

Rosaline J. Hill Architect Inc.

Architect • Development Consultant
414 Churchill Ave. N, Ottawa, ON, K1Z 5C6
613-853-2822 www.rjhill.ca

designing urban alternatives

May 19th, 2022

Development Review, Central
Planning, Real Estate and Economic Development Department
City of Ottawa
110 Laurier Avenue West,
Ottawa, Ontario, K1P 1J1

Re: 368 Tweedsmuir Design Brief for Site Plan Control Application

This brief will provide an overview of the proposed development and highlight the rationale for design choices and the appropriateness within the context of applicable planning policies. The proposed development aligns with the city's Official Plan and is appropriate and compatible with the surrounding area.

The proposed development is a 3 storey (plus basement) apartment building with a total of 16 units. 25% of the dwelling units have 2 bedrooms and the remaining are a mix of 1 bedroom units and bachelors. No on-site parking is proposed. The lot is 15.24m wide and 32.62m deep with a total area of 497sqm. It is legally described as Lot 41, Registered Plan 263. The lot is zoned R4UB and is in the General Urban Area subject to the Mature Neighbourhoods Overlay.



Image 1 – Proposed development of 368 Tweedsmuir



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Architect & Development Consultant
www.rjhill.ca | info@rjhill.ca

Our client would like to build a beautiful small apartment building that mixes contemporary and traditional architecture and provides a height transition between future Richmond Road development and the 2-storey neighbourhood to the north. The proposed building would provide much needed housing. Westboro will be enriched through added diversity. Increased density will help to support local businesses and public transit.

SITE CONTEXT

The site is located in Westboro village, less than 100m from Richmond Road which is designated a Traditional Main Street.

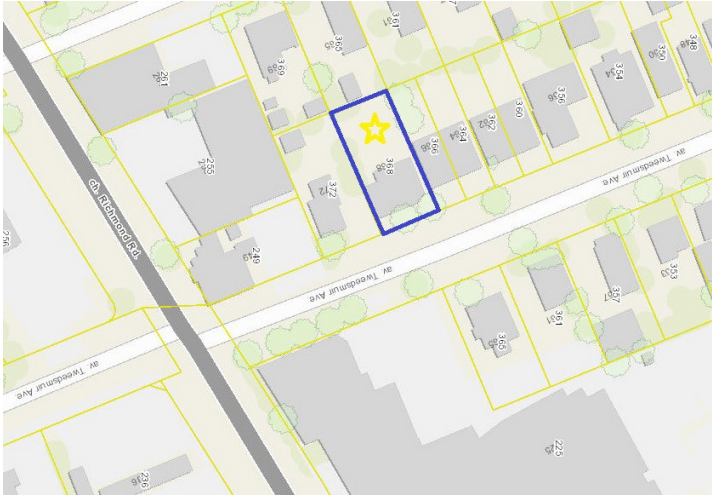


Image 2 – 368 Tweedsmuir parcel as shown on Geo Ottawa



Image 3 – 3D aerial view of 368 Tweedsmuir in immediate surrounding context



Image 4 – wider 3D aerial view of 368 Tweedsmuir in surrounding context

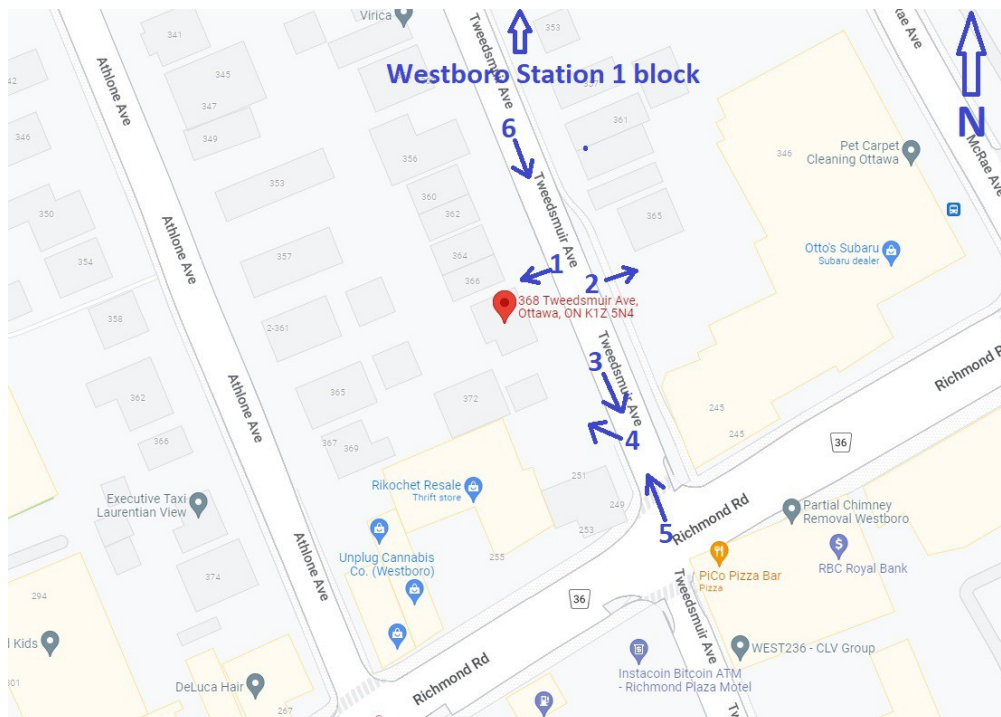


Image 5 – 100m radius of 368 Tweedsmuir (views 1-5 correspond with images 5.1 to 5.5 that follow)



