

# 2885 Carp Road

## Planning Rationale

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Site Plan Control

**Date |**  
January 2023

**Client |**  
Bekim Holdings Inc.

JD | PLANNING

**Prepared for:**

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## SECTION 1: INTRODUCTION

### 1.1 Overview

JD Planning has been retained by Bekim Holdings Inc. (the 'Owner') to prepare a Planning Rationale Report for an application for Site Plan Control with respect to the development of a proposed low-rise warehouse building located at 2885 Carp Road, in the City of Ottawa. The site is a vacant generally rectangular-shaped property with frontage on the west side of Carp Road, just southeast of the Village of Carp. The Owner is proposing to construct a new one-storey light industrial warehouse building with a mezzanine on the site. The following is a Planning Rationale in support of the proposed development.

### 1.2 Subject Site

The site is a large predominantly vacant lot on the west side of Carp Road, municipally known as 2885 Carp Road. There are presently two temporary buildings on the site roughly in the centre of the property, including a movable trailer and a temporary storage shed. Each of the existing buildings will be removed once the proposed new warehouse building is constructed. The site is bordered by some trees along the front (east), interior side (south), and rear (west) lot lines. Figure 1 below shows the site and approximate property lines per 2021 GeoOttawa mapping.

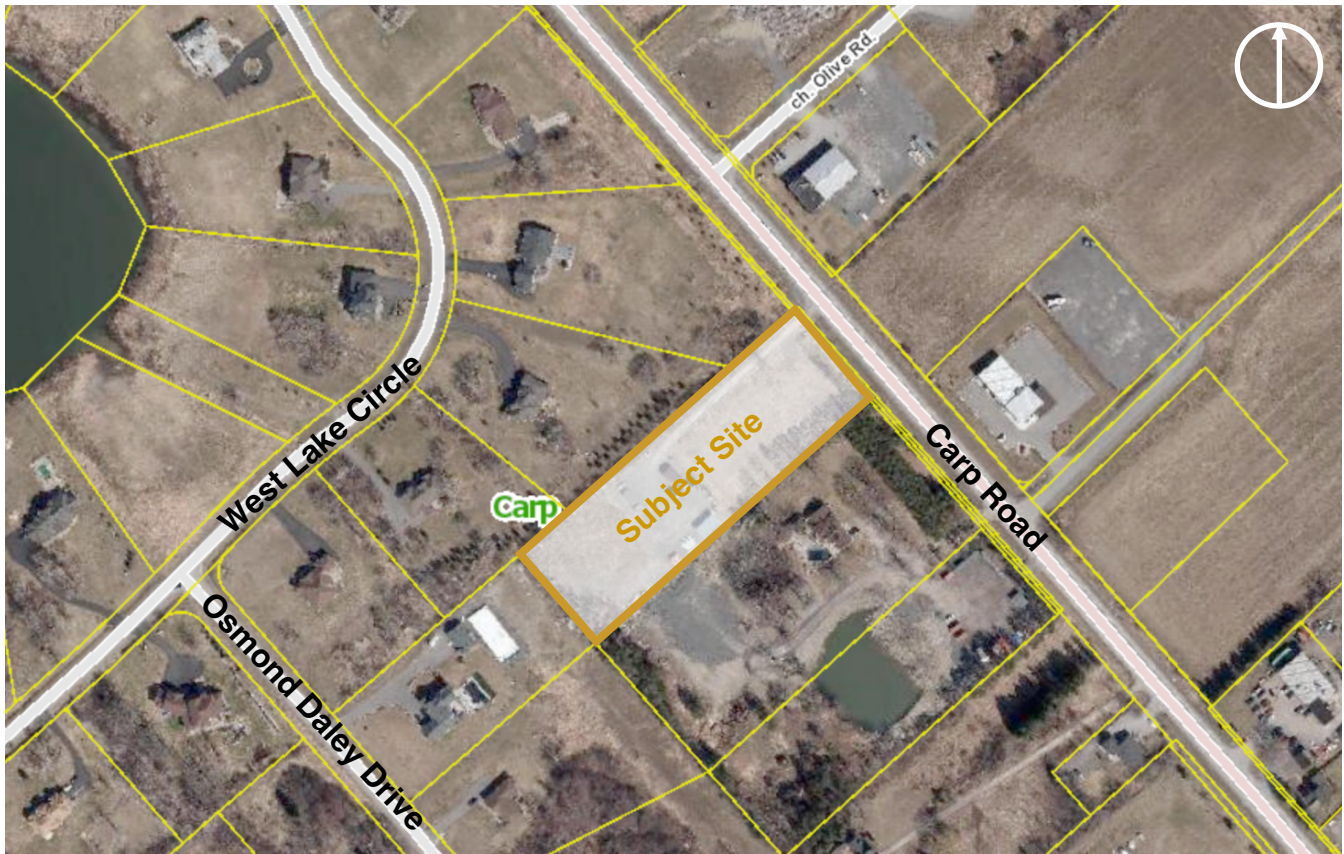


Figure 1. Subject site showing property lines and surrounding context (GeoOttawa, 2021)

### 1.2.1 Site Statistics

The site is a large rectangular shaped lot which located on the west side of Carp Road. The respective site statistics are set out below, and the property is subject to the following legal description:

|                   |   |
|-------------------|---|
| Area              | 12,041.0 m <sup>2</sup>   |
| Frontage          | 61.56 m (Carp Road)   |
| Legal Description | Part of Lot 9, Concession 3, Geographic Township of Huntley, City of Ottawa |
| PIN               | 04538-0128  |

An extract of the existing Plan of Survey is presented in Figure 2 below.

