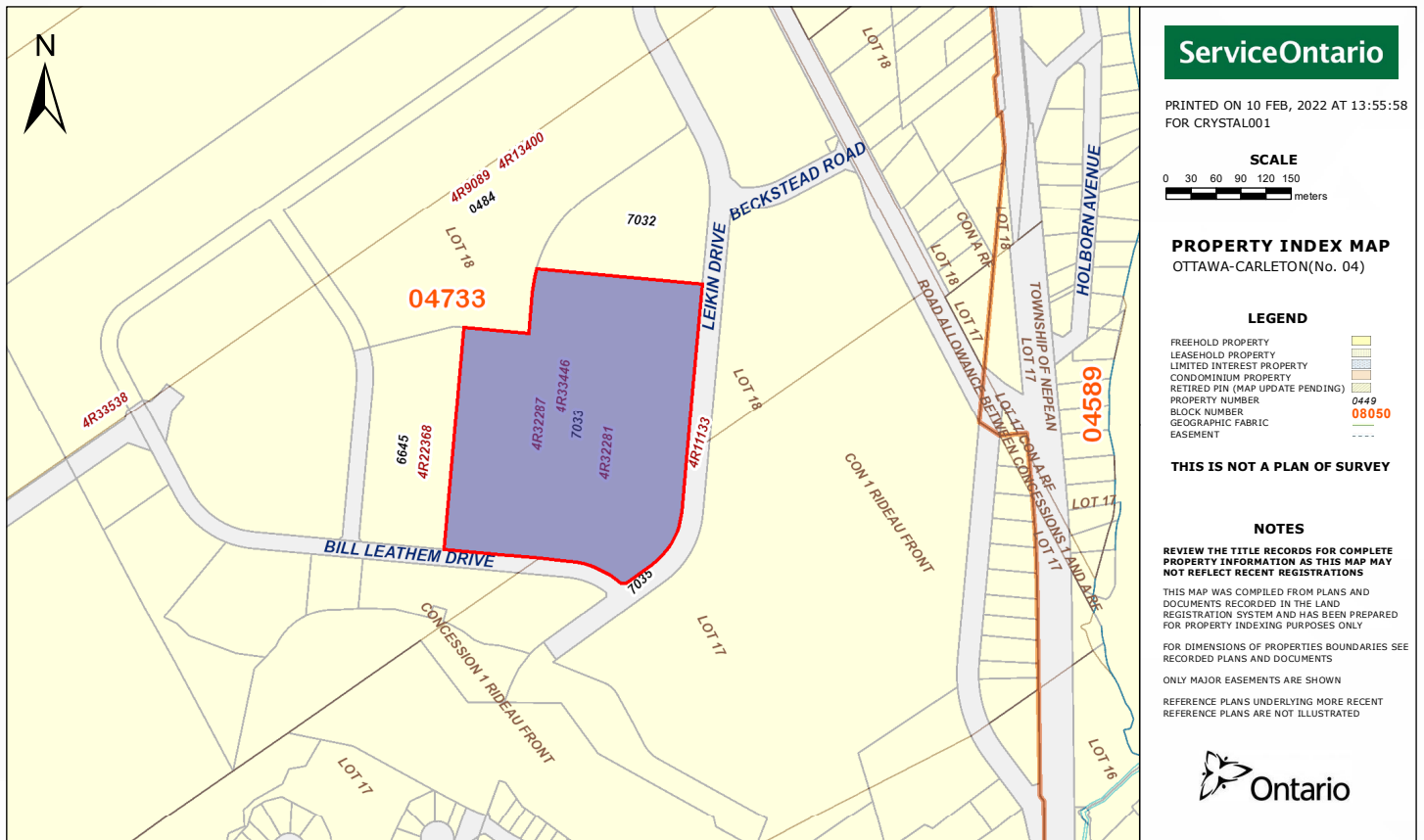


Figure 1 – Subject Site Location - highlighted in Purple.

2.2 Surrounding Area

The South Merivale Business Park can be described as partially developed with major buildings and vacant land surrounding the site.

North: The subject site is bounded to the North by 2 parcels of Mixed Industrial designated of vacant land zoned IL9 and IL9 [2707]. IL9 is a Light Industrial Zone, subzone 9 – South Merivale Business Park and exemption 2707 is reserved for future use as development occurs.

South: The subject site is bound to the South by Bill Leathem Road and south of the road is an additional piece of Mixed Industrial designated vacant lands zoned IL9 with an exemption that allows a place of worship, a place of assembly and a community centre as additional uses and prohibits daycares as a use on the site.

West: To the west of the subject site is new 4 storey class A - 146,000 square foot office building with 510 car surface parking spaces and ev charging station.

East: The National Headquarters of the RCMP is located to the east of the subject site. The HQ is a 999,988 square foot office complex consists of eight interconnected buildings surrounded by surface parking lots.

Photo Context



Photo #1 – Looking northward from in front of the RCMP National HQ



Photo #2 – Looking towards the northeast from the southwest corner of the site.



Photo #3 – Looking northward from in front of the RCMP National HQ



Photo #4 – Office building at 61 Bill Leathem Dr immediately west of the site.

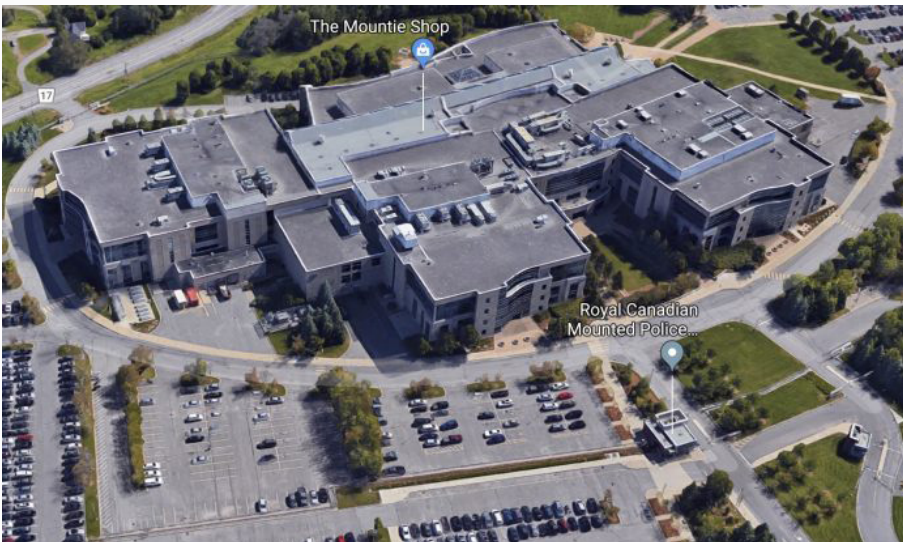


Photo #5 – RCMP Headquarters at 73 Leikin Drive immediately east of the site.

