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Recreation
Community &
Residential
Commercial &
Institutional
Environmental
Restoration

2750 & 2760 Sheffield Road, 2713 Lancaster Road, 2865 F Walkley Road and two unaddressed parcels

Planning Rationale



Prepared for: Richcraft Properties Ltd.

**2750 & 2760 SHEFFIELD ROAD, 2713 LANCASTER ROAD, 2865 F
WALKLEY ROAD AND TWO UNADDRESSED PARCELS**

**PLANNING RATIONALE
IN SUPPORT OF A
SITE PLAN CONTROL APPLICATION**

Prepared By:

NOVATECH
240 Michael Cowpland Drive
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May 16, 2023

Novatech File: 122003
Ref: R-2023-075

May 16, 2023

City of Ottawa
Planning, Real Estate and Economic Development
110 Laurier Avenue West, 4th Floor
Ottawa, ON K1P 1J1
By email only: tracey.scaramazzino@ottawa.ca

Attention: Tracey Scaramazzino, Planner II

**Reference: Site Plan Control Application
2750 & 2760 Sheffield Road, 2713 Lancaster Road. 2865 F Walkley Road and
two unaddressed parcels
Our File No.: 123060**

Novatech has prepared this Planning Rationale on behalf of Richcraft Properties Ltd. to support a Site Plan Control application on a number of properties between Sheffield and Lancaster Roads.

Richcraft Properties Ltd. proposes to demolish part of an existing warehouse at 2760 Sheffield Road and construct a new 10,374m² light industrial building. Access will be alongside the existing building at 2713 Lancaster Road and parking associated with the new building is located on the two adjoining unaddressed parcels.

Sincerely,

NOVATECH



James Ireland, MCIP, RPP
Project Planner – Planning & Development

Table of Contents

1.0	INTRODUCTION	1
2.0	SITE DESCRIPTION AND SURROUNDING USES	1
3.0	DEVELOPMENT PROPOSAL	4
4.0	PLANNING ASSESSMENT	6
4.1	Provincial Policy Statement, 2020	6
4.2	City of Ottawa Official Plan	11
4.3	City of Ottawa Zoning By-law 2008-250	16
5.0	CONCLUSION	17

1.0 INTRODUCTION

Novatech has prepared this Planning Rationale on behalf of Richcraft Properties Ltd. to support a Site Plan Control application on a number of properties between Sheffield and Lancaster Roads being 2750 & 2760 Sheffield Road, 2713 Lancaster Road, 2865 Walkley F Road and two unaddressed parcels. Together these are the Subject Site.

Richcraft Properties Ltd. proposes to demolish part of an existing warehouse 2760 Sheffield Road and construct a new 10,564m² light industrial building across two parcels (2760 Sheffield Road and 2865 Walkley F Road). Access will be alongside the existing building at 2713 Lancaster Road and parking associated with the new building is located on the two adjoining unaddressed parcels.

This Planning Rationale describes the Subject Site and surrounding context, details the proposed Site Plan, and demonstrates that it is consistent with the Provincial Policy Statement (2020), conforms to the City of Ottawa Official Plan (2022), and complies with the City of Ottawa Zoning By-law 2008-250.

2.0 SITE DESCRIPTION AND SURROUNDING USES

The Subject Site comprises six parcels totalling 8.45 ha shown on the map below and in the following table.

To the **north** of the Subject Site is a trucking company with access from Sheffield Road and a multi-tenant light industrial / logistics developments with access from Humber Place. Also to the north is the continuation northwards of the former railway. The tracks were removed from this section around 2018.

To the **east** across Sheffield Road is a Bell communications tower with an associated service building, and two building material companies in smaller light industrial buildings with large yards. Further east are Ramsay Creek, a railway and Highway 417.

To the **south** is an equipment rental company along with the continuation southwards of the former railway. The tracks were removed from this section around 2018.

To the **west** are office and light industrial buildings fronting on Lancaster Road. Further west is a low-rise residential neighbourhood.