Image 5.1 – view from the street looking at the existing home at 368 Tweedsmuir



Image 5.2 – view from the street looking across the street from 368 Tweedsmuir

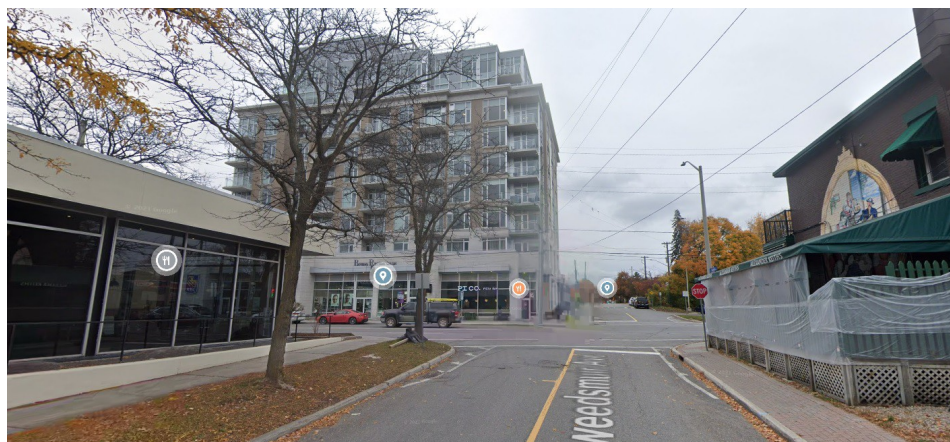


Image 5.3 – view looking north down Tweedsmuir toward the corner of Richmond Road



Image 5.4 – view of neighbours to the north of 368 Tweedsmuir



Image 5.5 – view looking south down Tweedsmuir from the corner of Richmond Road

The site of our proposed development is only two lots north from Richmond Road which is well served with public transit options and a designated shared lane for cyclist and automobile traffic. The site is only one block south of Westboro Station, a major rapid transit hub and future site of an upgraded Phase II light rail station.

Within short walking distance from the site is a large grocery store in addition to a wide variety of restaurants, shops, and other services.

Tweedsmuir Avenue north of Richmond Road has a sidewalk on the other (eastern) side of the street but there is no sidewalk on the street directly in front of this site. There is a parking lot across the street from this site. The neighbourhood has a mixture of singles, semis, and other types of low density multi-unit buildings and secondary dwelling units.

A proposal has been submitted to the city to consolidate and redevelop the property immediately to the north and on the corner of Richmond Road. According to the planning rationale submitted to the city by Fotenn, *“The proposed development would redevelop and intensify the properties located at the northwest corner of the Richmond Road and Tweedsmuir Avenue intersection in the heart of Westboro, with a nine-storey mixed-use building. The proposed development would consist of a nine-storey, mid-rise, mixed-use building fronting along Richmond Road, transitioning to a three-storey built form where it abuts the Tweedsmuir Avenue property to the north.”*



Image 6 – Proposed development at the corner of Richmond Road and Tweedsmuir Avenue

The proposed development would include ground floor commercial space, 87 dwelling units, 196 bicycle parking spaces, and an underground parking garage containing 98 parking spaces. The entrance and exit to the parking garage is proposed to be located off of Tweedsmuir Avenue. The height of the building steps down incrementally from 9 to 4 storeys towards the low rise homes to the north along Tweedsmuir Avenue. Fotenn's planning rationale goes on to say *“The Tweedsmuir fronting part of the building is proposed with architectural features and a residential built form that is complimentary to the residential character of the surrounding low-rise neighbourhood to the north.”*

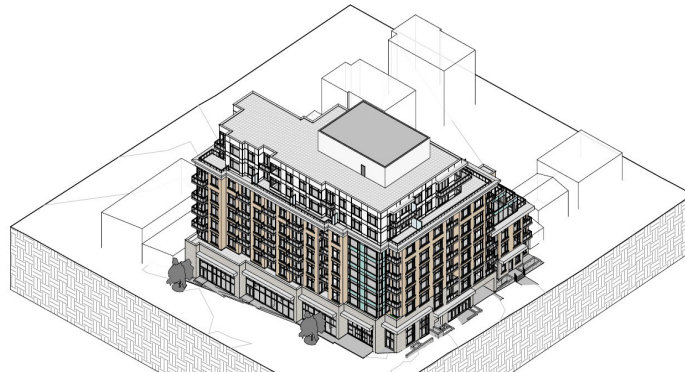


Image 7 – Massing of proposed development at the corner of Richmond and Tweedsmuir

This property is located within a neighbourhood in transition, close to a Transitional Mainstreet that is rapidly being redeveloped to about a 9 storey height. Given this pattern of redevelopment along Richmond and the empty parking lot across the street from this site, one could anticipate more change coming.

But perhaps the most significant aspect of context, is that this is a genuinely walkable location, where tenants could enjoy walking year round to small shops, parks, services, and a nearby rapid transit station.

The subject property is positioned between the multi-storey redevelopments and commercial activity of Richmond Road and the lower density neighbourhood to the north.