Road Network

The surrounding transportation network is provided as Figure 2. Both Leikin and Bill Leatham Drive are major collector roadways and Beckstead Road which connects Leikin to Merivale Road is classified as a collector.

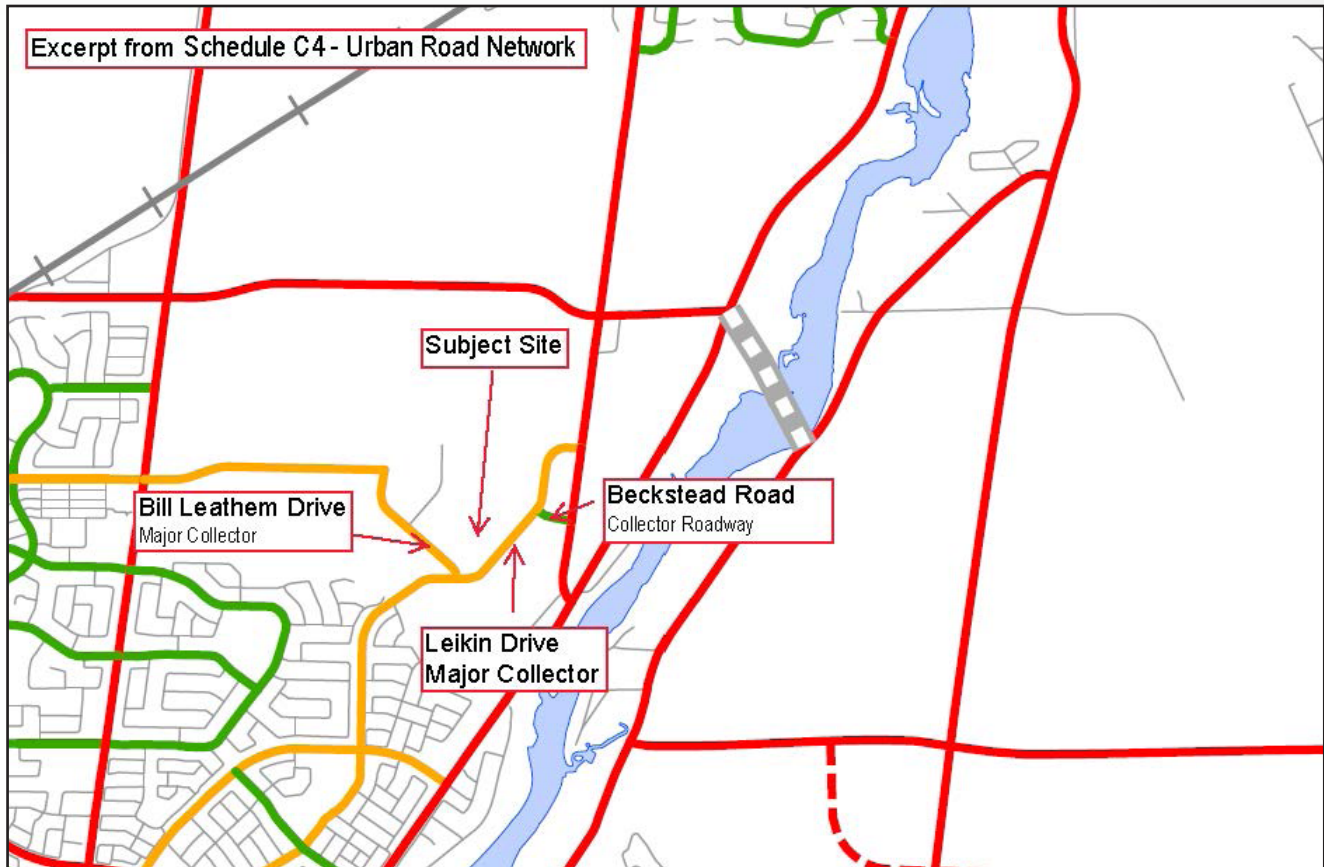


Figure 2 - Road Classifications

Canada Post is proposing a new parcel processing facility of approximately 21,398 square metres (230,326.2 square feet), comprised of 5768 square metres (62,082.4 square feet) of office space and 18,031 square metres (194,072 square feet) of processing and warehouse space. This new facility will incorporate state-of-the-art parcel processing technologies. The site plan shows appropriate loading and parking facilities. The site plan shows approximately 45 high docks, 80 parking stalls for 53' and 60' trailers and approximately 39 parking stalls for 5-ton trailers. The proposed site plan will also support alternative and active transportation including dedicated stalls for accessibility restrained drivers, visitors motorcycle, and bicycle parking. Electric vehicle parking/charging stalls are also indicated on the site plan.

The site plan reserves the far north end of the site for future Canada Post Development. The reserved area is approximately 9,565 sq. m. in size and provides a secondary entrance onto Leikin Drive. Canada Post is reserving this site for its own use which could be for example, a co-located letter carrier depot.

