

Planning Rationale
140 Reis Road



Site Plan Control

Prepared for:

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Table of Contents

SECTION 1: INTRODUCTION	- 4 -
1.1 Overview	- 4 -
1.2 Subject Site	- 4 -
1.3 Surrounding Context	- 6 -
1.3.1 <i>Transit & Transportation Network</i>	- 6 -
1.3.2 <i>Surrounding Context</i>	- 7 -
SECTION 2: PROPOSED DEVELOPMENT	- 9 -
2.1 Proposed Site Plan	- 9 -
SECTION 3: POLICY CONTEXT	- 13 -
3.1 Provincial Policy Statement	- 13 -
3.2 Official Plan – 2003	- 14 -
3.2.1 <i>Carp Road Corridor Community Design Plan</i>	- 16 -
3.3 New Official Plan – 2021	- 17 -
3.4 Zoning By-law	- 17 -
SECTION 4: SUPPORTING STUDIES & REPORTS	- 20 -
4.1 Hydrogeological Study and Terrain Analysis	- 20 -
4.2 Site Servicing and Stormwater Management Report	- 20 -
4.3 Geotechnical Investigation	- 20 -
SECTION 5: CONCLUSION	- 21 -

SECTION 1: INTRODUCTION

1.1 Overview

JD Planning has been retained by Winch Holdings Limited (the ‘Owner’) to prepare a Planning Rationale Report for an application for Site Plan Control (Rural Small) with respect to the development of a proposed low-rise warehouse building located at 140 Reis Road, in the City of Ottawa. The site is a vacant rectangular-shaped property with frontage on Reis Road, located in a mostly developed light industrial business park subdivision, northeast of Carp Road. The Owner is proposing to construct a new one-storey light industrial building on the site. The following is a Planning Rationale in support of the proposed development.

1.2 Subject Site

The site is in Ottawa’s Carp neighbourhood in Ward 5 – West Carleton – March and is generally located north and east of Highway 417, west of Terry Fox Drive, and south of Thomas A. Dolan Parkway. Carp is a primarily rural area that contains a mix of vacant rural lands, agricultural lands, light industrial uses, and mineral aggregate resource areas. The Village of Carp is located to the northwest of the site, and is a community characterized by locally oriented shops and restaurants as well as slightly more dense areas of low-rise residential development.