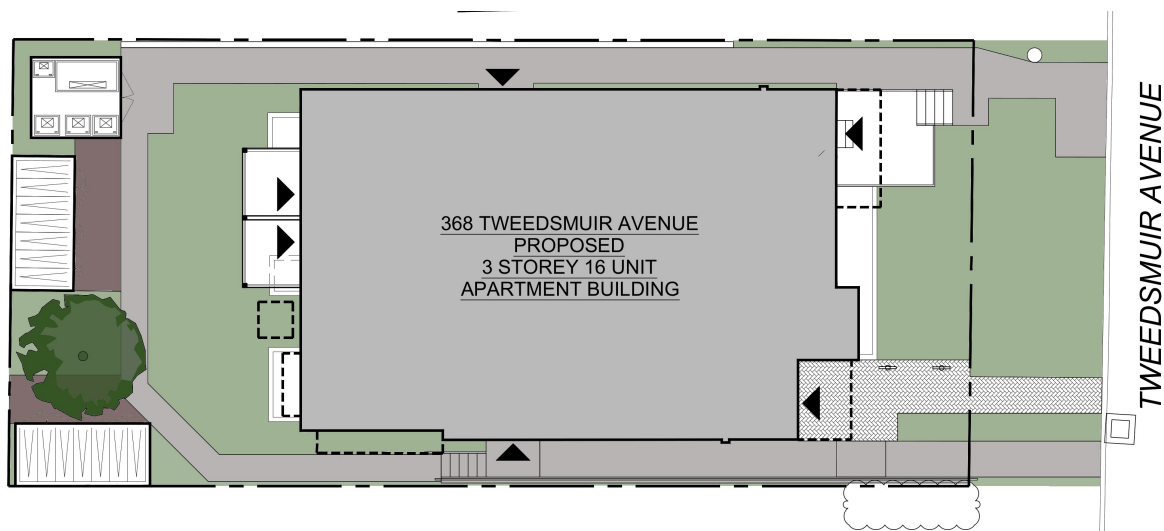


Image 8 – Concept Plan of proposed development

DESIGN OVERVIEW

This apartment has been designed to provide 16 small high quality rental units in a simple wood frame walk-up apartment building. Unit sizes vary from bachelors (with sleeping nooks) to 2 bedroom units. All units are designed to be bright and to feel spacious with the use of; combined spaces, layered views that pass through different spaces, and interior frosted glass walls with windows beyond. Ten of the twelve above grade units have balconies, and the ground floor front facing 2 bedroom unit has a walk out terrace.

Exterior cladding is a combination of brick, wood-look siding and horizontal lapped siding. Large contemporary windows contrast the more traditional use of scale. Although most of the roof is flat, a front portion of the front 3rd floor wall is tipped back and clad in metal roofing, to give the impression of a sloped roof.

Accessibility is provided along a ramp starting at the front entrance walkway, leading to a side door. Two accessible units are located on the ground floor; a 2 bedroom unit and a bachelor.

The main entrance is at the front left corner of the building and contains the mailboxes. Mechanical and electrical equipment and meters are located on or next to the back and side walls, out of sight, as well as on the roof. Roof top equipment is toward the center of the roof, and surrounded by a sound barrier.

Garbage will be stored in a shed in the back yard, accessed by tenants through the north side exit door, then along a short path. Garbage will be rolled from the shed to the garbage pad (next to the road) on collection days.

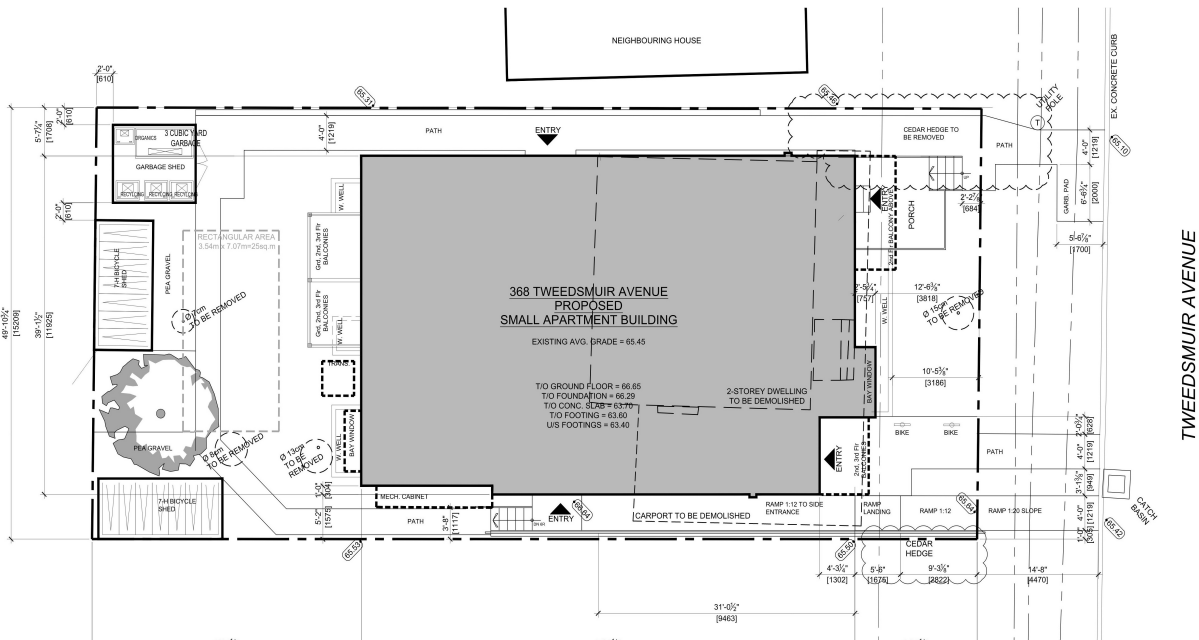


Image 9 – Site Plan of proposed development

Front Facade and Street Context

The building was designed as a transition element between higher density new construction along Richmond, and the 2 storey neighbourhood to the north. The design is contemporary, but with some traditional features, including a front facade third floor feature akin to a mansard roof. This 'eave' is at the level of the third floor structure, provides a visual transition to the 2 storey neighbourhood to the north.