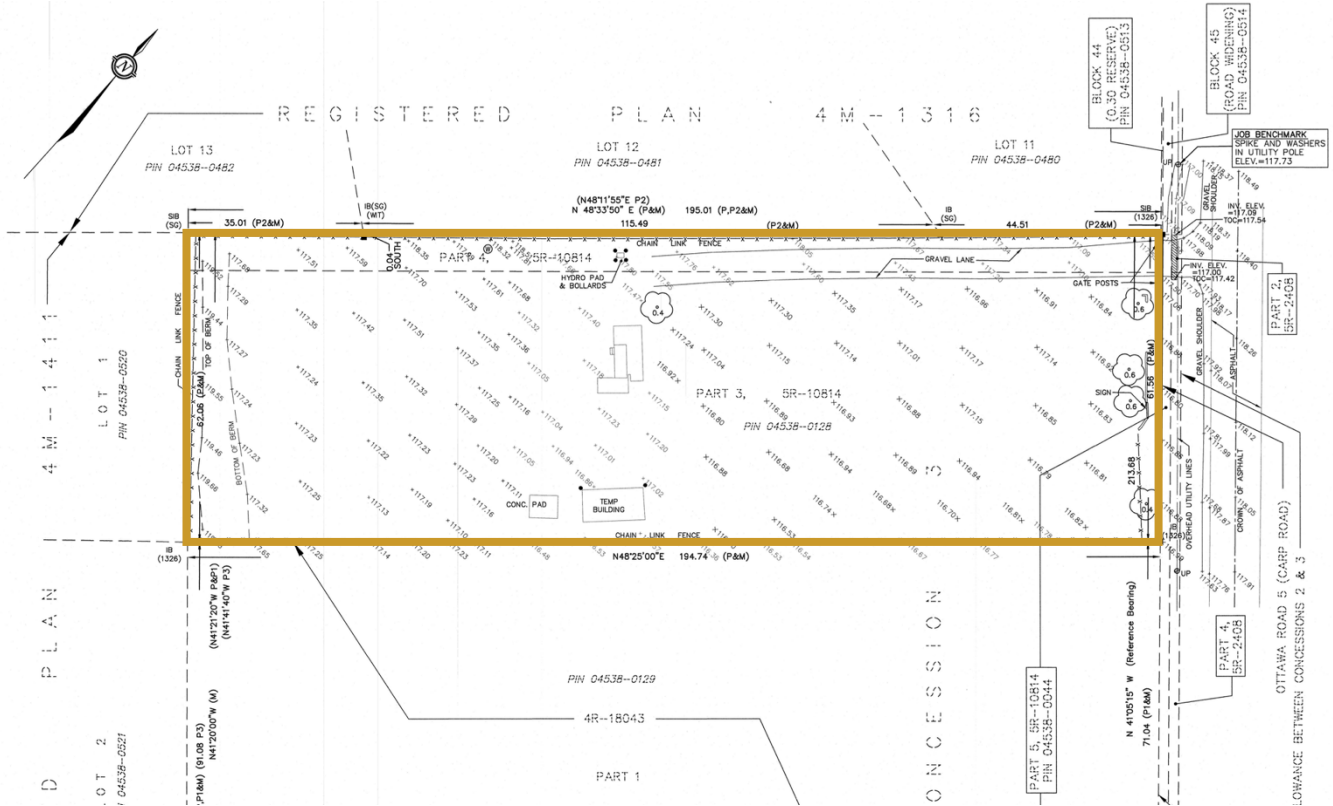


Figure 2. Extract of survey

### 1.2.2 Existing Site Conditions

The subject site is a 2.98-acre rural lot which presently contains a gravel laneway along the most northerly side of the lot, accessed from Carp Road. The laneway travels along the north property interior lot line until roughly the centre of the lot, leading to the two temporary building on site. Each of the buildings are movable and include a site office trailer and a storage shed, each of which will be removed following construction of the proposed warehouse. The site is otherwise vacant.

Images 1, 2 and 3 below represent the existing site conditions (January 2023) looking south from Carp Road. Note that the existing building on site is temporary and movable and will be removed upon development of the proposed warehouse building. Images 4 through 7 present the immediately surrounding context (Google Streetview, 2019).



Image 1. Subject site looking south from Carp Road (January 2023)



Image 2. Subject site looking south from Carp Road (January 2023)



Image 3. Subject site looking south from Carp Road (January 2023)



*Image 4. Office building opposite subject site looking north from Carp Road (Google Streetview, 2019)*



*Image 5. Residential subdivision adjacent to subject site looking southwest from Carp Road (Google Streetview, 2019)*



*Image 6. Commercial use located opposite subject site looking east from Carp Road (Google Streetview, 2019)*



*Image 7. Storage/light industrial use adjacent to subject site looking southwest from Carp Road (Google Streetview, 2019)*



Figure 3. Aerial imagery of subject site and immediately surrounding land uses (GeoOttawa, 2021)

### 1.3 Surrounding Context

The site is situated in Ottawa’s westerly rural area within the Carp neighbourhood in Ward 5 – West Carleton-March which is generally located north of Stittsville, west of Kanata Lakes and Dunrobin, south of Kinburn and east of Corkery. More specifically, the site is located on the west side of Carp Road, to the south of the intersection of McGee Side Road and north of the intersection of Arbourbrook Boulevard and Carp Road. A small low-rise residential subdivision is located to the northwest of the site, each of which front onto West Lake Circle. The subject site is bordered to the north by the rear yards/lot lines of the single-detached homes, with additional buffering from a treeline along the north lot line. To the west of the site is an additional rural estate lot subdivision with single-detached dwellings fronting onto Osmond Daley Drive, which is accessed via West Lake Circle. The adjacent residential uses each feature deep lots with generous rear yard setbacks allowing for appropriate separation between the proposed and permitted warehouse development. To the south are similar type light industrial and rural commercial uses including outdoor storage immediately bordering the south interior lot line. Along the west side of Carp Road opposite the site there is a multi-unit office building as well as rural commercial uses, along with vacant rural commercially zoned lands.

#### 1.3.1 Transit & Transportation Network

The site fronts onto the west side of Carp Road, which is a two-lane generally north-south Arterial which connects Stittsville to Fitzroy, travelling from Stittsville Main Street to the southeast, to Galetta Side Road to the northwest. Carp Road provides direct access to Highway 417 which travels east-west across the City of Ottawa and is easily navigable for trucking routes. Given the rural location of the site, there is limited transit accessible to the site, with the closest transit stations being located to the southeast at Tanger Outlets, located Palladium Drive and Campeau Drive.



