

Engineering

Land/Site
Development
Municipal
Infrastructure
Environmental/
Water Resources
Traffic/
Transportation
Recreational

Planning

Land/Site
Development
Planning Application
Management
Municipal Planning
Urban Design
Expert Witness
(LPAT)
Wireless Industry

Landscape Architecture

Streetscapes &
Public Amenities
Open Space, Parks &
Recreation
Community &
Residential
Commercial &
Institutional
Environmental
Restoration

U-Haul Moving & Storage of Ottawa 30 Frank Nighbor Place Ottawa, Ontario Planning Rationale



Prepared for: U-Haul Co. (Canada) Ltd.

**U-HAUL MOVING & STORAGE OF OTTAWA
30 FRANK NIGHBOR PLACE
OTTAWA, ONTARIO**

**PLANNING RATIONALE
IN SUPPORT OF APPLICATIONS FOR
SITE PLAN CONTROL AND LIFTING HOLDING BY-LAW**

Prepared For:



Prepared By:



Suite 200, 240 Michael Cowpland Drive
Ottawa, Ontario
K2M 1P6

May 20, 2022

Novatech File: 121326
Ref: R-2022-049

May 20, 2022

City of Ottawa
Planning, Real Estate, & Economic Development Department
110 Laurier Avenue West, 4th Floor
Ottawa, Ontario
K1P 1J1

Attention: Colette Gorni – Planner I

**Reference: U-Haul Moving & Storage of Ottawa
Planning Rationale in Support of Applications for Site Plan Control and
Lifting Holding By-law
30 Frank Nighbor Place
Our File No.: 121326**

Novatech has been retained by U-Haul Co. (Canada) Ltd. to prepare this Planning Rationale in support of applications for *Site Plan Control* and *Lifting Holding By-law* for their property municipally known as 30 Frank Nighbor Place in Ward 23 – Kanata South, Ottawa, Ontario. The herein will be referred to as the ‘Subject Site’.

U-Haul Co. (Canada) Ltd. is proposing to develop a self-storage facility with an automobile rental establishment on the Subject Site. The proposed ‘U-Haul Campus’ will feature two (2) storage buildings, ‘Building A’ comprising of 182,090 sq. ft. (16,916.72 m²) and ‘Building D’ comprising of 16,724 sq. ft. (1,554.71 m²), and two (2) outdoor enclosed storage buildings, ‘Buildings B and C’ each comprising of 2,342 sq. ft. (217.58 m²). A total of ten (10) bicycle parking spaces will be provided and are conveniently located near the entrance of ‘Building A’. A total of one hundred one (101) surface parking spaces will be provided for the proposed development and includes two (2) barrier-free parking spaces.

The Subject Site formed part of a previous approved *Consent* (City File No.: D08-01-17/B-00325 to D08-01-17/B-00327) application filed in 2017 with the City of Ottawa’s Committee of Adjustment. The purpose of the application was to sever 20 Frank Nighbor Place at the time, into three separate parcels for development. A Joint Use and Maintenance Agreement was created to allow both 20 and 30 Frank Nighbor Place access over the portion of the drive aisle. The Subject Site is identified on plans 4M-1012 and 4R-30745.

This Planning Rationale demonstrates how the proposed development is consistent with the *Provincial Policy Statement (PPS)*, conforms to the *City of Ottawa Official Plan*, and complies with the provisions of *Zoning By-law 2008-250*.

Yours truly,

NOVATECH



Robert Tran, M.PL.
Planner, Planning & Development

Table of Contents

1.0 INTRODUCTION AND PROPOSED DETAILS	1
1.1 Site Description and Surrounding Uses.....	1
1.2 Proposed Development	3
2.0 ADDITIONAL REPORTS	4
3.0 PLANNING POLICY AND REGULATORY FRAMEWORK	5
3.1 Provincial Policy Statement (PPS)	5
3.2 City of Ottawa Official Plan	7
3.2.1 Existing Official Plan	8
3.2.2 New Official Plan	12
3.3 Zoning By-law 2008-250	13
4.0 PUBLIC CONSULTATION STRATEGY.....	17
5.0 CONCLUSION	17

Figures

FIGURE 1: SUBJECT SITE AND SURROUNDING USES.....	2
FIGURE 2: EXCERPT FROM SITE PLAN PREPARED BY IBI GROUP, DRAWING A-1001, REV 2, DATED MAY 18, 2022.	4
FIGURE 3: EXCERPT FROM THE EXISTING CITY OF OTTAWA OFFICIAL PLAN SCHEDULE B – URBAN POLICY PLAN....	8
FIGURE 4: EXCERPT FROM NEW CITY OF OTTAWA OFFICIAL PLAN SCHEDULE B5 – SUBURBAN (WEST) TRANSECT.	12
FIGURE 5: EXCERPT OF THE SUBJECT SITE’S EXISTING ZONING FROM GEOOTTAWA.	14
FIGURE 6: EXCERPT FROM ZONING BY-LAW 2008-250 URBAN EXCEPTION 1414.	16
FIGURE 7: EXCERPT FROM ZONING BY-LAW 2008-250 URBAN EXCEPTION 1932.	16

1.0 INTRODUCTION AND PROPOSED DETAILS

Novatech has been retained by U-Haul Co. (Canada) Ltd. to prepare this Planning Rationale in support of applications for *Site Plan Control* and *Lifting Holding By-law* for their property municipally known as 30 Frank Nighbor Place in Ward 23 – Kanata South, Ottawa, Ontario. The herein will be referred to the as the ‘Subject Site’.

U-Haul Co. (Canada) Ltd. is proposing to develop a self-storage facility with an automobile rental establishment on the Subject Site. The proposed ‘U-Haul Campus’ will feature two (2) storage buildings, ‘Building A’ comprising of 182,090 sq. ft. (16,916.72 m²) and ‘Building D’ comprising of 16,724 sq. ft. (1,554.71 m²), and two (2) outdoor enclosed storage buildings, ‘Buildings B and C’ each comprising of 2,342 sq. ft. (217.58 m²). A total of ten (10) bicycle parking spaces will be provided and are conveniently located near the entrance of ‘Building A’. A total of one hundred one (101) surface parking spaces will be provided for the proposed development and includes two (2) barrier-free parking spaces.

