

**PLANNING RATIONALE REPORT FOR SITE PLAN
APPLICATION**

**98 - 100 BEARBROOK
CITY OF OTTAWA**

**PREPARED BY P H ROBINSON CONSULTING
MAY 2022**



This report has been prepared on behalf of Landric Bearbrook Property Inc. in support of a Site Plan Control Application for their lands at 98 100 Bearbrook in Blackburn Hamlet.

The legal description of the property is Part of Lot 14, Concession 2 (Ottawa Front) geographic Township of Gloucester, City of Ottawa (P.I.N 04746-0637 and 04746-0638) and it consists of two adjacent parcels of land.

The property is located on the west side of Bearbrook Road, north of Innes Road and is in an AM11 zone (see **Figure 1 Location plan**).



Figure 1 Location Plan. Subject property shown in black outline

Adjacent lands and context

Lands immediately to the south consist of a commercial plaza (a 1 storey plaza in places and other parts of the building are 2 storeys) that have commercial and

office uses that is also in an AM11 zone. To the north of the property are semi-detached units that are in a different zoning category (R2N). To the west is a school (École Élémentaire Catholique Sainte-Marie) and a park (South Park) and to the east (opposite side of Bearbrook Road) is also a school (Good Shepherd School) and a park (Bearbrook Park). See image below showing the **property, , parkland areas** and the nearby school. A full size version of Figure 2 is in the **Appendix section** of this document.



Figure 2

Existing property

The lands at 98 and 100 Bearbrook have approximately 78 metres of frontage (256') on the west side of Bearbrook and a depth of approximately 50 metres (165'). The total lot area is approximately 3,960 m² (42626 ft²). See the attached survey plan (in **Appendix Section**) for further details.

The property consists of two single family homes that are presently vacant. (see aerial view on next page) Large grassed areas with some trees around the perimeter of the property and along the frontage exist on the property. There are two driveways to Bearbrook Road at the present time.

Proposed development

The intent of the development application is to build a mid rise rental apartment building with some townhouse units along Bearbrook Road with surface and underground parking on the property. The townhouse units are part of the overall building footprint and design for the building. (see **Site Plan in the Appendix Section**). The proposal calls for 168 units (159 apartment units and 9 townhouse units) and 209 parking spaces.(see **Figures in the Appendix Section of the Site Plan, Ground Floor Plans, Underground Parking Plans and Architectural Elevation Drawings**)

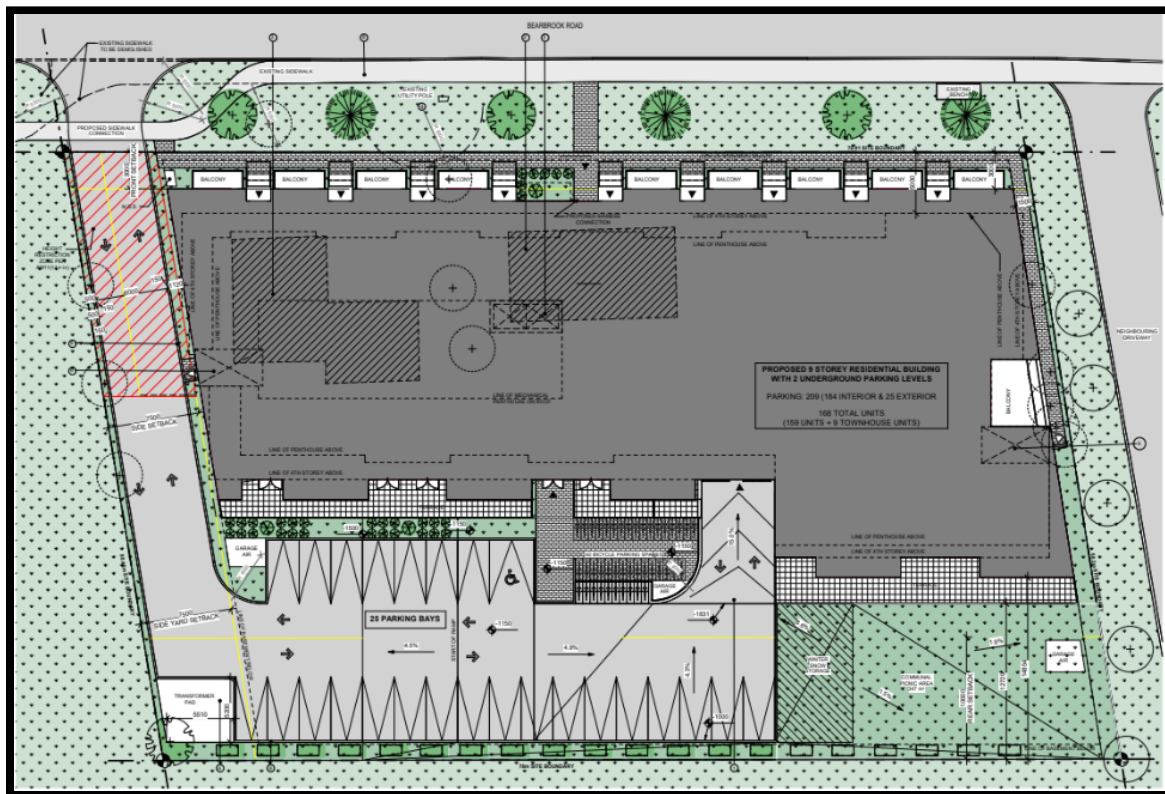


Figure 3 Site Plan extract. Bearbrook Road at the top of the figure

The parking requirements for the unit count and types of units would result in 234 parking spaces and the total number of parking spaces proposed will be the subject of a Minor Variance application that will be filed near the conclusion of the Site Plan approval process. The Minor Variance for parking will only be for resident parking requirements and not for visitor parking requirements. All residents will be made fully aware of the parking situation and the provision of a dedicated parking space in the building will be a separate component of the overall rent that they pay.

It should be noted that all the provided resident parking will be located in the underground parking. The rear yard surface parking will be for the visitor parking requirements which will simplify access to the underground parking and will reduce the need for visitor access.

There will be seven (7) studio units, thirty seven (37) 1 bedroom units, seventy five (75) 1 bedroom + den units, thirty nine (39) 2 bedroom units and one (1) 3 bedroom units in the overall development plus the nine (9) ground level orientated townhouse (three and four bedroom units) facing Bearbrook Road. .

A total area for bicycle storage (84 spaces) will be provided in the underground parking level of the building (42 spaces) and also an outside area (42 spaces) at the rear of the building .

The vehicular site access to the property is through a full turning movement driveway location located at the north edge of the Bearbrook Road frontage. The existing sidewalk along the frontage of the property will be slightly realigned at the north edge of the property.

The access to the underground parking levels are located at the rear of the building and there are two levels of underground parking with 184 underground parking spaces (and 25 surface parking spaces at the rear of the property) and 84 bicycle parking spaces in total (42 in the underground parking level and 42 to the rear of the building). The underground parking footprint is greater than the footprint of the proposed building in order to have the maximum number of parking spaces in the underground levels. The Parking Garage structure is fully covered and does not exceed grade level.

In addition to the underground vehicular parking and the bicycle parking, the basement level will also contain the garbage rooms and some areas for residents storage lockers. Garbage will be hauled outside for removal by building operational staff.

The designs call for balconies facing the east, west and south sides of the property. Balconies are not proposed on the north side of the building adjacent to residential uses.

There will be landscaped areas along the frontage of the property, along the Bearbrook frontage, the north and south sides of the property and the largest area of landscaping will be at the rear of the property adjacent to the building at the south west corner of the property. See Figure 5 (extract of the Landscaping Plan) on page 7.

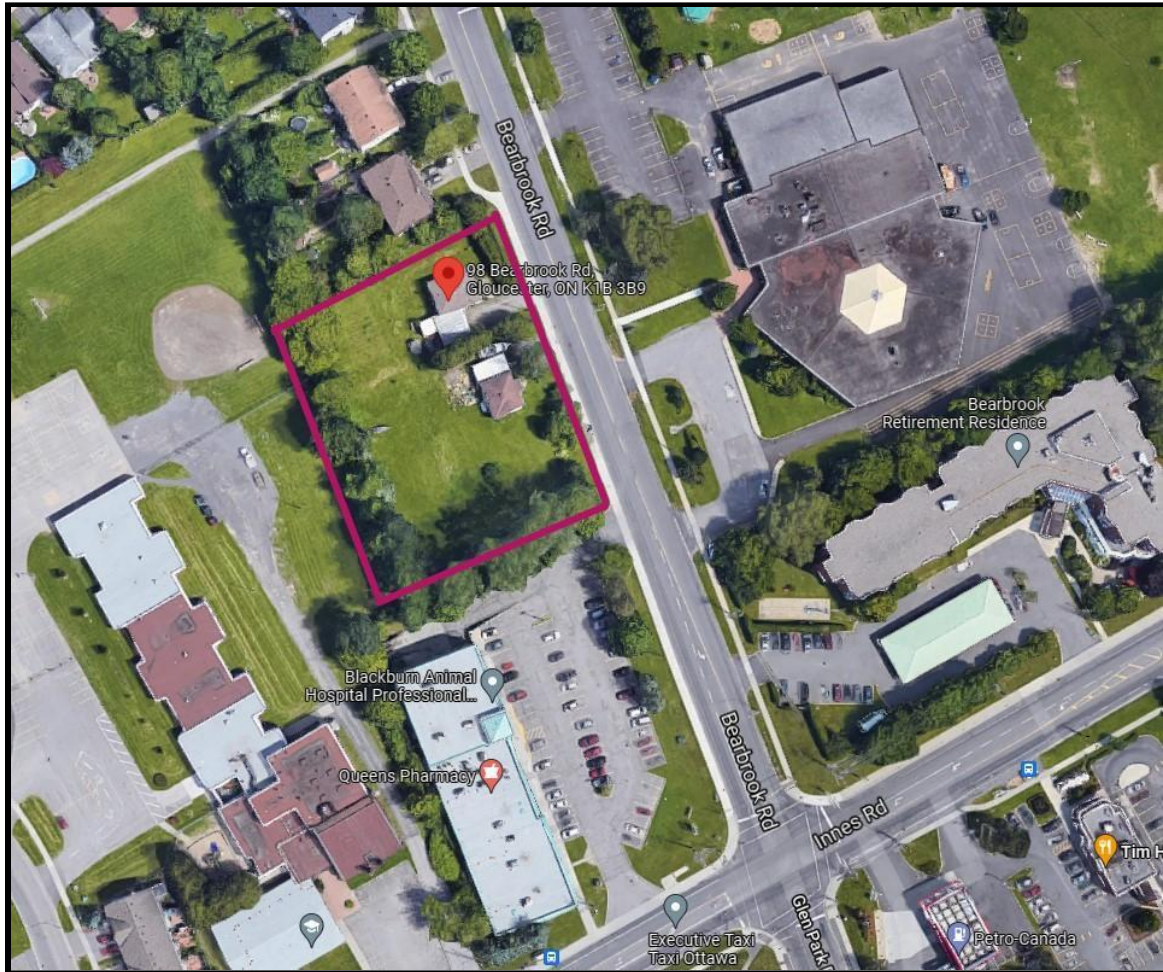


Figure 4 Aerial view of the subject property. There are 2 single family homes on the property.

The new building will have a total building footprint of 1812 m² (19 497 ft²). The building height is 29 m and the total building area is 14674 m² (157890 ft²). The height limit under the current zoning for this property is 30 m and the building is zoning compliant in terms of height, setbacks and landscaped areas and amenity area requirements. A minor variance will be required to allow for the proposed modifications to the amount of parking to be provided for the residents of the building. The zoning requirement would be for 234 parking spaces to be provided and the submission is proposing a total of 209 parking spaces which is bringing the project to almost 90% parking compliance.

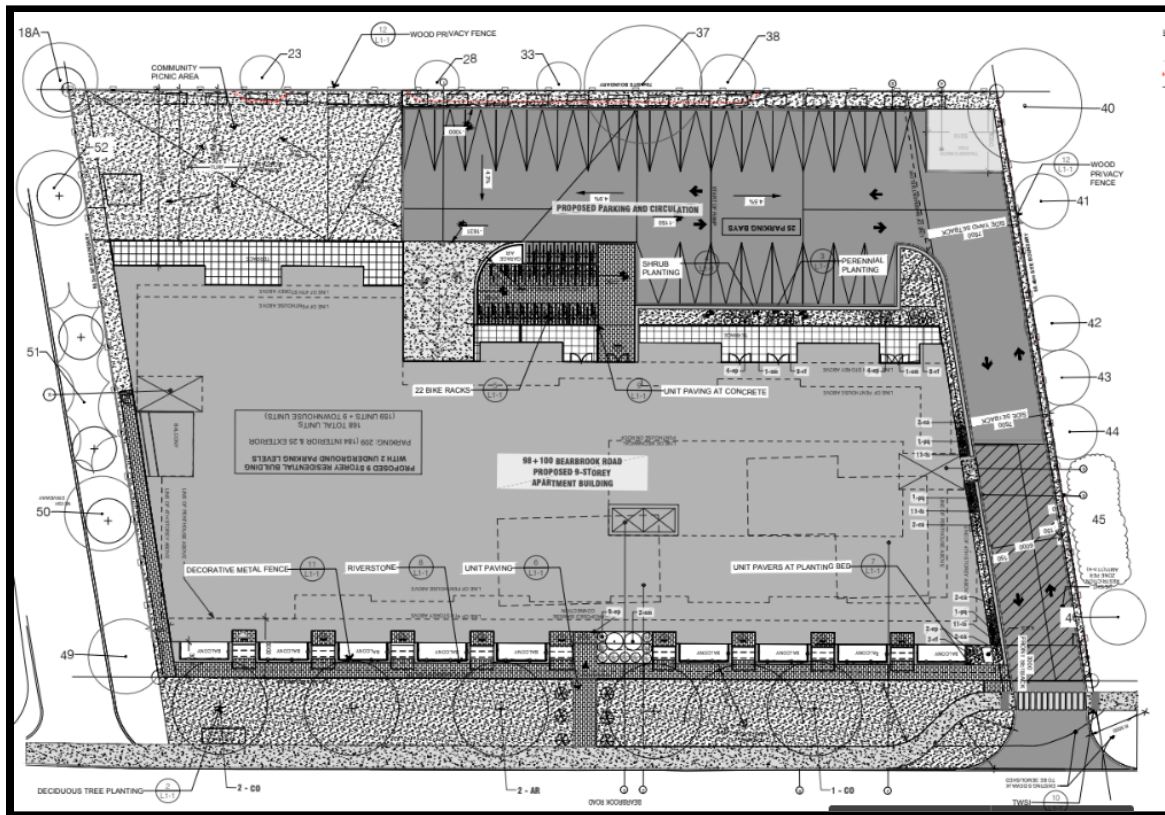


Figure 5 Extract of Landscaping Plan

Urban Design

Massing

The proposed development builds up, as seen on page 9, to the height of nine (9) storeys through a trinity of building mass, along with a recessed penthouse level, aimed at complementing the context in which it is planned as set out in Section 4.11 of the Official Plan.

These portions were initially explored through conceptual massing and gradually evolved through city interaction and design progression (Figures 6 through 8 on the next two pages) which led to the current proposal, expressing form and massing through architectural articulation as seen on page 9 as Figure 8.