### 1.3.2 Surrounding Context

More specifically, the site is located on the west side of Carp Road and the following building types and land uses surround the property:

- **North (Side Yard)** Low-rise residential subdivision (single-detached dwellings)
- **South (Side Yard)** One-storey building; Outdoor storage
- **East (Front Yard)** Automobile/machinery dealership; Vacant rural lands
- **West (Rear Yard)** Low-rise residential subdivision (single-detached dwellings)

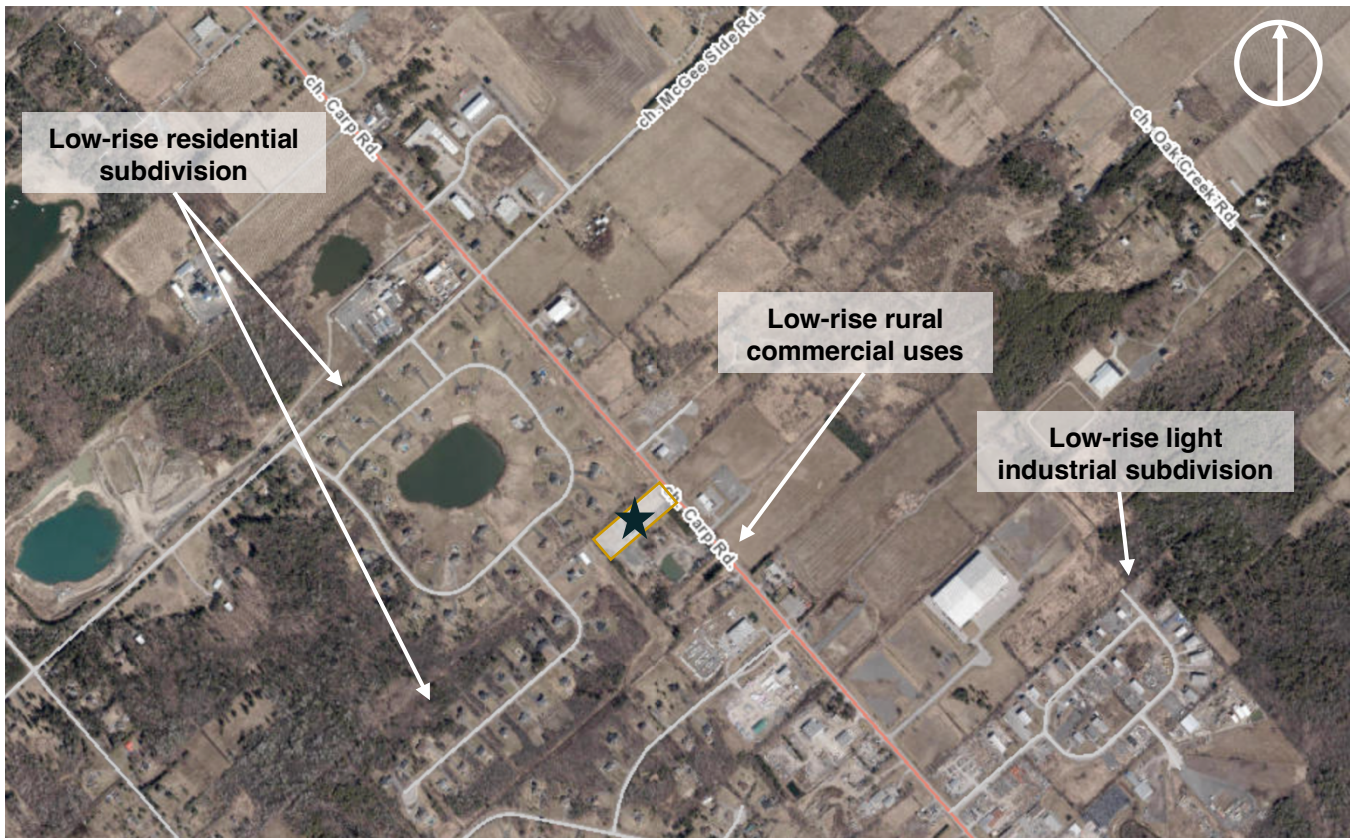


Figure 4. Subject site and surrounding context (GeoOttawa, 2021)

## SECTION 2: PROPOSED DEVELOPMENT

### 2.1 Design Overview

The proposed development is a one-storey warehouse building with a pitched roof and a mezzanine towards the southwest rear corner of the lot. As shown in the proposed elevations, the proposed materiality is to metal siding, typical for warehouse type buildings. The building is situated with its primary access on the northeast façade with the mezzanine above, with three loading bays along the front façade.

### 2.2 Access & Layout

The building features three loading bays along the front façade, accessed from the gravel driveway along the north interior side lot line. There are nine proposed parking spaces for visitors and employees. Snow storage is proposed along the southeast side of the parking lot. The proposed septic bed is located to the east of the building.

### 2.3 Landscaping

The overall site is proposed to be landscaped with topsoil and native low maintenance hydroseed. There are five trees to be retained, and eleven proposed deciduous trees (red maple and red oak) to be planted in the front yard and along the driveway. Coniferous trees including balsam fir, eastern larch/tamarack, and white spruce are also proposed in the front yard, as well as along the south and north interior lot lines.

### 2.5 Development Statistics

As noted, the proposed development is a one-storey warehouse building with a mezzanine space. The proposed site and development statistics are indicated in Table 1 below. The design elements and site layout of the building are presented in the elevations, site plan, and landscape plan to follow.

Table 1. Development details

|                                    |                         |                         |                       |
|------------------------------------|-------------------------|-------------------------|-----------------------|
| Lot Area                           | 12,041.0 m <sup>2</sup> | Building Height         | 7.5 m                 |
| Front Yard Setback                 | 61.0 m                  | Building Footprint Area | 701.42 m <sup>2</sup> |
| Interior Side Yard Setback (North) | 4.50 m                  | Bicycle Parking         | 3 spaces              |
| Interior Side Yard Setback (South) | 26.88 m                 | Vehicular Parking       | 9 spaces              |
| Rear Yard Setback                  | 10.09 m                 | Drive Aisle Width       | 6.0 m                 |