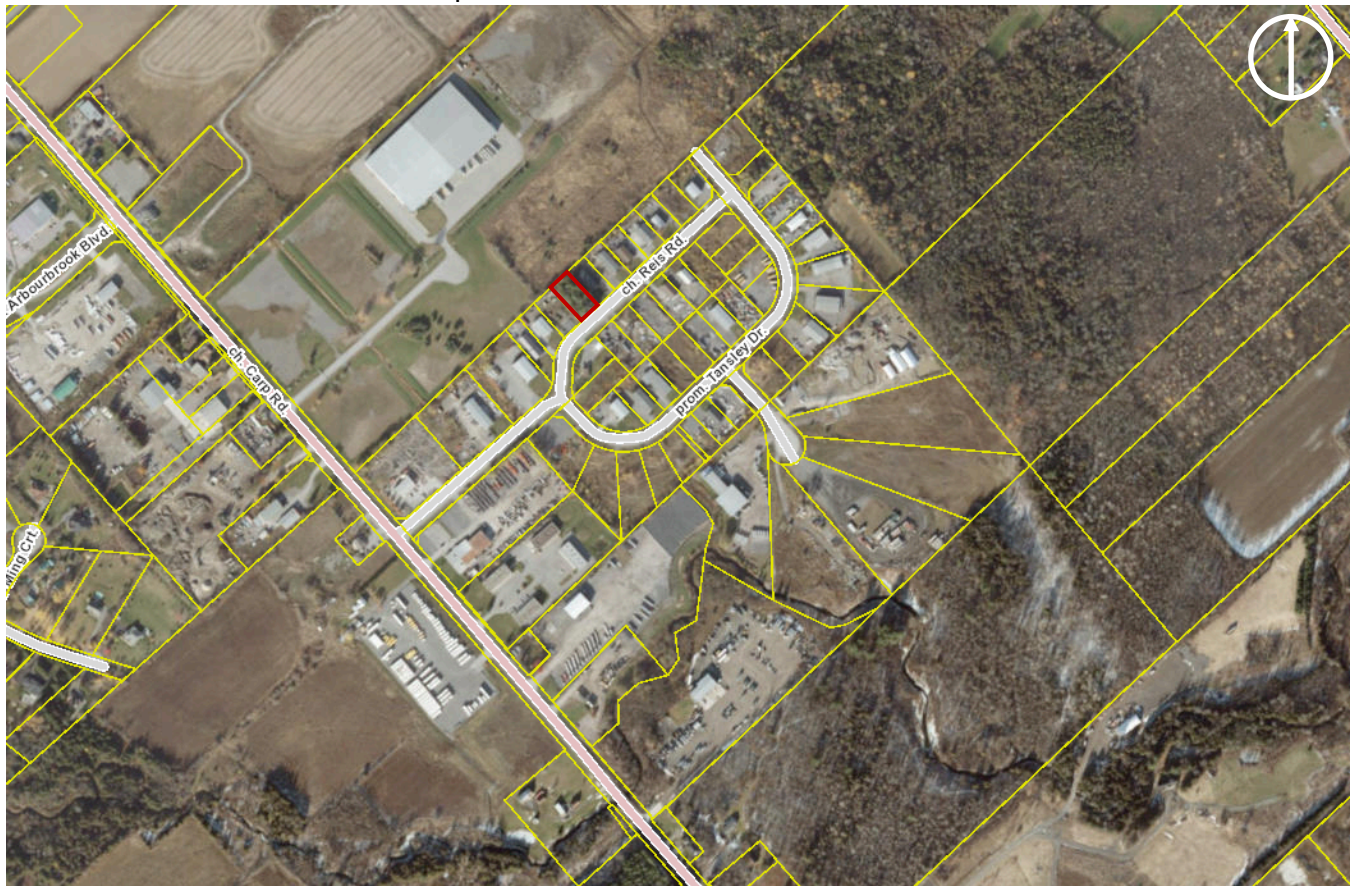


Figure 1. Subject site and surrounding context (GeoOttawa, 2019)

The site is presently vacant featuring a gravel laneway roughly in the centre of the property, as well as mixed grasses and shrubs. The site consists of the following specifications and legal description:

Area 1818.04 m²
Frontage 30.25 m
Depth 60.18 m
Legal Description Part 1, Part of Block 2, Plan 4M-745
PIN All of 04537-0208

Error! Reference source not found. shows the site and property lines, while **Error! Reference source not found.** is an extract of the existing plan of survey, which show the property size and