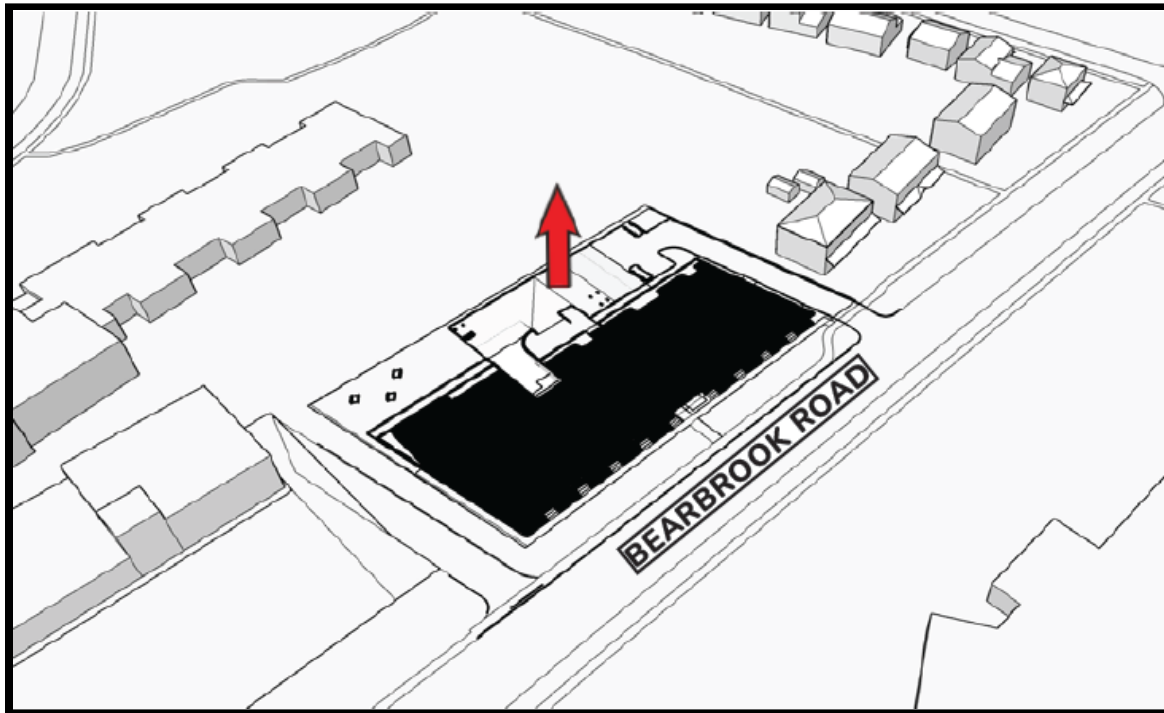


Figures 6,7,8. Initial massing exploration, massing development and massing evolution of the design for the building of the Bearbrook facade

Base: The base or podium is positioned parallel to the street as well as the lot lines and expressed through a combination of a heavier masonry material and a series of 'townhouse' balconies which are projected from the building footprint. **See Figure 9 on the following page.**

Given that the building is nine storeys in height, this two-storey base with projected patios brings the footprint closer to the street allowing for a more relatable human scale transition and relationship between the pedestrian realm of Bearbrook Road and the built form of the proposed development.

Similarly, the material and colours help define the heavier base with its own distinct, masonry cladding, anchoring the building to the site and enriching the quality of the street realm while accentuating the corridors of Bearbrook Road leading to Innes Road. This masonry cladding is also present in select sections up to the full height of the building in order to vertically articulate the higher portions of the building while breaking the cladding in recessed areas to reduce the overwhelming sense of its horizontal dominance and overall weight along Bearbrook Road.



Base Building. Base form of the building. Figure 9

Middle: The middle section is setback from the building footprint along all facades, including Bearbrook Road, and is used to horizontally break the base from the top, while being utilised to emphasise the primary pedestrian entrance as well as the individual 'Townhouse' entries along Bearbrook Road. This middle 'band' furthermore facilitates the transition of the building from a parallelogram to a more traditional rectangular building form. This section is highlighted using a dark aluminium panel cladding system which increases its distinction from the base and top.

Top: The top portion of the project, while large in size, is broken up vertically and horizontally through the use of various, much lighter, cladding materials to soften the sense of weight created. This element also descends to the first storey in some areas along the side and rear, tying the building together vertically and horizontally. **See Figure 10 on the following page.**

Notably, the project embraces the street along Bearbrook Road through a series of private entrances to townhouse type units. This helps in creating intimate, private units for the residents while promoting pedestrian connectivity. The proposed development has been analysed in its context through various

transitional studies and contextual placement of the form as seen in Figures 11 and 12 on the following page. Figures 13-16 inclusive show the final architectural elevation plans of all facades.



Upper Volume - Recessed upper 'mid rise' component of the midrise building. Figure 10

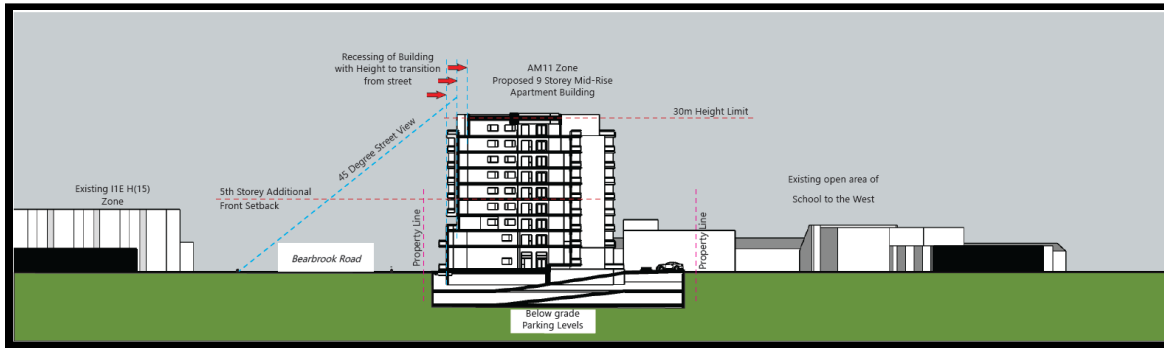


Figure 11 Longitudinal section through Bearbrook Street and Property



Figure 12 Proposed transitional through property



Figure 13. East elevation from Bearbrook Road



Figure 14 West elevation from the rear of the property



Figure 15 North elevation. No balconies overlooking the north property line



Figure 16 South elevation with commercial plaza to the south

More details related to design issues can be found in the Design Brief, prepared by the project architect, that is being filed with this submission.

Background studies

Background studies and plans addressing soils, environmental issues, landscaping, lighting, traffic and civil engineering design are being filed as part of this Site Plan application.

The proposal has a compact building footprint with a high density of units on a road with good public transit options with commercial facilities within walking distance which will reduce the reliance on the private automobile.

Along Bearbrook Road OC Transpo Route 28 provides 7 day a week service from 6:15 am until 11:25 pm at night providing service to and from Blair Station on the LRT line from the Blackburn Hamlet community. [Subject property](#) shown on Figure 17 on the following page in [blue](#)

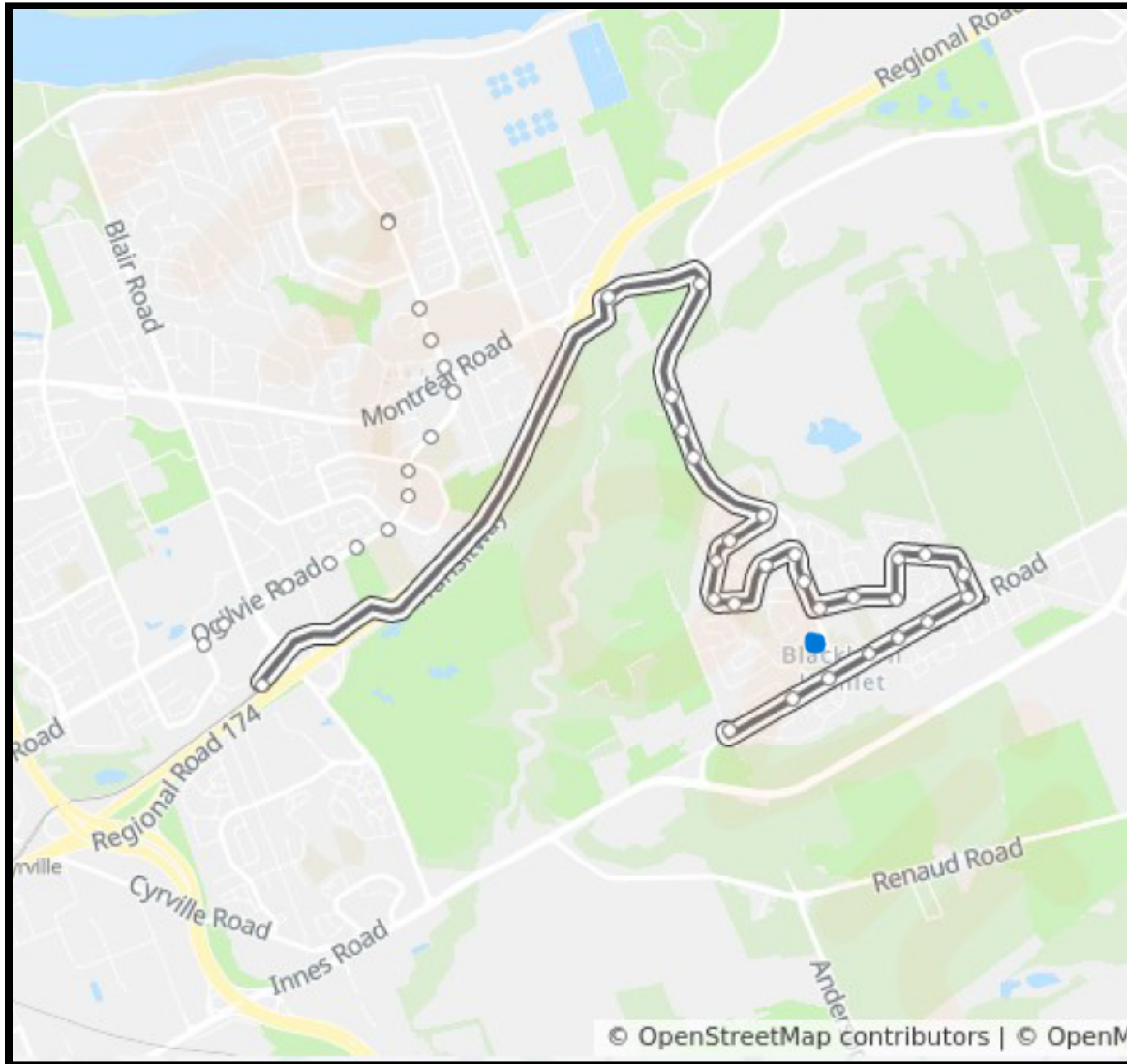


Figure 17 OC Transpo Route 28 which connects the community to the LRT network

The size and type of unit options (a mix of one, one- bedroom plus den and two bedroom units) supports social sustainability by offering good quality small unit rental options to the public. There is also the flexibility for larger units by merging together individual units to form a larger unit.

No major issues or concerns were identified in these studies. The civil engineering design for the project has been undertaken to allow the building to be self draining/graded to have no undue impacts on adjacent properties and the onsite lighting will not spill on to adjacent properties.

Zoning

Under Zoning By-law 2008-250 the **subject lands** are zoned **AM11** (Arterial Main Street Subzone 11). (Shown in the centre of Figure 18 below). The north boundary of the AM11 zone is the north property line of the subject property.



Figure 18 Zoning By-law extract

This is a zone which permits mid rise apartment dwellings and has specific setbacks listed in the AM11 component of the Arterial Mainstreet zoning which is a specific zoning that applies in parts of Blackburn Hamlet.

The proposed uses of the site are in conformity with the existing zoning and all setback requirements that apply to this site. The only variances required will be for the resident parking being provided as indicated previously in this report

It is important to note that the design of the building and its setbacks recognizes the specific interior side yard setbacks that are in place for the AM11 zone.

The following are the **AM11** zone provisions and how the proposed development meets these requirements.

	REQUIREMENT	PROPOSED/PROVIDED
Minimum frontage	No minimum	79 m
Minimum Lot Area	No minimum	3553 m ²
Minimum front yard	3 m	3 m
Minimum interior side yard (N) (to a residential zone)	7.5 m	7.5 m
Minimum interior side yard (S) (to a non residential zone)	No minimum	1.5 m
Minimum Rear yard	7.5 m	14.85 m
Maximum Building Height	30 m	29 m
Parking	234 spaces	209 spaces *

*** Subject to a Minor Variance application**

In terms of building heights/side yard setbacks based on the adjacent zoning on properties the following section (Section 186 (11) (n) in the By-law applies.

Note that the first 4 points refer to building heights within certain proximities to rear lot lines that are adjacent to **R1,R2,R3, R4** properties. The subject property has an I1F zoned property at the rear (a school) so only the item in **purple** applies and the design is compliant with that **requirement**.

As per Section 186 (11) (n) of Zoning By-law 2008-250, as amended for lands in an AM11 zone the following building heights are in effect. The **red** and **purple** text has been added by the author of this report to highlight the key elements.

despite Table 185(f) the following maximum building heights apply:


- In any area up to and including 20 metres from a rear lot line abutting a **R1, R2 or R3** zone: 11 m
- In any area up to and including 20 metres from a rear lot line abutting an **R4** zone: 15 m
- In any area over 20 metres and up to and including 30 metres from a rear lot line abutting a **R1, R2, R3, or R4** zone: 20 m

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- In any area:
 - outside of the areas identified in (i) through (iii) above; and,
 - up to 7.5 metres from that part of a side lot line within 20 metres of a street and abutting a R1, R2, R3 or R4 zone: 15 m (property to the north is in an R2 zone)
- In all other cases: 30 m (By-law 2015-49)

It is our understanding that the building heights proposed are zoning compliant. With our clients property abutting an I1F zone at the rear, the requirements related to stepped back setbacks to residential zones do not apply. The zoning permits a maximum building height of 30 m and any portions of the building along the north property line that are within 7.5 m of the north property line and within 20 metres of the street itself that abut an R1-R4 zone have a maximum building height of 15 m.

The project has been designed so that the minimum side yard setback on the north side is 7.5 m all along the north property line and as a result, the proposed building is zoning compliant with the specific zoning requirements that are in effect for this property.

The area that is subject to the specific height limit along the north property line has been shown with a hatching symbol on the site plan drawing  and this is a portion of the building with no balconies facing a lot line which will prevent overlook and privacy issues with the residential properties to the north. The proposed building on the property is compliant for the area shown with the hatching symbol and beyond the area subject to the specific height limit. **See Figure 19 on the following page.**

The project will require a Minor Variance on the amount of parking being provided for the residents of the building. The required parking ratio for 'resident' parking is 1.2 spaces per unit and the proposal calls for a provided parking total of 166 spaces when the requirement for the 'residents' component of the apartment units is 191 spaces. This results in a provided parking ratio of 1.04 spaces/unit instead of 1.2 spaces/unit. It should be noted that there is a clause in the By-law that when all of the required parking spaces for a use are provided underground the parking requirement can be reduced by 10% (Section 101 (6) (c)). All of our required parking spaces for the resident component can be provided underground and the final interpretation of how this section of the By-law is interpreted will be discussed with the City By-law writers as it can reduce the scope of our parking variance or may be able to remove the need for the variance.