The front facade has been designed to break up the visual mass into elements in scale with northern neighbours. A front bay window extends the full height of the building, dividing breaking the massing vertically. The lower eave line breaks the front facade with a strong horizontal feature. And the grouping of large windows (with door) at the ground and second floor on the right side of the facade sets this part of the building apart at this smaller scale.



Image 10 – 3D rendering of proposed building in context (looking from the right)



Image 11 – 3D rendering of proposed building in context (looking from the left)

There are two entrances on the front facade. The primary entrance is the visual focus of the facade. It is glassy, protected by a balcony above, and lit with pot lights so that it is a safe and inviting highlight feature after dark. The secondary front entrance is dedicated to the largest ground floor unit, leading out onto its patio. Front balconies provide needed private amenity and also add interest and animation to the facade, providing a semi private interface between the public street and private interior spaces.

Low canopy trees will be planted in the front, under overhead wires.

Rear Yard

The rear yard is designed for regular use by tenants, for playing, gardening, parking bikes, and eating outdoors. Private balconies overlook the rear yard, but are toward the centre of the rear facade such that they do not significantly overlook neighbour's yards. One large canopy tree will be planted to contribute to our City's 40% tree canopy goal. They will provide a shaded amenity space for outdoor summer eating. Bike sheds are designed for convenient and regular bike use. Bay windows add articulation and interest to the rear facade on the exposed southern side. In combination with proposed balconies, the amenity space exceeds zoning requirements, allowing for private and shared outdoor spaces.



Image 12 – Rear yard view of proposed development



Image 13 – View from rear yard of northern neighbour looking south (future redevelopment site)

Sustainability

The proposed 16 unit apartment building would replace a single family home with on-site parking. This existing house supports a single household in their car-dependent lifestyle. The proposed apartment would support 16 households in a walking / biking / transit lifestyle. More residents without more cars is a very significant step toward sustainable living.

Unlike the existing home, this new development is designed to manage storm water and hold water on-site during extreme storm events.

Sun Shadowing

As per the study below, sun shadow effects are insignificant because the proposed building is within the shadow cast by the proposed southern neighbour. Below and included with our application are the full results of the study.

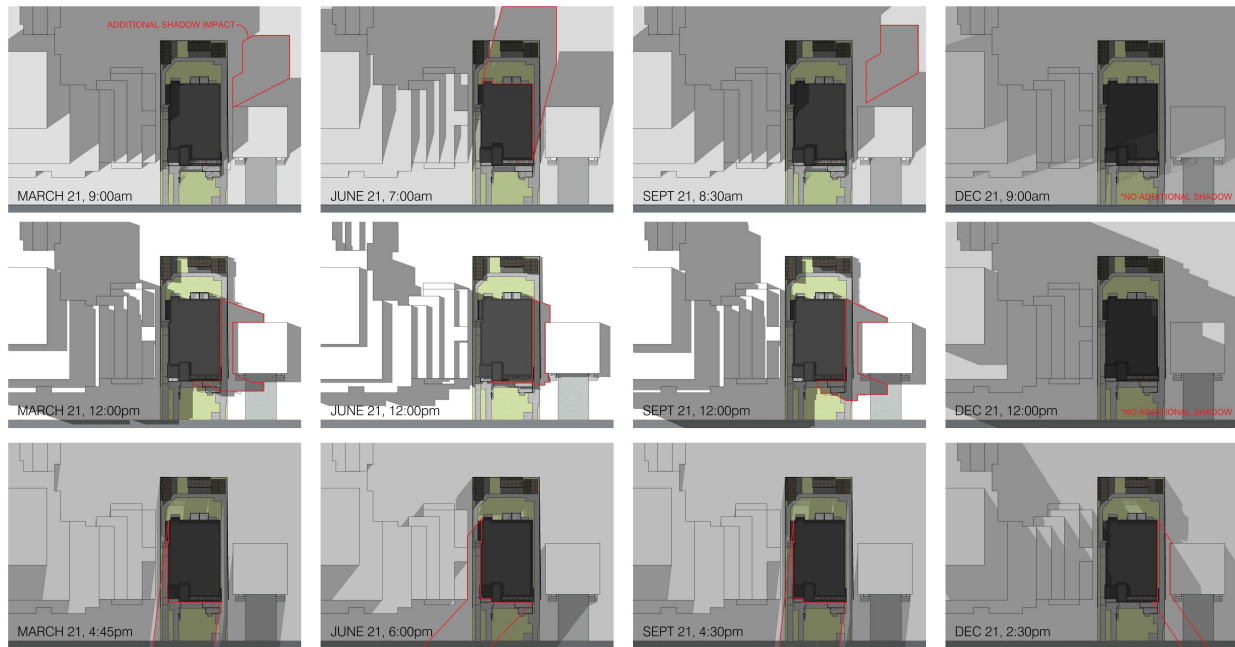


Image 14 – Shadow analysis for proposed building

Bike Storage

Two front bike parking spaces are provided near the front entrance. Two rear yard bike storage sheds are proposed, each housing 7 bikes. Rear yard sheds are proposed to be in line with the fence (a continuation of the fence line), and at the same height as the wood board privacy fence. These sheds have been designed with their back wall at the same height as the privacy fence, and to shed water only onto the subject property. Their eaves do not overhang onto neighbouring properties. Where a small retaining wall is needed at the property line, to meet the needs for site grading and rain water management, the retaining wall will be poured with the floor slab of the shed. From the neighbour's view, the sheds would look like a continuation of the fence line. Zoning requires a 2' setback, however by eliminating this setback we have eliminated a 'weed-zone' and increased the usable space of the rear yard for amenity and the enjoyment of tenants.

