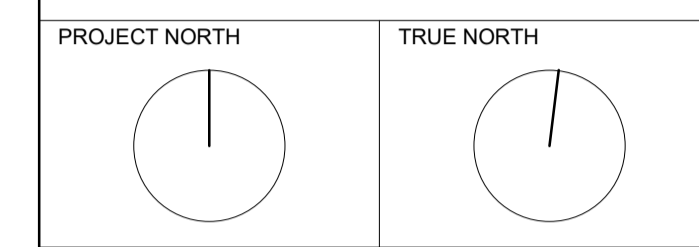


SEAL

**NOT FOR CONSTRUCTION**

GENERAL NOTES  
DO NOT SCALE DRAWINGS.  
CONTRACTOR TO VERIFY ALL DIMENSIONS & CONDITIONS  
AND REPORT ANY DISCREPANCIES.



NOTE: THE PROPERTY OWNER IS UPGRADING THE EXISTING CROSSWALKS UNDER A SEPARATE PROJECT

EXISTING ROAD LIGHT STANDARD TO REMAIN

EXISTING DEPRESSION CURB AT CROSSWALK TO REMAIN

EXISTING CROSSWALK

EXISTING CROSSWALK

EXISTING ROAD LIGHT STANDARD TO REMAIN

EXISTING DEPRESSION CURB AT CROSSWALK TO REMAIN

EXISTING CROSSWALK

EXISTING CROSSWALK

EXISTING ROAD LIGHT STANDARD TO REMAIN

EXISTING DEPRESSION CURB AT CROSSWALK TO REMAIN

EXISTING CROSSWALK

EXISTING CROSSWALK/PLAZA

EXISTING ROAD LIGHT STANDARD TO REMAIN

EXISTING CROSSWALK

EXISTING ROAD LIGHT STANDARD TO REMAIN

EXISTING DEPRESSION CURB AT CROSSWALK TO REMAIN

EXISTING CROSSWALK

CONCRETE PAVING

CONCRETE PAVING

TWISI AT DEPRESSION CURB

TWISI AT DEPRESSION CURB

CONCRETE CROSSWALK/PLAZA

EXISTING ROAD LIGHT STANDARD TO REMAIN

EXISTING CROSSWALK

EXISTING ROAD LIGHT STANDARD TO REMAIN

EXISTING DEPRESSION CURB AT CROSSWALK TO REMAIN

EXISTING CROSSWALK

CONCRETE PAVING

CONCRETE PAVING

TWISI AT DEPRESSION CURB

TWISI AT DEPRESSION CURB

CONCRETE CROSSWALK/PLAZA

EXISTING ROAD LIGHT STANDARD TO REMAIN

EXISTING CROSSWALK

EXISTING ROAD LIGHT STANDARD TO REMAIN

EXISTING DEPRESSION CURB AT CROSSWALK TO REMAIN

EXISTING CROSSWALK

CONCRETE PAVING

CONCRETE PAVING

TWISI AT DEPRESSION CURB

TWISI AT DEPRESSION CURB

CONCRETE CROSSWALK/PLAZA

EXISTING ROAD LIGHT STANDARD TO REMAIN

EXISTING CROSSWALK

EXISTING ROAD LIGHT STANDARD TO REMAIN

EXISTING DEPRESSION CURB AT CROSSWALK TO REMAIN

EXISTING CROSSWALK

CONCRETE PAVING

CONCRETE PAVING

TWISI AT DEPRESSION CURB

TWISI AT DEPRESSION CURB

CONCRETE CROSSWALK/PLAZA

EXISTING ROAD LIGHT STANDARD TO REMAIN

EXISTING CROSSWALK

EXISTING ROAD LIGHT STANDARD TO REMAIN

EXISTING DEPRESSION CURB AT CROSSWALK TO REMAIN

EXISTING CROSSWALK

CONCRETE PAVING

CONCRETE PAVING

TWISI AT DEPRESSION CURB

TWISI AT DEPRESSION CURB

CONCRETE CROSSWALK/PLAZA

EXISTING ROAD LIGHT STANDARD TO REMAIN

EXISTING CROSSWALK

EXISTING ROAD LIGHT STANDARD TO REMAIN

EXISTING DEPRESSION CURB AT CROSSWALK TO REMAIN

EXISTING CROSSWALK

CONCRETE PAVING

CONCRETE PAVING

TWISI AT DEPRESSION CURB

TWISI AT DEPRESSION CURB

CONCRETE CROSSWALK/PLAZA

EXISTING ROAD LIGHT STANDARD TO REMAIN

EXISTING CROSSWALK

