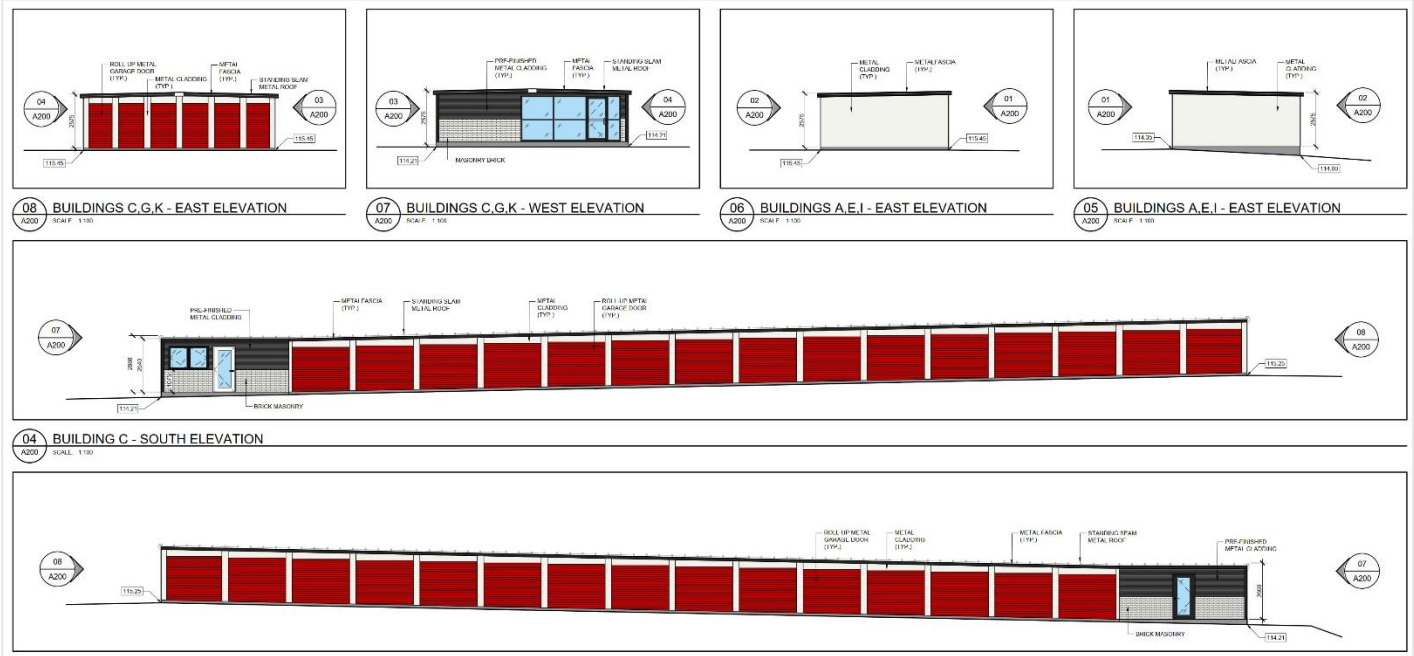


PLANNING RATIONALE

273 & 275 RUSS BRADLEY ROAD



Project No.: OCP-22-1643-01

Prepared for:

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TABLE OF CONTENTS

1.0 OVERVIEW / INTRODUCTION..... 2

2.0 SUBJECT LANDS & SITE CONTEXT 3

3.0 DEVELOPMENT PROPOSAL 5

4.0 PLANNING POLICY & REGULATORY FRAMEWORK 6

 4.1 *Provincial Policy Statement..... 6*

 4.2 *City of Ottawa Official Plan, 2021..... 7*

 4.3 *Carp Road Corridor Community Design Plan 8*

 4.4 *City of Ottawa Zoning By-law No. 2008-250..... 9*

5.0 DESIGN BRIEF 12

6.0 PUBLIC CONSULTATION STRATEGY 14

7.0 CONCLUSION..... 15

1.0 OVERVIEW / INTRODUCTION

This Planning Rationale is submitted in support of a Site Plan Control application for the development of a self storage warehouse facility. The proposed development is for lands known municipally as 273 & 275 Russ Bradley Road. This rationale and the Site Plan Control application are submitted on behalf of Trevor Watkins.

A pre-application consultation meeting was held with City of Ottawa staff on June 3, 2021, to identify the reports and plans required in support of this application. Preconsultation follow-up meetings were held on March 23, 2022 and August 31, 2022 to discuss groundwater and infiltration issues relating to the proposed development. Subsequent correspondence with City and review agency staff was carried out via email.

This Planning Rationale will address the appropriateness of the proposed development in the context of the existing planning policy and regulatory framework.