Below is a detail of the proposed bike shed designed to retain rain water on site. The rear wall height and style are designed to match the privacy fence as seen by the neighbour.

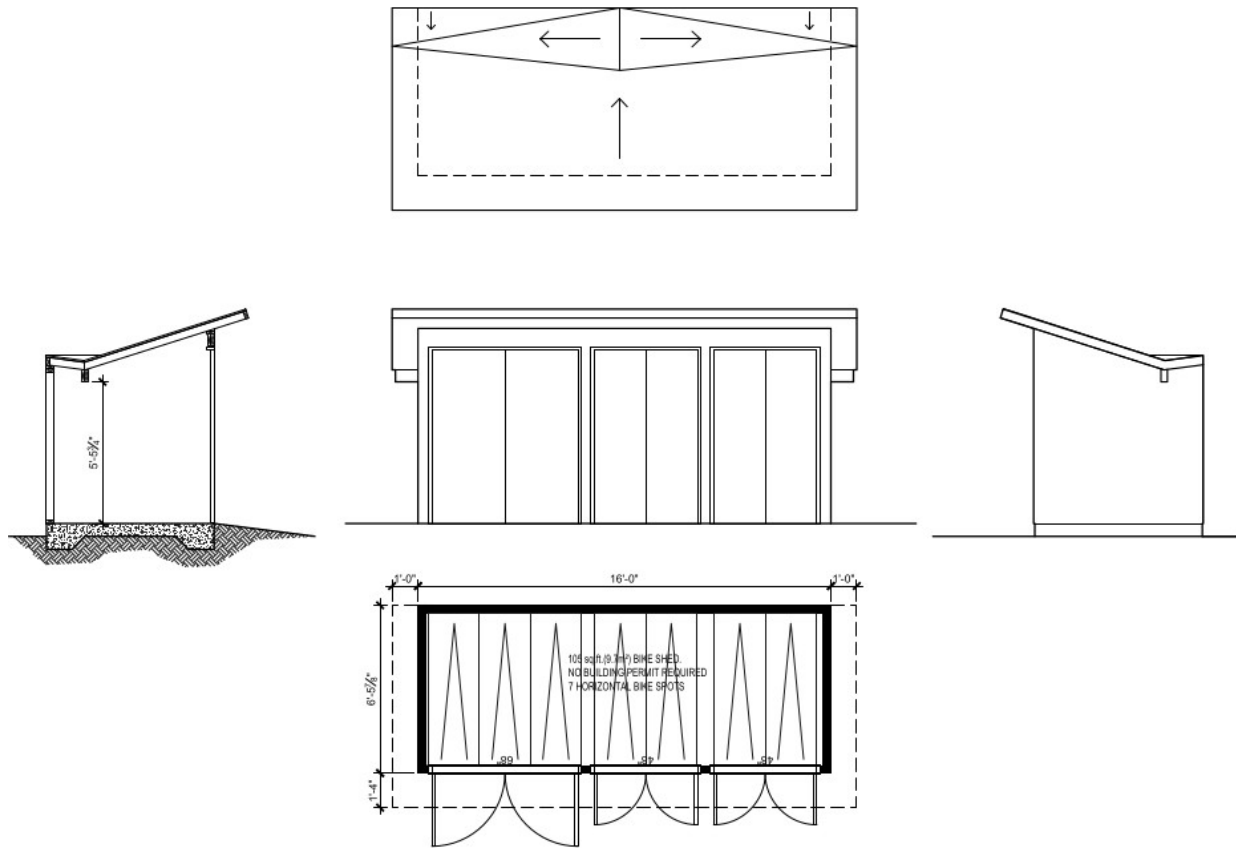


Image 15 – Detail of proposed bike shed

PLANNING FRAMEWORK

Richmond Road/Westboro Secondary Plan

The site is subject to the Richmond Road / Westboro Secondary Plan, which includes the following Overlying Objectives and Principles:

- *Ensure the area develops in such a way as to ensure that it is pedestrian and cyclist "friendly"*
- *Preserve the scale and character of existing neighbourhoods and ensure the compatibility of new development.*

The proposed development includes bike parking a double the zoned amount. The front facade is animated with windows into living spaces, active entrances, balconies and a front porch. This degree of animation and semi-private outdoor spaces will enliven the pedestrian experience on the street.

The proposed front facade has been designed to mediate the change of scale from south to north on the street. Using a roof-like front feature, an eave line relates to the eaves and height to the north. Groupings of windows, material changes and a full height bay window feature break up the mass of the facade into elements that are in scale with northern neighbours.

The proposed development satisfies the unifying vision of the Secondary Plan which states: *"...Westboro Village will continue to be an attractive and liveable urban community... with... a range of housing types and choices, excellent transit service and well-designed, compact and inclusive development that will enhance the area's diversity and vibrancy."*

The animated front facade will make this street more attractive and liveable, building community by adding semi-private spaces to the edge of the street – the perfect stage for casual social interaction and community building. The mix of small dwellings will add diversity of housing choice, and compact living options within ground related units.

Furthermore, the proposed development also satisfies Objective 1 of the Plan which states: *"Encourage infill/intensification at a human scale that is compatible with the existing community on appropriate key potential redevelopment sites."*

The front facade has been carefully designed to divide the massing to compliment northern neighbours. This sensitivity to the scale and rhythm of architectural features in the existing neighbourhood it key to retaining a human scale that is compatible.