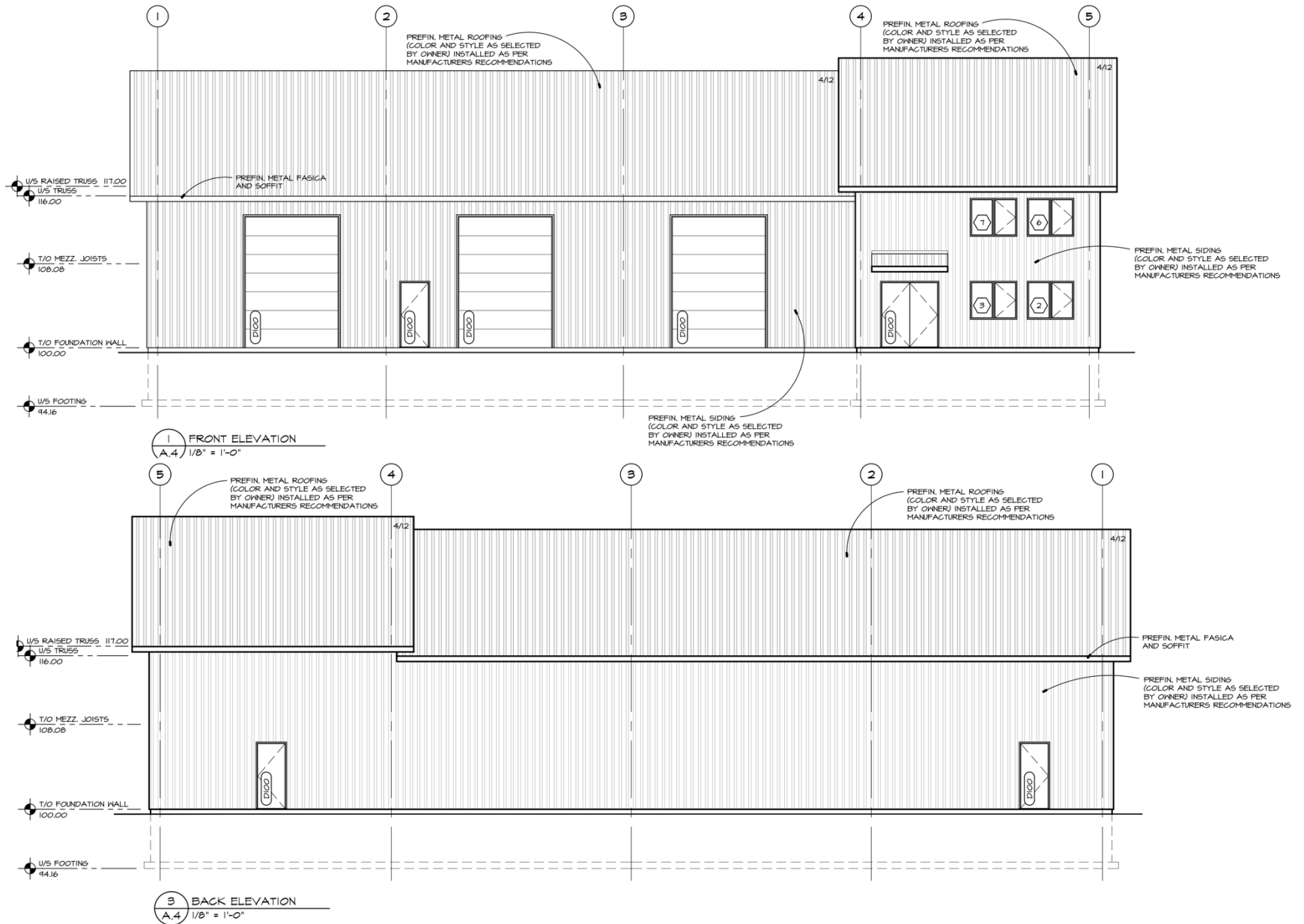


Figure 6. Proposed front and rear elevations prepared by Bell + Associates

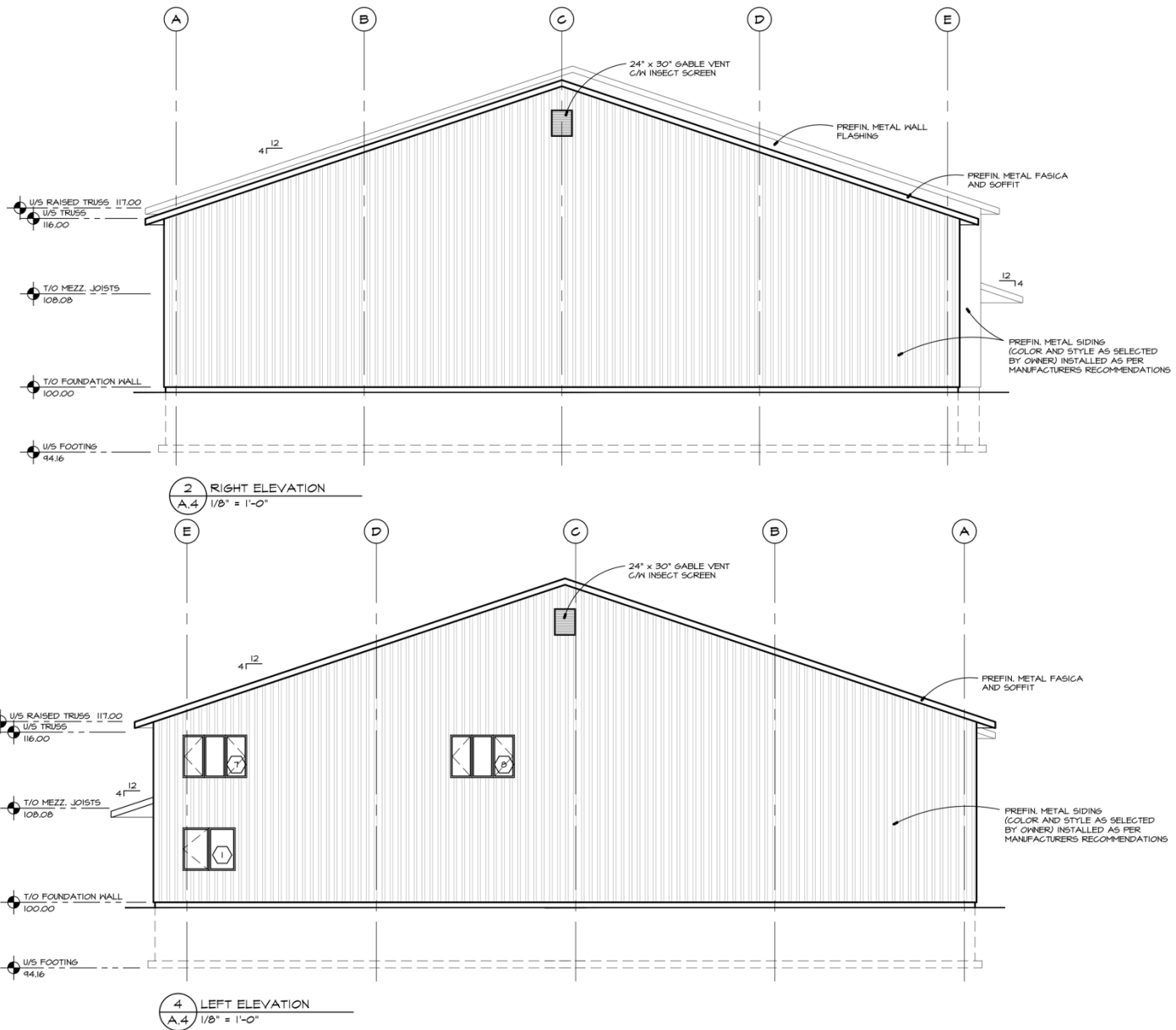


Figure 7. Proposed right and left side elevations prepared by Bell + Associates



## SECTION 3: POLICY CONTEXT

### 3.1 Provincial Policy Statement

The Provincial Policy Statement 2020 (PPS) provides policy direction on planning matters for the Province of Ontario. Decisions affecting all planning matters shall be consistent with the Provincial Planning Policies. The proposed development complies with the applicable policies of the PPS, as demonstrated below.

**Section 1.1.1** of the PPS states that healthy, livable, and safe communities are sustained by:

- a) *“promoting efficient development and land use patterns which sustain the financial well-being of the Province and municipalities over the long term;*
- b) *accommodating an appropriate affordable and market-based range and mix of ...employment (including industrial and commercial)...and other uses to meet long-term needs;*
- c) *avoiding development and land use patterns which may cause environmental or public health and safety concerns;*
- d) *avoiding development and land use patterns that would prevent the efficient expansion of settlement areas in those areas which are adjacent or close to settlement areas;*
- f) *improving accessibility for persons with disabilities and older persons by addressing land use barriers which restrict their full participation in society;*
- e) *ensuring that necessary infrastructure and public service facilities are or will be available to meet current and projected needs”...*

**Section 1.1.4** of the PPS states that rural areas are important to the economic success of the Province and quality of life. Per the PPS, “rural areas and urban areas are interdependent in terms of markets, resources and amenities. It is important to leverage rural assets and amenities and protect the environment as a foundation for a sustainable economy”. **Section 1.1.4.1** notes that healthy, integrated, and viable rural areas should be supported by:

- a) *“building upon rural character, and leveraging rural amenities and assets;*
- e) *using rural infrastructure and public service facilities efficiently;*
- f) *promoting diversification of the economic base and employment opportunities through goods and services, including value-added products and the sustainable management or use of resources...*

**Section 1.1.5.4** of the PPS states that “development that is compatible with the rural landscape and can be sustained by rural service levels should be promoted”. **Section 1.1.5.5** states that “development shall be appropriate to the infrastructure, which is planned or available, and avoid the need for the unjustified and/or uneconomical expansion of this infrastructure”. Finally, **Section 1.1.5.6** states that “opportunities should be retained to locate new or expanding land uses that require separation from other uses”.

According to the City of Ottawa’s New Official Plan, the site is located within the Rural Industrial and Logistics designation which is considered an “employment area” for the purposes of the Provincial Policy Statement. **Section 1.3.1** of the PPS sets out policies for employment areas and states that “planning authorities shall promote economic development and competitiveness by:

- a) *providing for an appropriate mix and range of employment, institutional, and broader mixed uses to meet long-term needs;*
- b) *providing for a diversified economic base, including maintaining a range and choice of suitable sites for employment uses which support a wide range of economic activities and ancillary uses, and take into account the needs of existing and future businesses.*