2.0 SUBJECT LANDS & SITE CONTEXT

The subject land is known municipally as 273 and 275 Russ Bradley Road and is in West Carleton-March in the City of Ottawa. The subject land is described legally as Block 15 and Part of Block 16 on Plan 4M-1511 and further as Parts 6, 7, 8, 9, 10, and 11 on Plan 4R-33191, City of Ottawa. The subject land is located on the south side of Russ Bradley Road, immediately west of Carp Road, as illustrated on Figure 1: Location of Subject Land. The subject land has an area of 24,247 square metres (5.99 acres). There is a 30cm reserve between the subject land and Russ Bradley Road, comprised of Parts 2, 3, 4, and 5 on Plan 4R-33191, and once lifted/reconveyed, the subject land will have frontage of 99.1 metres. There is a watercourse that traverses the north end of the subject land, immediately south of Russ Bradley Road. The subject land is currently vacant with a gravel entrance off of Russ Bradley Road and a small gravel pad at the northeast corner of the subject land.



Figure 1: Location of Subject Land (Source: Google Imaging)

The subject land is located approximately three (3) kilometers south of the Village of Carp, and less than six and a half (6.5) kilometers north of the Carp Road and Highway 417 interchange. The subject land is located within the business park at the Carp Airport that was approved as part of a largescale subdivision of the Carp Airport lands.

Located to the north of the subject land is vacant land within the business park, with agricultural uses beyond. Situated immediately east of the subject land is Carp Road with the Irish Hills Golf & Country Club beyond. South of the subject land is vacant land (beyond an existing runway), with a variety of rural commercial and industrial uses located beyond. Located to the west of the subject land are lands that were recently subject to planning applications to permit office, warehouse, and cannabis production facility uses, with the Carp Airport and future airport related residential uses beyond. Please see Figure 2: Surrounding Land Uses for additional details pertaining to the existing and proposed land uses in the vicinity.



Figure 2: Surrounding Land Uses (Source: GeoOttawa)

3.0 DEVELOPMENT PROPOSAL

This proposal contemplates the development of the subject land with a series of twelve (12) self storage warehouse buildings having a total gross floor area of 6,325.6 square metres. The gross floor areas of the individual buildings range from 241.6 square metres to 817.6 square metres. A total of 423 storage units are proposed, having individual unit areas ranging between 4.65 square metres and 27.8 square metres. The proposed buildings will be sited in four (4) strips running in a north/south fashion on the subject land. Much of the required parking will be located at the rear of the property, with small clusters of parking spaces located throughout the development. The proposed entrance, drive aisles, and parking areas will be gravel. The proposed development will retain the landscaping in the front yard that was established in conjunction with the relocated watercourse and will also provide additional landscaping features within the required 10 metre landscape buffer abutting Carp Road.

Access will be provided via a single entrance from Russ Bradley Road. An illustration of the development is provided below in Figure 3: Extract from Proposed Site Plan.