The Subject Site formed part of a previous approved *Consent* (City File No.: D08-01-17/B-00325 to D08-01-17/B-00327) application filed in 2017 with the City of Ottawa’s Committee of Adjustment. The purpose of the application was to sever 20 Frank Nighbor Place at the time, into three separate parcels for development. A Joint Use and Maintenance Agreement was created to allow both 20 and 30 Frank Nighbor Place access over the portion of the drive aisle. The Subject Site is identified on plans 4M-1012 and 4R-30745.

This Planning Rationale demonstrates how the proposed development is consistent with the *Provincial Policy Statement (PPS)*, conforms to the *City of Ottawa Official Plan*, and complies with the provisions of *Zoning By-law 2008-250*.

1.1 Site Description and Surrounding Uses

The Subject Site is an irregular shaped parcel of vacant land and is a remaining block within a registered subdivision planned for future development. The topography of the Subject Site is generally flat but slopes downwards towards the Carp River located to the west. The Subject Site has a total area of 3.8 hectares and is accessed by an existing drive aisle that is shared with Camp Mart’s secondary access/egress, located at 20 Frank Nighbor Place to the east as shown on **Figure 1**.

Two easements transect the Subject Site in an east to west direction for the purposes of sanitary, water, and stormwater pipes. The stormwater outlets to the Carp River with the infrastructure owned and maintained by the City of Ottawa and described in a series of easements described as Parts 2 on 4R-11331 and Part 1 on 4R-14133 (Instrument No.: N698162, N698164, and LT1174833).

The Subject Site has remained undeveloped with no known structures or buildings based on a review of 1976 aerial photography from GeoOttawa. The Subject Site was previously used for agricultural related purposes on the eastern portion of the land based on a review of 2014 aerial photography.



Figure 1: Subject Site and surrounding uses.

The following describes the land uses adjacent to the Subject Site as shown in **Figure 1**.

North: Highway 417 abuts the Subject Site immediately to the north. The Bridge Christian Church, undeveloped land as well as a stormwater pond are situated on the north side of Highway 417.

East: Commercial land uses including Camp Mart and The Home Depot are situated east of the Subject Site. As previously discussed, the drive aisle is shared with Camp Mart’s secondary access/egress located at 20 Frank Nighbor Place.

South: A vacant block of land intended for future development at 40 Frank Nighbor Place is situated southeast of the Subject Site. Further southeast of the Subject Site is a recreational and athletic facility. Office and recreational land uses are located south of the Subject Site.

West: An open space area owned by the City of Ottawa and the Carp River abut the Subject Site to the west. A surface parking lot that forms part of the Canadian Tire Centre is located on the west side of the Carp River.

The Subject Site is legally described as follows:

PART OF BLOCK 2 PLAN 4M1012, PARTS 2, 3, 7, 8, 9, 10, 11 AND 12 PLAN 4R30745 SUBJECT TO AN EASEMENT OVER PARTS 3, 10 AND 11 PLAN 4R30745 AS IN N698162 SUBJECT TO AN EASEMENT OVER PARTS 3, 10 AND 11 PLAN 4R30745 AS IN N698164 SUBJECT TO AN EASEMENT OVER PART 9 PLAN 4R30745 AS IN LT1174833 SUBJECT TO AN EASEMENT OVER PARTS 2, 3, 8 AND 11 PLAN 4R30745 IN FAVOUR OF PART OF BLOCK 2 PLAN 4M1012, PART 1 PLAN 4R30745 AS IN OC1955094 SUBJECT TO AN EASEMENT OVER PARTS 2 AND 3 PLAN 4R30745 IN

FAVOUR OF PART OF BLOCK 2 PLAN 4M1012, PARTS 4, 5, 6 AND 13 PLAN 4R30745 AS IN OC1955094 TOGETHER WITH AN EASEMENT OVER PART OF BLOCK 2 PLAN 4M1012, PARTS 5 AND 13 PLAN 4R30745 AS IN OC1955094

1.2 Proposed Development

As previously discussed, U-Haul Co. (Canada) Ltd. is proposing to develop a self-storage facility with an associated automobile rental establishment on the Subject Site as shown on **Figure 2**. 'Building A' consists of a five storey self-storage building and will serve as the main anchor in welcoming guests to the U-Haul Campus. 'Building D' consists of a one storey storage warehouse and will only be accessible to guests once they have checked-in at 'Building A'. 'Buildings B and C' are outdoor enclosed storage buildings which provide for additional self-storage options on the U-Haul Campus.

The location and orientation of the buildings have been carefully sited to capitalize on U-Haul Campus' proximity to Highway 417. No permanent buildings or structures are proposed within the required 14.0 m setback from the Highway 417 Right-of-Way (ROW) as per the *Ministry of Transportation's Highway Corridor Management Manual – April 2022*.

The U-Haul Campus will be accessed by an extension of the drive aisle off of Frank Nighbor Place which is shared with 20 Frank Nighbor Place. Pedestrian circulation for the U-Haul Campus will be achieved by extending the existing concrete sidewalk as part of the drive aisle to a series of internal pathway connections. A total of ten (10) bicycle parking spaces will be provided for the U-Haul Campus and are conveniently located near the entrance of 'Building A'. A total of one hundred one (101) surface parking spaces will be provided throughout the Subject Site and includes two (2) barrier-free parking spaces. Please refer to the accompanying Architectural Design Brief prepared by IBI Group, dated May 16, 2022 for building design details and how the U-Haul Campus conforms to the *City of Ottawa Official Plan's* applicable urban design policies.

Stormwater management for the U-Haul Campus will include both stormwater quantity and quality control measures prior to releasing flows towards the Carp River. This will be achieved by a treatment train of grass swales, an on-site stormwater management facility (dry pond) located in the southwest corner of the Subject Site, and the use of an oil/grit separator. Further detailed information is available in the Development Servicing Study and Stormwater Management Report prepared by Novatech dated May 20, 2022.