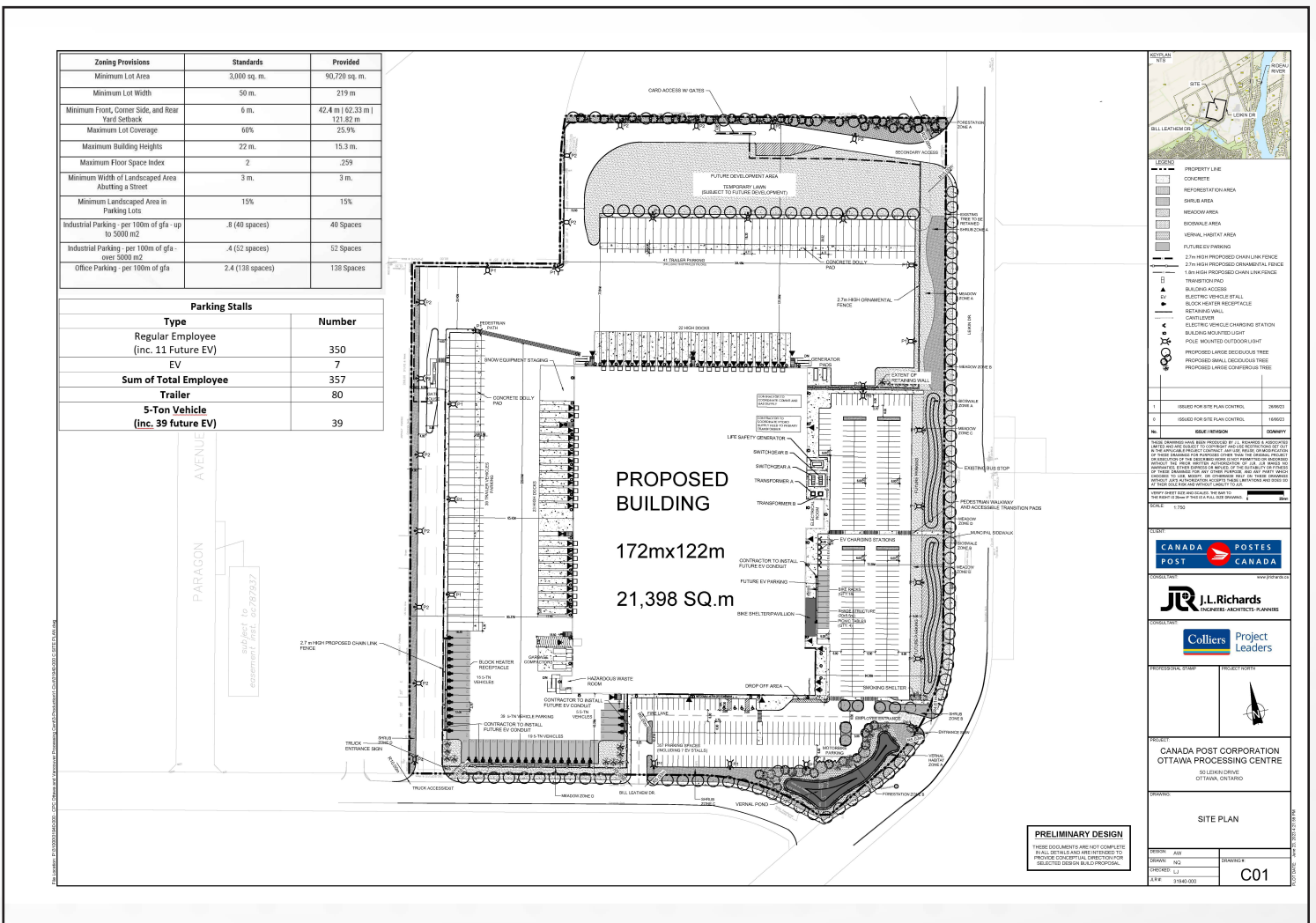


Figure 3 - Proposed Processing Centre Site Plan

3.1 Key Recommendations from Supporting Studies

Airport Noise

The proposed development resides within the 25 NEF/ NEP as illustrated on Schedule C14 - Land Use Constraints Due to Aircraft Noise. The subject site is within Airport Operating Influence Zone.

Official Plan Policy 10.2.2 Protection of airport and aircraft operations requires a noise control study consistent with the Council-approved Environmental Noise Control Guidelines.

Submitted with this rationale is a noise study by Gradient Wind Engineering Inc. who were retained Colliers Project Leaders on behalf of Canada Post Corporation, to undertake an aircraft noise assessment for the industrial development for the subject site.

The noise study found that for aircraft fly-overs, sound exposure will approach the NEF/ NEP 32 which is equivalent to a 24 hour Leq of 64 dBA outside the buildings. The study required that the noise inside the dwellings would need to be reduced to 47 dBA for office / administration area. The study noted that typical commercial windows and curtain wall systems with STC rating of 35 are expected to be sufficient to attenuate aircraft noise.

Implementing the recommendations contained in the report will ensure that the proposed development will comply with all required City of Ottawa and Province of Ontario airport noise requirements.

4.1 Provincial Policy Statement

The Provincial Policy Statement (PPS), issued under Section 3 of the Planning Act (PA), in effect since May 1, 2020, identifies provincial interests in land use planning and development through policy direction. Under the PA, the PPS must be considered by approval authorities in the review of development applications. The proposed site plan was reviewed in relation to the various policies and principles in the PPS.

Policy 1.1.1

Healthy, liveable and safe communities are sustained by:

- promoting efficient development and land use patterns which sustain the financial well-being of the province and municipalities over the long term.
- accommodating an appropriate affordable and market-based range and mix of residential types (including single-detached, additional residential units, multi-unit housing, affordable housing and housing for older persons), employment (including industrial and commercial), institutional (including places of worship, cemeteries and long-term care homes), recreation, park and open space, and other uses to meet long-term needs.
- avoiding development and land use patterns which may cause environmental or public health and safety concerns.
- avoiding development and land use patterns that would prevent the efficient expansion of settlement areas in those areas which are adjacent or close to settlement areas.
- promoting the integration of land use planning, growth management, transit-supportive development, intensification and infrastructure planning to achieve cost-effective development patterns, optimization of transit investments, and standards to minimize land consumption and servicing costs.
- improving accessibility for persons with disabilities and older persons by addressing land use barriers which restrict their full participation in society.
- ensuring that necessary infrastructure and public service facilities are or will be available to meet current and projected needs.
- promoting development and land use patterns that conserve biodiversity; and preparing for the regional and local impacts of a changing climate.

The proposed site plan provides a needed distribution hub which utilizes land designated and zoned for that purpose and supports the economic well-being of the community. It does not cause any environmental or health concerns. The proposed site plan is consistent with direction to create cost effective development patterns.

Policy 1.1.3.1

Settlement areas shall be the focus of growth and development.

The proposed site plan is within the current settlement area and the land proposed is designated and zoned for that purpose.

Policy 1.1.3.2

Requires land use patterns within settlement areas to be based on densities and a mix of land uses which:

- a) efficiently use land and resources.
- b) are appropriate for, and efficiently use, the infrastructure and public service facilities which are planned or available and avoid the need for their unjustified and/or uneconomical expansion.
- c) minimize negative impacts to air quality and climate change and promote energy efficiency.
- d) prepare for the impacts of a changing climate.
- e) support active transportation.
- f) are transit-supportive, where transit is planned, exists or may be developed,
- g) are freight supportive.

The proposed site plan is proposed for lands that are designed and zoned for the purpose proposed. The site plan is appropriate for the public infrastructure and public service facilities in the area and compatible with the existing adjacent uses. The proposed site plan is freight supportive.

Section 1.3 of the PPS provides Policy direction on employment and the development of employment lands. It sets out clear directions to maintain sufficient well-located and well-serviced employment areas.