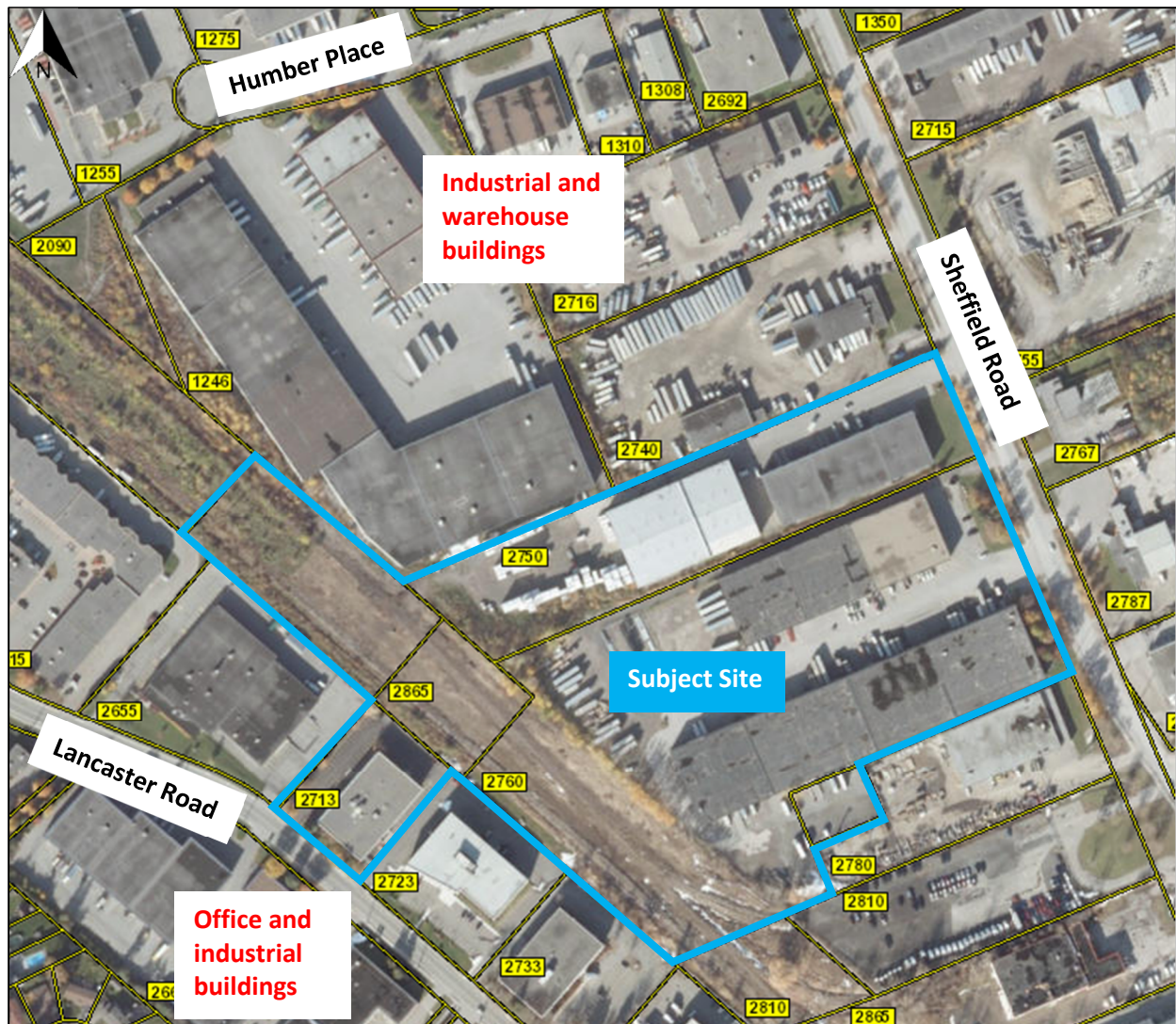


Figure 1: Subject Site and Surrounding Area

Municipal Address	Legal description	Current Development / Use	Area
2750 Sheffield Road	PT LTS 24 & 25, CON 3OF , PTS 1, 2 & 3 , 5R2860 ; S/T CT101558 OTTAWA	Two warehouse buildings	1.88 ha
2760 Sheffield Road	PART LOTS 24 & 25, CONCESSION 3OF , PARTS 7 & 8 , 5R4921 ; S/T NS82953 ; S/T CT166911 OTTAWA; PART LOTS 24 & 25, CONCESSION 3OF , PARTS 5 & 6 , 5R4921 , T/W NS82954 ; S/T N723466,OT74613 OTTAWA; PART OF LOTS 24 AND 25, CONCESSION 3, OTTAWA FRONT (GLOUCESTER) PART 4 ON PLAN 4R-34045 AND PART OF LOT 25 CONCESSION 3, OTTAWA FRONT (GLOUCESTER) PART 5 ON PLAN 4R-34045, TOGETHER WITH AN EASEMENT OVER PARTS 2, 4 TO 7, 9, 15, 17, AND 19 TO 22 ON PLAN 4R-16285 AS IN LT1351640; TOGETHER WITH AN EASEMENT OVER PART LOT 24 CONCESSION 3 OTTAWA FRONT (GLOUCESTER), BEING PART 20 4R-33542 AS IN OC2321552; TOGETHER WITH AN EASEMENT OVER PART LOTS 24 AND 25, CONCESSION 3, OTTAWA FRONT (GLOUCESTER), BEING PART 14, 4R33542 AS IN OC2327032; TOGETHER WITH AN EASEMENT OVER PART LOT 25, CONCESSION 3 OTTAWA FRONT (GLOUCESTER), BEING PART 12, 4R-33542 AS IN OC2335588; TOGETHER WITH AN EASEMENT OVER PART LOT 24, CONCESSION 3, OTTAWA FRONT, GLOUCESTER, PARTS 16 AND 18, PLAN 4R-33542 AS IN OC2335588 SUBJECT TO AN EASEMENT OVER PARTS 5 & 6 PLAN 5R4921 IN FAVOUR OF PART LOT 24 CONCESSION 3OF PARTS 1,2,3 & 4 ON 5R4921 SAVE AND EXCEPT PART 1 ON 4R25599 AS IN OC1292539 SUBJECT TO AN EASEMENT OVER PART 5, PLAN 4R34045 IN FAVOUR OF PART LOT 25, CONCESSION 3, PARTS 1,2,3, PLAN 4R34045 AS IN OC2410797 SUBJECT TO AN EASEMENT OVER PART 5, PLAN 4R34045 IN FAVOUR OF PART LOT 25, CONCESSION 3, PARTS 1,2,3, PLAN 4R34045 AS IN OC2410800 TOGETHER WITH AN EASEMENT OVER PART LOTS 24 AND 25, CONCESSION 3, PARTS 12,14,16,18 AND 20, PLAN 4R33542 AS IN OC2410800	Two warehouse buildings	4.72 ha
2713 Lancaster Road	PCL A-15, SEC 4M-121 ; PT BLK A, PL 4M-121 , PART 5 , 4R6646 , T/W LT601576; T/W LT496170 ; OTTAWA/GLOUCESTER	Two storey office building	0.48 ha
2865 Walkley F Road	ART OF LOT 25, CONCESSION 3, OTTAWA FRONT, GLOUCESTER, PARTS 2 AND 3, PLAN 4R-34045; TOGETHER WITH AN EASEMENT OVER PARTS 2, 4 TO 7, 9, 15, 17, AND 19 TO 22, PLAN 4R-16285 AS IN LT1351640 ; TOGETHER WITH AN EASEMENT OVER PART LOT 24, CONCESSION 3, OTTAWA FRONT, GLOUCESTER, PART 20, PLAN 4R-33542 AS IN OC2321552 ; TOGETHER WITH AN EASEMENT OVER PART LOTS 24 AND 25, CONCESSION 3, OTTAWA FRONT, GLOUCESTER, PART 14, PLAN 4R-33542 AS IN OC2327032 ; TOGETHER WITH AN EASEMENT OVER PART LOT 25, CONCESSION 3, OTTAWA FRONT, GLOUCESTER, PART 12, PLAN 4R-33542 AS IN OC2335588 ; TOGETHER WITH AN EASEMENT OVER PART LOT 24, CONCESSION 3, OTTAWA FRONT, GLOUCESTER, PARTS 16 AND 18,	Vacant	0.40 ha