Image 16 – Planning Area (Schedule A) in Richmond Road / Westboro Secondary Plan

Sector 3 (Westboro Village) of the Secondary Plan pertains mostly to Richmond Road as a Traditional Mainstreet. It encourages the reinforcement of the existing character of Westboro Village through the updated design guidelines, as well as maintaining a sense of human scale with mixed-use buildings generally in the 4-6 storey range and a minimum of 2 storeys, with buildings located close to the street.

The site is located just on the edge of sector 3 which is designated as “Westboro Village” in the Plan. Our development proposal to add density will contribute to increased foot traffic and clientele to the small shops on Richmond Road, which is a Traditional Mainstreet.

R4 Zoning

The R4 zoning for this site permits only 12 dwelling units. At the time that the R4 zoning was drafted, it was thought that a lot of this size could fit only 12 dwelling units. Since that time, real estate costs and rents have sky rocketted and need for smaller units is dire. It is therefore appropriate that our proposed building would house 16 small households rather than 12. The addition of these units does not change the outside of the building in any way. It simply allows for more smaller units, and more affordable rents.

Front articulation is prescribed by R4 zoning, to encourage compatibility with neighbours. The proposed design exceeds these requirements, adding further articulation to the roofline to compliment northern neighbours.

R4 zoning is more prescriptive than any other zone with respect to landscaping. This proposal exceeds all landscaping and amenity requirements.

We propose to located bike sheds in the rear yard, set at the property line, and a garbage shed in the rear yard set 0.6m from lot lines. Proposed sheds are low, and predominantly out of sight from neighbours. A wood board fence will be installed around the rear yard, such that neighbours will see the proposed trees, not the proposed sheds.

Because this site is entirely within 100m of a Traditional Mainstreet, designated Area Y on Schedule 1A of the zoning bylaw, no onsite parking is required.

Committee of Adjustment

Minor variances are needed in order to proceed with this proposed development, as follows:

1. Eliminate the 0.6m setback required for a bike shed in the rear yard,
2. Permit 16 dwelling units whereas the bylaw limits this building to 12 dwelling units.

PRE-APPLICATION CONSULTATION

On February 15th, we met with city planning staff along with our client and our client's planner. The following were the comments from city planning staff included in the minutes from the meeting, as well as our response to these comments.

Comments from City Planning Staff	RJH Architect Responses
PLANNING	
<p>A Minor Rezoning would be required. In the case of a rezoning such as this in advance of the comprehensive ZBL where you're seeking to go beyond densities such as those permitted in the R4 subzones, the new OP sets out that the applicant will have to demonstrate that their requested zoning changes (both the unit increase and parking relief) are appropriate.</p> <p>Evolving Overlay policies in the new OP potentially contemplate increases in density, but it will be up to you to demonstrate you are within the intent of Section 3 and 5 of the OP, as well as the applicable (Inner Urban in this case) transect policies.</p>	<p>We've been informed by the authours of the R4 zoning that the R4 zoning report indicated that an increase in units as we propose should be achieved through minor variance rather than rezoning.</p>
<p>Prior to making a complete submission, I also encourage you to discuss the proposal with the area Councillor, Jeff Leiper, immediate neighbours and local community associations.</p>	<p>We met with the city councilor on March 13. Following that we had a meeting with neighbours on May 7. The neighbours had no comments or questions about the proposal, but were concerned in general about intensification without parking.</p>
WASTE MANAGEMENT	
<p>Waste Management requires a minimum pathway width of 2 metres. If this distance cannot be met then a staging area for pickup day will need to be included at the front of the property.</p>	<p>We have added a garbage pad of 3.2m x 1.7m dimensions next to the curb.</p>
<p>The following containers are required: Garbage: 1 x 2 yard bin Fiber: 2 x 360L carts Glass metal plastic: 1 x 360L cart Organics: 1 x 240L cart</p>	<p>Provide as required.</p>

TECHNICAL REQUIREMENTS	
The R4 provisions include parking prohibitors to ensure the front yard does not become a parking lot. The city would like to see an active front yard to act as a prohibitor, this can be combined with a landscape element.	We have designed the site plan so as to limit the potential of vehicular parking in the front yard in the future. Landscaping includes a boulder and a couple of trees in the front yard.
If required, ensure that you include railings to the ramp and stairs, as its proximity to the side yard might be impactful to the neighboring property.	Railings are shown on the site plan. The railing is largely behind the neighbour's shed.
There are three separate walkways, consider combining the two to the north may help maximize soft landscaping at the front.	The two walkways on the northeast corner have been combined into one.
Although there are overhead wires, any opportunity to provide trees in the front yard is great.	Trees will be planted as per landscape drawing
URBAN DESIGN COMMENTS	
There are currently four entrances to the building, is a ramp required? Consider developing an internal lift to make up for the accessibility requirement. A lift is beneficial as it is a more discrete approach if it is underused.	A lift scenario has been considered, however it significantly reduces leasable space on the ground and basement floors. The ramp has been designed to integrate well with the grading and landscaping.
We appreciate the strong approach this proposal makes to relate and fit within its neighbourhood context for its street facing facade, however, we have the following comments/questions that will need to be addressed when a full submission is made:	Thank you.
Rear yard amenity: Is suitable amenity space provided? Will new trees be provided?	Yes
Entrances: What do each of the entrances service? Will they have canopies? Are they all necessary?	There are four significant entrances to the building. One is the front entrance that is the shared formal entry to the building that has a welcome presence on the street. Another is the front facing entrance to a dwelling unit on the ground floor that animates the facade and allows the tenants of that unit to use their front facing outdoor patio. This entrance is meets zoning intent to animate the streetscape. On the north side, there's an exit door for fire safety, and on the south side there is an entry point that is fully accessible.