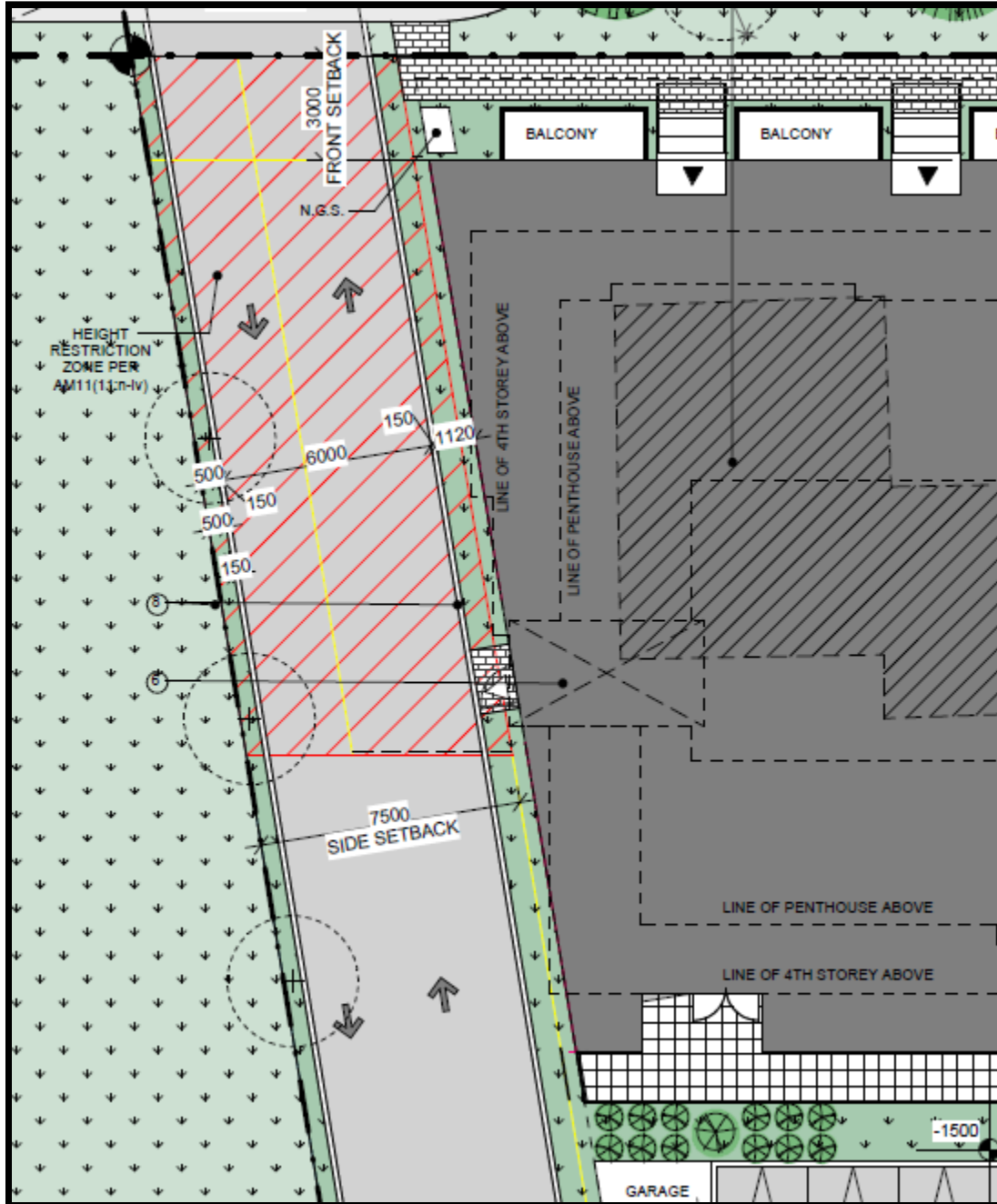


Figure 19. North lot line with area subject to a maximum building height of 15 m shown in red hatching with portions of the property further away from the street and the interior of the site subject to a 30 m height limit.

Provincial Policy Statement

The current Provincial Policy Statement (PPS) came into effect in 2020 and it outlines the key matters of Provincial planning interest with respect to land use planning decisions made by municipal approval authorities. Any decisions that are made by municipal approval authorities must be consistent with the policies of the PPS.

The following are sections of the PPS that are applicable to the subject property :

- 1.1.1 Healthy, liveable and safe communities are sustained by:
 - a) promoting efficient development and land use patterns which sustain the financial well-being of the Province and municipalities over the long term; *The proposed development on the site represents an efficient intensification of the site and will not create inefficient land use patterns in the area.*
 - e) promoting cost-effective development standards to minimise land consumption and servicing costs. *The intensification of the site will be achieved through a 9 storey building on the site and a significant proportion of the site's parking requirements will be through underground parking. The existing infrastructure within the City Right of Way will be connected to the new building.*
- 1.1.3.1 *Settlement areas* shall be the focus of growth and their vitality and regeneration shall be promoted. *The property is within a settlement area of the City of Ottawa.*
- 1.1.3.2 Land Use Patterns within *settlement areas* shall be based on:
 - a) densities and a mix of land uses which: 1) efficiently use land and resources. *This redevelopment of the property is an efficient use of the land and will allow for a sensitive infill development within appropriate height limits for this use and with significant underground parking to ensure that there is a maximum of greenspace maintained on the site.* 2) are appropriate for, and efficiently use, the infrastructure and public service facilities which are planned or available, and avoid the need for their unjustified and/or uneconomical expansion; *The proposed new development on the property will efficiently utilise the existing infrastructure available within the City Right of Way on Bearbrook Road.*
- 1.4.3 Planning authorities shall provide for an appropriate range of housing types and densities to meet projected requirements of current and future residents of the regional market area by:
 - c) directing the development of new housing towards locations where appropriate levels of infrastructure and public service facilities are or will be available to support current and projected needs. *The proposed development is between an established residential neighbourhood with*

nearby infrastructure, public transit on a number of routes within a short walking distance and nearby parks and schools for residents' needs and an arterial main street/commercial/mixed use corridor to the south.

- d) promoting densities for new housing which efficiently use land, resources, infrastructure and public service facilities, and support the use of alternative transportation nodes and public transit in areas where it exists or is to be developed; *The proposal represents an efficient use of land on the property, allows for some passive and active open space areas within the site, utilises existing infrastructure and is located close to OC Transpo bus routes.*

Official Plan Designation – City of Ottawa Official Plan

The new Official Plan, which was adopted by City Council on October 27, 2021 has the property within an Outer Urban Transect Policy Area and a Mainstreet Corridor and the lands to the north are in a Built Form Overlay- Evolving Neighbourhood designation. After the late October adoption date, the Official Plan was forwarded to the provincial Ministry of Municipal Affairs and Housing for the final approval. At the date of the submission of this report, it is our understanding that the final approval of the Council adopted Official Plan has not been granted. It is a policy document that is being used by City Planning Staff at the time of the writing of this report as it has been adopted by City Council.

Where a side street (Bearbrook) intersects with a Corridor (along Innes), the Corridor designation may include one or more lots on the side street so as to extend the Corridor designation along the side street to the average depth of the Corridor designation along the rest of the Corridor block. In the Case of Mainstreet Corridors, a maximum depth of 220 metres from the centreline of the street is identified as a Mainstreet Corridor. The north property line of the subject property is approximately 182 m from the centreline of Innes Road so the entirety of the subject property is within the Mainstreet Corridor designation as shown on the figure

Further, there is a Built Form Overlay, Evolving Neighbourhood, that applies to the adjacent area in Schedule B3-Outer Urban Transect. The Evolving overlay is applied to areas in close proximity to Hubs and Corridors to signal a gradual evolution over time that will see a change in character to support intensification, including guidance for a change in character from suburban to urban to allow new built forms and more diverse functions of land. **See Figure 20 on the following page** which is an extract from the Official Plan showing the designations.

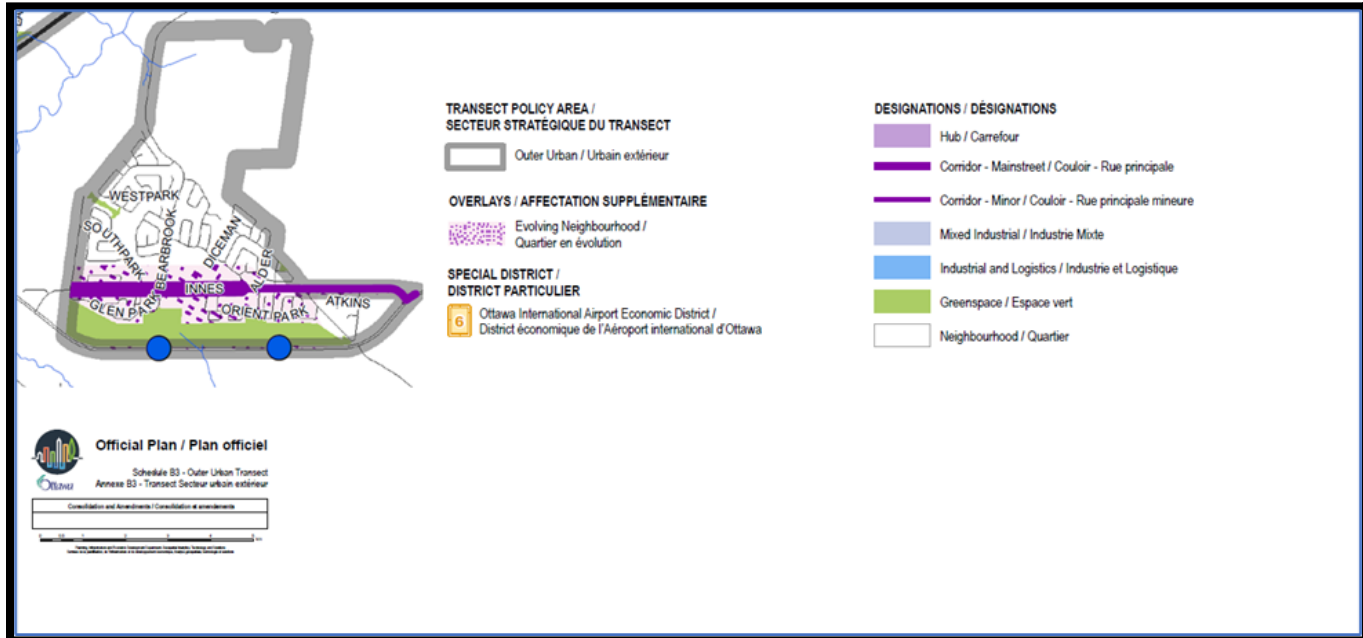


Figure 20 Extract from Official Plan showing land use designations. Property is in a Mainstreet Corridor and is adjacent to an Evolving Neighbourhood Overlay

On the following page, in **Figure 21**, there is a closer view of the Official Plan mapping with the **subject property** shown in **red** and within the Mainstreet Corridor Designation and with the Evolving Neighbourhood designation further to the north. As indicated previously, the extent of the Mainstreet Corridor actually extends further to the north than the north limit of the subject property as the Mainstreet Corridor is intended to be 220 m from the centreline of Innes Road.

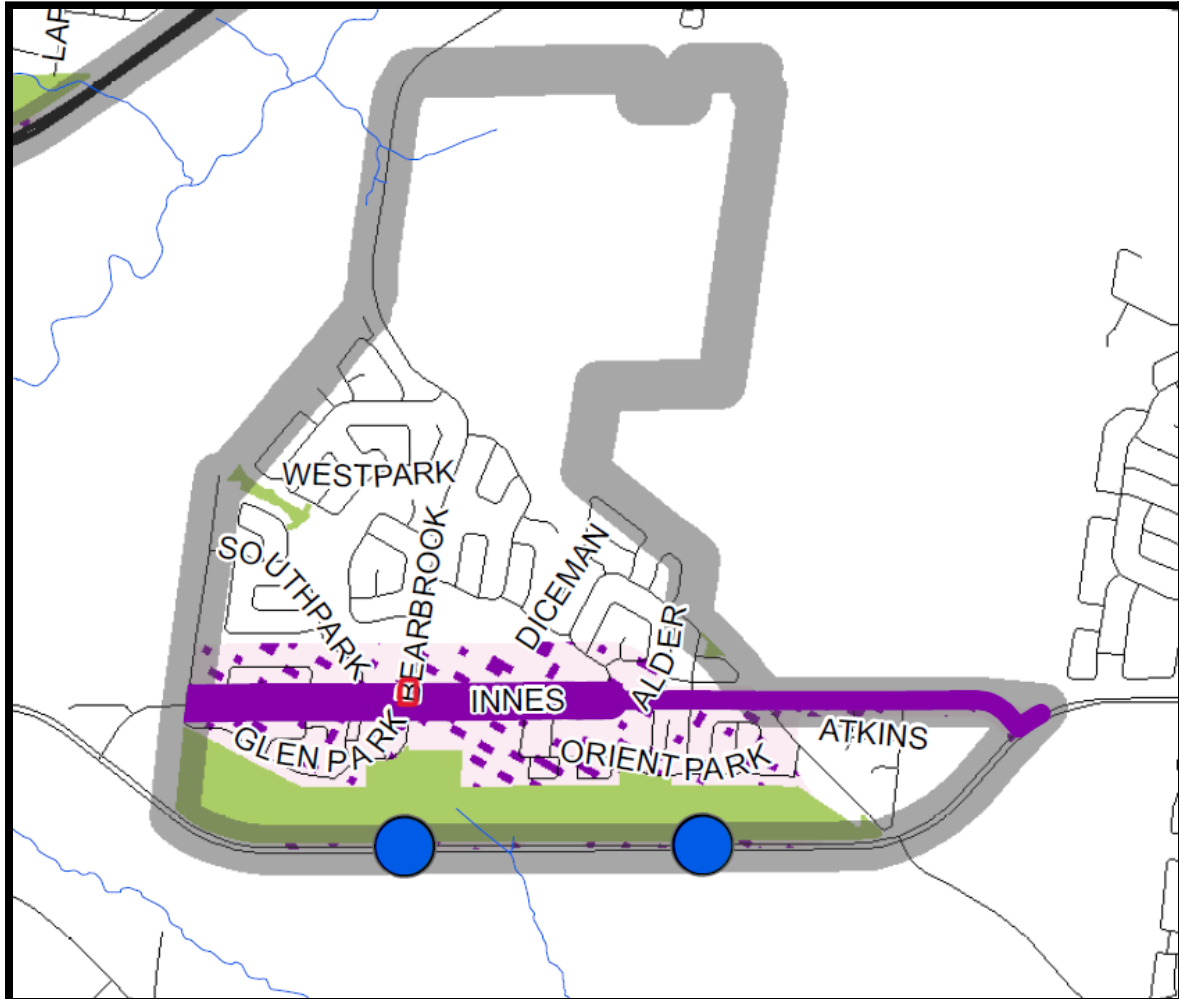


Figure 21. Extract of Official Plan mapping for Blackburn Hamlet with subject property highlighted

As indicated in the figure on the following page (**Figure 22 using GeoOttawa mapping**) the extent of the Main Street corridor designation by the prescribed distance goes beyond the north edge of the subject property (north edge of the subject property is approximately 182 m north of the centreline of Innes Road). **As per Section 5.6.1.1.1.** of the 'new' Official Plan indicates that the depth of the Evolving Neighbourhood designation *'will be applied generally to the properties that have a lot line along a Minor Corridor; lands 150 m from the boundary of a Hub or Mainstreet designation; and to lands within a 400 m radius of a rapid transit station.* As a result, the Mainstreet designation extends beyond the north edge of the subject property and extends to a point beyond the first pair of adjacent semis to the north and a portion of the next lot north of the first pair of

semis. The Evolving Neighbourhood designation would extend to north of Centrepark Drive to the north of the semis.



Figure 22 showing the distance from the centreline of Innes Road to the north edge of the subject property as being 182 m

The properties rear lot line abuts a zoned minor institutional, école élémentaire Catholique Sainte-Marie, the northern interior lot line abuts a lot zoned Residential Second Density, while the southern interior lot line abuts a lot also zoned Arterial Mainstreet, Subzone 11.

As per Section 4.6.6 of the new Official Plan which speaks to transition between different building heights of multi unit residential buildings adjacent to lower density residential properties,

1) To minimise impacts on neighbouring properties and on the public realm, transition in building heights shall be designed in accordance with applicable design guidelines. **The setback of the proposed building to the north property line has been located to be in conformity with the interior side yard setback requirements that are in effect for the AM11 zone and the maximum building height is in.** In addition, the Zoning By-law shall include transition requirements for Mid-rise and High-rise buildings, as follows:

- a) Between existing buildings of different heights;
- b) Where the planned context anticipates the adjacency of buildings of different heights;
- c) Within a designation that is the target for intensification, specifically:
 - i) Built form transition between a Hub and a surrounding Low-rise area should occur within the Hub; and
 - ii) Built form transition between a Corridor and a surrounding Low-rise area should occur within the Corridor. **The proposed new building will be setback from the north property line through a setback of 7.5 m (25') with portions of the building above the 4th floor set a little further back (8 + m) and the penthouse set approximately 10 m from the north property line. The existing semis to the north are approximately 2.9 m from the mutual property line with the subject property giving a building to building setback of 10.4 m. The entirety of the closest pair of semis to the north property line are within the prescribed corridor width of the Main Street (Innes)**

2) Transitions between Mid-rise and High-rise buildings, and adjacent properties designated as Neighbourhood on the B-series of schedules, will be achieved by providing a gradual change in height and massing, through the stepping down of buildings, and setbacks from the Low-rise properties, generally guided by the

application of an angular plane as may be set in the Zoning Bylaw or by other means in accordance with Council-approved Plans and design guidelines.