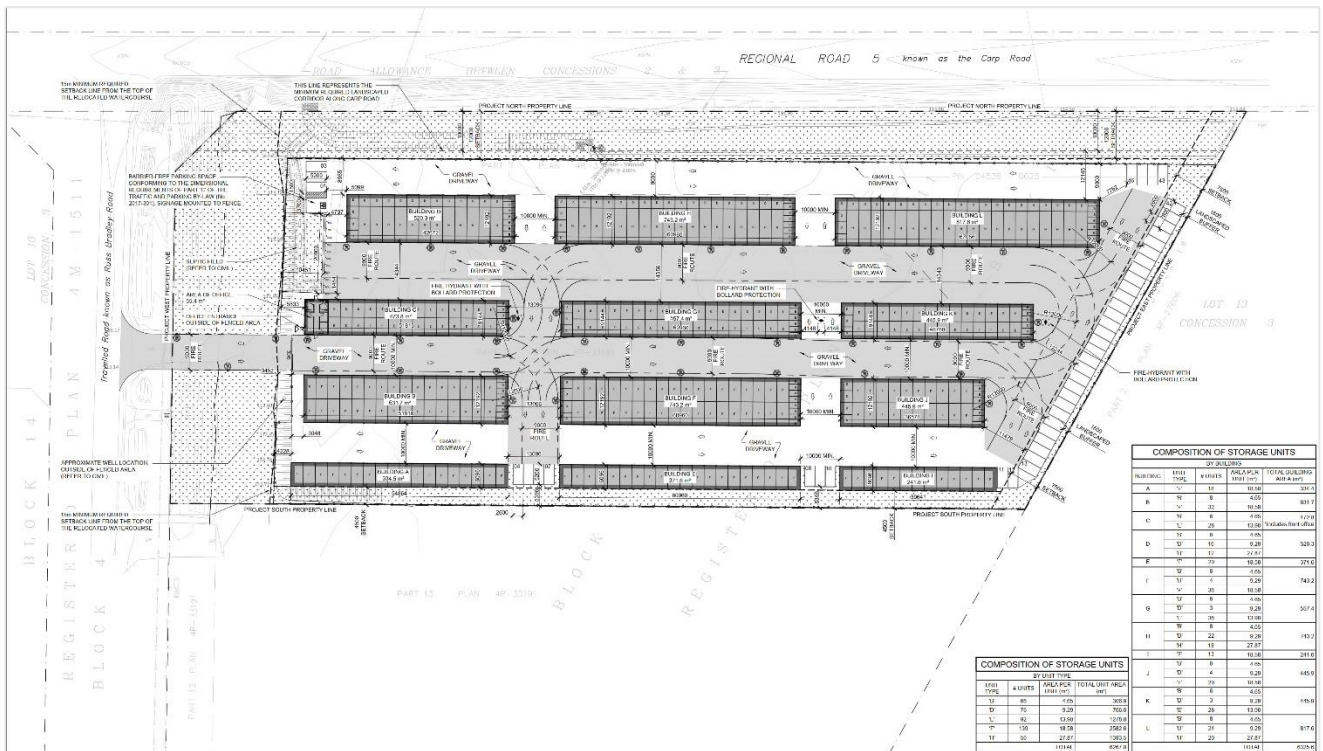


Figure 3: Extract from Proposed Site Plan

4.0 PLANNING POLICY & REGULATORY FRAMEWORK

4.1 Provincial Policy Statement

The 2020 Provincial Policy Statement (the “PPS”) provides policy direction on land use planning matters of provincial interest, including development in rural areas. The PPS includes policies that guide development in the rural area, permitting development when the rural landscape can be respected, and the development can be adequately serviced by rural infrastructure and private services. Healthy, liveable, and safe communities are sustained in part through providing for the development of various uses, including commercial and light industrial uses such as the proposed self storage warehouse development.

The PPS provides that municipalities shall promote economic development through various measures, including ensuring suitable land and related infrastructure is available for the development of a mix of employment uses. The subject land is within a parcel that has been approved for use as a large-scale business and industrial park. The proposed warehouse and storage development will contribute to the diversity of the light industrial uses within the WestKan Business Park, as well as the rural Carp community generally.

The PPS provides that where municipal water and municipal sewage services are not available, individual on-site sewage services and individual on-site water services may be used where it can be demonstrated that site conditions are appropriate for the long-term provision of such, and that there will be no negative impacts stemming from such private services. The proposed development will utilize a private well (existing) and a private septic system (proposed) to service the proposed development, specifically the office and washroom located in Building C. A Hydrogeological Assessment & Terrain Assessment was prepared by McIntosh Perry Consulting Engineers Ltd. that concluded the on-site well can adequately supply the proposed development and further, that there is sufficient spatial area and suitable soils for the proposed private septic system. Additional details can be found in the Hydrogeological Assessment & Terrain Analysis dated December 23, 2022. Water for firefighting purposes will be provided via the existing infrastructure within Russ Bradley Road. A series of hydrants are proposed to be located throughout the site to meet firefighting access requirements.

As noted previously, the proposed development is in close proximity to the privately owned Carp Airport facility. Per the policies of the PPS, the development of land surrounding airports shall not hinder the long-term viability of the airport. No negative impacts on the existing airport are anticipated given the subject land was designated for employment and commercial development through a broad land use planning exercise.

The PPS indicates that natural areas and features must be protected, and that development on lands adjacent to natural heritage features are prohibited unless it is demonstrated that there will be no negative impacts on such features or their ecological functions. An Environmental Impact Statement and Tree Conservation Report has been prepared by McIntosh Perry Consulting Engineers Ltd. indicating that the subject land is comprised of a variety of natural heritage features. Upon the completion of various field/site visits and the review of natural areas and features on the subject land, a series of mitigation measures have been provided to ensure the protection of the various features. Further information can be found in the Environmental Impact Statement and Tree Conservation Reports dated December 23, 2022.

The proposed development represents commercial intensification that can be adequately serviced within an identified business park and as such, is consistent with the policies of the Provincial Policy Statement, 2020.

4.2 City of Ottawa Official Plan, 2021

The subject land is designated Rural Countryside pursuant to Schedule B9: Rural Transect, of the City of Ottawa’s Official Plan, as shown on Figure 4: Extract from Schedule B9: Rural Transect.