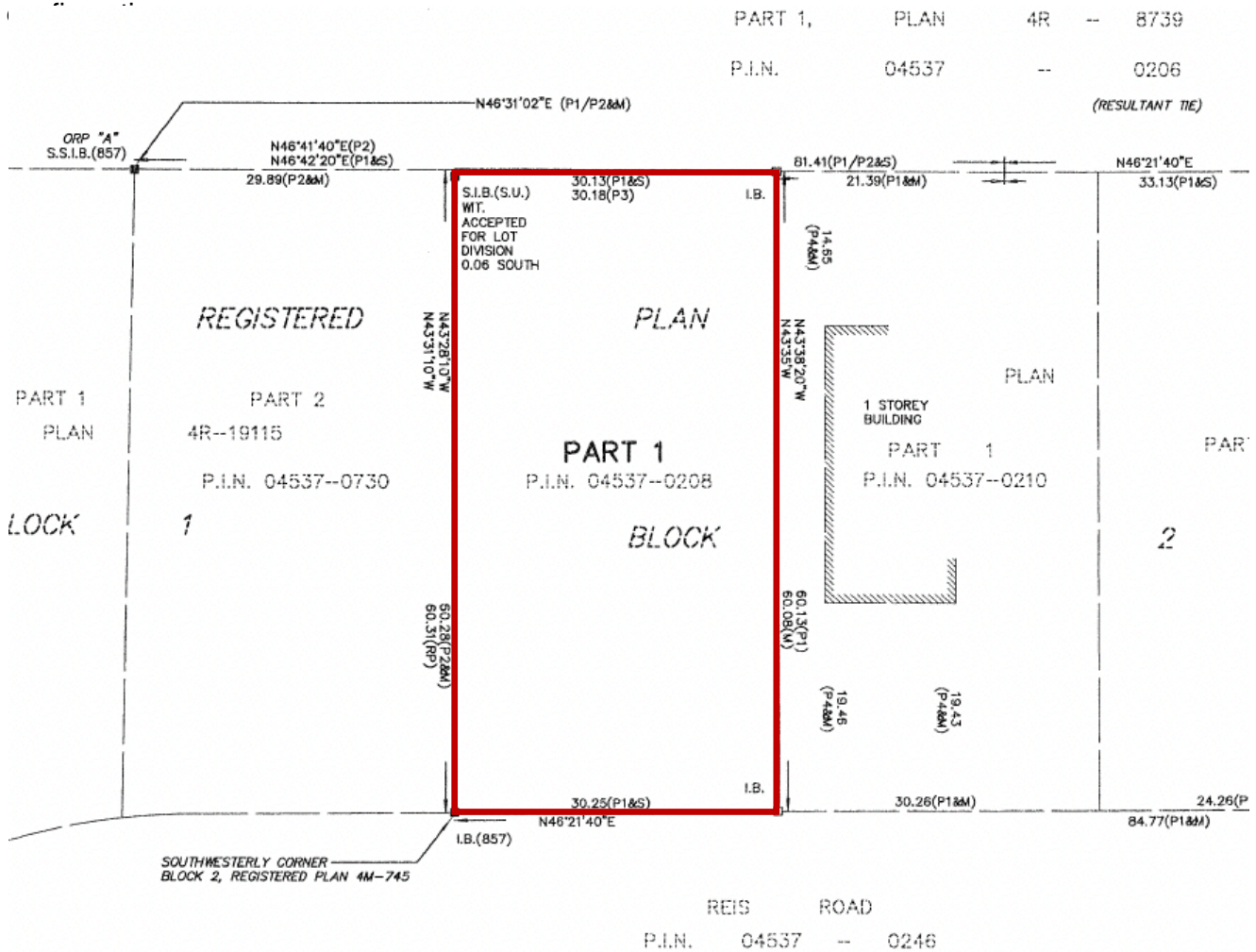


Figure 2. Extract of Plan of Survey prepared by H.A.Ken Shipman Surveying Ltd.

Image 1 and Image 2 below are images of the subject site as it presently exists today. **Error! Reference source not found.** is an aerial image of the site via Google Earth, which generally demonstrates the lot within its surrounding context.



Image 1. Streetview of subject site looking northwest from Reis Road



Image 2. Streetview of subject site looking north from Reis Road

1.3 Surrounding Context

The immediately surrounding area is characterized by generally equally sized lots ranging from approximately half an acre and up, containing low-rise light industrial type uses, warehouses, and open storage. The subject site is within a Plan of Subdivision, called the Reis Road Business Park, which was approved by the former Township of West Carleton, with the agreement registered in December 1989. According to GeoOttawa mapping, approximately 21 lots within the immediately surrounding area and RG4 zoning appear to be developed with a building, while the remaining lots contain outdoor storage or are still vacant.

1.3.1 Transit & Transportation Network

The site is located on the north side of Reis Road which travels generally north-east, south-west and connects to the east side of Carp Road to the southwest. To the northeast, Reis Road connects to Tansley Drive which is a semi-circular road that connects back to Reis at the southwest. Carp Road is a generally north-south arterial road, running from Stittsville Main Street to the south-east and Galetta Side

Road, south of the Ottawa River, to the northwest. Carp Road connects directly to Highway 417 which travels east-west through the City of Ottawa.

1.3.2 Surrounding Context

More specifically, the site is located on the north side of Reis Road within an existing light industrial subdivision, and the following building types and land uses directly abut the site:

- **North (rear yard)** Low-rise warehouse/distribution centre
- **South (front yard)** Reis Road: auto services use, open storage
- **East (side yard)** Low-rise light industrial type use
- **West (side yard)** Low-rise storage yard and machinery sales/warehousing

Images 3 through 5 represent immediately surrounding properties and the surrounding context.