The zoning in place for this property has angular plane calculations in place when an AM zoned property abuts a residential property along the rear lot line which does not permit mid or high rise uses but there is only a prescribed minimum interior side yard setback for AM11 lots and the proposal meets those requirements. The property to the rear of the subject property is a school and it is in an I1F zone and the rear yard setback that has been provided is approximately 15 m and the required rear yard setback is 10 m.

In terms of the setback to the property to the north, the provided setback is zoning compliant and in terms of impacts of shadowing on the properties to the north there is brief minimal impact in terms of shading during the mid day period on September 21 (fall equinox)



and also some impact in the mid day period on December 21 (winter equinox)



Summer shadowing does not cast a shadow on the adjacent properties due to the increased height of the sun in the sky and the impacts are primarily restricted to mid day impacts.

As per Section 4.6.6.7 of the new Official Plan, Mid-rise buildings shall be designed to respond to context, and transect area policies, and

should:

a) Frame the street block and provide mid-block connections to break up large blocks; **The proposed development is the most northerly portion of the AM zoned lands and mid block connections are not applicable as the lands to the rear are part of a school property.**

b) Include a base with active frontages, and a middle portion that relates to the scale and character of the surrounding buildings, or, planned context; **As indicated in the Design Brief prepared by the project architects the base of the building contains ground level townhouse units with active frontages and interaction with the sidewalk and the design is cognizant of the nearby uses and will mitigate the impact of the building on adjacent properties through the use of building materials, and a design in which no unit balconies will overlook the properties to the north and above the 4th floor there is a slightly greater setback that is in excess of minimum side yard setbacks.**

c) Be generally proportionate in height to the width of the right of way, with additional height permitted in the Downtown Core Transect; **The proposed building height is 29 m and the existing width of the Right of Way is 26 m and the permitted maximum building height is 30 m, and;**

d) Provide sufficient setbacks and step backs to:

i) Provide landscaping and adequate space for tree planting; **landscaping and tree planting provided along the frontage of the property and some limited landscape strips along the north and south property lines. The north property line contains the driveway access to the rear yard parking and underground parking entrance and as a result, additional landscaped areas are not provided in this area.**

ii) Avoid a street canyon effect; **on the opposite side of Bearbrook Road there is a school which will not result in a street canyon effect with the development of this property and**

iii) Minimise microclimate impacts on the public realm and private amenity areas. **The private amenity areas are located primarily in the balcony areas of the property and will not result in adverse microclimate impacts and the public amenity area is at the rear of the property and will have afternoon and evening sunlight in most seasons and will not have microclimate impacts.**

The subject property meets the policy directions for this property as a Main Street Corridor property in close proximity to an Evolving Neighbourhood designation in the new Official Plan and has been designed to result in a minimal impact on the neighbouring properties.

Public Consultation Details

A pre consultation meeting was held with City Staff in the Planning, Infrastructure, Transportation and Urban Design Departments on October 20 2021 and then further email correspondence between that date and mid November related to zoning and submission requirements.

The owners and planning consultant have had video meetings and an initial discussion in May 2021 and then on March 11 and then on March 17 2022 with Councillor Dudas to discuss the project. In addition, a video meeting was held between the owners and the planning manager for the area on April 8 to discuss the project.

In addition to the Public Hearing that will be held when the Minor Variance application is brought before the Committee of Adjustment, an additional Public Open House is proposed. The Public Open House would be held jointly with Councillor Dudas's office to give members of the public the opportunity to comment on the proposed development.

Conclusions

The proposed development of a 9 storey rental apartment building on the property is consistent with the majority of current zoning and Official Plan and policies for these lands.

The site represents a sensitive infill development that will allow for:

- a well designed contemporary rental building to be located on the periphery of an established neighbourhood that will add to the character of the street and the neighbourhood

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- a centralised underground parking facility which results in a reduced amount of surface parking on the site and also will serve as the location for a significant component of the bicycle parking for the development.
- a new development that meets the majority of the current zoning regulations
- buffering through vegetation and setbacks to adjacent properties .

Through the site plan process there are a number of key engineering studies and drawings required in support of the development. These studies show that the site can be serviced through existing service connections along Bearbrook Road. The overall site drainage and grading has been designed based on current City guidelines and will utilise roof top storage in order to control the 100 year peak post development flows to that of the 5 year pre development flows.

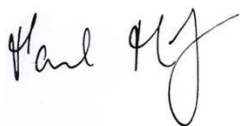
Landscaping will be provided along the side, front and rear yards to provide screening and buffering to adjacent properties. In addition there will be private amenity areas on the balconies of the units and a common amenity area within the building.

The proposed building height of the buildings is just below the permitted maximum building height as outlined in the zoning by-law (29 m proposed versus 30 m permitted) and privacy and noise issues will be mitigated as the plans do not call for a rooftop garden.

It is our opinion that the proposed development is consistent with the Provincial Policy Statement and the City of Ottawa Official Plan and the majority of relevant zoning regulations. It is being proposed at an appropriate scale of development and will be compatible with surrounding land uses and will contribute to the overall housing supply in the community surrounding the property.

Should you have any questions or require anything further, please do not hesitate to contact the undersigned.

P H Robinson Consulting



Paul Robinson RPP

APPENDIX

**PROPERTY SURVEY
SITE PLAN
LANDSCAPING PLAN
ARCHITECTURAL ELEVATION PLANS
TYPICAL FLOOR PLANS
UNDERGROUND PARKING PLANS**

30 July 2021 10:44 AM

ASSOCIATION OF ONTARIO LAND SURVEYORS PLAN SUBMISSION FORM 2169193



THIS PLAN IS NOT VALID UNLESS IT IS AN EMBOSSED ORIGINAL COPY ISSUED BY THE SURVEYOR In accordance with Regulation 1026, Section 29(3)

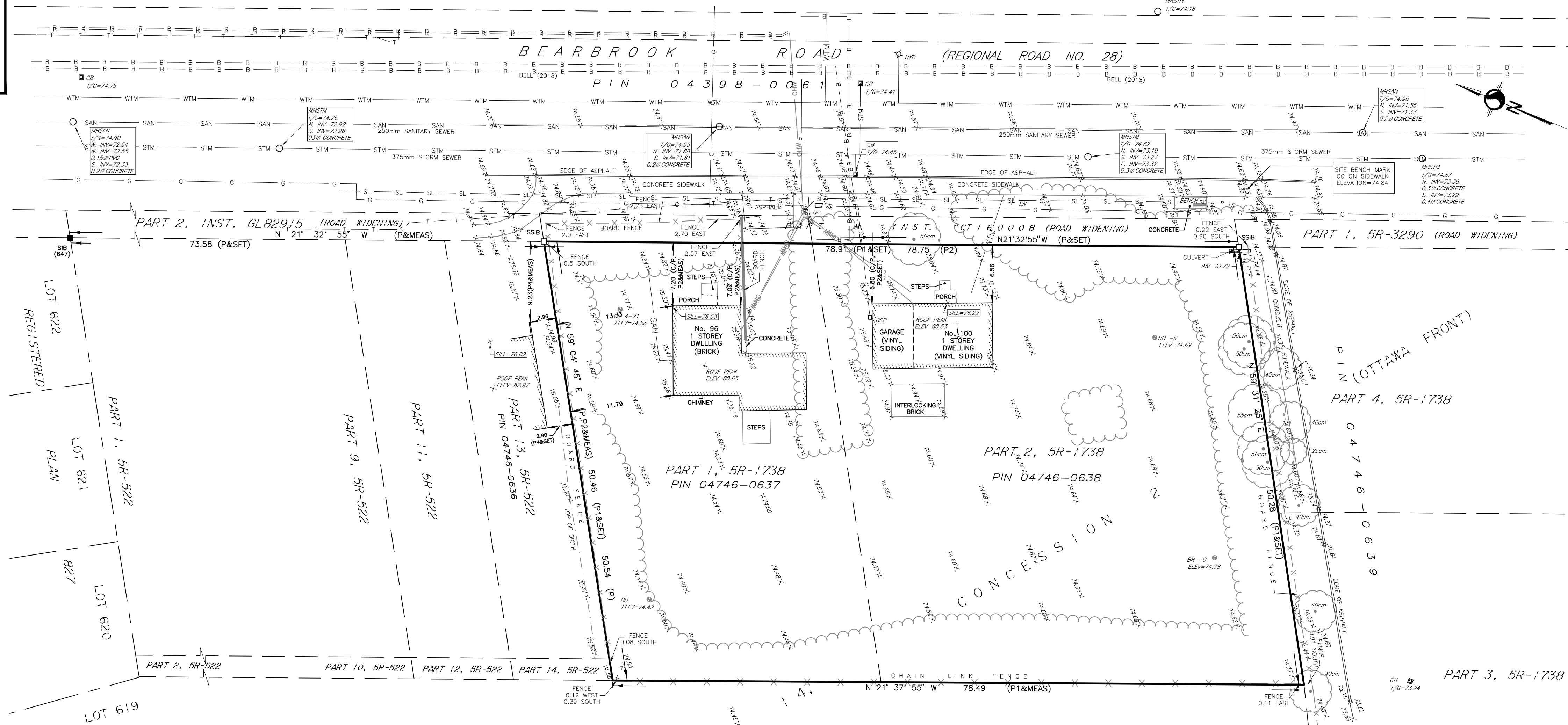
SURVEYOR'S REAL PROPERTY REPORT PART 1 - PLAN OF SURVEY PART OF LOT 14 CONSESSION 2 (OTTAWA FRONT) (GEOGRAPHIC TOWNSHIP OF GLOUCESTER) CITY OF OTTAWA

Scale 1:100

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LEGEND (IF APPLICABLE)

Table with columns for symbols, DENOTES, and FOUND MONUMENTS. Includes symbols for iron bars, manholes, valves, and various utility markers.



NOTE: THIS PLAN OF SURVEY IS TO BE READ IN CONJUNCTION WITH THE REPORT SUMMARY NOTED AS PART 2 HEREON. PART 2: This Report was prepared for Landric Homes and the undersigned accepts no responsibility for the use by other parties.

METRIC CONVERSION

DISTANCES AND COORDINATES SHOWN ON THIS PLAN ARE IN METRES AND CAN BE CONVERTED TO FEET BY DIVIDING BY 0.3048

BEARING NOTE

BEARINGS ARE GRID, DERIVED FROM CAN-NET VRS NETWORK GPS OBSERVATIONS ON NCC HORIZONTAL CONTROL MONUMENTS 19773035 AND 19680191, CENTRAL MERIDIAN, 76° 30' WEST LONGITUDE MTM ZONE 9, NAD83 (ORIGINAL).

19773035 N:5006060.42 E:324888.04 19680191 N:5033564.26 E:388064.94

FOR BEARING COMPARISONS, A ROTATION OF 0°41'05" COUNTER-CLOCKWISE HAS BEEN APPLIED TO BEARINGS ON (P) AND A ROTATION OF 0°41'25" COUNTER-CLOCKWISE HAS BEEN APPLIED TO BEARINGS ON (P1).

SURVEYOR'S CERTIFICATE

I CERTIFY THAT: 1. THIS SURVEY AND PLAN ARE CORRECT AND IN ACCORDANCE WITH THE SURVEYS ACT, THE SURVEYORS ACT AND THE REGULATIONS MADE UNDER THEM. 2. THE SURVEY WAS COMPLETED ON THE 28th DAY OF JULY, 2021.

July 30, 2021 DATE

Francis Lau Ontario Land Surveyor

Stantec Geomatics Ltd. logo and contact information: 1331 CLOYE AVENUE, SUITE 400 OTTAWA, ONTARIO, K2C 3G4 TEL. 613.722.4420 stantec.com

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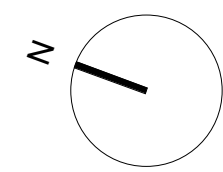
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GENERAL NOTES

- NOTE-A :**
• ALL DRAWINGS ARE TO BE READ IN CONJUNCTION WITH ALL OTHER DRAWINGS AND SPECIFICATIONS, INCLUDING OTHER CONSULTANTS DRAWINGS AND SPECIFICATIONS. ANY DISCREPANCIES BETWEEN DRAWINGS WILL BE REPORTED TO THE PROJECT LEAD IMMEDIATELY FOR CLARIFICATION PRIOR TO COMMENCING ANY CONSTRUCTION.
- NOTE-B :**
• ALL GENERAL SITE INFORMATION AND CONDITIONS HAVE BEEN COMPILED FROM EXISTING PLANS AND SURVEYS.
- NOTE-C :**
• CONTRACTOR IS RESPONSIBLE TO CHECK AND VERIFY ALL DIMENSIONS ON SITE AND REPORT ALL ERRORS AND / OR OMISSIONS TO THE ARCHITECT.
- NOTE-D :**
• REFER TO LANDSCAPE PLAN FOR ALL EXTERIOR LANDSCAPING.
- NOTE-E :**
• DO NOT SCALE DRAWINGS.
- NOTE-F :**
• ALL CONTRACTORS MUST COMPLY WITH ALL APPLICABLE CODES AND REGULATIONS.

SURVEY INFO

TOPOGRAPHIC SURVEY OF :
Stantec Geomatics Ltd.