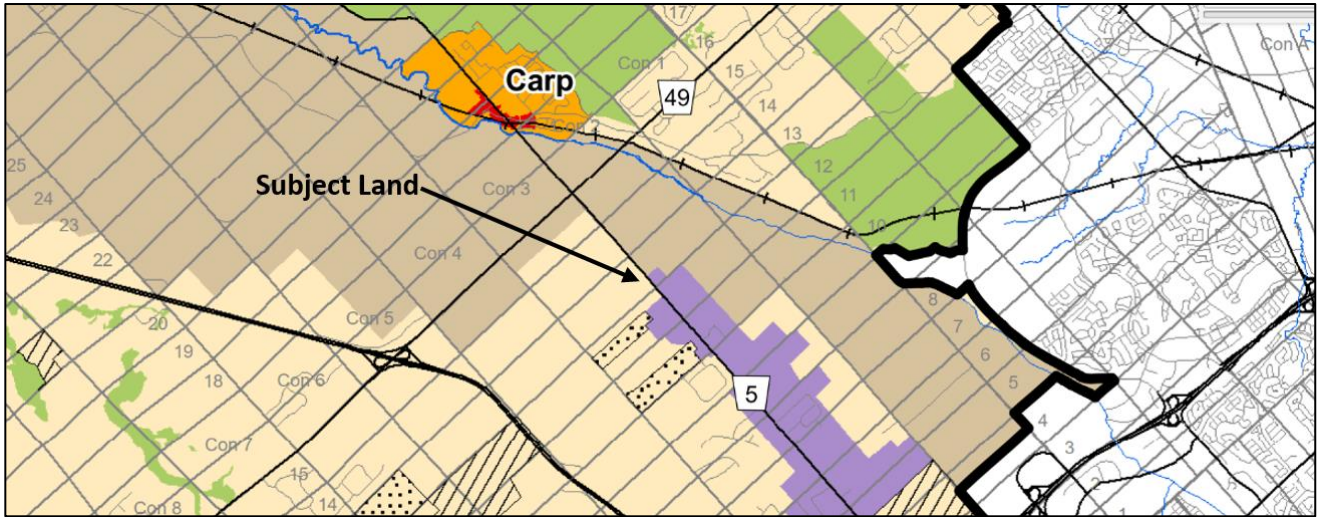


Figure 4: Extract from Schedule B9: Rural Transect

The subject land is further identified as Carp Airport Area within the Carp Road Corridor Policy Area pursuant to Area Specific Policy Plan Schedule 8.A – Designation Plan, as illustrated on Figure 5: Extract from Schedule 8.A Designation Plan (Carp Road Corridor).

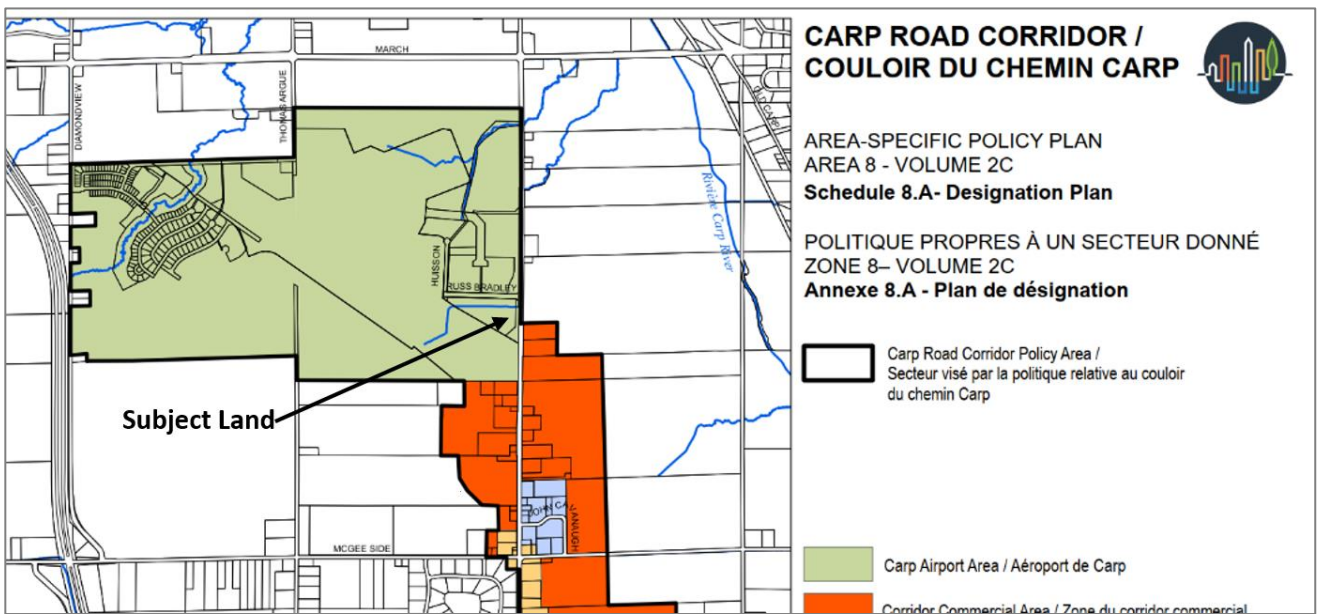


Figure 5: Extract from Schedule 8.A – Designation Plan (Carp Road Corridor)

The purpose of the Carp Airport Area designation is to recognise and protect lands for the provision of airport facilities and uses that serve aviation needs generally. Light industrial uses, such as the proposed self storage warehouse, are permitted within this designation. The proposed warehouse/storage use will be situated within an existing business park that was contemplated in the Carp Airport master land use planning exercise and will diversify the commercial uses in the Carp Airport area. The proposed development will not conflict with or present a risk to the operational requirements of the existing Carp Airport.