Policy Section 1.3.1:

Planning authorities shall promote economic development and competitiveness by:

- providing for an appropriate mix and range of employment, institutional, and broader mixed uses to meet long-term needs.
- providing opportunities for a diversified economic base, including maintaining a range and choice of suitable sites for employment uses which support a wide range of economic activities and ancillary uses, and take into account the needs of existing and future businesses.
- facilitating the conditions for economic investment by identifying strategic sites for investment, monitoring the availability and suitability of employment sites, including market-ready sites, and seeking to address potential barriers to investment.
- ensuring the necessary infrastructure is provided to support current and projected needs.

The proposed site plan is consistent with this Policy section and the approval of this plan will allow the construction of a facility which will support the applicants market competitiveness.

Section 1.3.2.6:

Planning authorities shall protect employment areas in proximity to major goods movement facilities and corridors for employment uses that require those locations.

The proposed site plan is located in a settlement area that is designed and zoned for employment and major goods movement facilities.

Policy Section 1.8.1:

Planning authorities shall support energy conservation and efficiency, improved air quality, reduced greenhouse gas emissions, and preparing for the impacts of a changing climate through land use and development patterns which:

- focus freight-intensive land uses to areas well served by major highways, airports, rail facilities and marine facilities.
- promote design and orientation which maximizes energy efficiency and conservation, and considers the mitigating effects of vegetation and green infrastructure; and
- maximize vegetation within settlement areas, where feasible.

The proposed site plan is located on lands designed for freight intensive uses. The buildings proposed in via the site plan will include significant environmental sustainability components including the integration of the Canadian Green Building Council (CaGBC) Zero Carbon Building Design Standard, high efficiency heating/cooling, high performance envelope, and roof-mounted PV array. The proposed landscaping includes tree and shrubs planting as well as meadows, naturalized bioswales and 2 vernal ponds.

As demonstrated the proposed site plan is consistent with the direction of the relevant policies of the Provincial Policy Statement.

4.2 City of Ottawa Official Plan (2021)

The City of Ottawa's new Official Plan was adopted by Ottawa City Council on November 24, 2021. Under the new Official Plan, the Subject Site is designated as Mixed Industrial as shown on Schedule B6 – Suburban (West) Transect of the new Official Plan, Figure 6, below.

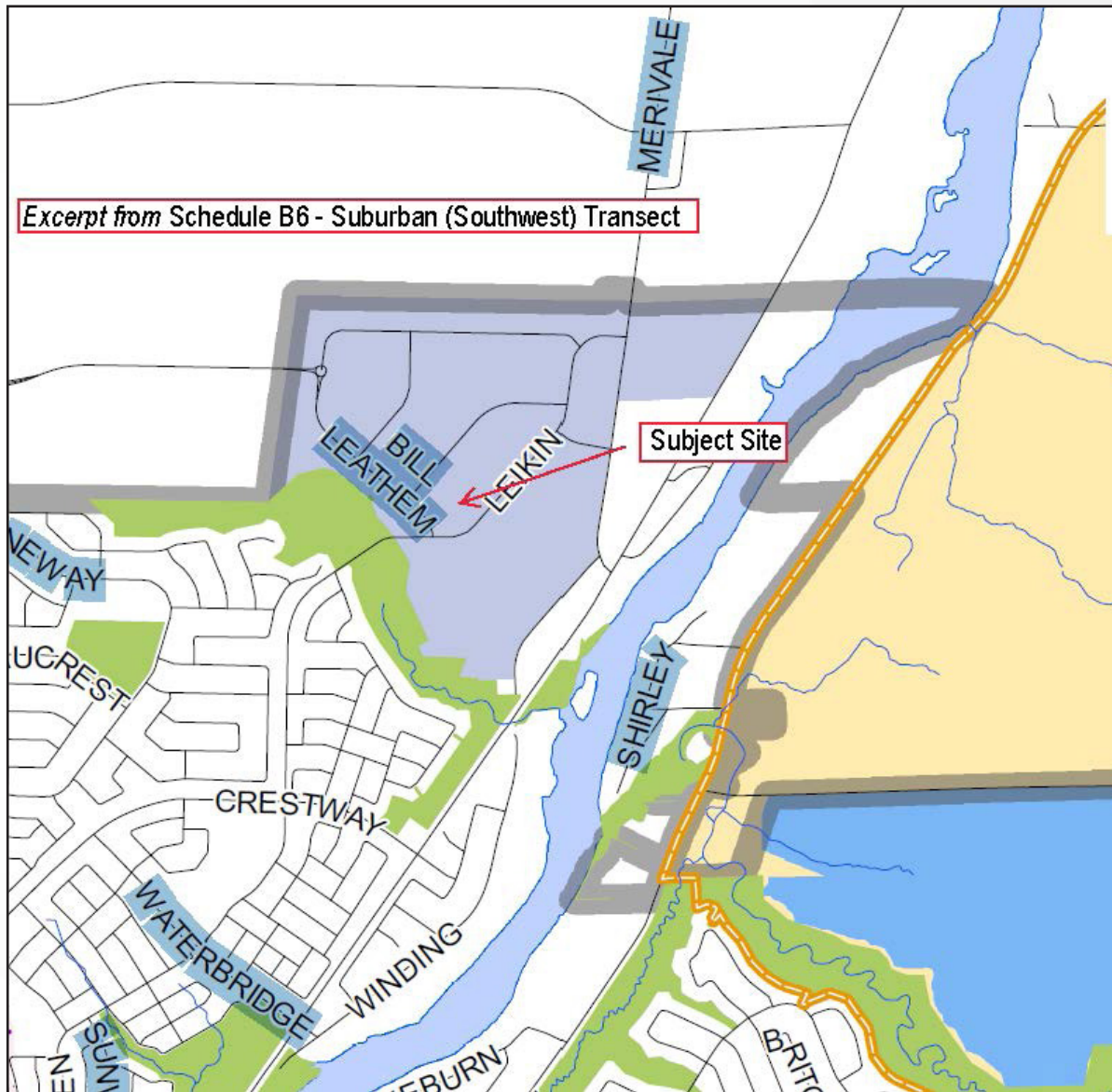


Figure 4 - Excerpt from Schedule B5 - Suburban (West) Transect

The Official Plan designates Mixed Industrial areas as clusters of economic activity that are less impactful and provide a broader range of non-residential uses than Industrial areas.

Policy 6.5.1.2 states that Major office, Small-scale office (less than 107,639 square feet), warehousing, distribution and storage are permitted uses in the Mixed Industrial designation. The proposed site plan conforms to the requirements of the Official Plan.