PROJECT INFORMATION

SITE SUMMARY
ADDRESS: 98-100 BEARBROOK RD, GLOUCESTER, OTTAWA
CURRENT ZONING: AM11
SITE AREA: 3553.55 m²
PROPOSED USE: RESIDENTIAL
BUILDING AREA: 1812.30 m²

ZONING SUMMARY

	REQUIRED	PROPOSED
LOT AREA	3553.55 m ²	3553.55 m ²
LOT WIDTH	0.00 m	78.81 m
MIN. LOT WIDTH	0.00 m	0.00 m
MAX. BUILDING HEIGHT	30.00 m	29.00 m
MAX. PARAPET HEIGHT	0.00 m	0.00 m
Above max. building		
SET BACKS:		
- FRONT YARD	3.00 m (min.)	3.00 m
	5.00 m (max.)	
- CORNER SIDE YARD	0.00 m (min.)	0.00 m
	0.00 m (max.)	
- INTERIOR SIDE YARD	7.50 m (min.)	7.50 m
Abutting Residential Zone		
- INTERIOR SIDE YARD	0.00 m	1500
All other case		
- REAR YARD	10.00 m (min.)	12.73 m
	0.00 m (max.)	
MIN LANDSCAPED AREA	0.00 m ²	813.90 m ²

VEHICULAR PARKING

	REQUIRED	PROPOSED
TOWNHOUSE: 1 / UNIT	9	9
MID-RISE: 1.2 / UNIT	191	166
VISITORS: 0.2 / UNIT	34	34
Resident Parking Requirement Attainment: 89.3%		
BICYCLE: 0.5 / UNIT	80	84

AMENITY AREA

	REQUIRED	PROPOSED
PRIVATE	477.0 m ²	1527.5 m ²
COMMUNAL	477.0 m ²	577.4 m ²

(CALC: 6 m² / UNIT - MIN 50% MUST BE COMMUNAL)

SUMMARY OF PROPOSED:

9 x 3 / 4 Bedrm Townhouse: Each ±80 m² over 2 floors
159 APARTMENTS: Studio x 7;
1 Bed x 37;
1 Bed + Den x 75;
2 Bed x 39;
3 Bed x 1;
PARKING BAYS: 25 Exterior + 184 Interior = 209 Total
PARKING GARAGE: 3305.77m² x ±=
6811.54 m²
TOTAL BUILDING AREA: 14674.0 m² (excl. garage)
PROPOSED COVERAGE: 51.00 % (1812.3 m²)

KEYNOTE DESCRIPTIONS

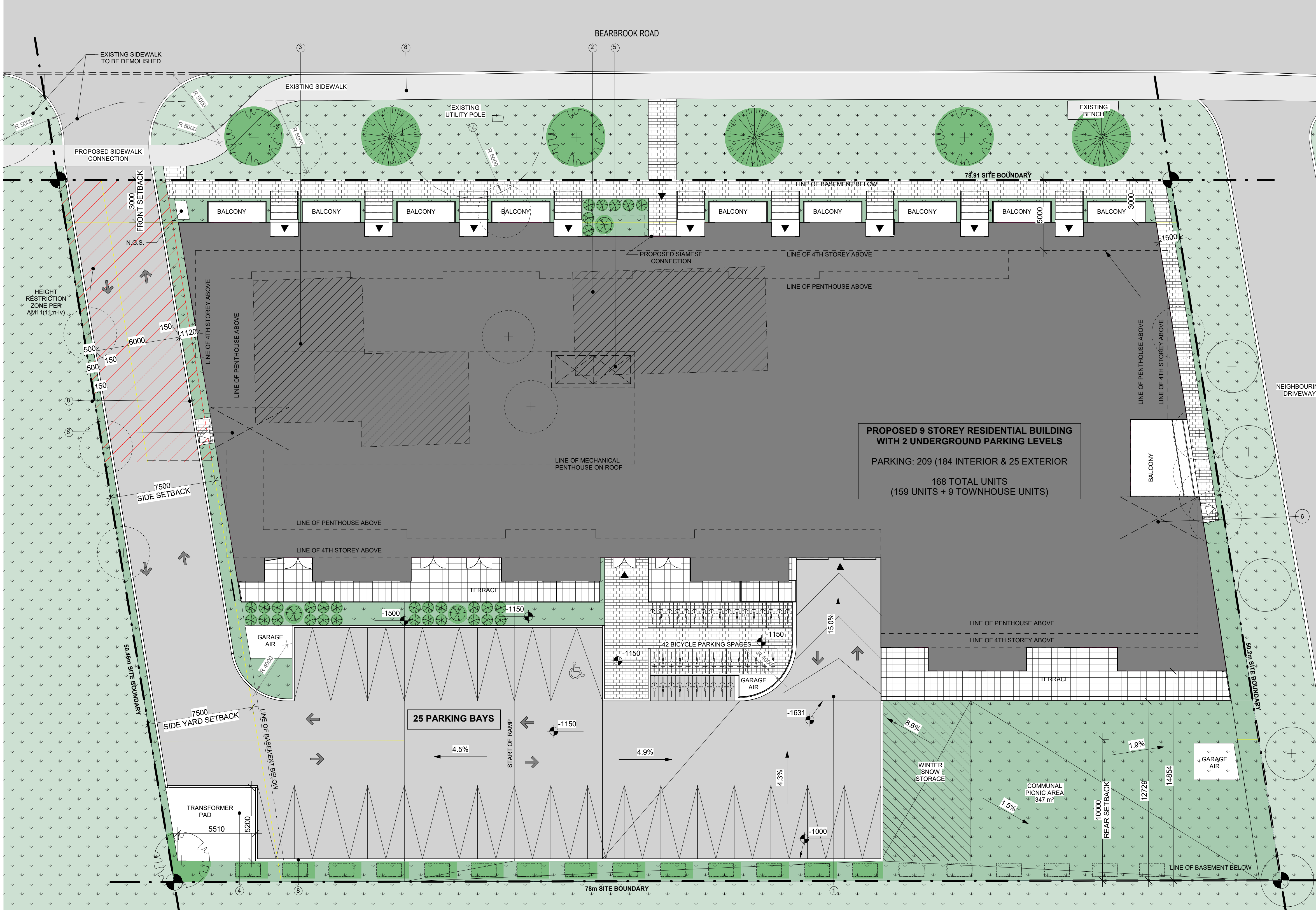
- 1 GARAGE ENTRANCE
- 2 100 BEARBROOK TO BE DEMOLISHED
- 3 98 BEARBROOK TO BE DEMOLISHED
- 4 TRANSFORMER PAD
- 5 ELEVATOR SHAFT
- 6 STAIRCASE SHAFT
- 7 TOWNHOUSE ENTRANCE
- 8 CONCRETE CURB

SITE CONTEXT



LEGEND

- SURFACES**
- GRASS
 - POURED CONCRETE
 - CONCRETE PAVERS
 - POURED CONCRETE
 - ASPHALT PAVING
 - PROPOSED NEW BUILDING
 - EXISTING BUILDING TO DEMOLISH
- LINES**
- PROPERTY LINE
 - SETBACK LINE
 - EXISTING FENCE
 - NEW FENCE
 - OVERHEAD WIRES
- VEGETATION**
- TREE: EXISTING TO REMAIN
 - TREE: EXISTING TO BE REMOVED
 - TREE: NEW PROPOSED
 - SHRUB: NEW PROPOSED
- SYMBOLS**
- DIRECTIONAL ARROWS
 - BUILDING ACCESS
 - BUILDING EGRESS
 - SIAMESE CONNECTION
 - UTILITY POLE
 - FIRE HYDRANT
 - CATCH BASIN / MANHOLE
 - DEPRESSED CURB
 - LANDSCAPE LIGHT
 - LIGHT POLE
 - WALL MOUNTED LIGHT
 - EXISTING GRADE ELEVATION
 - PROPOSED GRADE ELEVATION
 - LOT CORNERS
- PARKING**
- BIKE PARKING
H: HORIZONTAL 0.6M x 1.8M
V: VERTICAL 0.5M x 1.5M
S: STACKED 0.37M x 1.8M
 - CAR PARKING
R: RESIDENTIAL
V: VISITOR
 - BF PARKING (TYPE A)
R: RESIDENTIAL
V: VISITOR
 - BF PARKING (TYPE B)
R: RESIDENTIAL
V: VISITOR



PROPOSED 9 STOREY RESIDENTIAL BUILDING WITH 2 UNDERGROUND PARKING LEVELS
PARKING: 209 (184 INTERIOR & 25 EXTERIOR)
168 TOTAL UNITS (159 UNITS + 9 TOWNHOUSE UNITS)

UNIT COUNT	TOWNHOUSE	STUDIO	1 BEDROOM	1 BED + DEN	2 BEDROOM	3 BEDROOM	TOTAL
GROUND FLOOR	9		3	4	2		18
2ND FLOOR	(9)	1	4	5	2		12
3RD FLOOR		1	5	11	4		21
4TH FLOOR		1	5	11	4		21
5TH FLOOR		1	5	11	4		21
6TH FLOOR		1	5	11	4		21
7TH FLOOR		1	5	11	4		21
8TH FLOOR		1	5	11	4		21
9TH FLOOR					11	1	12
TOTAL	9	7	37	75	39	1	168