The relocated watercourse is identified on Schedule C11-A Natural Heritage System (West). Additional information pertaining to the assessment and protection of the natural heritage features is provided in the accompanying Environmental Impact Statement & Tree Conservation Reports prepared and submitted in support of this application for Site Plan Control approval.

The proposed development will be serviced with private services as anticipated and expected for uses within the Rural Countryside designation within the Carp Road Corridor Policy Area. On site stormwater management facilities will be provided on the subject land. Addition details are available in the Servicing & Stormwater Management Report dated December 23, 2022, where it is demonstrated that the proposed development can be adequately serviced on private services.

The proposed development confirms to the policies of the City of Ottawa Official Plan, 2021.

4.3 Carp Road Corridor Community Design Plan

The subject land is designated Carp Airport pursuant to Schedule 1 of the Carp Road Corridor Community Design (CRCCDP) plan, as shown on Figure 6: Extract from Schedule 1, Carp Road Corridor Community Design Plan.

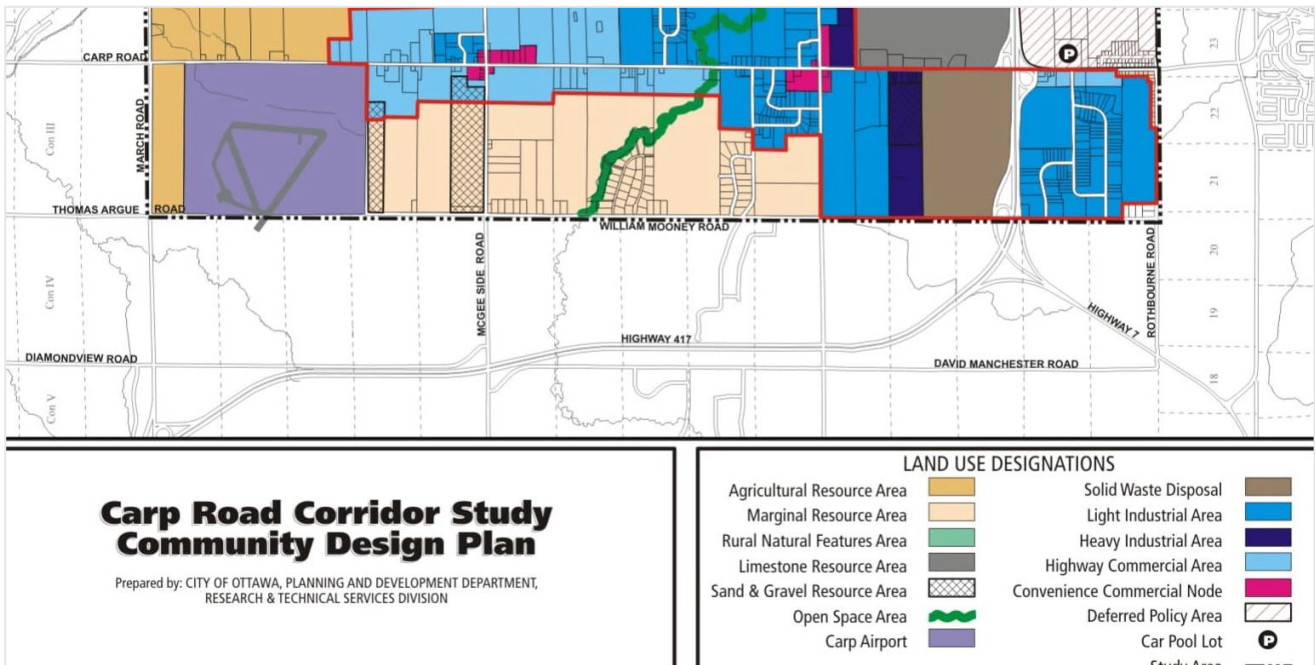


Figure 6: Extract from Schedule 1, Carp Road Corridor Community Design Plan

The CRCCDP indicates that the Carp Airport is designated within the Ottawa Official Plan and identifies the permitted uses as aviation uses and those uses associated with an airport. The Community Design Plan further notes that the policies for the Carp Airport included in the City of Ottawa Official Plan shall apply; the proposed development’s conformance with such policies is addressed above.

The subject land is further identified as being located within a High Recharge area pursuant to Schedule 2 of the Carp Road Corridor Community Design (CRCCDP) plan, as shown on Figure 7: Extract from Schedule 2, Carp Road Corridor Community Design Plan Environmental Features.