	PLAN 4R-33542 AS IN OC2335588 SUBJECT TO AN EASEMENT OVER PART 3, PLAN 4R34045 IN FAVOUR OF PART LOT 25, CONCESSION 3, PART 1, PLAN 4R34045 AS IN OC2410797 TOGETHER WITH AN EASEMENT OVER PART LOTS 24 AND 25, CONCESSION 3, PARTS 12,14,16,18 AND 20, PLAN 4R33542 AND PART 5, PLAN 4R34045 AS IN OC2410797 TOGETHER WITH AN EASEMENT OVER PART LOT 25, CONCESSION 3, PART 5, PLAN 4R34045 AS IN OC2410800 CITY OF OTTAWA		
Unaddressed parcel #1 (south)	PART OF LOT 24, CONCESSION 3, OTTAWA FRONT, GLOUCESTER, BEING PART 1 ON PLAN 4R-25599. SUBJECT TO AN EASEMENT OVER PT 1 PLAN 4R25599 IN FAVOUR OF PT LT 24 CON 3OF PTS 1,2,3 & 4 ON 5R4921 SAVE AND EXCEPT PT 1 ON 4R25599 AS IN OC1292539 CITY OF OTTAWA	Ramps and parking associated with 2760 Sheffield Road	0.14 ha
Unaddressed parcel #2 (north)	PART OF LOT 25, CONCESSION 3, OTTAWA FRONT, GLOUCESTER, PART 1, PLAN 4R-34045; TOGETHER WITH AN EASEMENT OVER PARTS 2, 4 TO 7, 9, 15, 17, AND 19 TO 22, PLAN 4R-16285 AS IN LT1351640 ; TOGETHER WITH AN EASEMENT OVER PART LOT 24, CONCESSION 3, OTTAWA FRONT, GLOUCESTER, PART 20, PLAN 4R-33542 AS IN OC2321552 ; TOGETHER WITH AN EASEMENT OVER PART LOTS 24 AND 25, CONCESSION 3, OTTAWA FRONT, GLOUCESTER, PART 14, PLAN 4R-33542 AS IN OC2327032 ; TOGETHER WITH AN EASEMENT OVER PART LOT 25, CONCESSION 3, OTTAWA FRONT, GLOUCESTER, PART 12, PLAN 4R-33542 AS IN OC2335588 ; TOGETHER WITH AN EASEMENT OVER PART LOT 24, CONCESSION 3, OTTAWA FRONT, GLOUCESTER, PARTS 16 AND 18, PLAN 4R-33542 AS IN OC2335588 TOGETHER WITH AN EASEMENT OVER PART LOT 25, CONCESSION 3, PART 3, PLAN 4R34045 AS IN OC2410797 TOGETHER WITH AN EASEMENT OVER PART LOT 25, CONCESSION 3, PART 5, PLAN 4R34045 AS IN OC2410800 TOGETHER WITH AN EASEMENT OVER PART LOTS 24 AND 25, CONCESSION 3, PART 12,14,16,18 AND 20, PLAN 4R33542 AND PART 5, PLAN 4R34045 AS IN OC2410797 CITY OF OTTAWA	Vacant	0.83 ha
		Total Area	8.45 ha

3.0 DEVELOPMENT PROPOSAL

The development is concentrated in the western part of the Subject Site. There is no change to the access from Sheffield Road. The existing warehouse building in the southeast corner of the site will be partially demolished, reducing it's size by 20,243 sq.ft. to 78,916 sq.ft.

As shown on the excerpt of the Site Plan below, it is proposed to construct a new 10,564m² light industrial building across two parcels (2760 Sheffield Road and 2865 Walkley F Road). It will be a warehouse with a height of 12m. Access will be available from Sheffield Road and from Lancaster Road via a new entry alongside the existing office building at 2713 Lancaster Road. A driveway runs around the building, with loading bays arrayed along the east side of the building and car parking along the west side of the building. Additional parking truck parking is provided on the northern unaddressed parcel and car parking is provided on the southern unaddressed parcel. A total of 126 car parking spaces are provided. A total of ten bicycle parking spaces are also provided.