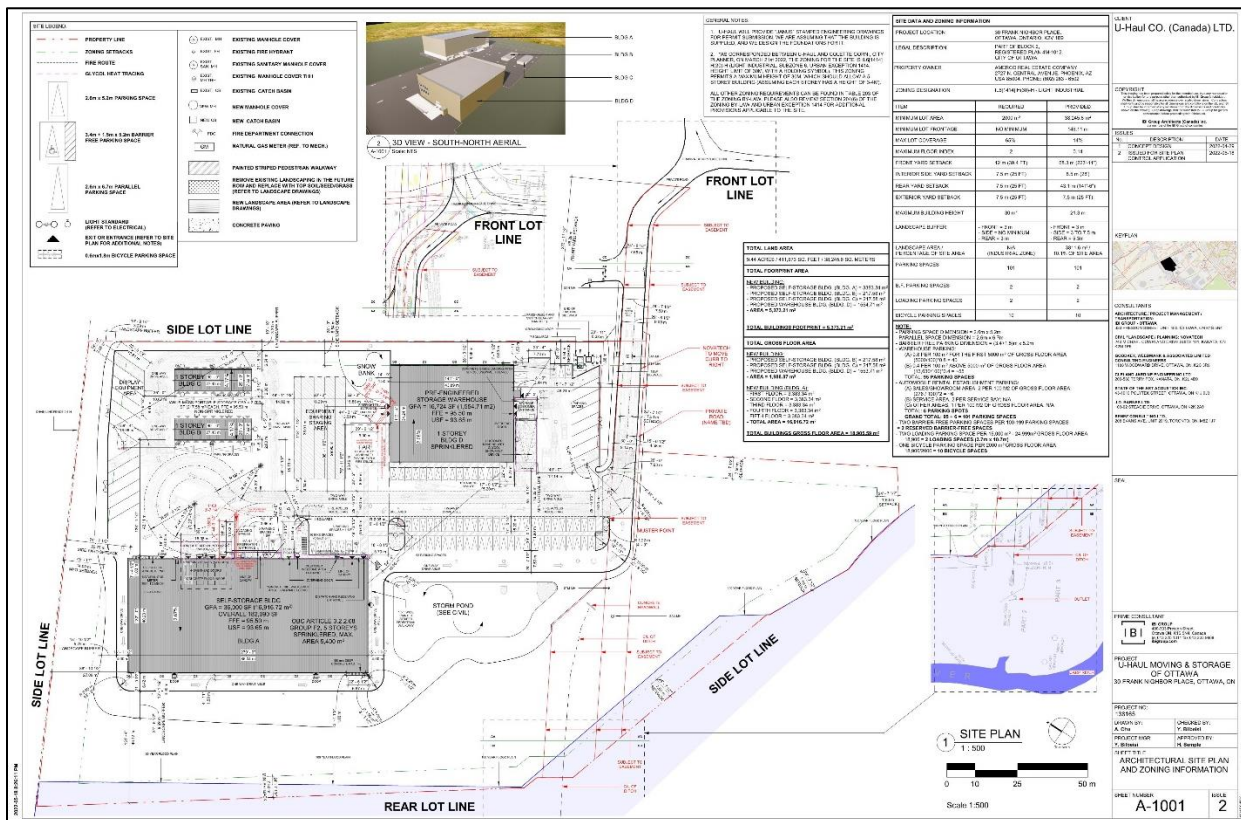


Figure 2: Excerpt from Site Plan prepared by IBI Group, Drawing A-1001, REV 2, dated May 18, 2022.

2.0 ADDITIONAL REPORTS

This report should be read in conjunction with the following reports and plans prepared by Novatech, IBI Group, WSP Golder, and Paterson Group.

- Development Servicing Study and Stormwater Management Report: R-2022-014, prepared by Novatech dated May 20, 2022.
- Landscape Plans, Drawings L1-L2, REV 1, prepared by Novatech dated May 20, 2022.
- Site Plan, Drawing A-1001, REV 2 prepared by IBI Group, dated May 18, 2022.
- Architectural Design Brief prepared by IBI Group, dated May 16, 2022.
- Architectural Building Elevations prepared by IBI Group dated April 11, 2022.
- Scoped Environmental Impact Statement, prepared by WSP Golder, dated May 2022.
- Geotechnical Investigation Proposed Commercial Development: 30 Frank Nighbor Place, Report: PG6153-1 Revision 1, prepared by Paterson Group dated April 28, 2022.
- Phase 1 Environmental Site Assessment, prepared by Golder Associates Ltd., dated April 2021.
- Phase 1 Environmental Site Assessment Addendum, prepared by WSP Golder dated May 19, 2022.

The Environmental Impact Statement (EIS) noted that a Tree Conservation Report (TCR) is not required per Tree Protection By-law 2020-340 as it was found there are no trees on the Subject Site greater than 10 cm diameter-at-breast-height (DBH).

3.0 PLANNING POLICY AND REGULATORY FRAMEWORK

3.1 Provincial Policy Statement (PPS)

The *Provincial Policy Statement (PPS) 2020* provides policy direction on land use planning and development matters of provincial interest by setting the policy foundation for regulating the development and use of land as set out in *Section 2* of the *Planning Act*. The decisions that affect all planning matters “*shall be consistent with*” relevant policy statements under the authority of *Section 3* of the *Planning Act*. The following is an overall review of the applicable *PPS* policies.

Building Strong Healthy Communities

Section 1.1 of the *PPS* speaks to managing and directing land use to achieve efficient and resilient development and land use patterns.

Policy 1.1.1 states that “*healthy, liveable, and safe communities are sustained by*”:

- a) *promoting efficient development and land use patterns which sustain the financial wellbeing of the Province and municipalities over the long term;*
 - c) *avoiding development and land use patterns which may cause environmental or public health and safety concerns; and*
 - g) *ensuring that necessary infrastructure and public service facilities are or will be available to meet current and projected needs;*
- **The U-Haul Campus will achieve an efficient land use pattern as the Subject Site is situated in a business park adjacent to existing commercial and light industrial land uses.**
 - **A Scoped Environmental Impact Statement (EIS) was prepared by WSP Golder dated May 2021. The report concludes that “based on the results of this preliminary review, no negative impacts to significant natural features are expected to result from the proposed development and the proposed development satisfies all relevant federal, provincial, and municipal laws, regulations and policies”.**
 - **The U-Haul Campus will be served by municipal infrastructure including water, stormwater, and sanitary services as well as an existing road network.**

Section 1.1.3 speaks to settlement areas which are urban areas and rural settlement areas, and include cities, towns, villages, and hamlets. The vitality and regeneration of settlement areas is critical to the long-term economic prosperity of our communities.