EXISTING ROAD LIGHT STANDARD TO REMAIN

EXISTING DEPRESSION CURB AT CROSSWALK TO REMAIN

EXISTING CROSSWALK

CONCRETE PAVING

CONCRETE PAVING

TWISI AT DEPRESSION CURB

TWISI AT DEPRESSION CURB

CONCRETE CROSSWALK/PLAZA

EXISTING ROAD LIGHT STANDARD TO REMAIN

EXISTING CROSSWALK

EXISTING ROAD LIGHT STANDARD TO REMAIN

EXISTING DEPRESSION CURB AT CROSSWALK TO REMAIN

EXISTING CROSSWALK

CONCRETE PAVING

CONCRETE PAVING

TWISI AT DEPRESSION CURB

TWISI AT DEPRESSION CURB

CONCRETE CROSSWALK/PLAZA

EXISTING ROAD LIGHT STANDARD TO REMAIN

EXISTING CROSSWALK

EXISTING ROAD LIGHT STANDARD TO REMAIN

EXISTING DEPRESSION CURB AT CROSSWALK TO REMAIN

EXISTING CROSSWALK

CONCRETE PAVING

CONCRETE PAVING

TWISI AT DEPRESSION CURB

TWISI AT DEPRESSION CURB

CONCRETE CROSSWALK/PLAZA

EXISTING ROAD LIGHT STANDARD TO REMAIN

EXISTING CROSSWALK

EXISTING ROAD LIGHT STANDARD TO REMAIN

EXISTING DEPRESSION CURB AT CROSSWALK TO REMAIN

EXISTING CROSSWALK

CONCRETE PAVING

CONCRETE PAVING

TWISI AT DEPRESSION CURB

TWISI AT DEPRESSION CURB

CONCRETE CROSSWALK/PLAZA

EXISTING ROAD LIGHT STANDARD TO REMAIN

EXISTING CROSSWALK

EXISTING ROAD LIGHT STANDARD TO REMAIN

EXISTING DEPRESSION CURB AT CROSSWALK TO REMAIN

EXISTING CROSSWALK

CONCRETE PAVING

CONCRETE PAVING

TWISI AT DEPRESSION CURB

TWISI AT DEPRESSION CURB

CONCRETE CROSSWALK/PLAZA

EXISTING ROAD LIGHT STANDARD TO REMAIN

EXISTING CROSSWALK

EXISTING ROAD LIGHT STANDARD TO REMAIN

EXISTING DEPRESSION CURB AT CROSSWALK TO REMAIN

EXISTING CROSSWALK

CONCRETE PAVING

CONCRETE PAVING

TWISI AT DEPRESSION CURB

TWISI AT DEPRESSION CURB

CONCRETE CROSSWALK/PLAZA

EXISTING ROAD LIGHT STANDARD TO REMAIN

EXISTING CROSSWALK

EXISTING ROAD LIGHT STANDARD TO REMAIN

EXISTING DEPRESSION CURB AT CROSSWALK TO REMAIN

EXISTING CROSSWALK

CONCRETE PAVING

CONCRETE PAVING

TWISI AT DEPRESSION CURB

TWISI AT DEPRESSION CURB

CONCRETE CROSSWALK/PLAZA

EXISTING ROAD LIGHT STANDARD TO REMAIN

EXISTING CROSSWALK

EXISTING ROAD LIGHT STANDARD TO REMAIN

EXISTING DEPRESSION CURB AT CROSSWALK TO REMAIN

EXISTING CROSSWALK

CONCRETE PAVING

CONCRETE PAVING

TWISI AT DEPRESSION CURB

TWISI AT DEPRESSION CURB

CONCRETE CROSSWALK/PLAZA

EXISTING ROAD LIGHT STANDARD TO REMAIN

No.	DATE	DESCRIPTION
5	2023-04-20	SITE PLAN 3RD SUBMISSION
4	2023-03-17	SITE PLAN 2ND SUBMISSION
3	2022-12-16	SITE PLAN SUBMISSION
2	2022-11-25	CHEO SITE REVIEW
1	2022-09-26	SITE PLAN PRECONSULT