1 SITE PLAN
A003 1:150

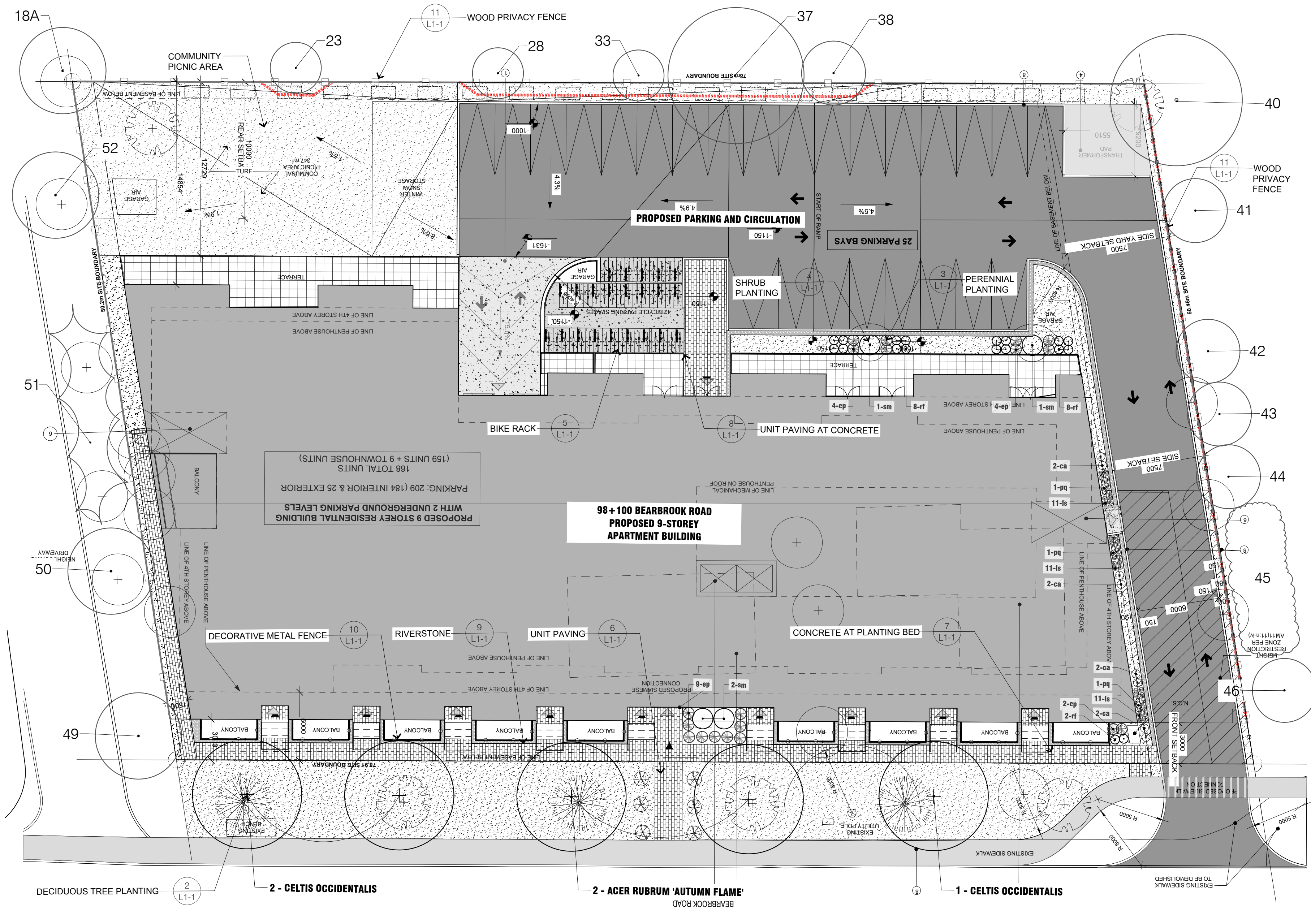


1.5 SPA Submission 22-03-15
revisions description date

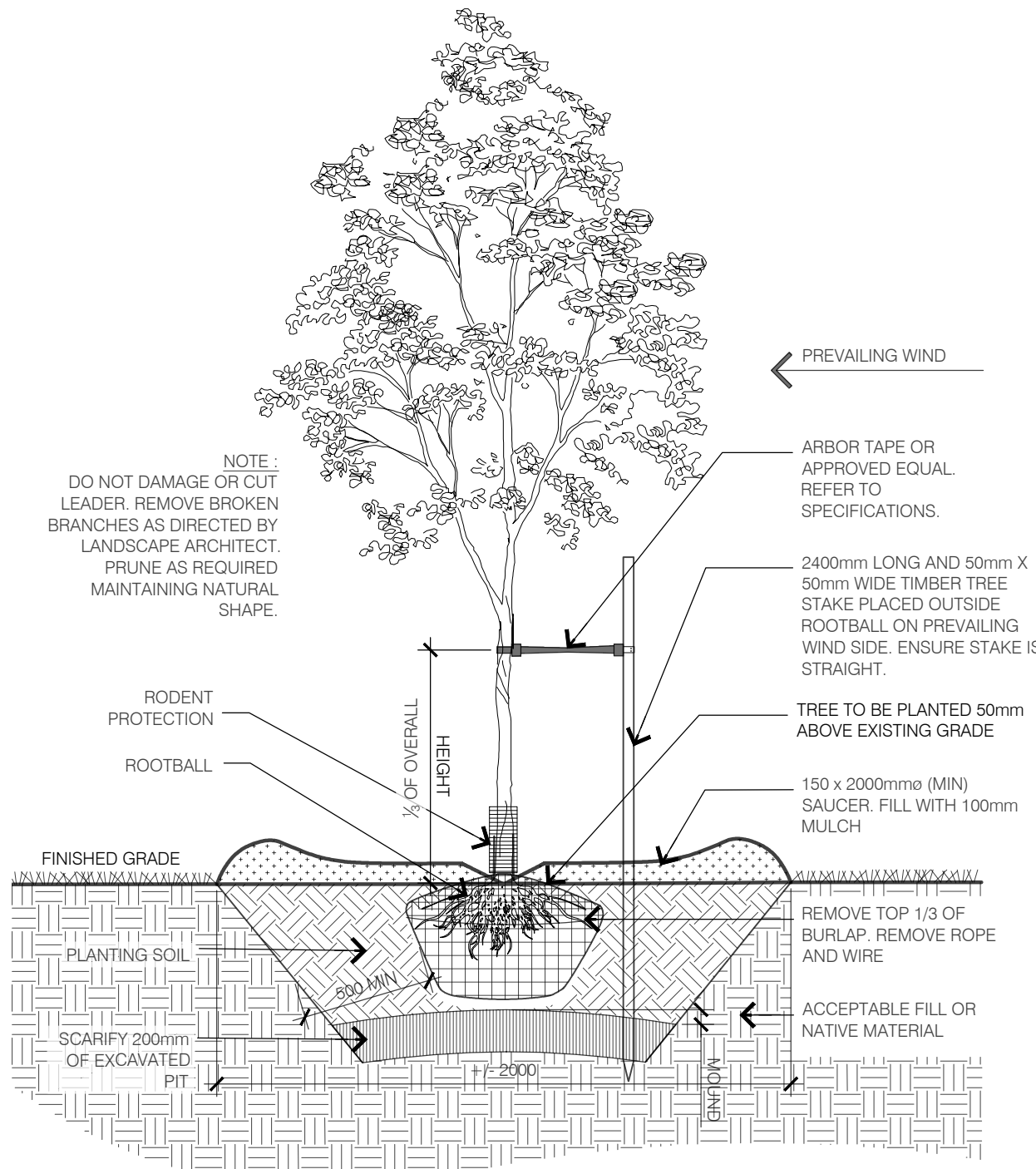
98-100 Bearbrook Rd

GENERAL SITE PLAN CONTROL

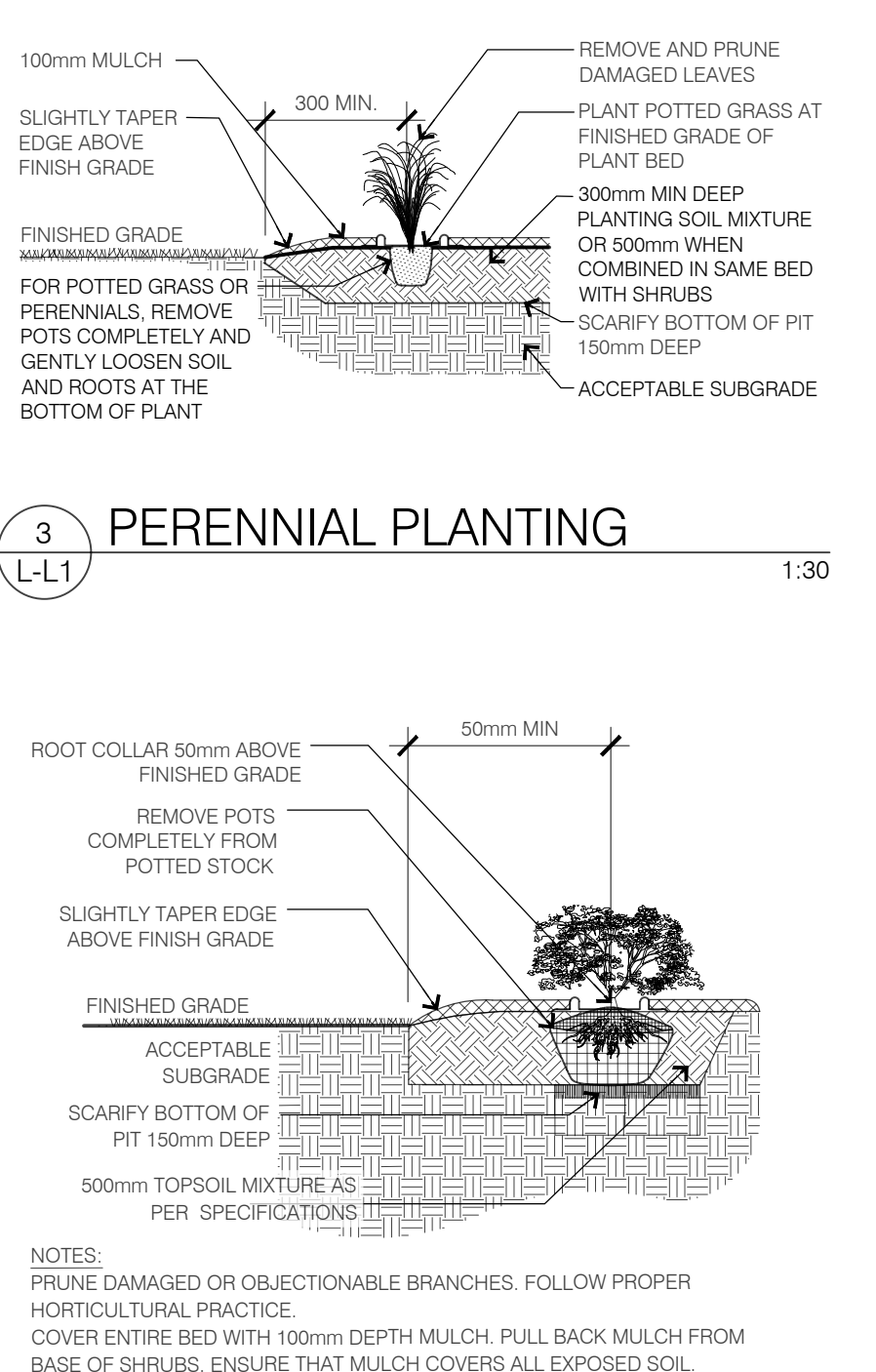
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PROJECT NO. / NO. DE PROJET : 21046
DATE : 22-03-15
DRAWN BY / DESSINÉ PAR : MA
REVIEWED BY / VÉRIFIÉ PAR : LaG
SCALE / ÉCHELLE : 1 : 150
PROJECT PHASE / PHASE DU PROJET : 0
DWG NO. / NO. DESSIN :
A003
REVISION NO. / NO. DE RÉVISION : 1.5



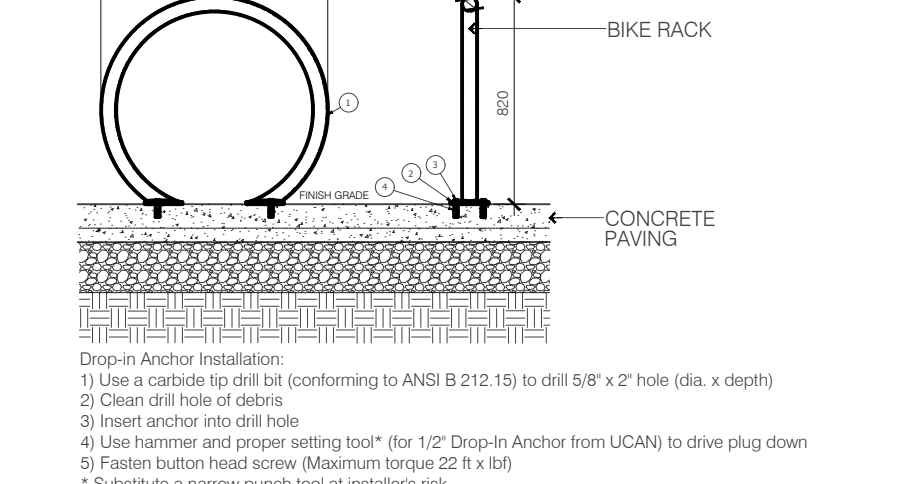
1 LANDSCAPE PLAN



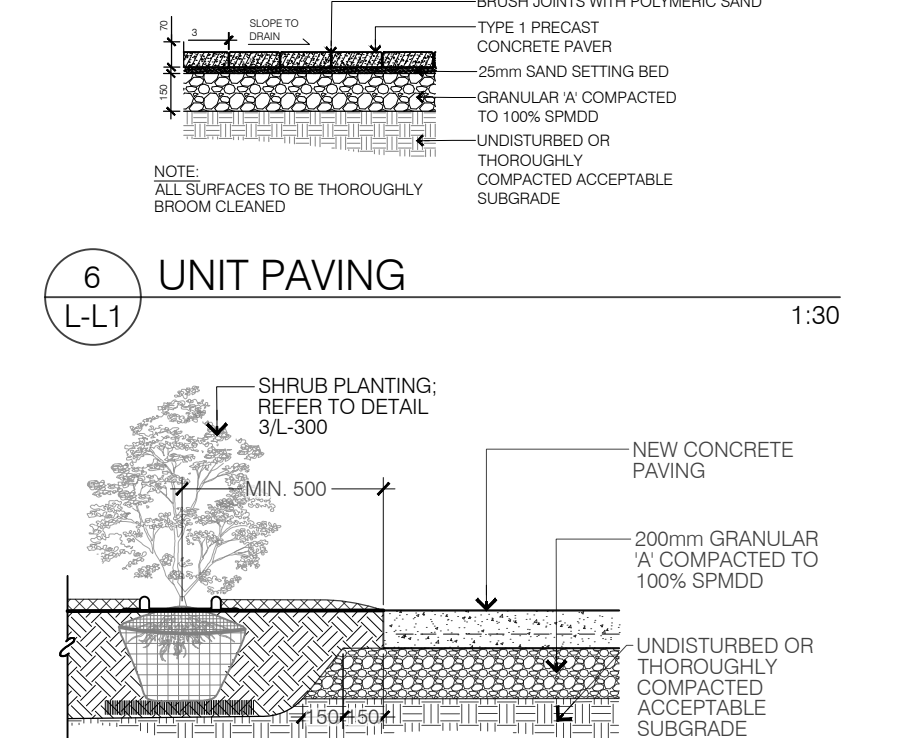
2 DECIDUOUS TREE PLANTING



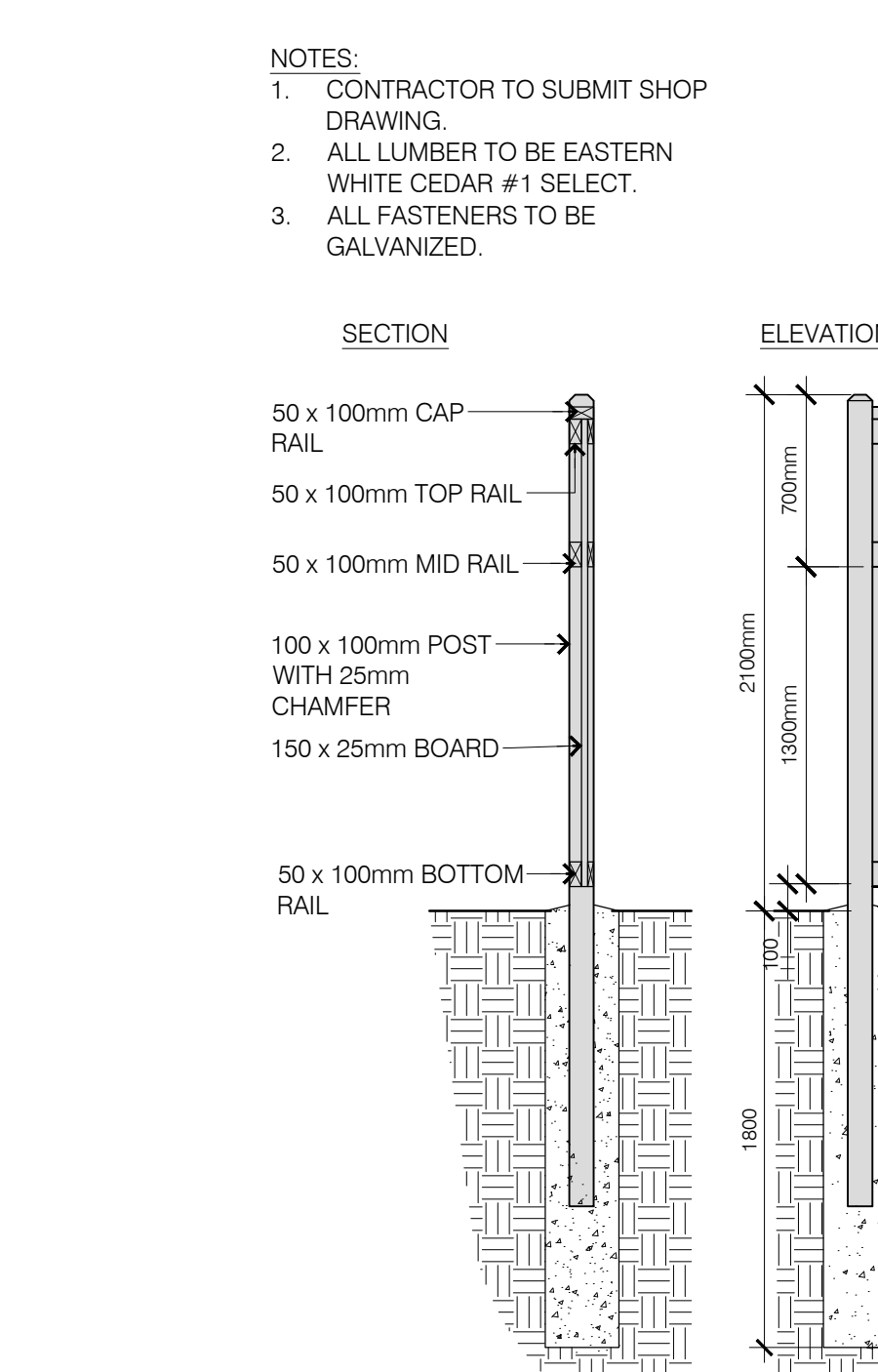
3 PERENNIAL PLANTING



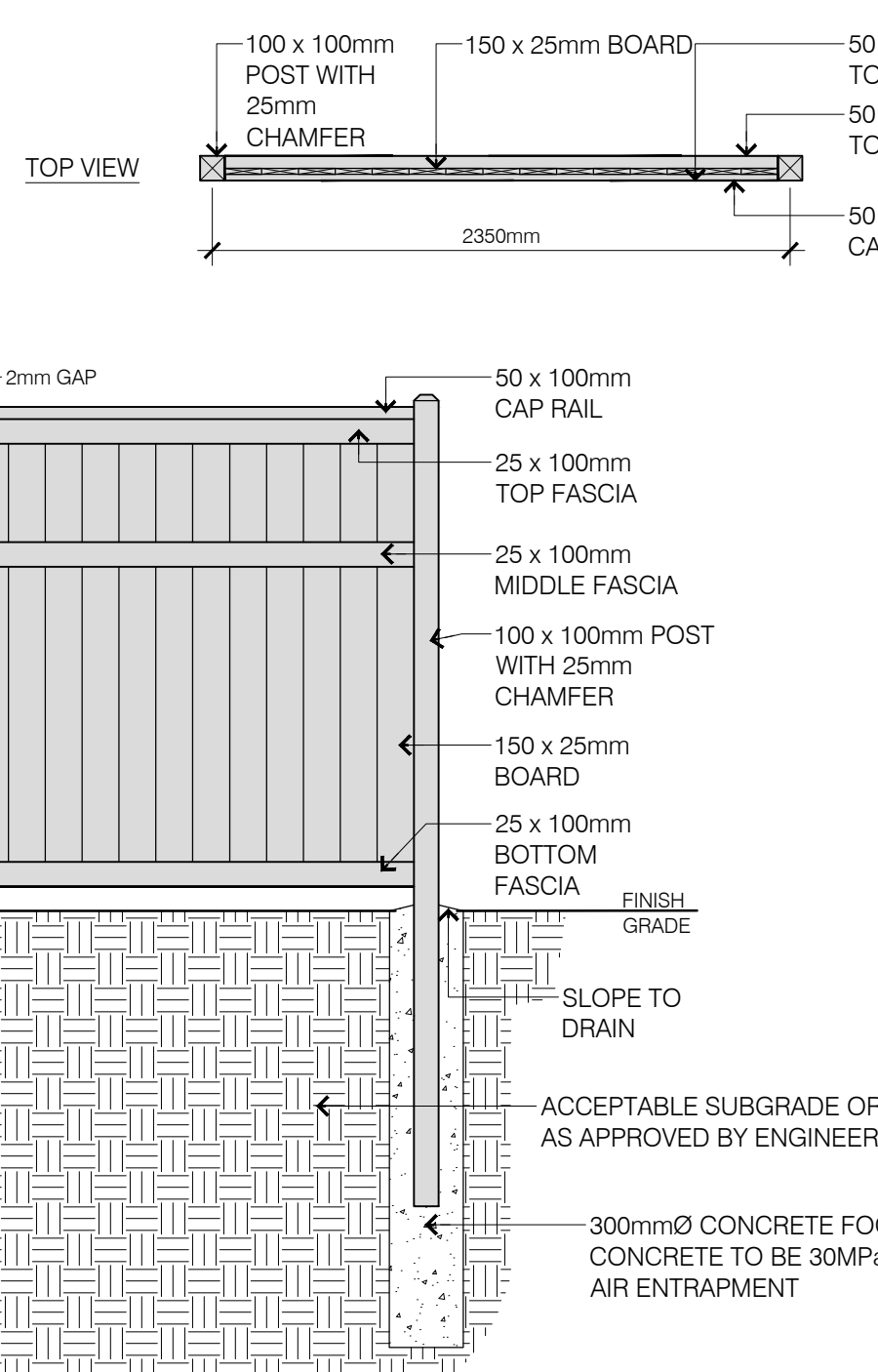
5 BIKE RACK



6 UNIT PAVING



7 CONCRETE AT PLANTING BED



11 WOOD PRIVACY FENCE

LEGEND

- PROPERTED LINE
- TREE PRESERVATION FENCE
- WOODEN FENCE(1.8m HEIGHT)
- 1.2m DECORATIVE METAL FENCE
- COIL BIKE RACK
- ENTRANCE
- UNIT PAVERS
- PATIO PAVERS
- RIVER STONE
- CONCRETE
- ASPHALT
- 800
- NEW DECIDUOUS TREE
- SHRUBS
- PERENNIALS
- VINES
- GRASSES

PLANT LIST

KEY	QTY	BOTANICAL NAME	COMMON NAME	SIZE	COMMENTS
DECIDUOUS TREES					
AR	2	<i>Acer rubrum autumn flame</i>	Freemans Maple	50mm CAL	W.B.
CO	3	<i>Celtis occidentalis</i>	Hackberry	50mm	W.B.
SHRUBS					
sm	4	<i>Syringa meyeri</i>	Dwarf Korean Lilac	1 gal	Potted
VINES					
pq	3	<i>Parthenocissus quinquefolia</i>	Virginia Creeper	1 gal	Potted
PERENNIALS					
ep	19	<i>Echinacea purpurea</i>	Purple Coneflower	1 tr	Potted
rf	18	<i>Rudbeckia fulgida goldstrum</i>	Black-eyed Susan	1 gal	Potted
ls	33	<i>Liatris spicata</i>	Dense Blazing Star	pl 72	Potted
GRASSES					
ca	8	<i>Calamagrostis acutiflora</i>	Karl Foerster	1 gal	Potted

8 UNIT PAVING AT CONCRETE 1:30

9 RIVERSTONE 1:20

10 DECORATIVE METAL FENCE 1:30

NOTES:

- CONTRACTOR TO SUBMIT SHOP DRAWING.
- ALL LUMBER TO BE EASTERN WHITE CEDAR #1 SELECT.
- ALL FASTENERS TO BE GALVANIZED.