The Subject Site is currently zoned IL9 – Light Industrial Zone, subzone 9 – South Merivale Business Park – under the City of Ottawa’s Zoning By-law 2008-250.

The purpose of the IL – Light Industrial Zone is to permit a wide range of low impact light industrial uses, as well as office and office-type uses in a campus-like industrial park setting, in accordance with the Employment Area designation of the Official Plan or, the General Urban Area designation where applicable.

In the IL9 zone office, warehouse (associated with a permitted use), truck terminal are all permitted uses subject to meeting the subzone provisions.

IL9 Zoning Provisions

Zoning Provisions	Standards	Provided
Minimum Lot Area	3,000 sq. m.	90,720 sq. m.
Minimum Lot Width	50 m.	219 m
Minimum Front, Corner Side, and Rear Yard Setback	6 m.	42.4 m 62.33 m 121.82 m
Maximum Lot Coverage	60%	25.9%
Maximum Building Heights	22 m.	15.3 m.
Maximum Floor Space Index	2	.259
Minimum Width of Landscaped Area Abutting a Street	3 m.	3 m.
Minimum Landscaped Area in Parking Lots	15%	15%
Industrial Parking - per 100m of gfa - up to 5000 m2	.8 (40 spaces)	40 Spaces
Industrial Parking - per 100m of gfa - over 5000 m2	.4 (52 spaces)	52 Spaces
Office Parking - per 100m of gfa	2.4 (138 spaces)	138 Spaces

Based on the analysis presented in this report, it is concluded that the proposed development is appropriate for the lands, builds upon and enhances the existing assets of the community, and is compatible with its surroundings. Furthermore, the development is consistent with the intent of the applicable policy and regulatory documents and is supported by the technical studies submitted as part of this application.

Submitted By:

Peter Hume

Peter Hume
Chief Strategist
HP Urban

National Capital Commission Federal Design Approval



APPROVAL LETTER

UNCLASSIFIED

1. FILE INFORMATION

File: CP2299-24888 **IAMIS:** 24888 **Approval Date:** 2023-MAR-16

Project Name: Canada Post Processing Facility Site Preparation, 50 Leikin Drive, Ottawa

Approval Level: 2

Fees: Yes

2. PROPONENT

Robert Loyst
Director, Project Delivery
Corporate Real Estate, Canada Post Corporation
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Ottawa, Ontario
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Email: robert.loyst@canadapost.ca

This person represents the Proponent Authority and has accountability for compliance with the terms and conditions of this Approval.

3. CONTACT LIST

NCC – Regulator

Federal Approvals Manager (main contact)	Nicole Howard	nicole.howard@ncc-ccn.ca
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Federal Approvals Director	Isabel Barrios	isabel.barrios@ncc-ccn.ca
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NCC – Advisors

Archaeology Program	Ian Badgley	archaeology-archeologie@ncc-ccn.ca
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Proponent

Colliers Project Leaders – Senior Project Manager	Luc Fréchette	luc.frechette@colliersprojectleaders.com
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4. PROJECT DESCRIPTION

The National Capital Commission's (NCC) Federal Approvals Division (the "Regulator") has received and reviewed a proposal for the site preparation for a Canada Post processing facility at 50 Leikin Drive, Ottawa (the "Proposal") as shown in the approved documents and drawings listed in Section 5 (below) submitted to the Regulator for review and approval by Colliers Project Leaders on behalf of Canada Post Corporation (the "Proponent").

Project Context and Objectives

A proposal for a new Canada Post processing facility to service the growing needs of Canadians is in the early planning stages and will be submitted to the Regulator for review and advice in fall 2023 prior to consideration by the NCC's Board of Directors. The project will consist of a new facility at 50 Leikin Drive, Ottawa, Ontario.

This approval consists of is site preparation (soil and tree removal) works to enable construction of the new facility in the near future.

Land Uses

9-hectare land parcel located within the South Merivale Business Park

Primary land use: light industrial

Secondary land use(s): offices

Ground floor level land use(s): primary building access, parking, warehouse, loading areas applicable].

Temporary land use(s): n/a

Project Scope

The current scope of work includes the removal of the topsoil and upper 1-2 metres of soft clays across the entire site to avoid long term settlement and cracking of the future building slab, loading dock aprons and parking lot. The soil / clay will be replaced with imported granular structural fill which will be placed and compacted in 300 metre lifts. This represents a volume of approximately 118,000 cubic meters of sub-excavation / disposal and replacement with the same quantity of imported structural fill.

In addition to soil removals, nine out of the existing 18 trees on the site are proposed to be removed. These trees will be compensated through proposed future plantings following these ratios: 1:1 ratio for dead trees, 2:1 ratio for DBH between 10 cm and 30 cm and 3:1 ratio for larger caliper trees. Overall, 80 new trees are proposed and will be confirmed in the next phase. The site preparation work is estimated to take five (5) to seven (7) months and is anticipated to start in mid-March 2023 with clearing of the vegetation to mitigate the risk of impact on wildlife, and then extend into early fall 2023.

5. SUBMITTED/REVIEWED DOCUMENTATION

- 2022-SEP – Phase I Environmental Site Assessment, Wood
- 2022-SEP – Phase II Environmental Site Assessment, Wood
- 2022-DEC – Preliminary Hydrogeological Investigation, WSP
- 2023-JAN-09 – Ottawa Plant Conceptual Site Plan
- 2023-JAN-12 – Federal Wetland Policy Review, WSP
- 2023-JAN-12 – Natural Environment Summary, WSP

- 2023-JAN-31 – Tree Conservation Report, WSP
- 2023-JAN – Draft CPC-OPC Site Preparation Specifications
- 2023-FEB-01 – Preliminary Stormwater Management – Memo, J.L.Richards
- 2023-FEB-01 – Soil Management Plan, WSP
- 2023-FEB-03 – Traffic Impact assessment – Scoping Report, CGHT Transportation
- 2023-FEB-02 – 2158-01 Canada Post (Landscape) Concept, CSW Landscape Architecture
- 2022-MAR-05 – CPC OPC General Design Overview

6. APPROVAL FRAMEWORK

NCC Plans:

- Plan for Canada’s Capital, 2017–2067 (2017)

NCC Strategies, Policies and Guidance Documents:

- N/A

Other Federal Plans:

- N/A

Other Federal Strategies, Policies and Guidance Document:

- N/A

National Interest Land Mass (NILM):

- No

Advisory Committees:

- N/A

Pre-Contact Archaeological Potential:

- Low

Consultations:

- N/A

Historic Archaeological Potential:

- Low

Heritage Designation(s):

- N/A

Board Approval Date:

- Approved by the Executive Committee of the / Board of Directors on 2023-MAR-07

Federal Environmental Impact

Analysis/Determination: Yes

- The project was assessed under the provisions of sections 81-91 of the *Impact Assessment Act* (IAA). The NCC has determined that project is not likely to cause any significant adverse environmental effects and may proceed, provided that all mitigation measures listed in the Environmental Effects Evaluation (EEE) (dated 2023-FEB-21) are implemented.