**Section 1.3.2.1** of the PPS states that *“planning authorities shall plan for, protect and preserve employment areas for current and future uses and ensure that the necessary infrastructure is provided to support current and projected needs.*

**Section 1.3.2.3** of the PPS states that *“within employment areas planned for industrial or manufacturing uses, planning authorities shall prohibit residential uses and prohibit or limit other sensitive land uses that are not ancillary to the primary employment uses in order to maintain land use compatibility. Employment areas planned for industrial, or manufacturing uses should include an appropriate transition to adjacent non-employment areas.*

→ In the above PPS policies, there is a significant emphasis on efficient development that is safe, respects the natural environment, uses available infrastructure, and provides for the needs of the community in both the short- and long-term. The PPS also emphasizes the need to respect existing rural character and provide opportunity for the retention of rural employment and logistics opportunities in the rural areas. The proposed development of a warehouse building in the City’s rural area is an appropriate and desirable use of the rural lands and meets the intent of the Provincial Policy Statement. The introduction of a warehouse to this site, which is zoned and designated to support this type of use, supports the rural employment area policies of the PPS. The warehouse supports diversification of the rural economic base and provides appropriate warehousing and interior storage for a rural-based company. There are several other examples of similar type warehouse, office, and light industrial uses along Carp Road, which has direct access to Highway 417. The proposed warehouse is appropriate for and compatible with these uses and provides separation from the neighbouring residential subdivision to the west. Overall, the proposed development meets the intent and purpose of the Provincial Policy Statement, as exemplified by policies set out above.

## **3.2 Official Plan**

The City of Ottawa’s New Official Plan was adopted by Council on November 24<sup>th</sup>, 2021 (By-law 2021-386) and approved (with modifications) by the Ministry of Municipal Affairs and Housing on November 4, 2022. The subject site is Designated Rural Industrial and Logistics in Schedule B9 – Rural Transect in the Official Plan, per Figure 9 below. The intent of the Rural Transect Area is to “provide for a range of social and economic activity” with policies that are “intended to ensure the responsible use of resources for the protection of public health and the environment while supporting economic development for rural businesses in recognition of their valuable contributions to the regional economy” (Section 5.5).

**Policy 5.5.1(b) and (c)** state that, *“[o]utside Villages, where development is permitted, built form and site design shall be premised on maintaining the rural character, image and identity; and... “sites shall be*



*designed to locate surface parking, storage and paved areas far from the road frontage, and access to such areas shall be designed to maintain rural character. The frontage along the road shall be landscaped and treed in a way that respects the rural landscape and enhances the green edge of rural roads. Elements such as low fences, hedges or landscape-based ornaments may be used to enhance the site frontage”.*

- The proposed development is located towards the rear of the lot to provide sufficient setbacks and buffering between the street and the proposed development. Storage will be indoors within the warehouse building and therefore fully screened from Carp Road. Further, plantings including coniferous and deciduous trees are proposed along the front lot line, and along the laneway to provide additional landscaped screening.