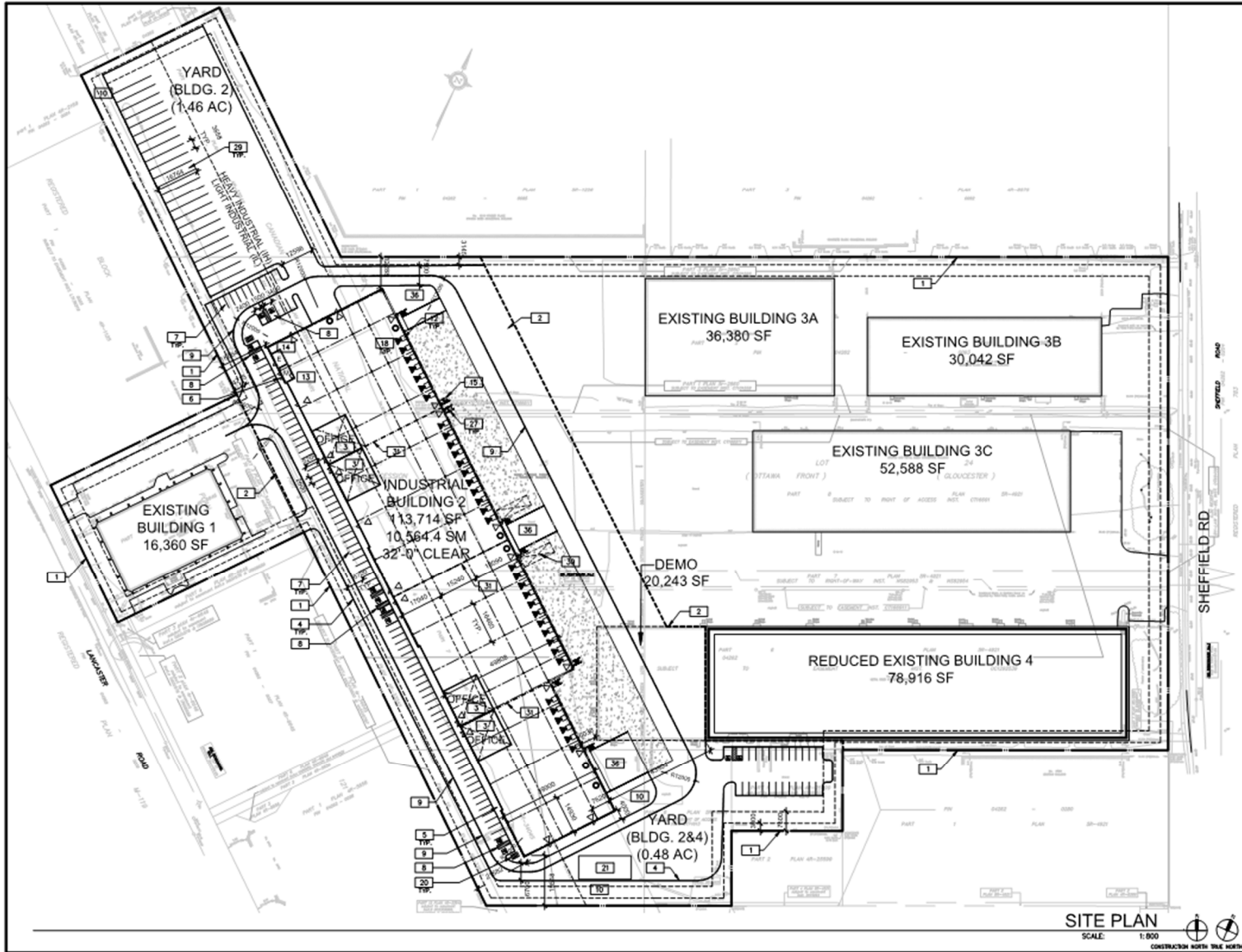


Figure 2: Excerpt of Site Plan by Ware Malcomb dated May 16, 2023

4.0 PLANNING ASSESSMENT

4.1 Provincial Policy Statement, 2020

Section 3 of the *Planning Act* requires that decisions affecting planning matters “shall be consistent with” policy statements issued under the Act. This includes the Provincial Policy Statement (PPS). The PPS is organized into three main policy sections: (1) Building Strong Healthy Communities, (2) Wise Use and Management of Resources, and (3) Protecting Public Health and Safety. The following sub-sections explain how the proposed development is consistent with the applicable PPS policies.

Building Strong Healthy Communities

Section 1.1 of the PPS is focused on managing and directing land use to achieve efficient and resilient development and land use patterns. The relevant policies are addressed below:

Policy 1.1.1 Healthy, liveable and safe communities are sustained by:

- a) *Promoting efficient development and land use patterns which sustain the financial well-being of the Province and municipalities over the long-term*
- b) *accommodating an appropriate affordable and market-based range and mix of residential types (including single-detached, additional residential units, multi-unit housing, affordable housing and housing for older persons), employment (including industrial and commercial), institutional (including places of worship, cemeteries and long-term care homes), recreation, park and open space, and other uses to meet long-term needs;*
- c) *avoiding development and land use patterns which may cause environmental or public health and safety concerns;*
- d) *avoiding development and land use patterns that would prevent the efficient expansion of settlement areas in those areas which are adjacent or close to settlement areas;*
- e) *promoting the integration of land use planning, growth management, transit-supportive development, intensification and infrastructure planning to achieve cost-effective development patterns, optimization of transit investments, and standards to minimize land consumption and servicing costs;*
- f) *improving accessibility for persons with disabilities and older persons by addressing land use barriers which restrict their full participation in society;*
- g) *ensuring that necessary infrastructure and public service facilities are or will be available to meet current and projected needs;*
- h) *promoting development and land use patterns that conserve biodiversity; and*
- i) *preparing for the regional and local impacts of a changing climate.*

The proposed development contributes to a healthy, liveable and safe community because it:

- is located in an existing business park adjacent to a 400 series highway representing efficient and context-appropriate development that minimizes servicing costs;
- does not create environmental or public health and safety concerns or prevent the efficient expansion of settlement areas.