KEY MAP, NTS

L1-1 GENERAL NOTES

- All general site information and conditions compiled from architect's and engineer's plans and surveys.
- Do not scale this drawing.
- Report any discrepancies prior to commencing work. No responsibility is born by the Landscape Architect for unknown subsurface conditions.
- Reinstate all areas and items damaged as a result of construction activities to the satisfaction of the Landscape Architect.
- Drawing may not be used for construction until signed by Landscape Architect as issued for construction.
- The accuracy of the position of utilities is not guaranteed.
- Individual utility co. must be contacted for confirmation of utility existence and location prior to digging.
- This drawing is an instrument of service and requires the permission of the Landscape Architect for use. Copyright is reserved by the Landscape Architect, David M. Lashley.

NO.	DATE	DESCRIPTION
4	2022-03-15	REVISED FOR SPA
3	2022-03-07	REVISED FOR SPA
2	2022-03-03	REVISED FOR SPA
1	2021-11-30	ISSUED FOR SPA

NO. DATE DESCRIPTION

SEAL: ASSOCIATION OF LANDSCAPE ARCHITECTS (OALA) MEMBER

NORTH: [Compass rose]

CONSULTANT: LASHLEY & ASSOCIATES LANDSCAPE ARCHITECTURE AND SITE ENGINEERING

201-860-GLADSTONE AVENUE OTTAWA, ON K1Y 3E6
T 613 233 8579 F 613 233 8580
W Lashley.com E Mail@Lashley.com

PROJECT: 98 + 100 BEARBROOK LANDRIC HOUSE
98 + 100 Bearbrook Drive
Gloucester, ON, K1B 3B9

DRAWING TITLE: LANDSCAPE PLAN

DATE: 2021-07-26 **DRAWING NO.:** NG-RP

SCALE: AS SHOWN

DRAWN BY: NG-RP

LA PROJECT NO.: 21813-1

L1-1

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1 EAST ELEVATION
A301 1 : 125



2 WEST ELEVATION
A301 1 : 125



revisions	description	date
1.5	SPA Submission	22-03-15

PROJECT NAME / NOM DU PROJET :

98-100 Bearbrook Rd

DRAWING NAME / NOM DU DESSIN :
ELEVATIONS

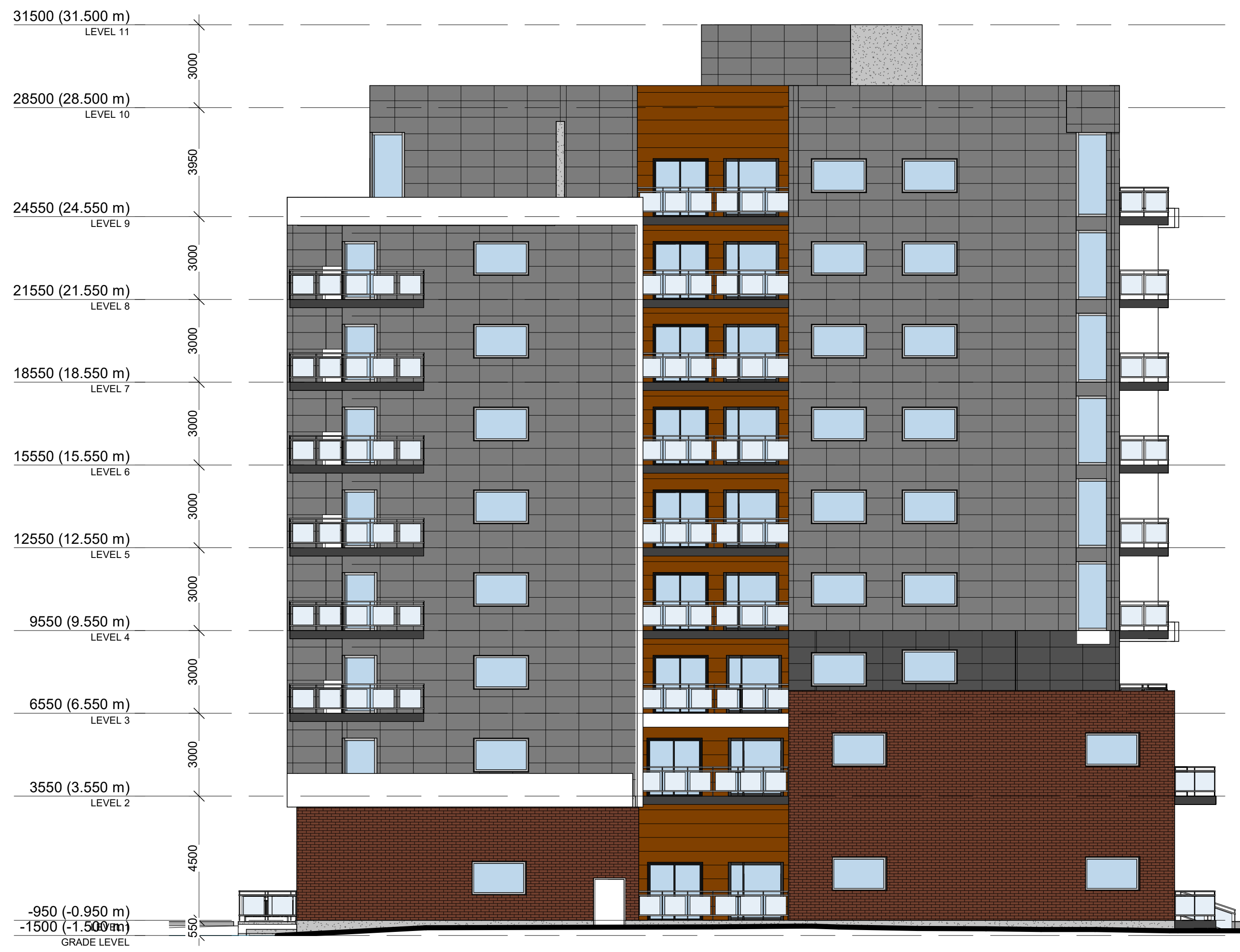
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DATE :	21-12-22
DRAWN BY / DESSINÉ PAR :	MA
REVIEWED BY / VÉRIFIÉ PAR :	LaG
SCALE / ÉCHELLE :	1 : 125
PROJECT PHASE / PHASE DU PROJET :	0
DWG NO. / NO. DESSIN :	

A301
REVISION NO. / NO. DE RÉVISION : 1.5

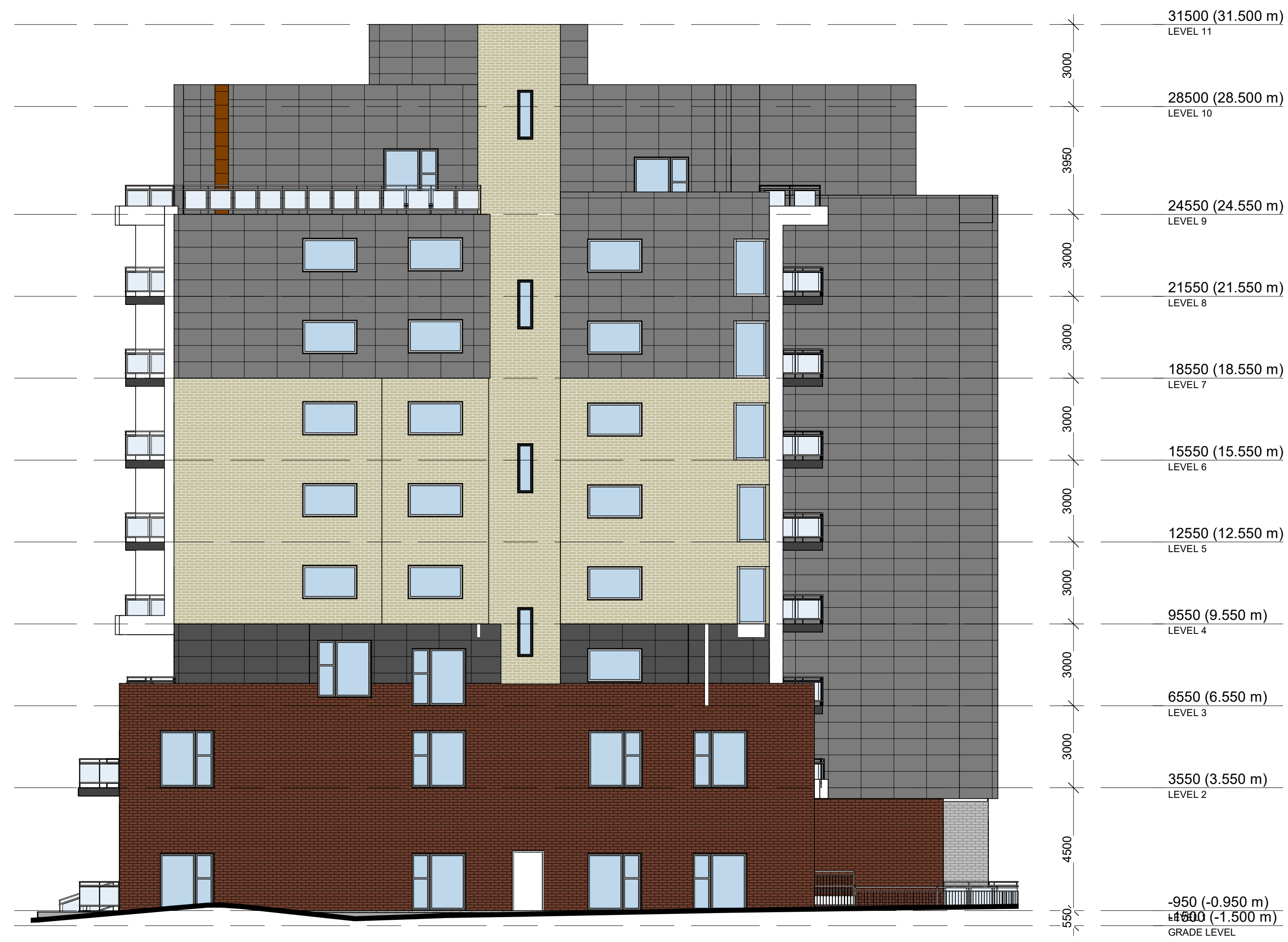
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1 SOUTH ELEVATION
A300 1 : 125



2 NORTH ELEVATION
A300 1 : 125



1.5	SPA Submission	22-03-15
revisions	description	date

PROJECT NAME / NOM DU PROJET :

98-100 Bearbrook Rd

DRAWING NAME / NOM DU DESSIN :