The subject site is designated Rural Industrial and Logistics in the Official Plan. Per Section 9.3 of the Official Plan, “Rural Industrial and Logistics are intended to support uses that are not suitable in the Urban area or Rural Countryside due to the requirements for large areas of land or separation from their noxious activity. These uses provide for a full range of activities across multiple industry sectors, which include warehouse, distribution, light and heavy industrial uses, and small offices”. Typically situated near 400 series highways, these designated areas place focus on movement of goods with minimal impact on traffic.

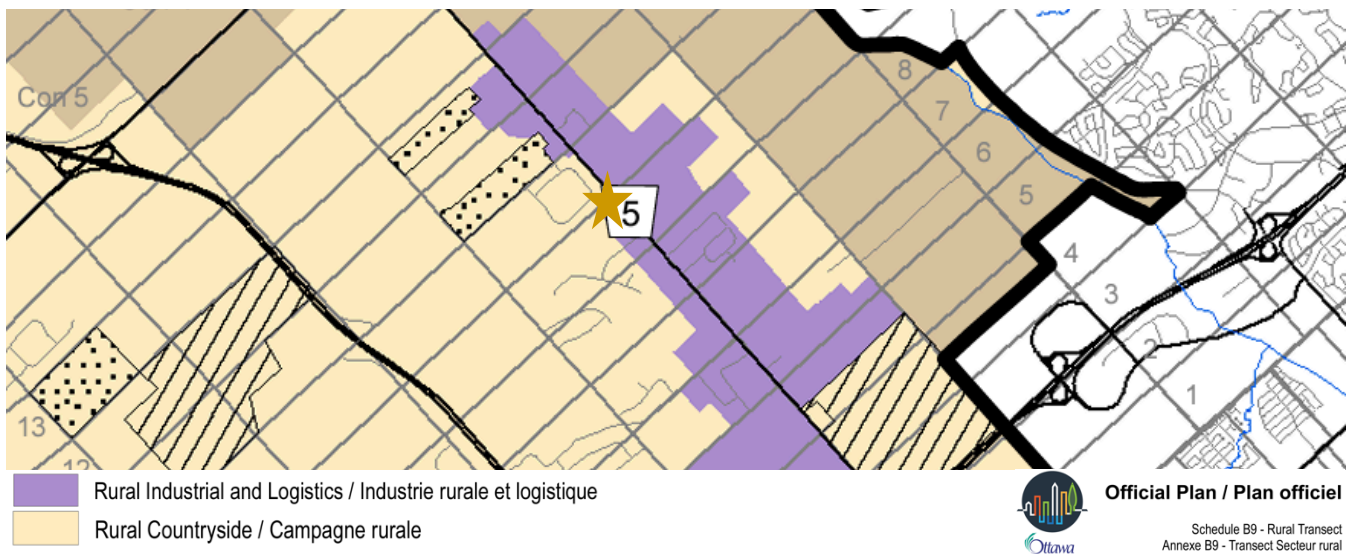


Figure 9. Extract of new Official Plan, Schedule B9 – Rural Transect

The intent of the Rural Industrial and Logistics designation is to “1) permit a range of industrial activity and functions to make the best use of rural locations; and 2) maintain clusters of industrial uses to reduce incompatibilities with the rural area”. Relevant policies to the proposed development are set out below.

**Section 9.3.1** sets out policies related to permitted industrial activity and functions in rural areas. Subsection 1 sets out that Rural Industrial and Logistics areas are located along arterial roads close to highway interchanges. In the case of the subject site, Carp Road is an arterial road located just north of Highway 417.

**Section 9.3.1(2)** states that: “development on lands designated as Rural Industrial and Logistics shall consider the following:

- a) *“Building design, site layout and landscape in a way that maintains and enhances the rural identity and feel of the area in which such development takes place;*
- b) *Appropriate screening from public roads and adjacent properties using natural vegetation, preferably existing vegetation where possible;*
- c) *Outdoor amenity areas for employees and landscaping that supports the City’s tree canopy targets; and,*
- d) *Accesses are designed to minimize hazards between the road on which the development fronts and its vehicular points of access”.*

**Section 9.3.1(3)** states that: “development shall be supported by individual private wells and sewage systems unless the City agrees to the development of multiple lots on a small water and wastewater system in conformity with the policies in this Plan”.

**Section 9.3.1(6)** states that: “lands designated as Rural Industrial and Logistics, located on Carp Road are subject to the policies of the Carp Road Corridor Area-Specific Policy Area”.

**Section 9.3.2** sets out permitted uses in the Rural Industrial and Logistics areas, which includes heavy and light industrial uses, equipment and supply centres, landscape and construction yards, as well as transportation, distribution, warehouse and large-scale storage operations, among others.

→ The proposed development of a warehouse building on this Rural and Industrial Logistics designated site is a permitted use of the land and appropriate for the context. The warehouse use supports the City’s Official Plan policies by providing appropriate open space and screening from Carp Road to maintain the rural character of the site. The OP policies also support redevelopment of this area with access to the arterial Carp Road and the introduction of employment-type uses in the City’s rural area. The proposed warehouse building is a permitted and desirable use of this land.

The site is also subject to the Carp Road Corridor Policy Area (per Area-Specific Policy Plan, Area 8 - Volume 2C) and designated Corridor Commercial Area as shown in Figure 10 below.