REVISIONS

PROJECT

**Ronald McDonald House Charities Ottawa (RMHCO)**

**Ronald McDonald House Expansion**

407 Smyth Road, Ottawa, ON K1H 8M8

IDEA # 19644

CLIENT #

SHEET NAME

**PARTIAL SITE PLAN AND CONTEXT PLAN**

DATE 2022-09-21

SCALE AS NOTED.

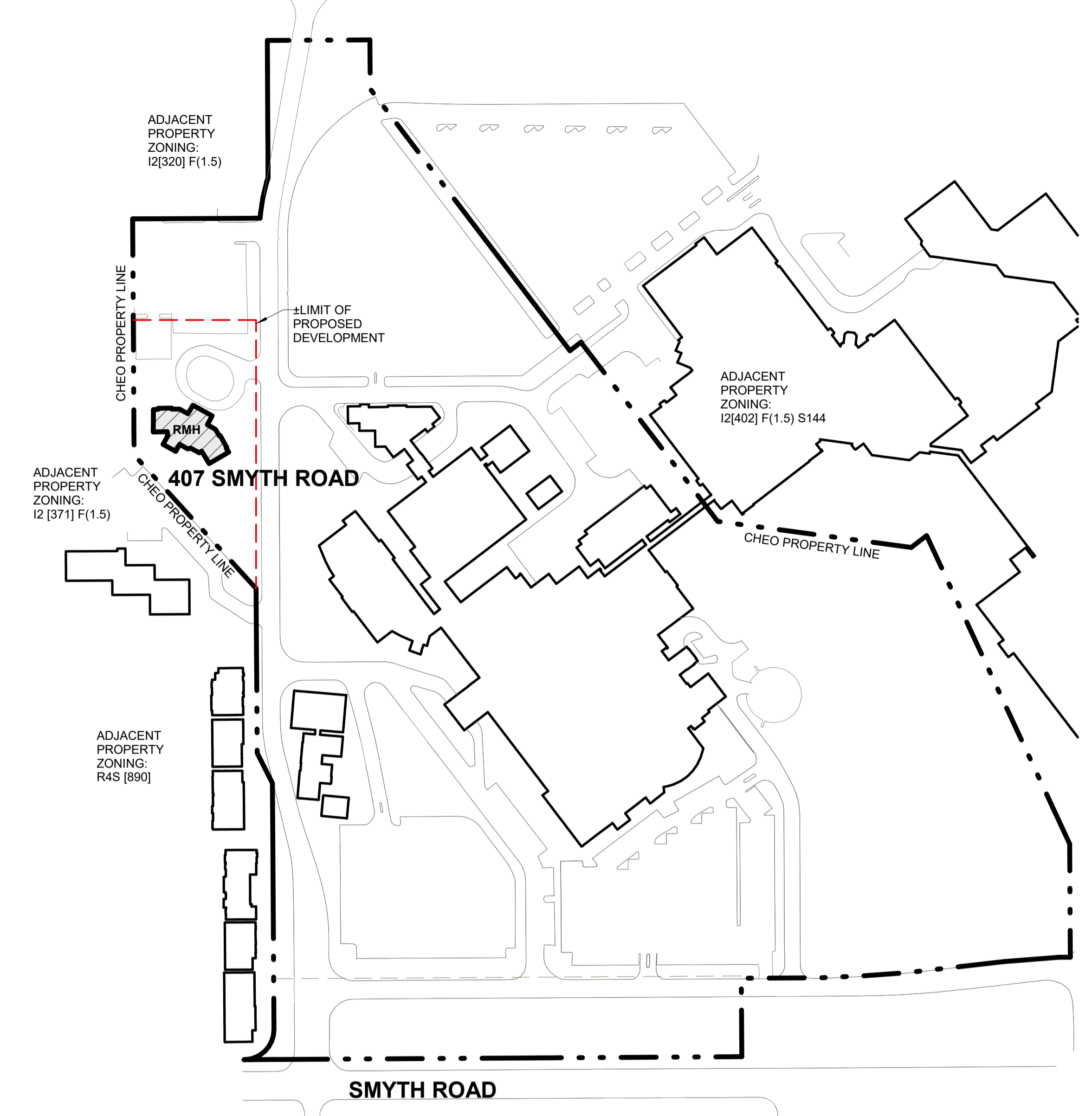
CHECKED BY

DRAWN BY

SHEET No. **SD101**

PLAN #18909 / D07-12-22-0176 A1 (841x594)

## HOSPITAL LINK ROAD



**A2** CHEO / TOH CONTEXT PLAN  
SD101 1 : 2000

### LEGAL DESCRIPTION

A CERTAIN PARCEL OF LAND SITUATE, LYING AND BEING IN THE CITY OF OTTAWA, ONTARIO, AND COMPOSED OF PART OF LOT 15, JUNCTION GORE, FORMERLY IN THE TOWNSHIP OF GLOUCESTER, NOW IN THE CITY OF OTTAWA, BEING PART OF PART 1 ON PLAN SR-8188, BEING THE FOOTPRINT OF THE 2 STOREY BRICK BUILDING REPRESENTING THE RONALD MCDONALD HOUSE AS DEPICTED ON PLAN SR-8188 ATTACHED, TOGETHER WITH A RIGHT-OF-WAY FOR ALL PURPOSES OVER PART 2 ON PLAN SR-8188.