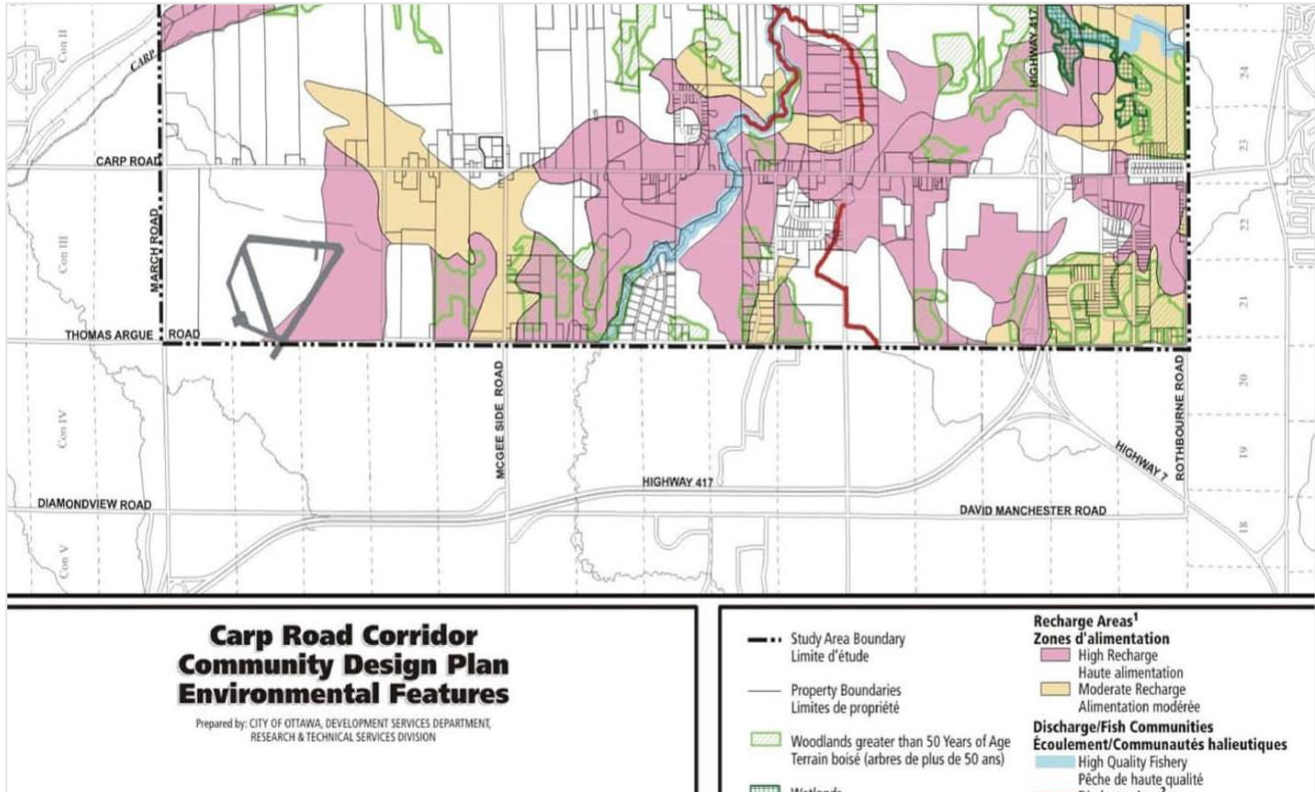


Figure 7: Extract from Schedule 2, Carp Road Corridor Community Design Plan Environmental Features

The location of the subject land within a recharge area and the permeability testing results are addressed in the accompanying Hydrogeological Assessment & Terrain Analysis.

The CRCCDP identifies a series of measures that are aimed at improving the visual appearance and aesthetics of the corridor and its associated rural landscape. The proposed development’s conformity with such measures is addressed in Section 5.1 Design Statement below.

The proposed development of the subject land will meet the objectives of the CRCCDP.

4.4 City of Ottawa Zoning By-law No. 2008-250

The subject land is zoned Air Transportation Facility Zone, Subzone B (T1B) pursuant to City of Ottawa Zoning By-law No. 2008-250 (the “zoning by-law”), as show on Figure 8: Extracts from GeoOttawa, Zoning Layer.