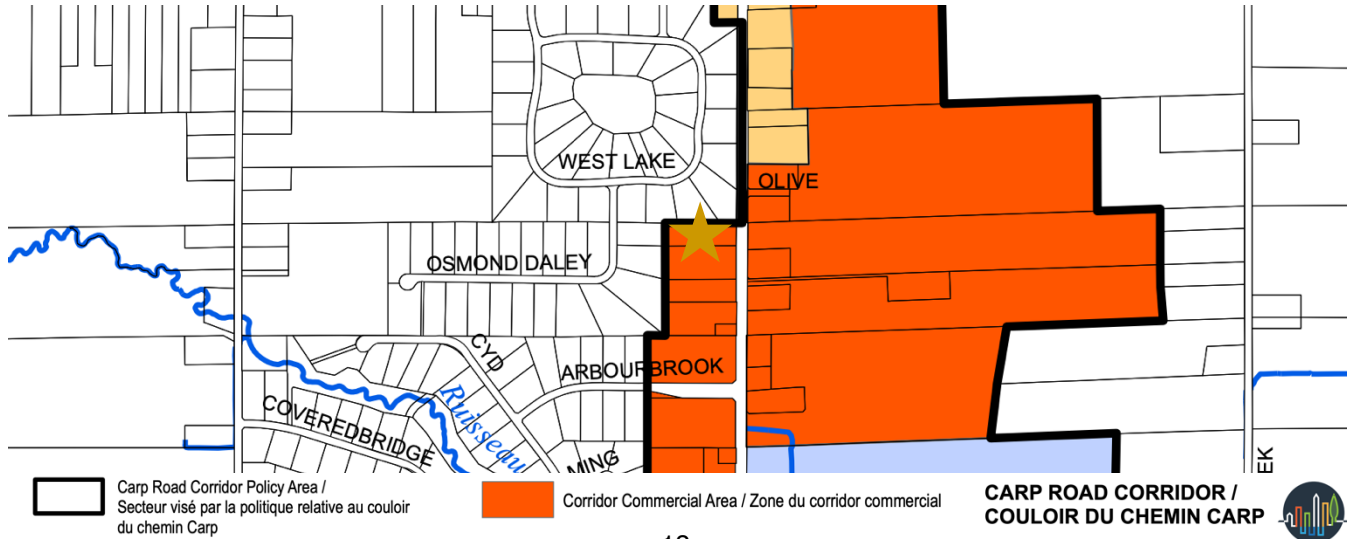


Figure 10. Extract of new Official Plan, Volume 2C, Schedule 8.A – Carp Road Corridor

**Per Section 8.4 of Volume 2C – Area Specific Policies**, permitted uses in the Corridor Commercial Area of the Carp Road Corridor Policy Area include: light industrial, automotive, recreational, and heavy vehicle sales and service.

→ The proposed warehouse is a permitted use in the Carp Road Corridor Policy Area and is appropriate given the surrounding Corridor Commercial uses adjacent to the site. The proposed development meets the intent of the planned function of the area.

### 3.3 Zoning By-law

The site is zoned RC9 – Rural Commercial Subzone 9 in the City of Ottawa’s Zoning By-law 2008-250, as demonstrated in Figure 11 below. The RC9 zone permits a variety of rural commercial and light industrial uses set out below. The purpose of the RC zone is to:

- (1) *permit the development of highway and recreational commercial uses which serve the rural community and visiting public in areas mainly designated as General Rural Area, Village and Carp Road Corridor Rural Employment Area in the Official Plan;*
- (2) *accommodate a range of commercial uses including services for the traveling public as well as agriculture-related, vehicle-oriented and construction products and services;*
- (3) *permit research facilities in areas designated Greenbelt Employment and Institutional Area in the Official Plan, and*
- (4) *regulate development in a manner that has a minimal impact on the surrounding rural area or villages.*

The RC9 zone permits the following uses:

- amusement centre
- amusement park
- animal care establishment
- animal hospital
- artist studio
- automobile rental establishment
- automobile dealership
- automobile service station (*may not retail fuel*)
- campground
- car wash
- click and collect facility
- detached dwelling (*limited to one total, accessory to a permitted use*)
- dwelling unit (*limited to one total, accessory to a permitted use*)
- heavy equipment and vehicle sales, rental and servicing
- hotel
- light industrial
- office
- parking lot
- printing plant
- retail food store, limited to a farmers’ market
- retail store
- service and repair shop
- research and development centre
- storefront industry
- warehouse



Figure 11. Extract of zoning by-law with subject sites shown in white (GeoOttawa, 2021)

Table 1. RC9 zoning provisions

| RC9 Zoning   |                           |   |                           |
|--|---------------------------|---|---------------------------|
| Provisions   |                           | Required  | Provided                  |
| Minimum lot area (m <sup>2</sup> )   |                           | 4000  | 12,041                    |
| Minimum lot width (m)  |                           | 30.0  | 61.0                      |
| Minimum front yard setback (m)   |                           | 10.0  | 161.0                     |
| Minimum interior side yard setback (m)                                       | Abutting residential zone | 4.5   | 4.5 (north)               |
|  | All other zones           | 3.0   | 26.88 (south)             |
| Minimum rear yard setback (m)  |                           | 10.0  | 10.0                      |
| Maximum height (m)   |                           | 11.0  | 7.5                       |
| Maximum lot coverage (%)   |                           | 25%   | 6%                        |
| Landscaping of yards   |                           | Front and corner side yards must be landscaped except for driveways crossing those yards leading to parking   | Yes – hydroseed and trees |
| Outdoor storage  |                           | Permitted in interior side and rear yard only<br>Must be screened and concealed and concealed from view abutting streets and from abutting non-commercial or non-industrial zones | Proposed interior storage |
| Minimum parking spaces (warehouse use)                                       |                           | 0.8 / 100 m <sup>2</sup> for the first 5000 m <sup>2</sup> of GFA   | 5 spaces<br>10 spaces     |
| Minimum drive aisle width for double traffic lane leading to parking lot (m) |                           | 6.0   | 6.0                       |
| Landscaping for parking lots   |                           | 15% of the parking lot area must be perimeter or interior between the lot line and the perimeter of the parking lot, and can include landscape islands, medians, pathways, plazas | Landscaped                |

|  |                           |          |          |
|--|---------------------------|----------|----------|
| Width and location of landscaped buffer for more than 10 but fewer than 100 spaces | Abutting a street (m)     | 3.0 m    | 12.0     |
|  | Not abutting a street (m) | 1.5 m    | 11.75    |
| Bicycle parking spaces   | 1/250 m <sup>2</sup> GFA  | 3 spaces | 3 spaces |

**SECTION 4: SUPPORTING PLANS & STUDIES**

A brief overview of the supporting studies prepared by other consultants is included below. Note that this is for ease of reference only and are a broad and incomplete overview of the submitted reports. Full details are found in the referenced reports prepared by others.