Policy 1.1.3.2 notes that land use patterns within *settlement areas* shall be based on densities and a mix of land uses which:

- a) *efficiently use land and resources;*
- b) *are appropriate for, and efficiently use, the infrastructure and public service facilities which are planned or available, and avoid the need for their unjustified and/or uneconomical expansion;*

- **The U-Haul Campus will efficiently utilize land and resources as the Subject Site is a remaining vacant block within a registered subdivision zoned for future commercial and industrial development.**
- **The U-Haul Campus will be served by municipal infrastructure including water, stormwater, and sanitary services as well as an existing road network.**

Section 1.3 speaks to employment and *Policy 1.3.1* states that “*planning authorities shall promote economic development and competitiveness by*”:

- a) *providing for an appropriate mix and range of employment, institutional, and broader mixed uses to meet long-term needs;*
 - b) *providing opportunities for a diversified economic base, including maintaining a range and choice of suitable sites for employment uses which support a wide range of economic activities and ancillary uses, and take into account the needs of existing and future businesses;*
 - d) *encouraging compact, mixed-use development that incorporates compatible employment uses to support liveable and resilient communities, with consideration of housing policy 1.4; and*
 - e) *ensuring the necessary infrastructure is provided to support current and projected needs.*
- **The self-storage facility and automobile rental establishment will contribute to creating additional employment opportunities while broadening the range of land uses in the area.**
 - **The Subject Site is situated in an business park adjacent to existing commercial and light industrial land uses with the proposed development representing a compatible and complementary land use.**
 - **The U-Haul Campus will be served by municipal infrastructure including water, stormwater, and sanitary services as well as an existing road network.**

Section 1.6.6 addresses sewage, water, and stormwater services.

Policy 1.6.6.2 notes that *municipal sewage services* and *municipal water services* are the preferred form of servicing for *settlement areas* to support protection of the environment and minimize potential risks to human health and safety. Within *settlement areas* with existing *municipal sewage services* and *municipal water services*, *intensification* and *redevelopment* shall be promoted wherever feasible to optimize the use of the services.

- **The U-Haul Campus will be served by future municipal infrastructure including water, stormwater, and sanitary services.**
- **Stormwater management for the U-Haul Campus will include both stormwater quantity and quality control measures prior to releasing flows towards the Carp River. This will be achieved by a treatment train of grass swales, an on-site stormwater management facility (dry pond) located in the Southwest corner of the Subject Site, and the use of an oil/grit separator.**

Wise Use and Management of Resources

Section 2.0 of the PPS speaks to conserving biodiversity and protecting the health of Great Lakes, natural heritage, water, agriculture, mineral aggregate, petroleum, cultural heritage and

archaeological resources for the long-term prosperity, environmental health, and social well-being of Ontario.

Policy 2.1.1 requires that natural features and areas shall be protected for the long term.

Policy 2.1.2 notes that the diversity and connectivity of natural features in an area, and the long-term *ecological function* and biodiversity of *natural heritage systems*, should be maintained, restored or, where possible, improved, recognizing linkages between and among *natural heritage features and areas, surface water features and ground water features*.

Policy 2.1.6 speaks to prohibiting development and site alteration in fish habitat except in accordance with provincial and federal requirements.

Policy 2.1.7 states that site alteration shall not be permitted in the habitat of endangered and threatened species, except in accordance with provincial and federal requirements.

- **A Scoped Environmental Impact Statement (EIS) was prepared by WSP Golder dated May 2021.**
- **The report concludes that “based on the results of this preliminary review, no negative impacts to significant natural features are expected to result from the proposed development and the proposed development satisfies all relevant federal, provincial, and municipal laws, regulations and policies”.**

Protecting Public Health and Safety

Section 3.0 considers the Province’s long-term prosperity, environmental health and social wellbeing which are dependent on reducing the potential for public cost or risk to Ontario’s residents from natural or human-made hazards. Development shall be directed away from areas of natural or human-made hazards where there is an unacceptable risk to public health, safety, property damage and not create new or aggravate existing hazards.

- **A Phase I Environmental Site Assessment (ESA) was prepared by Golder Associates Ltd. dated April 6, 2021. Detailed information and findings can be found in the Phase I ESA which will accompany this submission.**

3.2 City of Ottawa Official Plan

The City of Ottawa’s new *Official Plan* was approved by Planning Committee and the Agricultural and Rural Affairs Committee on October 14, 2021, and subsequently adopted by Ottawa City Council on November 24, 2021. It is noted that at the time this report was prepared, the new *Official Plan* did not yet receive approval from the Province of Ontario’s Ministry of Municipal Affairs and Housing.

The City of Ottawa has released a document titled “*Transition of In-stream Applications*” as part of the new *Official Plan* to guide how applications filed around the time of adoption of the new *Official Plan* should be addressed. It provides guidance for most types of applications but does not provide specific guidance for *Site Plan Control* applications like this one. Notwithstanding, the general guidance below has been followed for this application:

“Applications received after the day before the new Official Plan is adopted on October 27, 2021, but before Ministry approval of the Official Plan... must be evaluated against the existing Official Plan and must also include an evaluation of the application against the Council approved new Official Plan (and the new Secondary Plan, where applicable).”

3.2.1 Existing Official Plan

The Subject Site is designated under the existing *Official Plan* as an *Urban Employment Area* as per *Schedule B – Urban Policy Plan* as shown on **Figure 3**. As per *Section 3.6.5(1)* of the *Official Plan*, the *Urban Employment Areas* are “intended to be established clusters of business and economic activity capable of accommodating more than 2,000 jobs and with a broad spectrum of job densities”. The *Urban Employment Area* through the *Zoning By-law* will permit uses such as manufacturing, warehousing, distribution, storage, communications, storage as well as those that store products outdoors and require large land areas devoted to external storage, sale or service of goods for vehicle sales and service. Additional permitted uses within the *Urban Employment Area* include limited sample and showroom as well as ancillary uses such as recreational, health and fitness uses, child care, and service commercial uses.