<p>Ramp: If there is an entrance at grade can the external ramp be removed and replaced with a small internal lift? We have concerns about placing the ramp in the front yard as it will require a guard rail and retaining wall along the side lot line;</p>	<p>We drafted a layout with a small internal lift and found that it consumed space equivalent to a small bedroom on the ground and basement floors. On balance, given housing need, the ramp seems like a better choice.</p>
<p>Rear yard massing: Can the rear yard facing elevation provide similar articulation to soften a potential box massing?</p>	<p>A bay window has been added to the rear facade to add articulation and architectural interest to the back.</p>
<p>Materiality: As a variety of materials are envisioned, we recommend any metal/vinyl siding be limited in quantity as they do not improve the neighbourhood character. Perhaps a cementitious panel/siding would suit a pairing with masonry;</p>	<p>Neighbours tell us that they like our choice of brick at the front, and light coloured lapped siding on side walls.</p>
<p>Snow Storage: Any portion of the subject property which is intended to be used for permanent or temporary snow storage shall be as shown on the approved site plan and grading plan. Snow storage shall not interfere with approved grading and drainage patterns or servicing. Snow storage areas shall be setback from the property lines, foundations, fencing or landscaping a minimum of 1.5m. Snow storage areas shall not occupy driveways, aisles, required parking spaces or any portion of a road allowance. If snow is to be removed from the site, please indicate this on the plan(s).</p>	<p>No pavement and no parking and no designated area for snow storage, but there is room to clear pathways of snow.</p>
TRANSPORTATION	
<p>The purchaser, tenant or sub-lessee acknowledges the unit being rented/sold is not provided with any on-site parking and should a tenant/purchaser have a vehicle for which they wish to have parking that alternative and lawful arrangements will need to be made to accommodate their parking need at an alternative location. The Purchaser/Tenant also acknowledges that the availability and regulations governing on-street parking vary; that access to on-street parking, including through residential on-street parking permits issued by the City cannot be guaranteed now or in the future; and that a purchaser, tenant, or sub-lessee intending to rely on on-street parking for their vehicle or vehicles does so at their own risk.</p>	<p>Owner has been informed and will add on title</p>

<p>The Owner shall be required to enter into maintenance and liability agreement for all pavers, plant and landscaping material placed in the City right-of-way and the Owner shall assume all maintenance and replacement responsibilities in perpetuity.</p>	<p>This doesn't apply to our proposal. We have nothing except a pathway to the street. It is unnecessary to require such an agreement.</p>
<p>Bicycle parking spaces are required as per Section 111 of the Ottawa Comprehensive Zoning By-law. Bicycle parking spaces should be located in safe, secure places near main entrances and preferably protected from the weather.</p>	<p>Bike parking shed is provided for 14 spaces and they are covered. 2 bike parking spaces are provided at the front entrance. There is a total of 1 bike parking space per dwelling unit, which is twice the minimum required.</p>

COMMUNITY CONSULTATION

On March 14th, we met virtually with the city ward councilor in order to share the development proposal in its early stages and receive some preliminary feedback. We were advised to meet with immediate neighbours. Soon after, we delivered letters to immediate neighbours inviting them to a virtual information session to share with them the proposal in its early stages of design and get their feedback. On April 7th we met virtually with about 8 residents who were immediate neighbours of the proposed development site, as well as a representative of the Westboro Community Association, so that we could hear their suggestions and concerns, and include their perspective in our design problem solving. Although we had a good discussion about infill and the consultation process, the neighbours had no comments or questions about the proposal itself. They were however concerned in general about intensification in their neighbourhood without parking provided.



Image 17 – Building elevations

THE OFFICIAL PLANS

The new City of Ottawa Official Plan (“the adopted Official Plan”) was adopted by Ottawa City Council on November 24, 2021. It is noted that at the time this document was prepared, the adopted Official Plan has not received approval from the Province of Ontario’s Ministry of Municipal Affairs and Housing.

In-force Official Plan

The in-force City of Ottawa Official Plan was adopted by City Council on May 14, 2003 and modified by the Minister of Municipal Affairs on November 10, 2003. The Subject Property is designated General Urban Area on Schedule B of the in-force City of Ottawa Official Plan (see Figure 12).

Section 3.6.1 Policy 1 of the in-force Official Plan states that: *“The General Urban Area designation permits many types and densities of housing, as well as employment, retail uses, service, industrial, cultural, leisure, greenspace, entertainment and institutional uses.”* The proposed development of a low-rise apartment building is a permitted land use on the Subject Property.

Section 3.6.1 Policy 3 of the in-force Official Plan states that: *“Building height in the General Urban Area will continue to be predominantly Low-Rise. Within this range, changes in building form, height and density will be evaluated based upon compatibility with the existing context and the planned function of the area.”*

The proposed development of a three storey residential apartment building on the Subject Property is consistent with permitted building heights in the General Urban Area designation. The Subject Property is currently underutilized as a single family development, and is suitable for intensification. Policies of the in-force Official Plan are supportive of increased densities through intensification and redevelopment in the General Urban Area that will be predominantly low-rise.