Policy 1.1.3.1 Settlement areas shall be the focus of growth and development.

The Subject Site is in the Settlement Area.

Policy 1.1.3.2 Land use patterns within settlement areas shall be based on densities and a mix of land uses which:

- a) efficiently use land and resources;*
- b) are appropriate for, and efficiently use, the infrastructure and public service facilities which are planned or available, and avoid the need for their unjustified and/or uneconomical expansion;*
- c) minimize negative impacts to air quality and climate change, and promote energy efficiency;*
- d) prepare for the impacts of a changing climate;*
- e) support active transportation;*
- f) are transit-supportive, where transit is planned, exists or may be developed; and*
- g) are freight-supportive; and*

The proposed development efficiently use land and resources as it is in an existing business park. It is a freight supportive use located adjacent to a 400 series highway.

Section 1.3 *Employment* of the PPS is focused on planning authorities promoting economic development and competitiveness by:

- a) providing for an appropriate mix and range of employment, institutional, and broader mixed uses to meet long-term needs;*
- b) providing opportunities for a diversified economic base, including maintaining a range and choice of suitable sites for employment uses which support a wide range of economic activities and ancillary uses, and take into account the needs of existing and future businesses;*
- c) facilitating the conditions for economic investment by identifying strategic sites for investment, monitoring the availability and suitability of employment sites, including market-ready sites, and seeking to address potential barriers to investment;*
- d) encouraging compact, mixed-use development that incorporates compatible employment uses to support liveable and resilient communities, with consideration of housing policy 1.4; and*

- e) *ensuring the necessary infrastructure is provided to support current and projected needs.*

Relevant policies are addressed below:

1.3.2.3 Within employment areas planned for industrial or manufacturing uses, planning authorities shall prohibit residential uses and prohibit or limit other sensitive land uses that are not ancillary to the primary employment uses in order to maintain land use compatibility.

Employment areas planned for industrial or manufacturing uses should include an appropriate transition to adjacent non-employment areas.

1.3.2.6 Planning authorities shall protect employment areas in proximity to major goods movement facilities and corridors for employment uses that require those locations.

The proposed industrial warehouses have sufficient separation from residential areas (approximately 130m) to ensure an appropriate transition.

Section 1.6 of the PPS provides policies on infrastructure and public service facilities.

Policy 1.6.3 Before consideration is given to developing new infrastructure and public service facilities:

- (a) the use of existing infrastructure and public service facilities should be optimized*

A Functional Servicing and Stormwater Management Report by Ware Malcomb dated March 13, 2023 and included in this submission details how the proposed development will utilize municipal sewage, water, and stormwater services. Refer to the report for details.

Section 1.7 of the PPS provides policies on long-term economic prosperity. Relevant policies are:

1.7.1 Long-term economic prosperity should be supported by:

- a) promoting opportunities for economic development and community investment-readiness;*

c) optimizing the long-term availability and use of land, resources, infrastructure and public service facilities;

The Subject Site is in an existing business park and designated Industrial and Logistics in the Official Plan.

Section 1.8 of the PPS provides policies on energy conservation, air quality and climate change. Relevant policies are:

Policy 1.8.1 Planning authorities shall support energy conservation and efficiency, improved air quality, reduced greenhouse gas emissions, and preparing for the impacts of a changing climate through land use and development patterns which:

d) focus freight-intensive land uses to areas well served by major highways, airports, rail facilities and marine facilities;

g) maximize vegetation within settlement areas, where feasible.

The proposal is located adjacent to and with easy access to Highway 417. There is no vegetation of significance on the Subject Site.

Wise Use and Management of Resources

Section 2.1 of the PPS provides policies on Natural Heritage.

Policy 2.1.1 Natural features and areas shall be protected for the long term.

The Official Plan does not identify any Natural Heritage Features on the Subject Site.

Section 2.2 of the PPS provides policies on Water.

Policy 2.2.2 Development and site alteration shall be restricted in or near sensitive surface water features and sensitive ground water features such that these features and their related hydrologic functions will be protected, improved or restored.

No sensitive surface or ground water features exist on or adjacent to the Subject Site.

Section 2.3 of the PPS provides policies on Agriculture.

Policy 2.3.1 Prime agricultural areas shall be protected for long-term use for agriculture.

The Subject Site is not located on or adjacent to prime agricultural land.

Section 2.4 of the PPS provides policies on Minerals and Petroleum.

Policy 2.4.1 Minerals and petroleum resources shall be protected for long-term use.

No mineral or petroleum resources exist on or adjacent to the Subject Site.

Section 2.5 of the PPS provides policies on Mineral Aggregate Resources.

Policy 2.5.1 Mineral aggregate resources shall be protected for long-term use and, where provincial information is available, deposits of mineral aggregate resources shall be identified.

No mineral aggregate resources exist on or adjacent to the Subject Site.

Section 2.6 of the PPS provides policies on Cultural Heritage and Archaeology.

Policy 2.6.1 Significant built heritage resources and significant cultural heritage landscapes shall be conserved.

Policy 2.6.2 Development and site alteration shall not be permitted on lands containing archaeological resources or areas of archaeological potential unless significant archaeological resources have been conserved.

An Archaeological Resource Assessment was not required by City staff for this application.