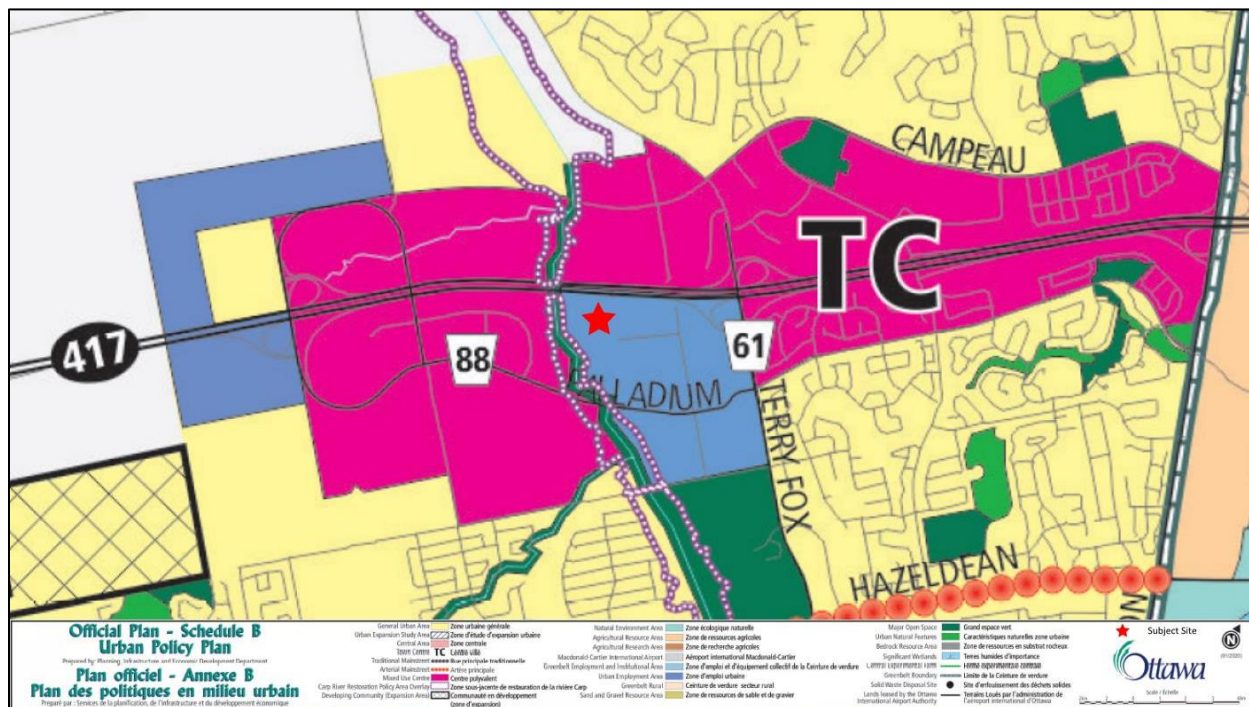


Figure 3: Excerpt from the existing City of Ottawa Official Plan Schedule B – Urban Policy Plan.

The applicable policies under *Section 3.6.5* are listed below with key sections highlighted. A description of how the proposal responds to the policies then follows:

- 1) *Urban Employment Areas* are designated on *Schedule B* and are intended to be established clusters of business and economic activity capable of accommodating more than 2,000 jobs and with a broad spectrum of job densities. *Urban Employment Areas* shall be distributed throughout the urban area to help provide access to jobs throughout the city. Established *Urban Employment Areas* have capacities ranging from 2,000 to

20,000+ jobs. The City shall maintain sufficient land in these Employment Areas to maintain the Employment objectives established in the City Employment Survey. New Urban Employment Areas shall have sufficient land to accommodate a minimum of 2,000 jobs based upon a diversified cluster of business types and economic activity and employment densities. These New Employment areas must also be located so that they have designated truck route access. Some Urban Employment Areas may not be contiguous as a result of natural or human-made barriers, or existing land uses.

- 2) In Urban Employment Areas, the Zoning By-law will:
- a) Permit traditional industrial uses such as manufacturing, warehousing, distribution, storage, communications, construction;
 - b) Permit uses that store most products outdoors and require large land areas devoted to external storage, sale or service of goods or for vehicle sales and service;
 - g) Distinguish uses with characteristics that are likely to impact negatively on surrounding areas (e.g., industrial uses that produce odours, dust, smoke, heavy equipment movement, large areas of outdoor storage, or noise) from those uses that are likely to have negligible such impacts (e.g., offices or research and development facilities);

- **The self-storage facility and automobile rental establishment will contribute to creating additional employment opportunities while broadening the range of land uses in the area.**
- **The Subject Site is situated in a business park adjacent to existing commercial and light industrial land uses with the U-Haul Campus representing compatible and complementary land uses.**
- **The Subject Site is zoned to permit a variety of commercial and light industrial land uses including self-storage facility (warehouse) and automobile rental establishment.**

Section 2.5.1 – *Designing Ottawa* considers elements of urban design such as “*how buildings, landscapes and adjacent public spaces look and function together*”. The components of communities where urban design play a key role include built form, public spaces, and infrastructure.