SURVEY INFORMATION DERIVED FROM OHSC SURVEY MAP PREPARED BY FAIRHALL, MOFFAT AND WOODLAND LIMITED, AND TOPOGRAPHICAL SKETCH PREPARED BY ANNIS O'SULLIVAN VOLLEBEK LTD.

### ZONING: I2 [370] F(1.5)

PRIMARY ZONE: INSTITUTIONAL  
SUB-ZONE: MAJOR INSTITUTIONAL ZONE  
EXCEPTION: 370  
SCHEDULE 1A CLASSIFICATION: -MINIMUM FRONT YARD SETBACK 46m (from SMYTH ROAD)  
-NO PARKING PERMITTED WITHIN FRONT YARD SETBACK  
B OUTER URBAN / INNER SUBURBAN

### SETBACKS

REQUIRED	PROVIDED
MINIMUM REAR YARD SETBACK: 7.5 m	8.0 m
MINIMUM INTERIOR SIDE YARD SETBACK: 7.5 m	8.0 m
MINIMUM CORNER SIDE YARD SETBACK: 7.5 m	n/a
MAXIMUM HEIGHT: NO MAXIMUM AS IT IS NOT ADJACENT TO R1, R2, OR R3 ZONE.	9.95 m
MINIMUM WIDTH OF LANDSCAPED AREA ALONG ALL LOT LINES: 3 m	4.8 m

### PARKING

THE SITE IS NOT LOCATED WITHIN 600m OF A RAPID TRANSIT STATION  
PARKING RATE FOR HOSPITAL USE IN AREA B IS 1.4 PER 100m<sup>2</sup>  
ADDITIONAL GFA = 1808.9m<sup>2</sup>  
NUMBER OF ADDITIONAL PARKING: (1808.9 / 100 \* 1.4): 25