Policy 2.6.3 Planning authorities shall not permit development and site alteration on adjacent lands to protected heritage property except where the proposed development and site alteration has been evaluated and it has been demonstrated that the heritage attributes of the protected heritage property will be conserved.

An Archaeological Resource Assessment was not required by City staff for this application.

Protecting Public Health and Safety

Section 3.1 of the PPS provides policies on Natural Hazards.

Policy 3.1.1 Development shall generally be directed, in accordance with guidance developed by the Province (as amended from time to time), to areas outside of:

- a) hazardous lands adjacent to the shorelines of the Great Lakes - St. Lawrence River System and large inland lakes which are impacted by flooding hazards, erosion hazards and/or dynamic beach hazards;*
- b) hazardous lands adjacent to river, stream and small inland lake systems which are impacted by flooding hazards and/or erosion hazards; and*
- c) hazardous sites.*

The proposed development is not occurring within natural hazard lands or sites.

Section 3.2 of the PPS provides policies on Human-Made Hazards.

Policy 3.2.1 Development on, abutting or adjacent to lands affected by mine hazards; oil, gas and salt hazards; or former mineral mining operations, mineral aggregate operations or petroleum resource operations may be permitted only if rehabilitation or other measures to address and mitigate known or suspected hazards are under way or have been completed.

Policy 3.2.2 Sites with contaminants in land or water shall be assessed and remediated as necessary prior to any activity on the site associated with the proposed use such that there will be no adverse effects.

A Phase 1 Environmental Site Assessment by EXP Services dated April 7, 2022 forms part of the application. It concludes that: "...no additional environmental investigation is recommended." Refer to the report for further details.

4.2 City of Ottawa Official Plan

Official Plan Designations

Schedule B3 – Outer Urban Transect designates the Subject Site Industrial and Logistics (blue):

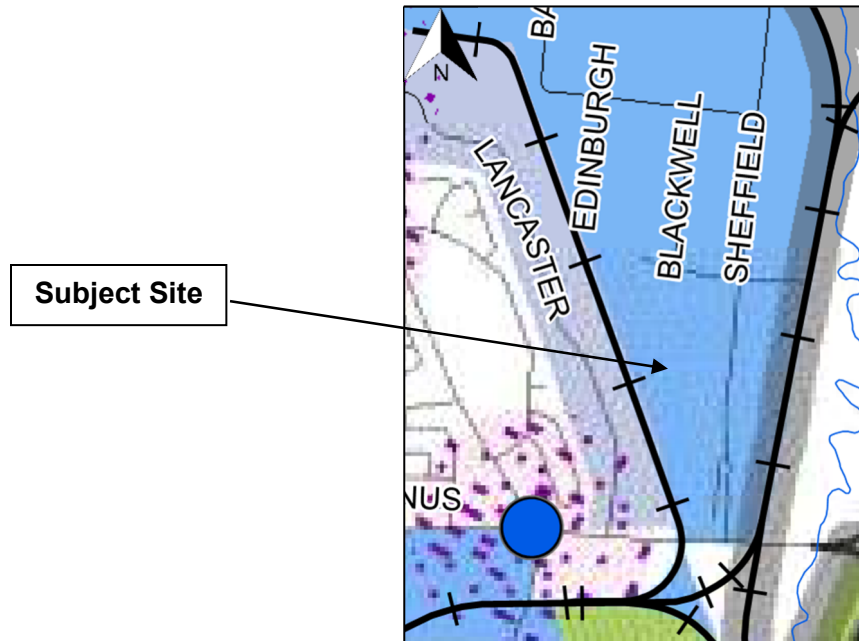


Figure 3: Schedule B6

Schedule C2 – Transit Network shows a Protected Transportation Corridor (green) on the Subject Site:

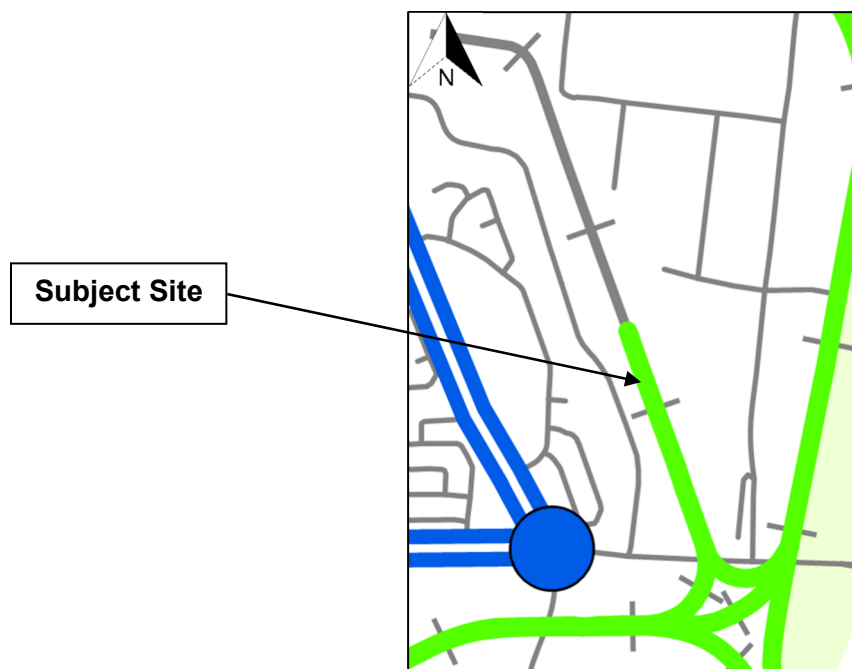


Figure 4: Schedule C2

Schedule C4 – Urban Road Network designates Sheffield and Lancaster Roads as Collectors (green) and Innes, Walkley and Russell Roads as Arterials (red) and Highway 416 (brown):



Figure 5: Schedule C4

The following Schedules are not shown here:

- C3 – Active Transportation Network shows the closest Major Pathway south of Walkley Road.
- C7-A – Design Priority Areas does not designate the Subject Site.
- Schedule C11-C – Natural Heritage System (east) identifies the Subject Site as Urban Area with no Natural Heritage designations.
- Schedule C13 – Scenic Routes designates Highway 416 as a Scenic Route.
- Schedule C15 – Environmental Constraints does not designate the Subject Site as having any environmental constraints.