The following is a review of the design objectives and summary of how the proposed development is consistent with the design objectives of Section 2.5.1:

- 1) *To enhance the sense of community by creating and maintaining places with their own distinct identity.*
- **The U-Haul Campus will enhance the sense of community within the existing business park by creating additional employment opportunities while broadening the range of land uses.**
- 2) *To define quality public and private spaces through development.*
- **The U-Haul Campus has been designed with a separate access from Frank Nighbor Place with pedestrian connections throughout the Subject Site.**
- 3) *To create places that are safe, accessible and are easy to get to, and move through.*

- As previously discussed, pedestrian circulation for the U-Haul Campus will be achieved by extending the existing concrete sidewalk as part of the drive aisle to a series of internal pathway connections throughout the Subject Site.
- 4) *To ensure that new development respects the character of existing areas.*
- The U-Haul Campus building designs respect the character of the existing business park through a planned theme to maximize the aesthetics by use of material changes, soft natural colours, accent trims, vertical and horizontal relief.
 - Please refer to the accompanying Architectural Design Brief prepared by IBI Group, dated May 16, 2022 for building design details and how the U-Haul Campus conforms to the *City of Ottawa Official Plan's* applicable urban design policies.
- 5) *To consider adaptability and diversity by creating places that can adapt and evolve easily over time and that are characterized by variety and choice.*
- The Subject Site is a remaining parcel of undeveloped land within an existing business park that is zoned to permit a self-storage facility and automobile rental establishment.
- 6) *To understand and respect natural processes and features in development design.*
- A Scoped Environmental Impact Statement (EIS) was prepared by WSP Golder dated May 2021.
 - The report concludes that ***“based on the results of this preliminary review, no negative impacts to significant natural features are expected to result from the proposed development and the proposed development satisfies all relevant federal, provincial, and municipal laws, regulations and policies”***.
- 7) *To maximize energy-efficiency and promote sustainable design to reduce the resource consumption, energy use, and carbon footprint of the built environment.*
- The U-Haul Campus has been designed to maximize energy-efficiency and promote sustainable design elements including the generous use of windows to allow for natural light while reducing the need for resource consumption. Insulated building materials will ensure proper heating and cooling to reduce energy use and overall costs as shown on the accompanying submitted plans

Section 4.11 – Urban Design and Compatibility includes policies that require *“high quality urban design in all parts of the city and design excellence in design priority areas. The policies within this Section are the responsibility of the development proponent to implement in the design of their site”*. The components of the urban design and compatibility include views, building design, massing and scale, outdoor amenity areas, and design priority areas.

- Please refer to the accompanying Architectural Design Brief prepared by IBI Group dated May 16, 2022 which evaluates the U-Haul Campus in conjunction with the criteria of **Section 4.11**.

Section 4.3 – Walking, Cycling, Transit, Roads and Parking Lots speaks to evaluating *“the adequacy of the transportation network to meet the needs of the proposed development”*. The

section outlines policies regarding proposed developments in relation to modes of transportation such as walking, cycling, transit, and automobile.

- **The U-Haul Campus will be accessible by various modes of transportation such as walking, cycling, and private automobile.**
- **As previously discussed, pedestrian circulation for the U-Haul Campus will be achieved by extending the existing concrete sidewalk as part of the drive aisle to series of internal pathway connections throughout the Subject Site.**
- **The Subject Site currently has frontage onto Frank Nighbor Place which is an existing local road.**

Section 4.4 – Water and Wastewater Servicing states that development within Public Service Areas will be on the basis of public services and requires an assessment of the adequacy of public services to support the development.

- **The U-Haul Campus will be served by municipal infrastructure including water, stormwater, and sanitary services as well as an existing road network. A Development Servicing Study and Stormwater Management Report was prepared by Novatech dated May 20, 2022.**

Section 4.6.2 – Archaeological Resources outlines policies for where development is proposed on land where archeological potential exists as identified on the City of Ottawa map of *Areas of Archaeological Potential*.

- **The Subject Site is identified as containing archaeological potential per the City of Ottawa’s GeoOttawa Archaeological Potential layer.**
- **A Stage 1 and 2 Archaeological Assessment was prepared by Golder Associates Limited dated May 30, 2018, for the development of 20 Frank Nighbor Place which included 30 Frank Nighbor Place in the study area.**
- **The study was reviewed and entered into the Ontario Public Register of Archaeological Reports by per the Ministry of Tourism, Culture, and Sport letter dated June 25, 2018.**

Section 4.7.8 – Environmental Impact Statement speaks to “*development within or adjacent to woodlands, wetlands, and other natural features has potential to impact the feature and its functions by removing vegetation, increasing the amount of paved or other impermeable surfaces, changing the grading of the site, or making other changes*”.

- **A Scoped Environmental Impact Statement (EIS) was prepared by WSP Golder dated May 2021.**
- **The report concludes that “based on the results of this preliminary review, no negative impacts to significant natural features are expected to result from the proposed development and the proposed development satisfies all relevant federal, provincial, and municipal laws, regulations and policies”.**
- **Detailed information and findings can be found in the EIS/TCR which will accompany this submission.**

Section 4.8.3 – *Unstable Soils or Bedrock* speaks to policies regarding “unstable soils or bedrock [that] could be unsafe or unsuitable for development and site alteration due to natural hazards or risk of damage to the structures built on these soils or bedrock”.

- A Geotechnical Investigation was prepared by Paterson Group dated April 8, 2022. The study notes that the Subject Site is considered suitable for the proposed development from a geotechnical perspective.
- Detailed information and findings can be found in the Geotechnical Investigation which will accompany this submission.

Section 4.8.4 – *Contaminated Sites* states that to prevent adverse effects relating to human health, ecological health, or the natural environment, it is important prior to permitting development on these sites, to identify these sites and ensure that they are suitable or have been made suitable for the proposed use in accordance with provincial legislation and regulations.

- A Phase I Environmental Site Assessment (ESA) was prepared by Golder Associates Ltd. dated April 6, 2021. Detailed information and findings can be found in the Phase I ESA which will accompany this submission.

3.2.2 New Official Plan

Under the new *Official Plan*, the Subject Site is now designated as *Mixed Industrial*. The Subject Site is located within the *Suburban (West) Transect* of *Schedule B5* as shown on **Figure 4**. The applicable policies under the relevant sections of the new *Official Plan* are listed below. A description of how the proposal responds to the policies then follows.