**4.1 Servicing & Stormwater Management Report**

A Servicing & Stormwater Management Report was prepared by Stantec Consulting Limited, dated December 2, 2022. The intent of the report is to provide an on-site servicing scenario free of conflicts, in accordance with City of Ottawa guidelines, and utilizing existing local infrastructure. Per the enclosed report, the site requires private servicing for domestic and emergency fire flows. Three underground potable water storage tanks are proposed to supplement low yielding wells to meet fire flow demands, which will receive water from the well during off-peak hours. The report also confirms that given the site is not serviced by municipal sanitary, a new septic tank and bed will be installed servicing the building via a 150mm diameter sanitary service lateral. The report also sets out the required measures to control the post-development runoff from the development quantity and quality of stormwater released to pre-development rates. The report proposes that stormwater from the site and external areas will be collected in a swale and ultimately conveyed to the roadside ditch on Carp Road over a rock weir as an outlet. A dry well in the northwest corner of the site is proposed. The swale will be used for stormwater detention, reduce peak flow discharge to the roadside ditch, promote infiltration, and improve water quality. The report concludes that the existing well is capable for fire flow supply, a new septic system will support the occupancy and usage demands, the stormwater management plan can control on-site runoff and meet target allowable release rates, site grading will be directed as much as possible to the Carp Road ditch east of the site, and erosion and sediment control measures will be implemented during construction to reduce environmental impacts to the receiving watercourse. For a detailed review of the findings and recommendations, refer to the full report enclosed with this submission.

**4.2 Hydrogeological Investigation & Terrain Analysis**

A Hydrogeological Investigation & Terrain Analysis was prepared by Gemtec Consulting Engineers and Scientists Limited, dated December 14, 2022. The report sets out the following objectives: to confirm construction of a new well is in accordance with the MECP requirements; confirm the water quality meets the Ontario Drinking Water Standards and maximum treatable limits; confirm the quantity of water meets MECP requirements; and confirm the septic impact assessment meets the MECP requirements. The report sets out the following conclusions:

- “The surficial soils encountered consist of silty sand fill material underlain by native sand and gravel glacial till.”

- “The maximum drawdown in the water level of the well was approximately 0.7 m after 8 hours of pumping at a flow rate of 56.8 litres per minute... Interference effects on neighbouring properties are not anticipated given the high well yields and negligible drawdown during pumping.”
- The groundwater quality exceeds the ODWQS for the aesthetic objective of manganese, warning level for sodium, and operational guideline for hardness. Initial well water samples reported exceedances for both fecal and total coliform; however, follow-up samples after chlorination were clear of bacterial exceedances...The water is suitable for consumption based on the bacteriological testing, although period sampling is recommended...”
- “The site is not considered to be hydrogeologically sensitive as thin soils, highly permeable soils, or outcropped bedrock were not encountered... Neighbouring dug wells are not expected to be impacted; however, one dug well may be in operation more than 415 m southeast and upgradient of the site. An incomplete well record was also located more than 400m away.”
- “Nearby borehole logs and design drawings for the swales and dry well suggest there will be greater than 2m of overburden separating these features from the bedrock aquifer; however, this should be confirmed during installation...”
- “Background nitrates are considered to be negligible...”
- “The average DDSSF for the proposed development must remain below the maximum allowable septic flow per MECP, which is calculated to be 7,020 l per day, assuming the use of an advance treatment septic system capable of reducing nitrates by 50%, or 2,340 l per day using a conventional system...”

For a complete overview of the findings, conclusions, and recommendations, refer to the report enclosed with this submission package.

### 4.3 Geotechnical Investigation

A Geotechnical Investigation was prepared by Gemtec Consulting Engineers and Scientists Limited, dated January 30, 2023. The purpose of the investigation was to identify the subsurface and groundwater conditions via limited boreholes and provide engineering guidelines on the geotechnical design of the project, including construction considerations. The report confirms that excavation for the development will be carried out through topsoil where encountered, fill material and into the glacial till deposit. The overburden soils can be classified as Type 3 and the sides of excavations should be sloped accordingly. The report sets out excavation recommendations, in addition to groundwater management. The report recommends that the development could be founded on footings bearing on or within the native undisturbed glacial till deposits, and the topsoil and fill should be removed from below any foundations and slabs on grade. The report proceeds to provide recommendations regarding grade raise restrictions, frost protection for the foundation, seismic design, foundation wall backfilling and drainage, and slab on grade support. The report sets out further recommendations regarding excavation and backfilling for servicing, as well as parking and drive aisle areas. Refer to the Geotechnical Investigation enclosed with this submission for full findings and recommendations.

### 4.4 Environmental Impact Statement

An Environmental Impact Statement and Tree Conservation Report was prepared by Gemtec Consulting Engineers and Scientists Limited, dated January 12, 2023, for the purpose of identifying the presence or

absence of natural heritage features and species at risk (SAR) on the site. The report concludes that no natural heritage features were identified on-site or within the study area, and the following SAR and their habitat were identified as having potential to occur: eastern small-foot myotis, little brown myotis, tri-coloured bat and loggerhead shrike. There were no butternut trees observed. The report indicates that there are no potential impacts anticipated to natural heritage features or significant wildlife habitat. The report sets out that if SAR are discovered throughout project works, operations should stop and the SAR biologist and MECP should be contacted for further direction, and best management practices and adherence to vegetation clearing windows should be followed. Overall, the report concludes that the project complies with the natural heritage policies of the PPS, and the natural heritage policies of the City of Ottawa Official Plan, Carp Road Corridor Community Development Plan, and Carp River Watershed/Subwatershed Study. Refer to the enclosed report for detailed report findings, recommendations, and conclusions.

**SECTION 5: CONCLUSION**

The subject is a large rural lot located on the west side of Carp Road, municipally known as 2885 Carp Road. The property currently contains a portable trailer which is to be removed following completion of construction of the proposed new low-rise warehouse building. The site is located within an area surrounded by other similar rural commercial and light industrial uses including warehouses, vehicle sales, and others generally to the south and east along Carp Road. The site is just south of a rural residential subdivision and north of a rural business park.

The proposed warehouse will be accessed by a driveway along the northwest side of the property which follows along the interior lot line. The proposed 9 parking spaces are situated in front of the warehouse building, which is located toward the most southwesterly corner of the site. The front yard area will be seeded with hydroseed mix with trees proposed along the drive aisle and the front lot lines. Several coniferous trees are proposed along the northwest interior side lot line.

Overall, the proposed warehouse building is consistent with the Provincial Policy Statement 2020, conforms to the policies of the City of Ottawa’s Official Plan and complies with the provisions of the City of Ottawa’s Zoning By-law 2008-250. The development and associated application for Site Plan Control represents good land use planning and is recommended for approval.

Respectfully submitted,



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Principal + Senior Planner