Image 3. Subject site and surrounding context looking northwest from Reis Road



Image 4. Subject site and surrounding context looking north from Reis Road

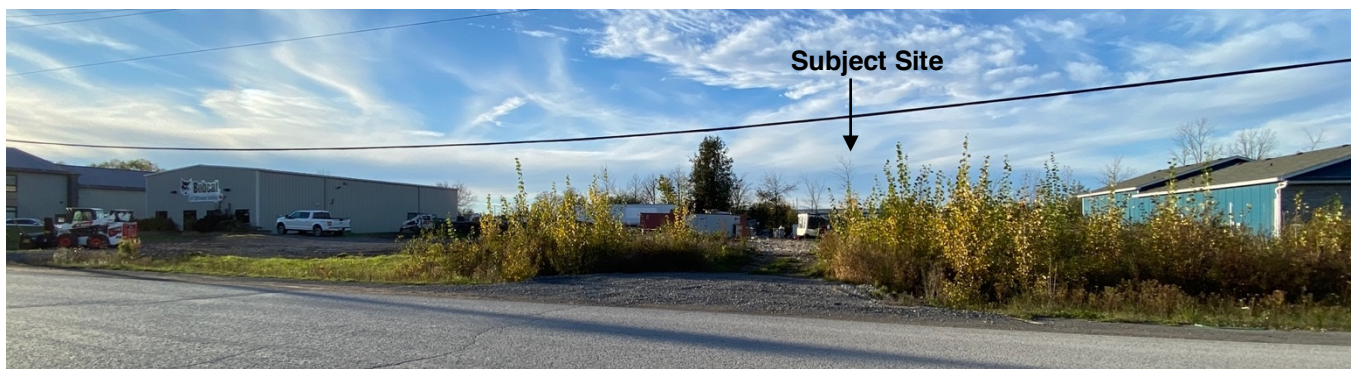


Image 5. Subject site and surrounding context looking northwest from Reis Road



Figure 6. Aerial imagery of subject site and surrounding context (Google Earth)

SECTION 2: PROPOSED DEVELOPMENT

2.1 Proposed Site Plan

The proposed development is a one-storey steel structure light industrial building with associated parking, drive aisle and landscape buffering. The proposed light industrial building is intended for warehouse use and storage with potential area for a mechanical workshop area.

The principal entrance of the building is from the southeast side of the building, with a pathway that connects from the door to the driveway. The site will also be accessed by a gravel driveway along the southwestern side of the property which wraps along the rear of the building. There are four garage doors proposed on the south side of the building, with three barrier free entrances. There is a gravel parking area in the rear with five parking spaces. There are landscape buffers between the proposed development and all property lines, including front, sides and rear.

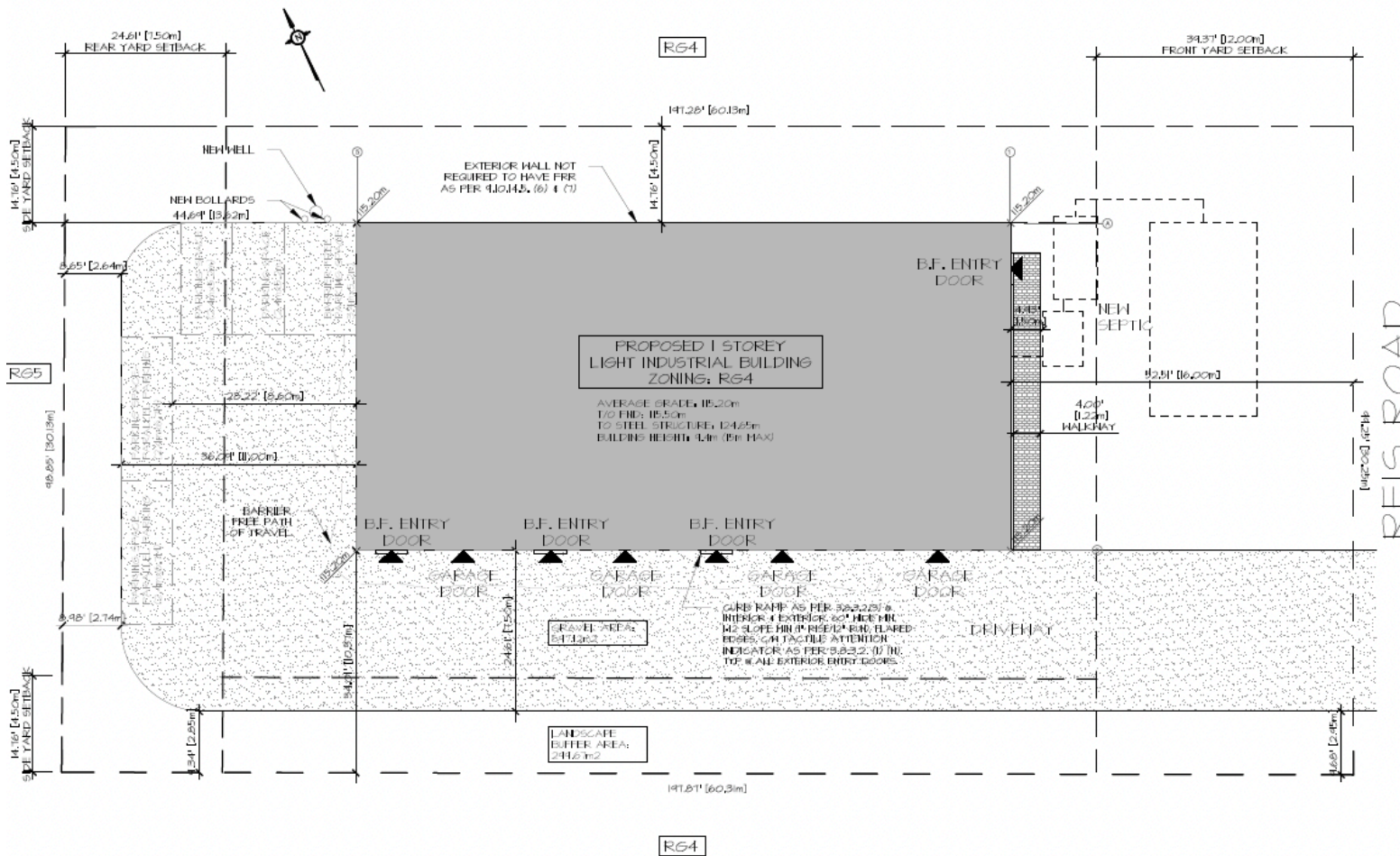
The development will be serviced by private on-site well and septic, with the septic located in the front yard and well located in the rear toward the northeast property line.