NUMBER OF EXISTING PARKING SERVING RONALD MCDONALD HOUSE TO BE RELOCATED: 17  
TOTAL PARKING SPACES (17+25): 45

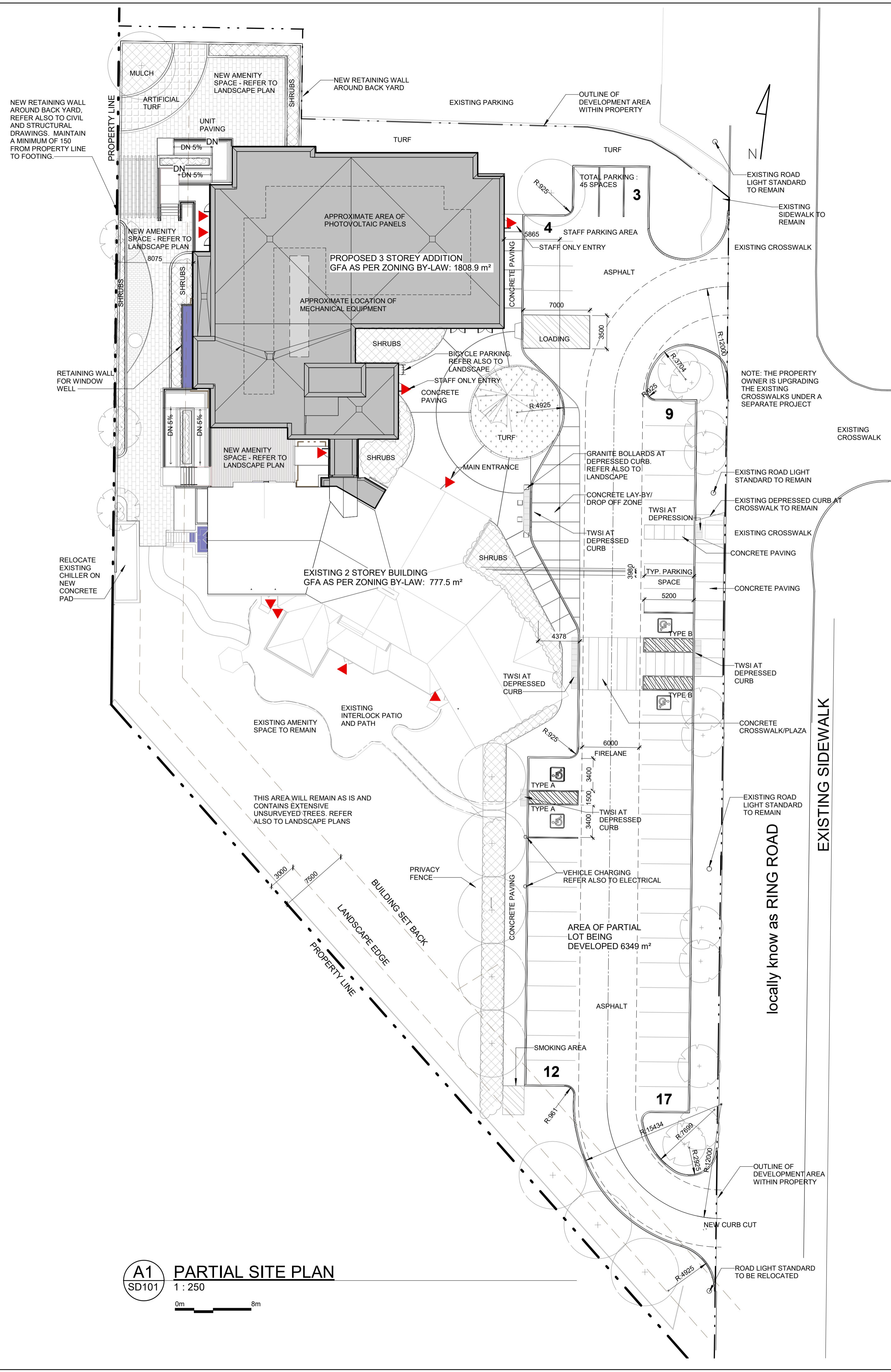
### BICYCLE PARKING

REQUIRED 1 PER 1000m<sup>2</sup> GFA (2586.4 / 1000) = 3 SPACES

### LOADING

REQUIRED FOR 2586.4m<sup>2</sup> GFA: 1

PROVIDED: 1



**A1** PARTIAL SITE PLAN  
SD101 1 : 250