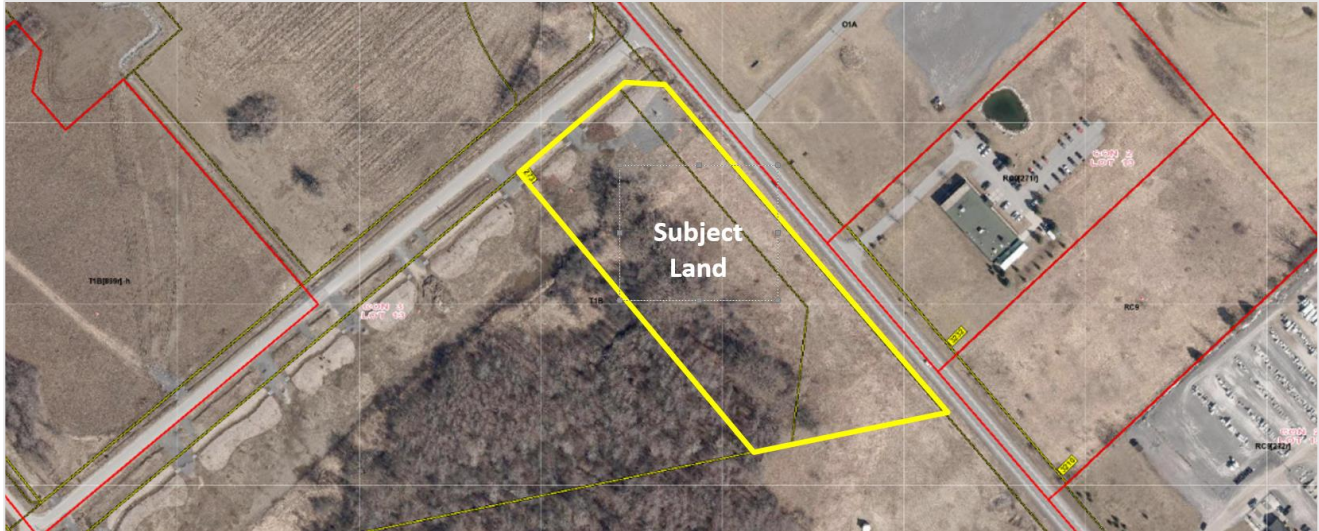


Figure 8: Extracts from GeoOttawa, Zoning Layer

The purpose of the Air Transportation Facility Zone, as it pertains to the Carp Airport, is to permit air transportation facilities and other uses related to the aviation sector. The zoning by-law identifies the uses which are permitted in the general Air Transportation Facility Zone, which include warehouse uses. The following table identifies the applicable zoning standards for the T1B zone, as well as general zoning standards relating to required parking and loading and demonstrates the proposed development’s compliance with same.

Table 1: Overview of Zoning Compliance

Regulation	Required	Provided	Compliance
Minimum Front Yard Setback	12m	>12m	Yes
Minimum Rear Yard Setback	7.5m	>7.5m	Yes
Minimum Interior Side Yard Setback	4.5m	4.5m	Yes
Minimum Corner Side Yard Setback	12m	>12m	Yes
Minimum Setback from Watercourse	15m From Top of Bank	>15m	Yes
Maximum Floor Space Index	1.0	<1.0	Yes
Maximum Lot Coverage	50%	26.1%	Yes
Minimum Distance Between Buildings on Same Lot	10m	10m	Yes
Maximum Building Height	Requirements Determined by Transport Canada		
Minimum Width Landscaped Area/Buffer (Abutting Street)	3m	>3m	Yes
Minimum Width Landscaped Area/Buffer (Not Abutting Street)	1.5m	1.5m	Yes
Minimum Width of Landscape Buffer Abutting Carp Road	10m	>10m	Yes
Minimum % of Parking Lot Provided as Landscaped Area	15%	988.8%	Yes
Minimum Required Parking Spaces (Vehicle)	46	46	Yes
Minimum Required Loading Spaces	1	Various	Yes

As detailed above, the proposed development will comply with the regulations of the Air Transportation Subzone B zone.

5.0 DESIGN BRIEF

The layout of the proposed development has been designed in accordance with the applicable standards and regulations of the City of Ottawa Comprehensive Zoning By-law, the Carp Road Corridor Community Design Plan, requirements outlined in an agreement relating to previous approvals on the Carp Airport lands, and the operational requirements of the proposed use.

The exterior of the proposed buildings will feature a cream-coloured metal cladding, black metal fascia, and red roll up metal garage doors. Proposed Building C will include office space, the elevation of which will feature pre-finished a dark-coloured metal cladding, masonry brick, and glazing. The buildings will feature a standing seam metal roof. These elements are illustrated in Figure 9: Extract from Proposed Elevations below.