The development overview is presented in Table 1 below.

Table 1. Development overview

Lot Area	1818.04 m ²	Building Height	9.40 m
Front Yard Setback	16.00 m	Building Area	466.85 m ²
Rear Yard Setback	13.62 m	Parking Spaces	5 spaces
Rear Yard Area	410.37 m ²	Lot Coverage	25.6%

The proposed site plan and elevations are presented in Figure 7 through Figure 9 below.



ZONING MATRIX			
ZONING:	R64		
	REQUIRED	PROPOSED	
MINIMUM LOT WIDTH	30m	30.25m	
MINIMUM LOT AREA	1800m ²	1818.04m ²	
MAXIMUM BUILDING HEIGHT	5m	4.4m	
MINIMUM SETBACKS	FRONT (EAST)	12m	16.00m
	SIDE 1 (SOUTH)	4.5m	10.31m
	REAR (WEST)	15m	15.62m
	SIDE 2 (NORTH)	4.5m	4.50m
LOT AREA:	1818.04m ²	-	
BUILDING AREA	466.85m ²	-	
MAXIMUM LOT COVERAGE	50%	25.6%	
PARKING SPACES	4	5	
BARRIER FREE PARKING SPACES	1	1	
LANDSCAPED BUFFERS	ABUTTING STREET	3m	N/A
	NOT ABUTTING STREET	NONE	2.85m, 2.64m, 4.50m
	RATIO WITH PARKING LOT	5%	42.1%

SITE PLAN

PROPOSED LIGHT INDUSTRIAL BUILDING

1/8"=1'-0"