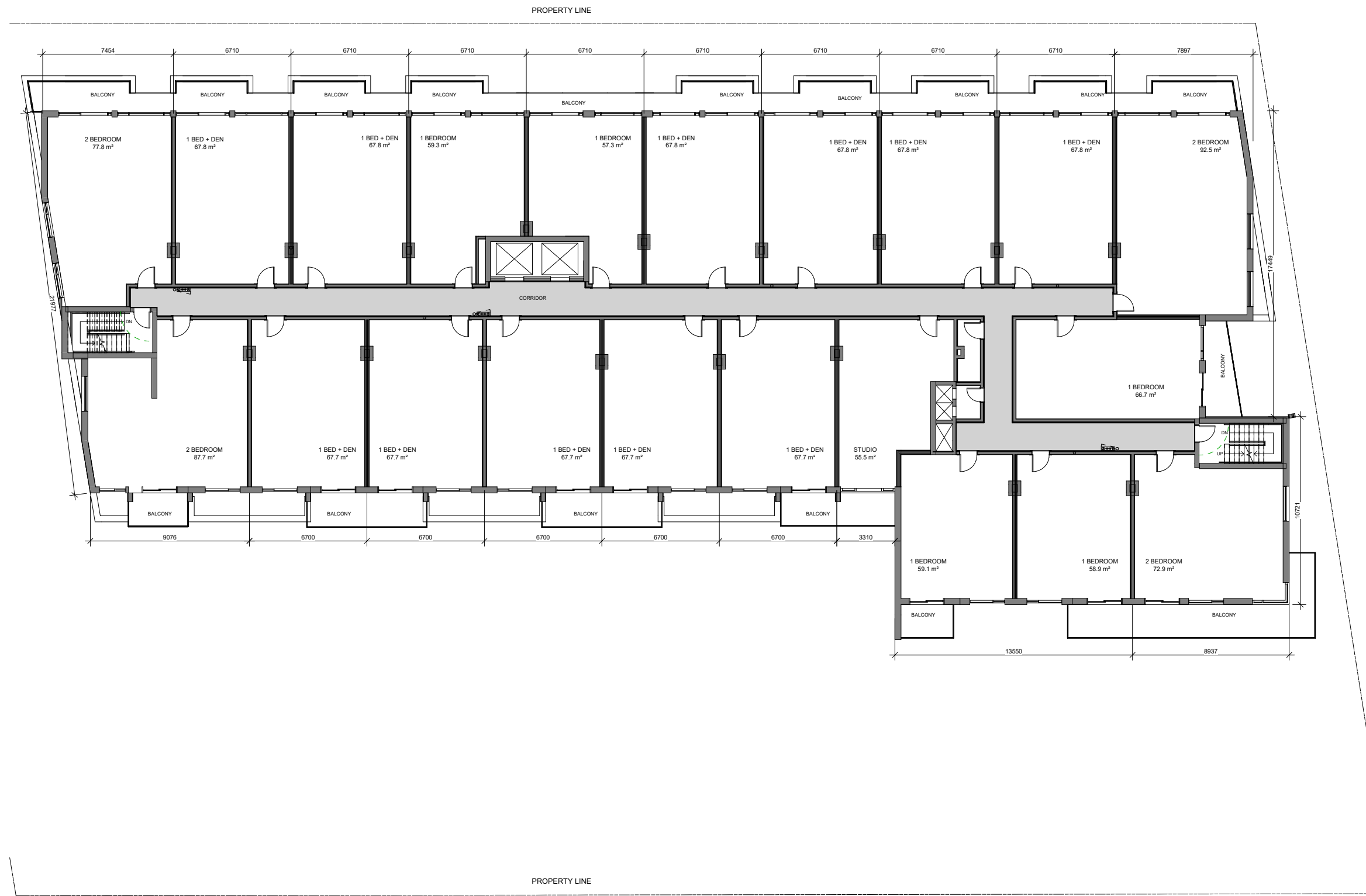
ELEVATIONS

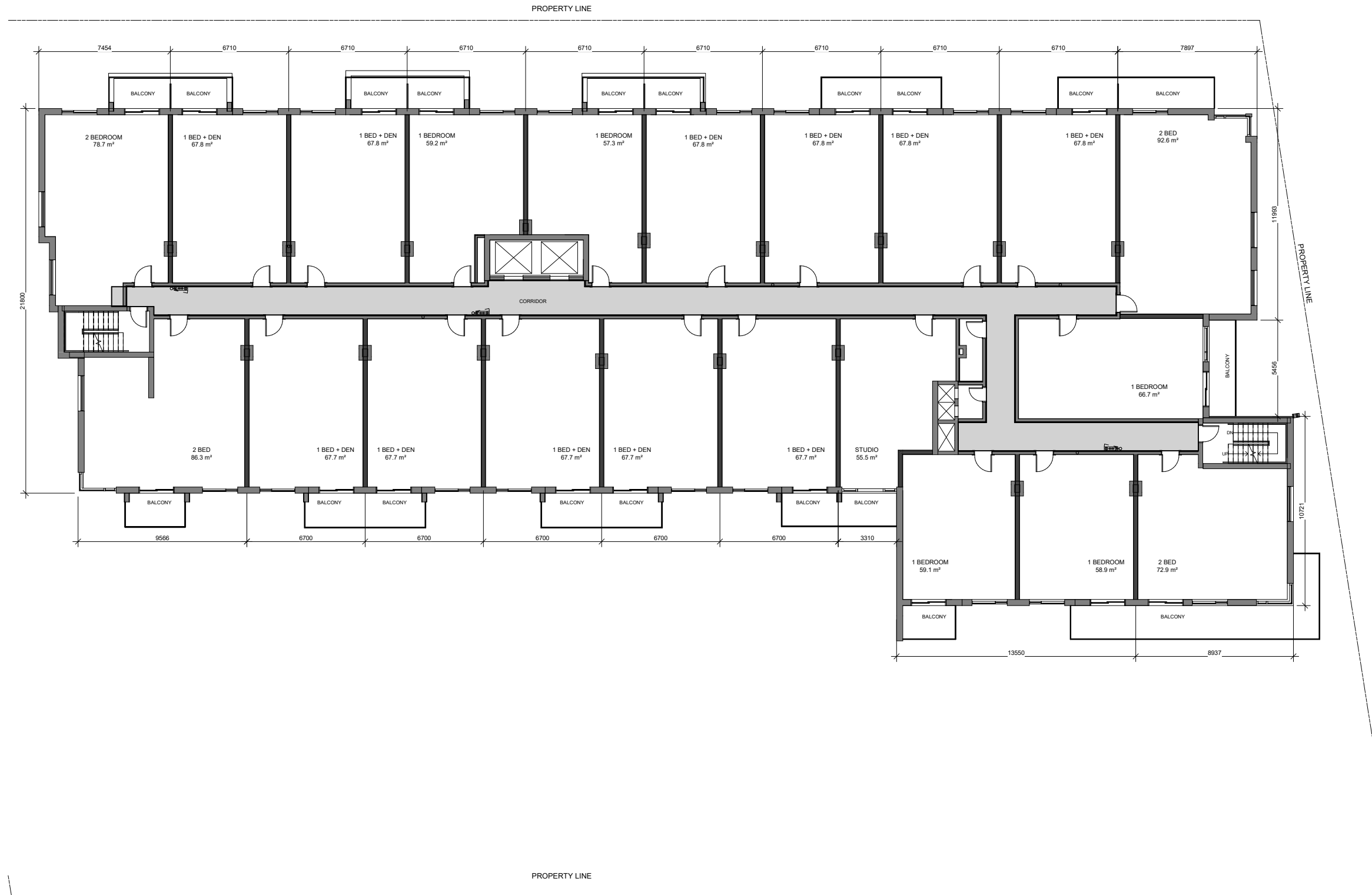
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DATE :	21-12-22
DRAWN BY / DESSINÉ PAR :	MA
REVIEWED BY / VÉRIFIÉ PAR :	LaG
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A300

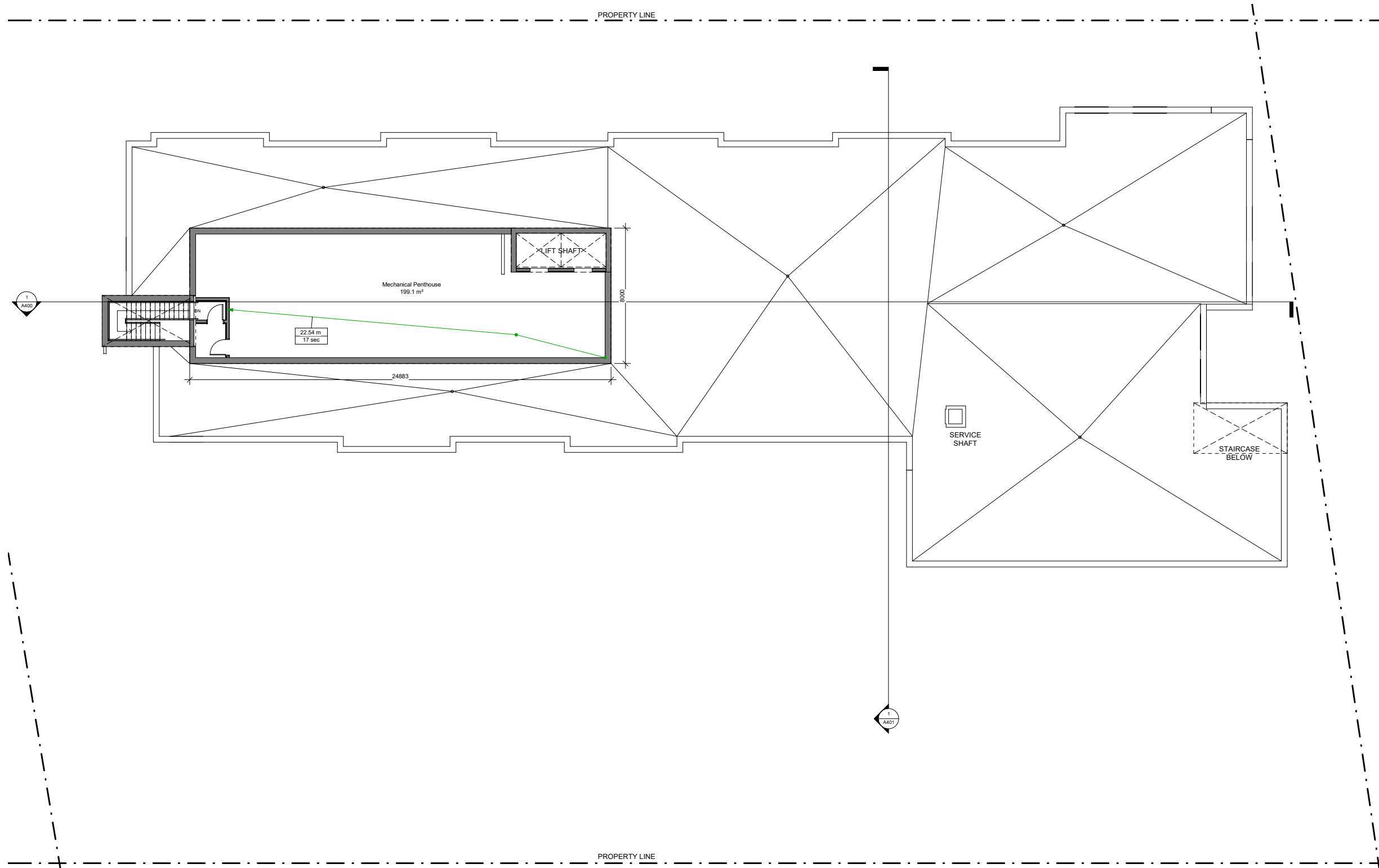
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1.5	SPA Submission	22-03-15
1.4	Preliminary Structural	22-02-25
1.2	COORDINATION	22-02-04
revisions	description	date

PROJECT NAME / NOM DU PROJET :

98-100 Bearbrook Rd

DRAWING NAME / NOM DU DESSIN :

P1

PROJECT NO. / NO. DE PROJET : 21046

DATE : 22-03-15

DRAWN BY / DESSINÉ PAR : MA

REVIEWED BY / VÉRIFIÉ PAR : LaG

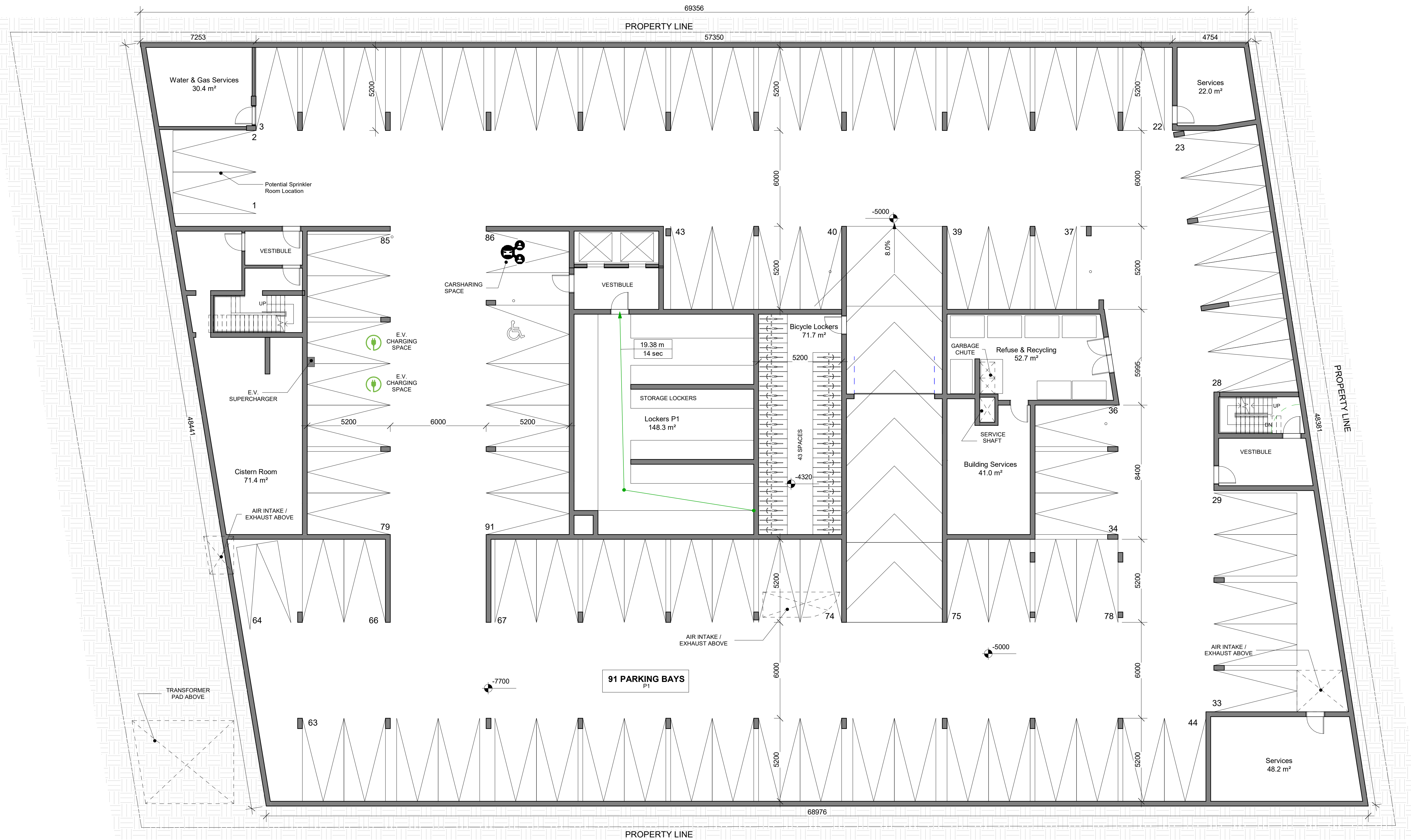
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PROJECT PHASE / PHASE DU PROJET : 0

DWG NO. / NO. DESSIN :

A100.1

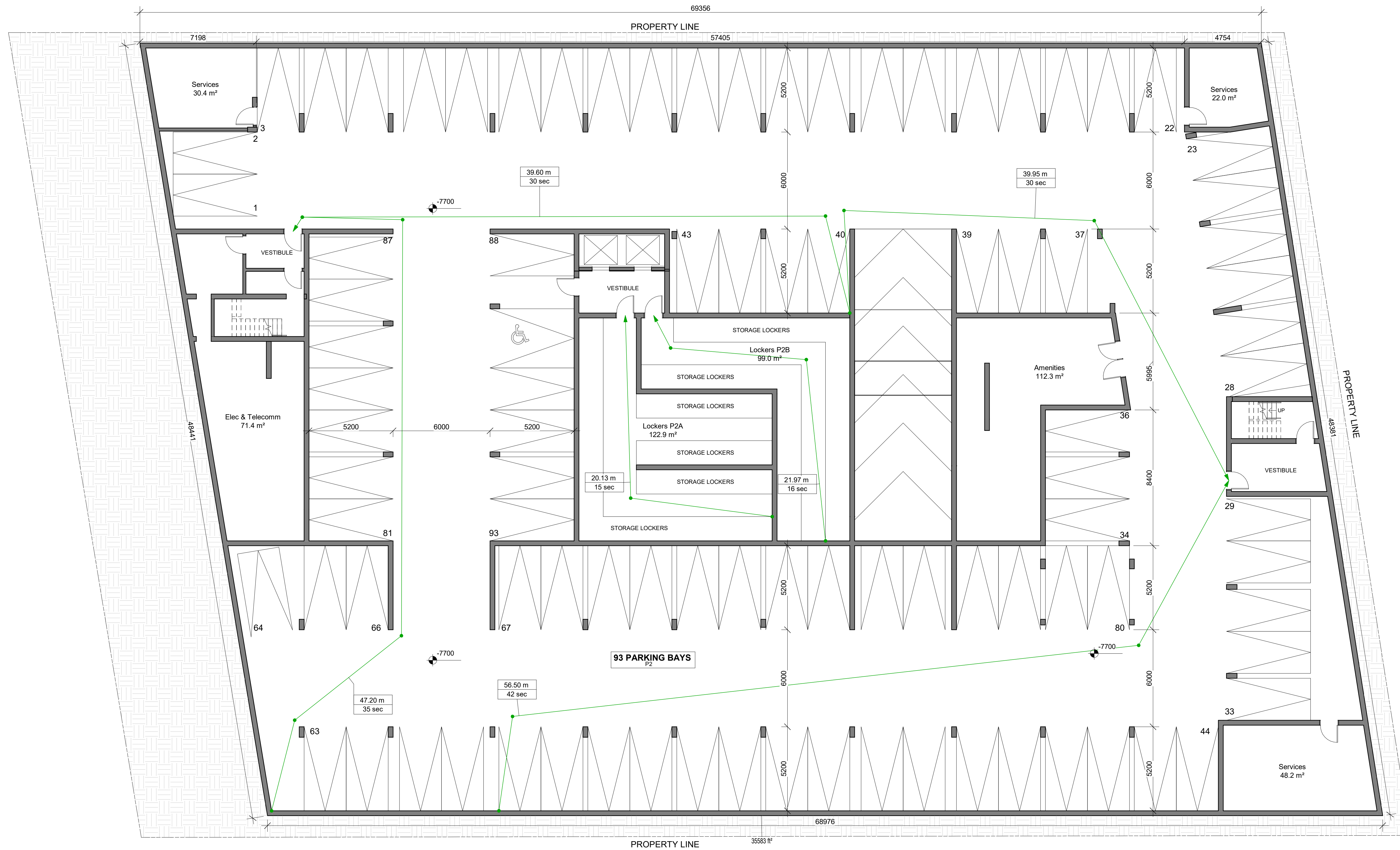
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CLIENT :



revisions	description	date
1.5	SPA Submission	22-03-15
1.4	Preliminary Structural	22-02-25
1.2	COORDINATION	22-02-04

PROJECT NAME / NOM DU PROJET :

98-100 Bearbrook Rd

DRAWING NAME / NOM DU DESSIN :

P2

PROJECT NO. / NO. DE PROJET : 21046

DATE : 22-03-15

DRAWN BY / DESSINÉ PAR : MA

REVIEWED BY / VÉRIFIÉ PAR : LaG

SCALE / ÉCHELLE : 1 : 125

PROJECT PHASE / PHASE DU PROJET : 0
DWG NO. / NO. DESSIN :

A100.0

REVISION NO. / NO. DE RÉVISION : 1.5