7. RELATED APPROVALS AND AGREEMENTS SIGNED

- N/A

8. APPROVAL STATEMENT

FEDERAL DESIGN APPROVAL FOR THE CANADA POST PROCESSING FACILITY SITE PREPARATION, 50 LEIKIN DRIVE, OTTAWA IS HEREBY GRANTED, PURSUANT TO THE NATIONAL CAPITAL ACT – SECTION 12, SUBJECT TO THE FOLLOWING CONDITIONS:

9. APPROVAL CONDITIONS

9.1 GENERAL CONDITIONS

A set of general conditions is presented below to frame the nature, terms, and limits of this Approval.

9.1.1. The Proponent must comply with all the terms and conditions of this approval. The NCC reserves the right to revoke this approval in case of non-compliance.

9.1.2. This approval is for, and limited to, the project scope described above and shown in the plans and documents listed in Section 5. The Proponent must complete all works in accordance with the approved plans and documents. Any modifications to the project must be submitted to the Regulator for review and approval prior to construction in a timeline acceptable to the Regulator.

9.1.3. The Proponent is to evolve the subsequent project phase(s) in consultation with the Regulator.

9.1.4. Subsequent project phase(s) must be consistent with the terms and conditions of this approval and responsive to the recommendations of the Regulator.

9.1.5. The NCC, in approving any plans, drawings and/or specifications, is not making any representations, nor is the NCC undertaking any responsibility of engineering, architectural or landscape architectural nature; the Proponent hereby assumes all such liability and risk.

9.1.6. The Proponent must satisfy the requirements of all other regulatory authorities and comply with all applicable laws and regulations related to the design and construction of the Proposal.

9.1.7. The Proponent must notify the Regulator in writing at the following milestones:

- a) Start of Site Preparation Mobilization
- b) Substantial Completion

9.1.8. The Proponent must initiate the implementation of this Proposal within 24 months of the date of this approval unless otherwise authorized in writing by the Regulator. A new application for review and approval by the NCC will be required after this period prior to construction.

9.1.9. The Regulator will follow up on the implementation of approval conditions during the construction period as required, this process may include project audit and site visits. The Regulator

will notify the Proponent prior to any site visit. The Proponent is responsible for demonstrating compliance with the approval conditions and/or providing the requested documentation in a timely manner to the satisfaction of the Regulator.

9.1.10. The Proponent must submit a 'Notice of Completion' (see last page of this approval) signed by the Proponent attesting that the works have been executed as per the approved plans and documents and that all of the conditions of this approval have been met. This document must be submitted to the Regulator one (1) month following the substantial completion of works.

9.1.11. No further NCC approvals will be granted for this site until all the conditions contained herein have been met.

9.2 PLANNING AND DESIGN CONDITIONS

The conditions below provide guidance to the Proponent for the subsequent phases of planning and design development of the Proposal and identify the information required by the Regulator.

9.2.1. The next phase shall address the challenges and opportunities of the site and include at a minimum:

- a) Executive summary
- b) Planning vision
- c) Accommodation requirements
- d) Municipal and stakeholder requirements
- e) Environment and sustainability strategies
- f) Proposed land uses, site and building massing, building heights, architectural and urban design guidelines, views, access and mobility (traffic, transit, connectivity), parking, universal accessibility, weather integration strategies (prevailing winds, sun/shadows, rain and snow) civil works, green spaces and landscape, physical security, public realm, servicing and material handling

9.2.2. The Proponent shall integrate the following recommendations in the Proposal prior to applying for NCC approval of the next phase:

- a) Noble and durable materials for building envelope and public areas
- b) Concrete of architectural quality for all areas exposed to view
- c) Architecturally exposed structural steel (AESS) standards
- d) Inconspicuous mechanical equipment well-integrated with the roofline
- e) Site amenities for employees in close proximity from entrances and fully integrated with the site landscape
- f) Sustainable and reconciled interior and exterior lighting design well integrated with the architecture and landscape
- g) Full-cut-off lighting fixtures
- h) GBA+ Principles

- i) CSA-B51 accessible design standards
- j) A landscape design that provides a comfortable and attractive environment for users in all seasons
- k) Winter-adapted vegetation, with particular regard for salt and drought tolerant species adapted to urban settings near roadways
- l) Landscape design that incorporates Indigenous perspectives
- m) Signage design integrated with the materials, details, and finishes of the building and its setting
- n) The design of all exterior signage is to be bilingual
- o) Back-lit 'box' signage is not permitted

9.2.3. A Transportation Impact Assessment and Transportation Demand Management Study is to be prepared to inform subsequent planning and design phases of the Proposal, notably:

- a) Modal split target and parking requirements
- b) Planning, design, and operational strategies to manage the transportation demand, meet the modal split target, and accommodate the anticipated pedestrian and vehicular movement.

9.2.4. In consultation with the Regulator, the Proponent must prepare and submit for NCC approval of the next phase the following:

- a) Complete sets of drawings at 60%, 90% and 100%, including but not limited to: location plan, site plan, architectural drawings, civil drawings, landscape drawings, exterior lighting and signage drawings, public art and interpretation drawings, labelled material samples, and mock-ups.
- b) 3D Renderings
- c) Energy Model Report (for information)
- d) Zero Carbon Building Scoring Plan (for information)
- e) A summary report demonstrating how the sustainability objectives/goals of this project will be achieved (for information)
- f) Description and location of mechanical, electrical and security equipment located on grade
- g) Indication of Rights-of-Way (ROWs) and clearances
- h) Construction Traffic Management Plan (eg, local closures)
- i) Soil erosion and sediment control plan

9.2.5. The Proponent must submit to the Regulator for validation prior to removal of the topsoil, the following:

- a) Traffic Management Plan (eg, local closures, detours)
- b) Pedestrian movement management plan for the construction site area (including all required signage), ensuring all public sidewalks have equivalent space on the same side of the street if existing sidewalks need to be occupied by construction activity
- c) Cycling movement management plan for the construction site area (including all required signage), ensuring that any protected or separated cycling facility has equivalent space on the same side of the street if an existing cycling lane or facility needs to be occupied by construction activity

- d) Construction vehicle and contractor vehicle parking management plan, specifically ensuring that contractors do not park their personal vehicles on any public sidewalk, public plaza, park or any other pedestrian-accessible public space in a way that disrupts pedestrian movement and desire lines. The Proponent shall require all contractors and sub-contractors to sign a site-specific parking management agreement and shall be responsible and accountable for the enforcement thereof.
- e) Temporary and permanent relocation plans for municipal traffic lights, signs, and controllers (if any are impacted)
- f) Temporary signage plans and drawings
- g) A copy of all outstanding documents identified in the approved Impact Assessment Act EEE (signed by NCC on 2023-FEB-21), no later than five (5) business days prior to construction start
- h) Schedules for all works

9.3 CONSTRUCTION CONDITIONS

The conditions below identify the requirements during project implementation and commissioning.