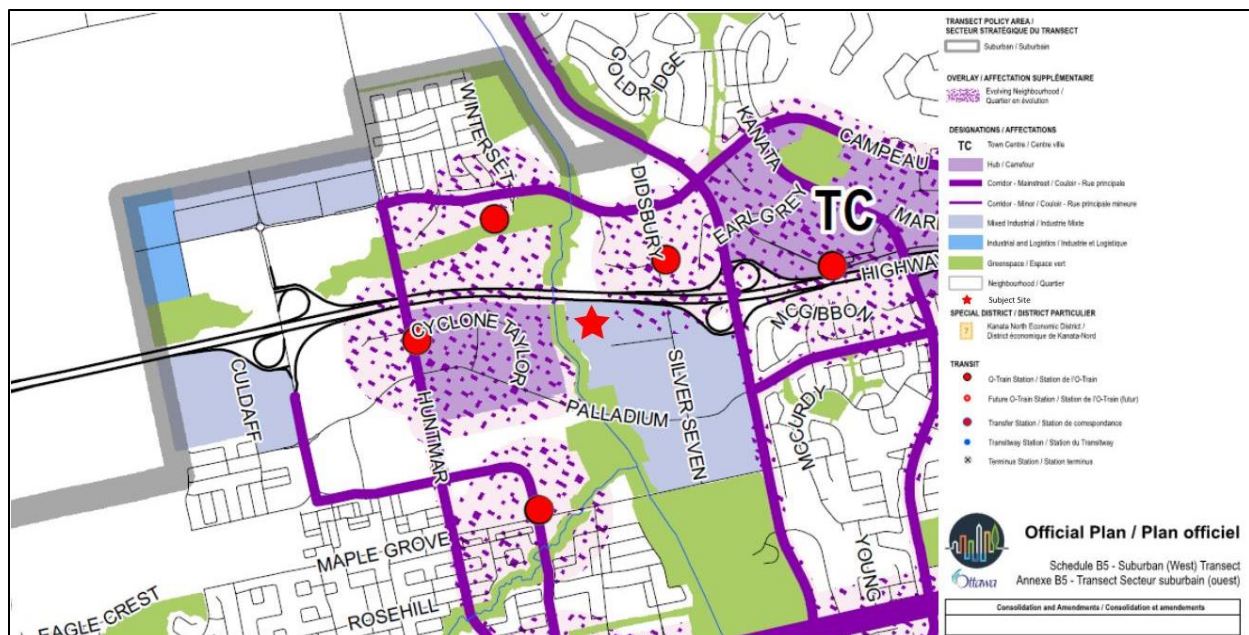


Figure 4: Excerpt from new City of Ottawa Official Plan Schedule B5 – Suburban (West) Transect.

The *Suburban Transect* comprises neighbourhoods within the urban boundary located outside the *Greenbelt*. The objectives of the *Suburban Transect* as per *Section 5.4* of the *Official Plan*

include an evolution towards 15-minute neighbourhoods, enhancing mobility options and street connectivity, and provide direction for new development.

Section 5.4.4(1) states that “Greenfield development in the Suburban Transect will contribute to the evolution towards 15-minute neighbourhoods to the extent possible by incorporating:”

- i) Screened parking lots, where surface parking is proposed, with visual impacts on the public realm mitigated by setbacks, landscaping, location on site or a combination of these measures;
- **The surface parking areas of the U-Haul Campus are appropriately setback from the Frank Nighbor Place with ‘Building D’ oriented towards the frontage of Frank Nighbor Place. The surface parking areas will be screened with soft landscaping elements and vegetation where appropriate.**

The *Mixed Industrial* designation applies to areas that are “clusters of economic activity that are less impactful and provide a broader range of non-residential uses than Industrial areas. These areas can provide a transition between Industrial and Logistics areas and Neighbourhoods, Hubs or Corridors and provide a supply of land for non-residential sensitive uses and smaller-scale light industrial and commercial uses”. These areas include a broad mix of uses such as small-scale office, light industrial, wholesale, small contractors, small-scale commercial service uses and non-residential sensitive uses such as places of worship, indoor recreational uses and stand-alone licensed care centres.

Section 6.5.1(2) states the following uses are permitted in the *Mixed Industrial* designation as shown on Schedules B1 through to B8:

- a) Low-impact light industrial uses including light manufacturing, warehousing, distribution and storage;
 - b) Automobile sales and service, heavy equipment sales and service;
- **The self-storage facility and automobile rental establishment will contribute to creating additional employment opportunities while broadening the range of land uses in the area.**
 - **The Subject Site is situated in a business park adjacent to existing commercial and light industrial land uses with the U-Haul Campus representing compatible and complementary land uses.**
 - **The current zoning of the Subject Site permits a variety of commercial and light industrial land uses including self-storage facility (warehouse) and automobile rental establishment.**

3.3 Zoning By-law 2008-250

The Subject Site is currently zoned as *Light Industrial, Subzone 6, Urban Exception 1414, Height 30 metres, Holding Zone – IL6[1414] H(30)-h, Parks and Open Space Zone, Urban Exception 1932, Holding Zone – O1[1932]-h* and *Parks and Open Space Zone – O1* as shown on **Figure 5**.

The purpose of the *Light Industrial Zone – IL* is to:

1. *permit a wide range of low impact light industrial uses, as well as office and office-type uses in a campus-like industrial park setting, in accordance with the Employment*

- Area designation of the Official Plan or, the General Urban Area designation where applicable;
2. allow in certain Employment Areas or General Urban Areas, a variety of complementary uses such as recreational, health and fitness uses and service commercial (e.g. convenience store, personal service business, restaurant, automobile service station and gas bar), occupying small sites on individual pads or in groupings as part of a small plaza, to serve the employees of the Employment or General Urban Area, the general public in the immediate vicinity, and passing traffic;
 3. prohibit retail uses in areas designated as Employment Area but allow limited sample and showroom space that is secondary and subordinate to the primary use of buildings for the manufacturing or warehousing of the product;
 4. prohibit uses which are likely to generate noise, fumes, odours, or are hazardous or obnoxious; and
 5. provide development standards that would ensure compatibility between uses and would minimize the negative impact of the uses on adjacent non-industrial areas.

The purpose of the Parks and Open Space Zone – O1 is to:

1. permit parks, open space and related and compatible uses to locate in areas designated as General Urban Area, General Rural Area, Major Open Space, Mixed Use Centre, Village, Greenbelt Rural and Central Area as well as in Major Recreational Pathway areas and along River Corridors as identified in the Official Plan, and
2. ensure that the range of permitted uses and applicable regulations is in keeping with the low scale, low intensity open space nature of these lands.