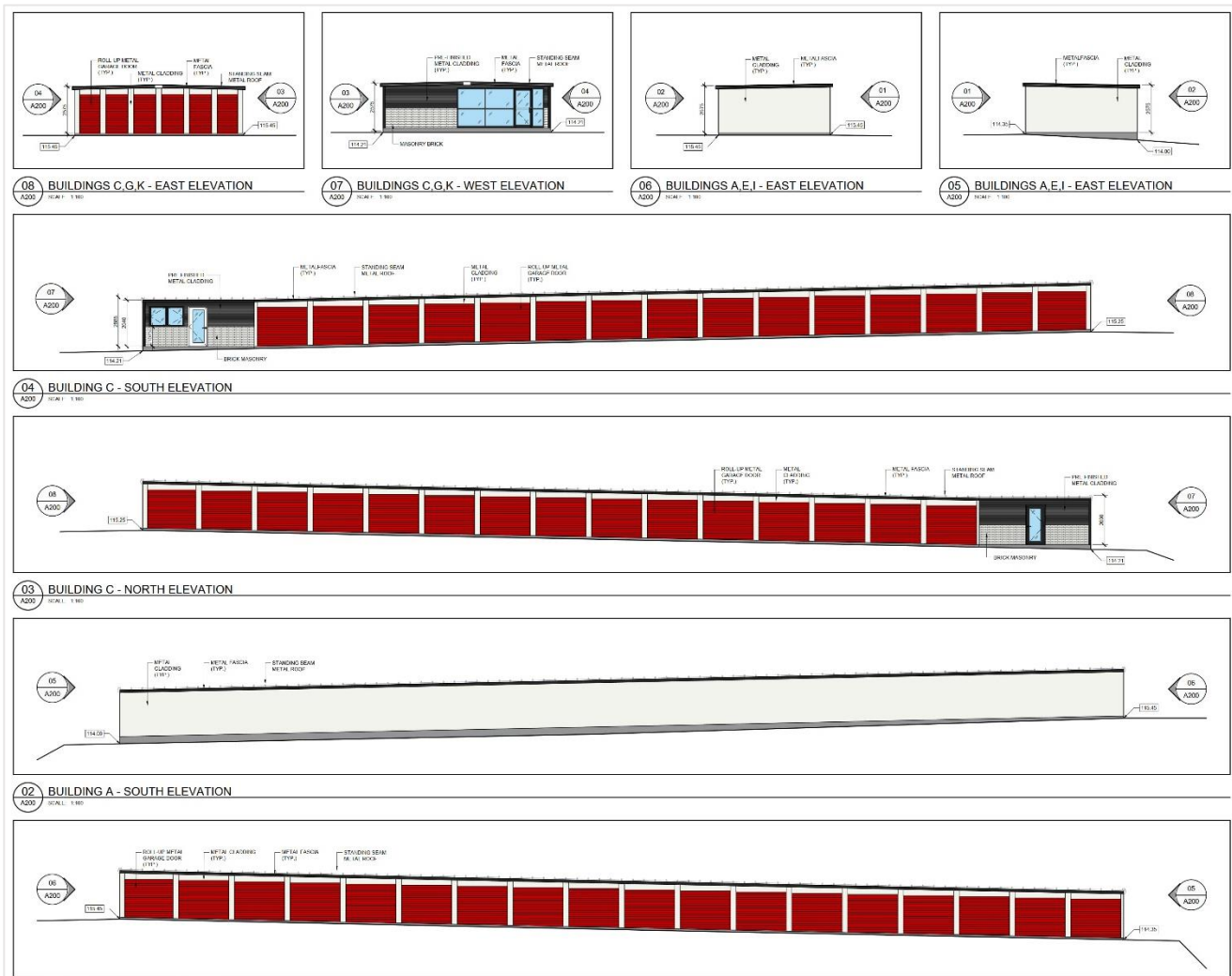


Figure 9: Extract from Proposed Elevations

The proposed landscaping will include deciduous tree plantings across the front and at the rear corners of the subject land, along with a large reforestation bed within the landscape buffer adjacent Carp Road. The reforestation bed will include a variety of deciduous and coniferous plantings, as well as shrubs, which is in

keeping with the design guidelines within the Carp Road Corridor Community Design Plan for industrial business parks and subdivisions. These features are in addition to the landscaping features located in the corridor of the relocated watercourse that was designed and provided by others.

The design guidelines of the Carp Road Corridor Community Design Plan also include measures such as limiting access to Carp Road and locating parking to the rear of buildings. The access to the proposed development will be from an internal subdivision road with no access to Carp Road proposed. The proposed parking is largely located to the rear of the subject land, with small pockets of parking located throughout the site.

The design and appearance of the proposed development is compatible with the surrounding neighbourhood.

6.0 PUBLIC CONSULTATION STRATEGY

This Site Plan Control application is subject to public consultation given the size of the proposed development.

A pre-application consultation meeting was held with City of Ottawa staff on June 3, 2021, to identify the reports and plans required in support of this application. Preconsultation follow-up meetings were held on March 23, 2022 and August 31, 2022 to discuss groundwater and infiltration issues relating to the proposed development. Subsequent correspondence with City and review agency staff was carried out via email.

Councillor Clarke Kelly (Ward 5, West Carleton-March) will be notified via email that the applications in support of the proposed development have been submitted. We would be happy to participate in a community information session should same be requested by and/or deemed necessary by the City of Ottawa or the Councillor.

7.0 CONCLUSION

The proposed development is consistent with the Provincial Policy Statement and conforms to the City of Ottawa Official Plan. The proposed self storage warehouse use is permitted pursuant to the City of Ottawa's Zoning By-law No. 2008-250 and the development of such use will follow the standards of the Air Transportation Facility Subzone B.

The proposal is an appropriate use of the subject property, is in the public interest, and represents good planning.

Respectfully submitted,

A handwritten signature in black ink that reads "Bridgette Alchawa". The signature is written in a cursive, flowing style.

Bridgette Alchawa, RPP
Land Use Planner