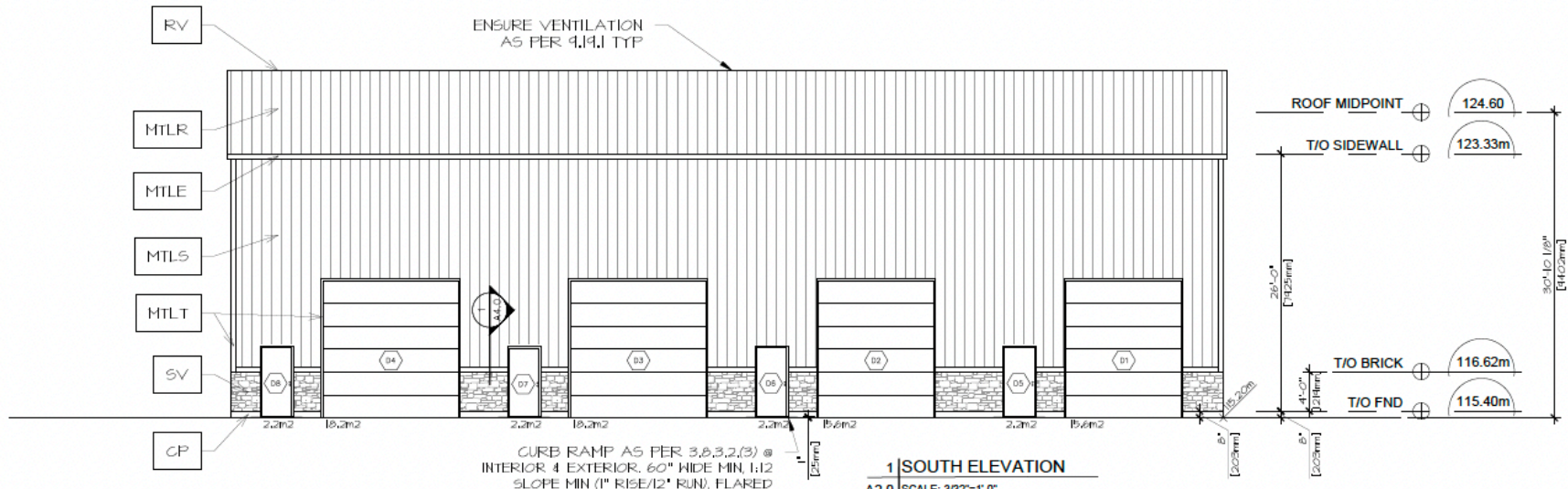
140 REIS ROAD

OCT 2021
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SP1.0

GRANT+HENLEY
DESIGN GROUP

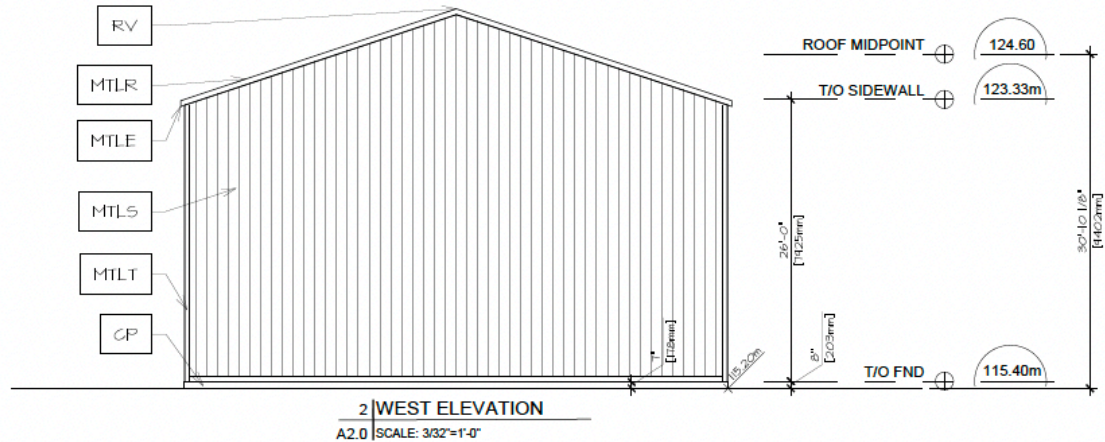
Figure 7. Site Plan prepared by Grant + Henley Design Group



MATERIALS		MTLS	MTLR
RV	RIDGE VENT BY OLYMPIA STEEL BUILDINGS	METAL LINEAR HALL SHEATHING* BY OLYMPIA STEEL BUILDINGS	METAL LINEAR ROOF SHEATHING* BY OLYMPIA STEEL BUILDINGS
MTLE	METAL EAVESSTROUGH TRIM AND DOWNPOUT BY OLYMPIA STEEL BUILDINGS	METAL TRIM BY OLYMPIA STEEL BUILDINGS	
MTLR	METAL LINEAR ROOF SHEATHING* BY OLYMPIA STEEL BUILDINGS	STONE VANEER BY SHOULDICE	CONCRETE PARGING

H				
ELEVATION	LIMITING DISTANCE	EBF	EBO	EBF/EBO RATIO
SOUTH	10.37m	248.1 m ²	76.4 m ²	30.1%
WEST	13.62m	143.7 m ²	0.0 m ²	0%
NORTH	4.50m	248.1 m ²	4.4 m ²	1.1%
EAST	16.00m	143.7 m ²	14.8 m ²	10.2%

REFER TO ENGINEER STAMPED DRAWINGS BY LAND AND BUILDING EXPERTS FOR ALL FOUNDATION & STRUCTURAL RELATED DETAILS. REFER TO DRAWINGS BY OLYMPIA STEEL BUILDING FOR STEEL FRAME CONSTRUCTION, WALL & ROOF DETAILS