Section 3.6.1 Policy 5 of the in-force Official Plan states that: *“The City supports intensification in the General Urban Area where it will complement the existing pattern and scale of development and planned function of the area. The predominant form of development and intensification will be semi-detached and other ground-oriented multiple unit housing. When considering a proposal for residential intensification through infill or redevelopment in the General Urban Area, the City will:”*

1. *Assess the compatibility of new development as it relates to existing community character so that it enhances and builds upon desirable established patterns of built form and open spaces;*

2. *Consider its contribution to the maintenance and achievement of a balance of housing types and tenures to provide a full range of housing for a variety of demographic profiles throughout the General Urban Area”*

Policies of the in-force Official Plan are generally supportive of intensification in the General Urban Area where it is comprised of low-rise housing forms that are compatible with the existing site context and community character. The proposed development will be boxed between a new 9 storey development that steps down to 4 storeys, and a neighbourhood of two storey singles, semi's and triplexes. The proposed development of a three storey apartment building represents appropriate intensification of the Subject Property with a compatible built form that is established within the existing site context. The proposed apartment building will contribute to expanding the range of available housing types and will create opportunities for a broad variety of residents to locate within the community.

Section 2.2 of the in-force Official Plan sets out policies to direct growth to target areas for intensification. The majority of growth is directed within the urban boundary where services are available or can be easily provided for new development to accommodate the creation of jobs, housing and increased transit use.

Section 2.2.2, Policy 1 a) of the in-force Official Plan identifies *“Redevelopment (the creation of new units, uses or lots on previously developed land in existing communities)”* as one of a range of supported methods for intensification within the Urban Area.

The Subject Property is designated General Urban Area on Schedule B of the in-force Official Plan. The Subject Property is located within the Urban Area and is within an appropriate location that can support residential intensification. The proposed development of a low-rise apartment building on the Subject Property is consistent with residential growth objectives of the in-force Official Plan.

The Official Plan also identifies Mainstreets as focal points or veins on which to target intensification.

Section 3.6.1 *Focusing intensification on Mainstreets allows for less disruption and more convenient services for adjacent communities and more efficient use of transit. The objective of the Mainstreet designation is to encourage more dense and mixed-use development that supports, and is supported by, increased walking, cycling and transit use.*

Authors of the City's zoning bylaw have extrapolated this to include all properties that are entirely within 100m of a Traditional Mainstreet. The subject property is within this 100m proximity, and therefore designed to maximize infill for residents with a walking, cycling or transit lifestyle.

Urban Design and Compatibility

Section 2.5.1 of the in-force Official Plan states: *“compatible development means development that, although it is not necessarily the same as or similar to existing buildings in the vicinity, can enhance an established community through good design and innovation and coexists with existing development without causing undue adverse impact on surrounding properties. It ‘fits well’ within its physical context and ‘works well’ with the existing and planned function. Generally speaking, the more a new development can incorporate the common characteristics of its setting in the design, the more compatible it will be. Nevertheless, a development can be designed to fit and work well in a certain existing context without being ‘the same as’ the existing development.”*



Image 18 – proposed development of 368 Tweedsmuir

The proposed development fits well by providing a visual transition from the proposed new development to the north, to the two storey neighbourhood.

Section 3.3.4 Designing Ottawa

Design Objective 1

To enhance the sense of community by creating and maintaining places with their own distinct identity.

The subject property has been designed to be a visual transition between high and low density development, and is within a setting that is rapidly evolving. In this context, it is significant that this design uses materials, colours and features that are contemporary, but scaled elements to suit the low rise neighbourhood.

Design Objective 2

To define quality public and private spaces through development.

The front elevation of the building continues the long tradition of high quality infill architecture in this area. The rear elevation is animated with bay window and balconies such that the rear yard space will support outdoor social activities, gardening and play.

Design Objective 3

To create places that are safe, accessible and are easy to get to, and move through.

The front entrance is well lit. Front balconies and windows will add lots of 'eye's on the street'. Bike sheds are easily accessed and safe. Accessibility is practical and in a prime location, and it is also affordable, making use of a ramp rather than an elevator which would inflate unit costs in such a small building. Pathways to the rear yard are simple and direct. The front entrance is clear and inviting.

Design Objective 4

To ensure that new development respects the character of existing areas.

The proposed design uses features and materials from the existing two storey neighbourhood to moderate the scale of the proposed building in this boundary location between higher and lower densities.

Design Objective 5

Consider adaptability and diversity by creating places that can adapt and evolve easily over time and that are characterized by variety and choice.

By introducing a low-rise apartment building to this area, people will have more housing choice. Different size units, bachelor, 1 and 2 bedrooms will appeal to different purchasers at different stages in their housing life cycle.

Design Objective 7

To maximize energy-efficiency and promote sustainable design to reduce the resource consumption, energy use, and carbon footprint of the built environment.

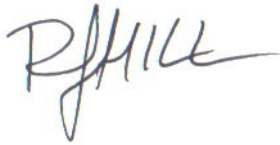
The proposed 16 unit apartment building would replace a single family home with on-site parking. This existing house supports a single household in their car-dependent lifestyle. The proposed apartment would support 16 households in a walking / biking / transit lifestyle. More residents without more cars is a very significant step toward sustainable living.

CONCLUSION

The proposed development at 368 Tweedsmuir Avenue is an opportunity to provide 16 new rental apartment units with no parking in a highly walkable neighbourhood in the City of Ottawa. This is an area that the City wishes to direct infill residential growth to help achieve intensification targets. The proposed development has been designed to meet the planning framework. It is consistent with the Official Plan. The intent of the zoning has been met, therefore the project can be built in accordance with the City's vision. The proposed building is a desirable contribution to this neighbourhood, and will provide much needed diversity of housing choice.

If you require any further information, please call me at 613-853-2822.

Regards,



Rosaline Hill

BES, BArch, OAA, MRAIC