Assessment of Official Plan Section 4.4 City Wide Policies

The proposed development conforms to the following applicable city-wide policies of Section 4.0 of the Official Plan:

Mobility

Policy 4.1.2(6)

New developments will provide direct connections to the existing or planned network of public sidewalks, pathways and cycling facilities.

- **The proposal connects to Lancaster Road.**

Policy 4.1.2(9)

Proponents of development shall provide an adequate number of bicycle parking facilities as follows:

- a) *Long-term bicycle parking facilities shall be secure, sheltered and usable by all types of cyclists. Where located inside buildings, long-term bicycle parking facilities shall provide safe, accessible, direct and convenient access to the exterior; and*
- b) *Short-term bicycle parking facilities shall be highly visible, well-lit, near building entrances and where appropriate, sheltered.*

- **Short-term bicycle parking is provided in accordance with the Zoning By-law 2008-250.**

Policy 4.1.4(11)

Surface parking lots should be designed to meet all of the following:

- a) *Minimize the number and width of vehicle entrances that interrupt pedestrian movement; and Provide safe, direct and well-defined pedestrian and cycling connections between the public street and all buildings, and between all buildings within the site; and*
- b) *Landscaping requirements shall be in addition to landscaping requirements for the right of way around the perimeter of parking lots; and*
- c) *Include regular spacing of tree islands that support the growth of mature shade trees and incorporate Low Impact Development measures for stormwater management where feasible; and*
- d) *Be designed to anticipate redevelopment of the site over time and facilitate future intensification, severance and infill; and*
- e) *Encourage the provision of electric vehicle charging spaces and dedicated car share spaces.*

- **The proposed surface parking area is accessed via one entrance. Landscaping is provided in accordance with the Zoning By-law.**

Policy 4.1.7(2)

The City shall protect rights of way for the street and road network shown on Schedules C4, C5, C9 and C10 and as listed in detail in Schedule C16.

- **Neither Lancaster Road nor Sheffield Road have right of way protection in Schedule C16 - Road Classification and Rights-of-Way Protection of the Official Plan.**

Policy 3) at 4.1.7 Protect and invest in rights of way is applicable:

3) Protected Transportation Corridors identified in this Plan, including in Schedule C2, the TMP, associated Plans and utility corridors shall be protected for future transportation purposes, such as active transportation, rapid transit, inter-regional passenger rail and high-speed rail.

- **The proposed building is located on a Protected Transportation Corridor on Schedule C2 (refer Figure 4). The applicant is currently discussing the basis and applicability of this corridor with City staff. Note that the corridor is not designated in the current Transportation Master Plan (2013). It is only identified as an existing railway (assuming the standard iconography for a railway – it is not in the legend). This section of railway was removed around 2018. The new TMP, currently underway, should correct this and clarify what status this corridor has.**

Drinking Water, Wastewater and Stormwater Infrastructure

Policy 4.7.1(23)

Applications for new development will demonstrate, to the City's satisfaction, that adequate services are available and can be allocated to support the proposal:

- a) Where adequate services do not exist or cannot yet be provided to support a development, the City may use holding provisions in accordance with Zoning By-law to regulate the timing of development*
- **A Functional Servicing and Stormwater Management Report by Ware Malcomb dated March 13, 2023 and included in this submission confirms that the proposed development can be adequately serviced by municipal services.**

Assessment of Official Plan Section 6.4 – Industrial and Logistics

Key introductory statements for the Industrial and Logistics designation include:

Industrial and Logistics areas are preserved to cluster economic activities relating to manufacturing, logistics, storage and other related uses.

The Industrial and Logistics designation is characterized by traditional industrial land uses such as warehousing, distribution, construction, light and heavy industrial, trades, outdoor storage and other uses requiring a range of parcel sizes.

- **The proposed industrial warehouse keeps with the intended character of the Industrial and Logistics designation as described above. Policy 2 of subsection 6.4.1 makes it clear that development in this designation should be for goods movement and logistics uses, such as the Warehouse and Light Industrial Uses proposed here (emphasis added):**
 - 2) The following uses are permitted in the Industrial and Logistics designation as shown on Schedules B1 through to B8:*
 - a. Traditional heavy and light industrial uses such as manufacturing, warehousing, distribution, storage, utilities and construction;*

- b. *Uses that store most products outdoors and require large land areas devoted to external storage, sale or service of goods;*
- c. *Auto service and body shops, heavy equipment and vehicle sales and service;*
- d. *Trades and contractors such as carpenters, plumbers, electricians and heating, ventilation and air conditioning;*
- e. *Major Office in accordance with Subsection 3.5, Policy 12); and*
- f. *Offices that are accessory to a primary use.*

The applicable policies of subsection 6.4.2 – *Ensure adverse impacts on sensitive uses are minimized* are addressed below:

- 1) *Residential, institutional and all other sensitive land uses are not permitted.*
 - 2) *Where permitted uses are in proximity to and potentially have adverse impacts on sensitive uses either within the same designation or an adjacent designation, amendments and minor variances to the Zoning By-law shall consider building setbacks to maximize the separation distance from sensitive use(s). Site plan control shall consider the siting of structures and/or outdoor operations to minimize potential adverse impacts to sensitive use(s).*
- **In relation to 1), no residential, institutional or all other sensitive land uses are proposed. In relation to 2), the proposed warehouse and light industrial uses will not have adverse impacts on sensitive uses as there are no sensitive uses within the Industrial and Logistics designation. The closest sensitive use is the residential area accessed from Southvale Crescent, approximately 130m from the Subject Site. Existing industrial uses are closer to this residential area than the proposal will be.**
 - **Subsection 6.4.3 – Limit ancillary uses to avoid conflict with industrial uses and minimize absorption of land for non-industrial uses is not applicable to these applications as ancillary uses are not proposed.**

4.3 City of Ottawa Zoning By-law 2008-250

The Subject Site is split zoned Heavy Industrial (IH) and Light Industrial (IL) as shown below. Light Industrial, Warehouse and Truck Transport Terminal uses are permitted in both zones (amongst others). The proposed Site Plan complies with the remaining zoning provisions with the exception of a small area of non-compliance for landscape buffer (see table below). This could be addressed via a Minor Variance application or a revision to the Site Plan. Note that the City staff confirmed at the pre-application consultation that only the new building needs to be assessed against zoning.

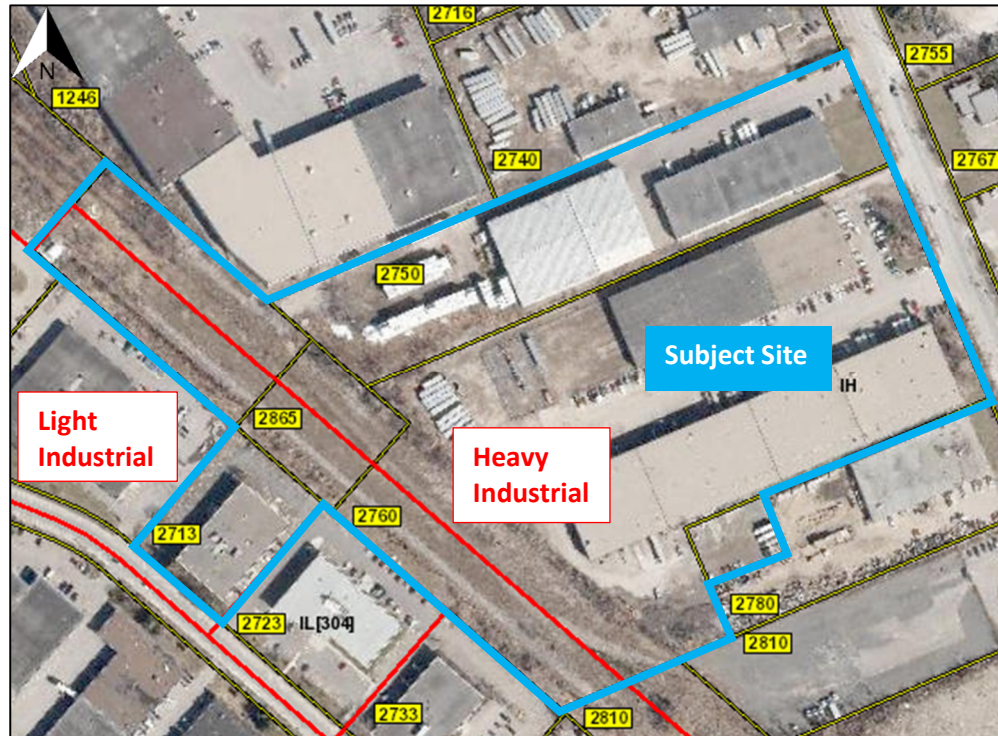


Figure 6: Zoning

Assessment against the **Light Industrial zone** (applies to the west part of the Subject Site):

Zoning Provision	Requirement	Provided
Minimum lot area	2,000 m ²	8.45 ha
Minimum lot width	No minimum	59.72 m
Maximum lot coverage	65%	36%
Minimum front yard and corner side yard	7.5 m	101 m and NA
Minimum interior side yard	(ii) all other cases: 7.5 m	12 m
Minimum rear yard	(iii) all other cases: 7.5 m	NA – no rear yard in the zone.
Maximum floor space index	2	0.36 (overall)
Maximum building height	((ii) in all other cases: 18 m	12 m
Minimum width of landscaping	(iii) in all other cases: No minimum	1.3 m

Assessment against the provisions of the **Heavy Industrial zone** (applies to the east part of the Subject Site):

Zoning Provision	Requirement	Provided
Minimum lot area	4,000 m ²	8.45 ha
Minimum lot width	No minimum	192.22 m
Minimum front yard and corner side yard setback	7.5 m	230 m and NA
Minimum interior side yard and rear yard	(ii) all other cases: 7.5 m	12 m and NA – no rear yard in zone.
Maximum floor space index	2	0.36 (overall)
Maximum building height	22 m	12 m
Minimum width of landscaped area	(iii) in all other cases: 3m	3 m

5.0 CONCLUSION

It is Novatech's assessment that the proposed development is consistent with the Provincial Policy Statement, conforms to the City of Ottawa's Official Plan, and is generally consistent with the Zoning By-Law 2008-250.

This planning rationale supports the proposed development. The proposed development functions well within the surrounding context. The proposed development is an appropriate and desirable addition to the neighbourhood and represents good planning.

NOVATECH

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