ELEVATIONS 1

PROPOSED LIGHT INDUSTRIAL BUILDING

3/32"=1'-0"

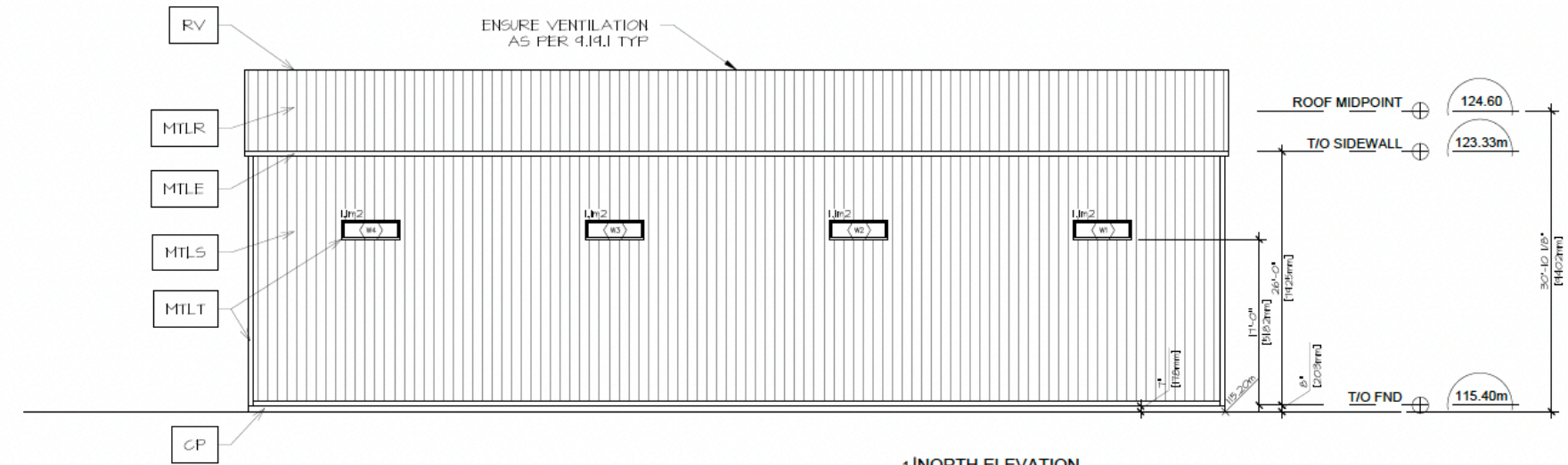
140 REIS ROAD

NOV 2020
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A2.0

GRANT+HENLEY
DESIGN GROUP

Figure 8. Elevations prepared by Grant + Henley Design Group

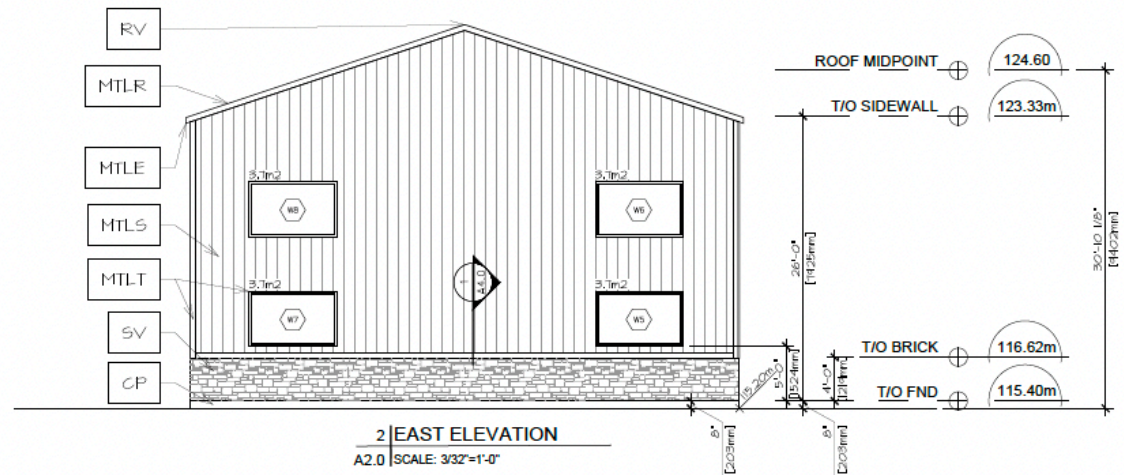


1 | NORTH ELEVATION
A2.0 | SCALE: 3/32"=1'-0"