9.3.1. Unforeseen site conditions requiring any deviation from the approved documents must be resolved in consultation with, and to the satisfaction of the Regulator prior to implementation. The Proponent must contact the Regulator at the shortest delay.

9.3.2. Existing trees to remain (including crown, trunk, and root system) are to be protected during all stages of work. No material, construction equipment, or vehicles are to be stored under the drip line of trees at any time. Any disturbed vegetation or landscaping will be repaired or replaced without delay to the satisfaction of the Regulator.

9.3.3. The Proponent shall install temporary bilingual construction signage informing the public and site users of the project. The signage must be removed following the completion of the project. Backlit, billboard and digital signage are not permitted on the hoarding, cranes or visible from the outside.

9.3.4. Third party advertising outside or visible from the outside is strictly prohibited.

9.3.5. The Proponent must undertake traffic management measures to ensure the safety of pedestrians and cyclist traffic during construction.

9.3.6. The Proponent must ensure that complete road closures are minimized and that pedestrian and cyclist traffic on adjacent streets is maintained at all times with adequate public safety protection and bilingual signage.

9.3.7. Traffic detours required during construction must accommodate universal accessibility.

9.3.8. The Proponent is to prevent damage to adjacent lands and assets. The Proponent will be responsible to repair at the Proponent's expense and without delay, any damage caused by the construction and warranty-related work associated with the Proposal.

9.3.9. The Proponent is responsible for the implementation of all the mitigation measures identified in the EEE (signed by NCC on 2023-FEB-21)]. The Proponent shall provide the EEE to the project Contractor and the mitigation measures have to be integrated into the project specifications.

9.3.10. The Proponent is responsible for compliance with relevant environmental regulations regarding disposal of material.

9.3.11. If any archaeological resources or human remains are discovered during excavations/construction, all works at the location concerned must be halted immediately and the NCC Archaeology Program must be notified immediately at archaeology-archeologie@ncc-ccn.ca. A professional archaeologist must be deployed on site by the Proponent to assess the discovery and to recommend measures for the protection of those resources or remains. A copy of the recommendations should be forwarded immediately to the NCC Archaeology Program for validation. Work shall not be resumed at the location concerned until the NCC Archaeology Program has received written confirmation that the validated measures have been put in place.

9.4 POST CONSTRUCTION CONDITIONS

The conditions below will continue to apply after project implementation.

9.4.1. The Proponent must maintain in state of good repair all infrastructure introduced by this Proposal in perpetuity.

9.4.2. All plant material retained as part of the Proposal is to be maintained by the Proponent in perpetuity, in order to ensure the greening, the healthy appearance and the full implementation of the proposed landscape. The Proponent must ensure the maintenance of all plant material in accordance with standard horticultural practice and the replacement of dead or diseased material at the earliest possible time. This maintenance shall be carried out on a regular basis, in all seasons, every year.

9.4.3. The Proponent is responsible for winter maintenance within the identified area owned by Canada Post at 50 Leikin Drive. The Proponent shall coordinate and mitigate with the local municipality any impacts to the sidewalk, including snow and ice control operations.

10. NCC REVIEW ANALYSIS

The Regulator has considered the following:

Planning

- The Proposal is located on federal lands owned by Canada Post Corporation.
- The federal lands subject to this Proposal are not part of the National Interest Land Mass (“NILM”).
- The Proposal is compatible with the strategic framework / policies of the Plan for Canada’s Capital, 2017–2067 (2017) including the following goals / policies:
 - Thriving and Connected: Support a liveable, attractive, resilient, accessible and economically competitive Capital Region.
- The Proposal will not affect protected views, as identified in Canada’s Capital Views Protection (2007):
 - The site is not located within a protected viewshed.

Design

- N/A

Heritage and Archaeology

- The Proposal is consistent with the NCC Policy and Procedures on Management of Heritage and Archaeology Assets.
- Archaeological assessment was undertaken, no project specific mitigation measures were required.

Environment and Sustainability

- The Federal Sustainable Development Strategy objectives have been met, as follows:
 - Wetland compensation was integrated into the design.
 - Re-creating pollinator habitat.
 - Sustainable design strategies with a focus on climate resiliency by designing storm water infrastructure that will assist to manage rainwater in extreme weather events.
 - Appropriate management of known site contamination was applied.
- Negative effects to migratory birds was avoided by planning vegetation removal prior to the active bird nesting season.

Coordination/ Consultation

- Internal NCC, federal, and municipal public consultation and reconciliation of requirements were needed and achieved.

Federal Approval Process and Timelines

- The FLUDTA process was tailored to fit project complexity.
- FLUDTA milestones were met on time.
- NCC advice was sought and followed.

11. ACCESS TO INFORMATION ACT

The Proponent acknowledges and understands that the NCC is subject to the provisions of the *Access to Information Act* and may, as a result of a request under that Act, be required to release this Approval, or any other documents arising out of, or related to, this Approval.