Figure 5: Excerpt of the Subject Site's existing zoning from GeoOttawa.

I Exception Number	II Applicable Zones	Exception Provisions		
		III Additional Land Uses Permitted	IV Land Uses Prohibited	V Provisions
1414 (By-law 2019-41) (By-law 2012-277) (By-law 2009-161) (By-law 2008-459)	IL6[1414] H(30)-h		all uses other than the following: - animal care establishment - animal hospital - automobile dealership - automobile rental establishment - bank - bank machine - broadcasting studio - catering establishment - convenience store - day care - emergency service - heavy equipment and vehicles sales, rental and servicing - hotel - light industrial uses - instructional facility - medical facility - office - park - parking garage - parking lot - personal brewing facility - personal service business - place of assembly - post office - printing plant - production studio - recreational and athletic facility - research and development center - restaurant - retail store - service and	- the provisions of Section 203(2)(b),(c) and 203(4) do not apply - garden centres associated with a permitted use must be located at least 0.3 m above the regulatory flood plain - all storage must be concealed or enclosed - automobile-related retail stores cannot exceed a floor area equal to 25% of the area of the lot and are not permitted abutting Highway 417 - the following provisions apply to retail stores and shopping centres: - minimum lot area is 0.81 ha - minimum lot width is 45 m - minimum yard setbacks is 3 m - maximum lot coverage is 45% - maximum building height is 11 m - a 6 m landscaped buffer must be provided between a parking lot or paved area and Highway 417 and Frank Nighbor Place - maximum aggregate gross floor area of 19,773 m ² for all retail stores - maximum floor space index- no limit- retail store and shopping centre are not permitted until the 'h' symbol is removed by City Council following: (1) City approval of the detailed design for the roadway modifications described in Exhibit 61 to the Ontario Municipal Board hearing regarding 15 Frank Nighbor Place and 737 and 777 Silver Seven Road, File numbers PL051066, PL060317 and PL060318 (2) conveyance to the City of all necessary road widenings to accommodate the roadway modifications noted above (3) entering into a registered cost sharing agreement to construct the roadway modifications noted above between the owners of 15 Frank Nighbor Place, 20 Frank Nighbor Place, 720 Silver Seven Road, 737 Silver Seven Road and 777 Silver Seven Road and a copy provided to the City. (4) provision to the City of an implementation plan including a construction schedule for the roadway modifications noted above . (5) provision of a performance deposit to the City for the roadway modifications noted above .

			repair shop - technology industry - training center - truck transport terminal - warehouse	
--	--	--	--	--

Figure 6: Excerpt from Zoning By-law 2008-250 Urban Exception 1414.

I Exception Number	II Applicable Zones	Exception Provisions		
		III Additional Land Uses Permitted	IV Land Uses Prohibited	V Provisions
1932 (By-law 2012-277)	O1[1932]-h DR[1932]-h IL[1932]-h L2[1932]-h		-all uses, except those that existed on July 11, 2012 are prohibited until such time as the holding symbol is removed	-the holding symbol can be removed only at such time as the following conditions are met: a. The Minister of the Environment will have approved the Carp River, Pool Creek and Feedmill Creek Restoration Class EA; b. The Kanata West Landowners Group, or the City, will have commenced Phase I of the Carp River restoration works; c. Mississippi Valley Conservation Authority will have issued a permit under Section 28 of the Conservation Authorities Act for the placement and removal of fill in accordance with the Carp River Restoration Plan and the EA; d. Filling of the property and an as-built survey will have been completed to demonstrate that the area is entirely removed from the flood plain; and e. At all times, the flood storage capacity of the corridor will be maintained at or above existing conditions.

Figure 7: Excerpt from Zoning By-law 2008-250 Urban Exception 1932.

- As shown above in Figures 6 and 7, the Subject Site is also conditioned to site-specific provisions outlined in both *Urban Exceptions 1414* and *1932*. The provisions contained within these site-specific zones include those related to specific land uses, road widening and design, and work in the flood plain adjacent to the Carp River. It should be noted that not all provisions contained within these site-specific zones are applicable to the proposed development.
- The applicant is requesting that the ‘h’ symbol under *IL6[1414] H(30)-h* be removed as it does not apply to the proposed self-storage facility and automobile rental establishment for the U-Haul Campus development.
- The ‘h’ symbol applies to the Carp Restoration works which are on City owned land and is a requirement of the Kanata West Landowner’s Group and not applicable to the Subject Site. It appears that this area was zoned in error and the area of land does not form part of the proposed development.
- The proposed self-storage facility (warehouse) and automobile rental establishment are permitted land uses within the *IL6[1414] H(30)-h* zone.
- The proposed development has been designed to comply with the applicable site-specific provisions such as outdoor storage requirements.

4.0 PUBLIC CONSULTATION STRATEGY

The public consultation strategy will involve a variety of methods as follows:

- The City will be posting signage on the Subject Site which provides members of the public with details of the proposed development and means of contacting the file lead to provide comments and/or questions.
- The City will post digital copies of all required supporting studies and plans will be made available for public viewing through the City of Ottawa's Development Applications webpage (<https://devapps.ottawa.ca/en/>).
- Pre-consultation will also be conducted with the local ward councillor.
- Community organization(s) will be notified of the details of the proposed development through a 'heads up' by City of Ottawa staff.

5.0 CONCLUSION

It is our assessment that the proposed development is consistent with the *2020 Provincial Policy Statement*, conforms with the *City of Ottawa Official Plan* and complies with *Zoning By-Law 2008-250*. This Planning Rationale along with the associated technical studies supports the development of the proposed U-Haul Campus.

The U-Haul Campus is an appropriate and desirable addition to the Kanata South community and represents good planning.

Yours truly,

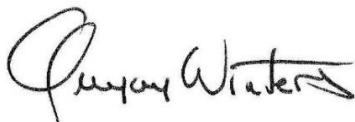
NOVATECH

Prepared by:



Robert Tran, M.PL.
Planner, Planning & Development

Reviewed by:



Greg Winters, MCIP, RPP
Director, Planning & Development

Appendix A
Site Plan Drawing A-1001, REV 2
Prepared by IBI Group
Dated May 18, 2022