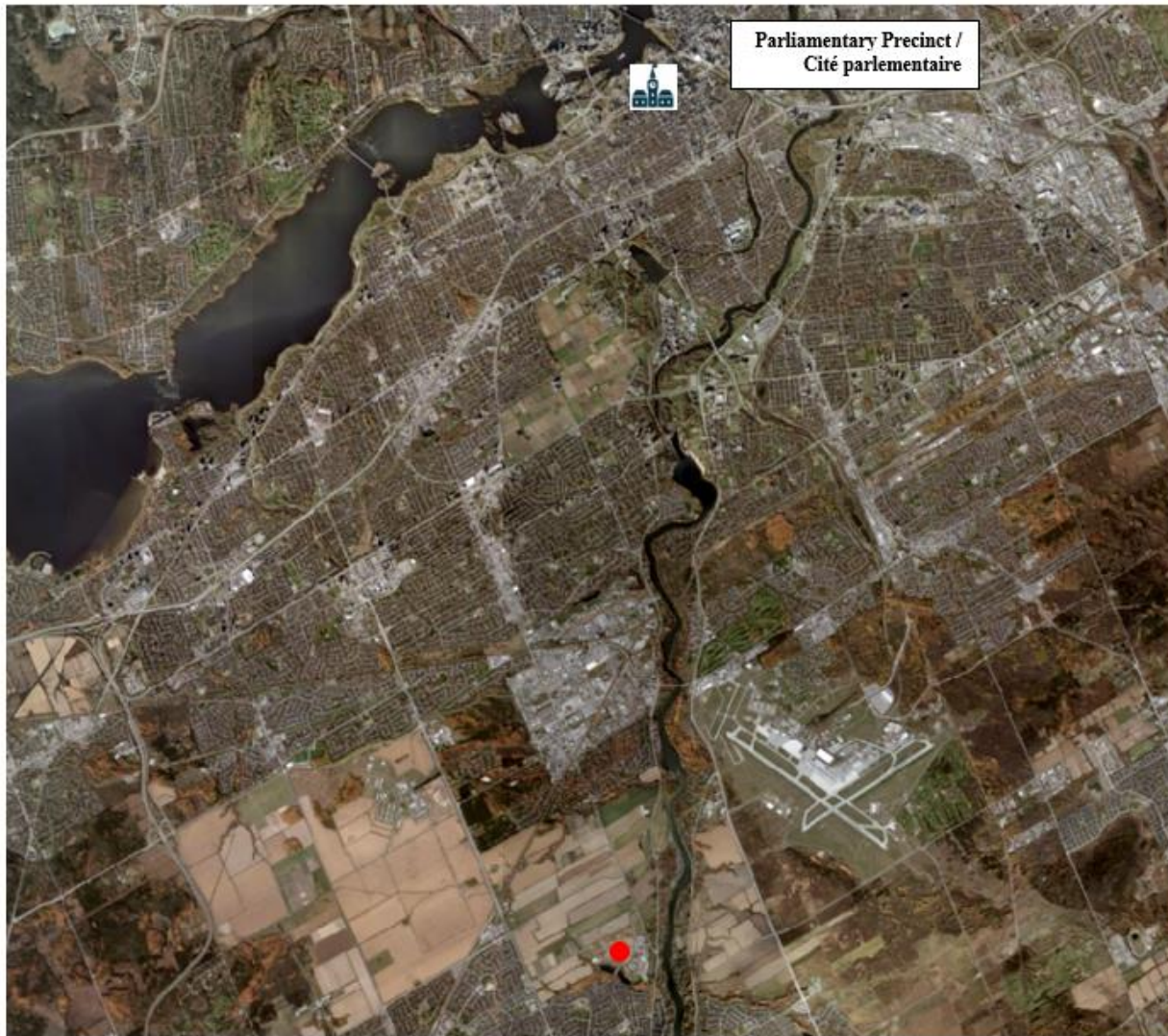
12. AUTHORIZED SIGNATURE (NCC)

<p>DocuSigned by:  8E8319D91759427... Alain Miguelez Vice-President, Capital Planning</p>	<p style="text-align: center;">2023-Mar-16 _____ DATE (YYYY-MMM-DD)</p>
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13. APPENDICES

The documents and drawings identified in the attached appendices are to be read with and form part of this NCC Approval.

Appendix	Title	Date	Provided By
1	Location Plan	2023-FEB-21	NCC
2	Ecological Land Classification Map (wetland area to be compensated and Trees #6-18 along the road to remain)	2023-JAN-12	Proponent
3	EEE Signature Page	2023-FEB-21	Proponent



Appendix 2 Ecological Land Classification Map (wetland area to be compensated and Trees #6-18 along the road to remain) 2023-JAN-12 Proponent



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Rationale:

Canada Post and the National Capital Commission have determined that the proposed project, the new Canada Post Processing Centre, Ottawa ON, is not likely to cause significant adverse environmental effects. This determination was based on a consideration of the following factors: 1. impacts on rights of Indigenous peoples, and Indigenous knowledge; 2. community knowledge; 3. comments received from the public, and 4. technically and economically feasible mitigation measures.

Canada Post and the National Capital Commission are satisfied that the carrying out of the project is not likely to cause significant adverse environmental effects given the identified mitigation measures. Therefore, the proponent Canada Post may carry out the project in whole or in part.

Section H: Signatures and Approval of Project Determination

Note: After making an environmental effect determination, authorities must post their Notice of Determination on the Registry no earlier than 30 days after posting their Notice of Intent (see Step 2 of the Guidance document). As outlined in the IAA, the Notice of Determination must also include information about any mitigation measures taken into account by an authority when making the determination (ss. 86(2)). Comments: Canada Post Corporation and the NCC have agreed that the project is not likely to cause significant adverse environmental effects and required mitigation measures described in this EEE are satisfactory for the first phase of the project involving the site preparation works in 2023. Where identified in this EEE, potential environmental effects and required mitigation measures will be reviewed again once the detailed drawings and specifications are available from the Design-Builder in 2024 for the second phase of the project.

Form completed by CIMA+:

	Valérie Bédard	2023/02/22
Signature	First and last name	Date

Form endorsed by Owner's Representative - Colliers:

	Luc Fréchette	2023/02/21
Signature	First and last name	Date

Approved by Canada Post Corporation:

 <small>Digitally signed by Allison Rogers Date: 2023.02.21 11:51:55 -05'00'</small>	Allison Rogers	2023/02/21/dd
Signature	First and last name	Date

Approved by Canada Post Corporation:

	Robert Loyst	2023/02/21
Signature	First and last name	Date

Approved by National Capital Commission:

 <small>DocuSigned by: Catherine Tardy Laporte E8550B1498DE442...</small>	Catherine Tardy Laporte	2023-Feb-23
Signature	First and last name	Date

NOTICE OF COMPLETION FORM

This page must be sent to the NCC's Director of Federal Approvals and Heritage, and Archaeology Programs (FApproval@ncc-ccn.ca) within **one (1) month** following the substantial completion of works set out in the attached Federal Approval (CP2299-24888, IAMIS 24888).

By signing below, I attest that all works related to the approved project, have been executed as per the approved plans and that all of the conditions have been satisfied.

NAME OF PROPONENT (PRINT)

SIGNATURE OF PROPONENT

DATE (YYYY-MM-DD)

TITLE

ORGANIZATION

PHONE #

E-MAIL

Once completed, please send to:

FApproval@ncc-ccn.ca