MATERIALS		MILT	METAL TRIM BY OLYMPIA STEEL BUILDINGS
RV	RIDGE VENT BY OLYMPIA STEEL BUILDINGS	SV	STONE VANEER BY SHOULDICE
MTLR	"METAL LINEAR ROOF SHEATHING" BY OLYMPIA STEEL BUILDINGS	CP	CONCRETE PARKING
MTLS	"METAL LINEAR HALL SHEATHING" BY OLYMPIA STEEL BUILDINGS		

M				
ELEVATION	LIMITING DISTANCE	EBF	EBO	EBF/EBO RATIO
SOUTH	10.31m	248J m2	76.4 m2	30.7%
WEST	13.62m	143.7 m2	0.0 m2	0%
NORTH	4.50m	248J m2	4.4 m2	1.7%
EAST	16.00m	143.7 m2	14.8 m2	10.2%

REFER TO ENGINEER STAMPED DRAWINGS BY LAND AND BUILDING EXPERTS FOR ALL FOUNDATION & STRUCTURAL RELATED DETAILS, REFER TO DRAWINGS BY OLYMPIA STEEL BUILDING FOR STEEL FRAME CONSTRUCTION, WALL & ROOF DETAILS



2 | EAST ELEVATION
A2.0 | SCALE: 3/32"=1'-0"

ELEVATIONS 2

PROPOSED LIGHT INDUSTRIAL BUILDING

3/32"=1'-0"

140 REIS ROAD

NOV 2020
FOR PERMIT

A2.1

GRANT+HENLEY
DESIGN GROUP

Figure 9. Elevations prepared by Grant + Henley Design Group

SECTION 3: POLICY CONTEXT

3.1 Provincial Policy Statement

The Provincial Policy Statement 2020 (PPS) provides policy direction on planning matters for the Province of Ontario. Decisions affecting all planning matters shall be consistent with the Provincial Planning Policies. The proposed low-rise office development is consistent with the applicable policies of the PPS, as demonstrated below.

Section 1.1.1 of the PPS states that healthy, livable, and safe communities are sustained by:

- a) *“promoting efficient development and land use patterns which sustain the financial well-being of the Province and municipalities over the long term;*
- b) *accommodating an appropriate affordable and market-based range and mix of ... employment (including industrial and commercial), institutional... and other uses to meet long-term needs;*
- c) *avoiding development and land use patterns which may cause environmental or public health and safety concerns;*
- d) *avoiding development and land use patterns that would prevent the efficient expansion of settlement areas in those areas which are adjacent or close to settlement areas;*
- e) *promoting the integration of land use planning, growth management, transit-supportive development, intensification and infrastructure planning to achieve cost-effective development patterns, optimization of transit investments, and standards to minimize land consumption and servicing costs;*
- f) *improving accessibility for persons with disabilities and older persons by addressing land use barriers which restrict their full participation in society;*
- g) *ensuring that necessary infrastructure and public service facilities are or will be available to meet current and projected needs”...*

Section 1.1.3.4 of the PPS states that *“appropriate development standards should be promoted which facilitate intensification, redevelopment and compact form, while avoiding or mitigating risks to public health and safety”.*

Section 1.1.3.6 of the PPS states that *“new development taking place in designated growth areas should occur adjacent to the existing built-up area and should have a compact form, mix of uses and densities that allow for the efficient use of land, infrastructure and public service facilities”.*

Section 1.3.1 states that *“planning authorities shall promote economic development and competitiveness by:*

- a) *providing for an appropriate mix and range of employment, institutional, and broader mixed uses to meet long-term needs;*
- b) *providing opportunities for a diversified economic base, including maintaining a range and choice of suitable sites for employment uses which support a wide range of economic activities and ancillary uses, and take into account the needs of existing and future businesses...”*

Section 1.3.2.1 states that *“planning authorities shall plan for, protect and preserve employment areas for current and future uses and ensure that the necessary infrastructure is provided to support current and projected needs.”*

Section 1.3.2.2 states that *“at the time of the official plan review or update, planning authorities should assess employment areas identified in local official plans to ensure that this designation is appropriate to the planned function of the employment area. Employment areas planned for industrial and manufacturing uses shall provide for separation or mitigation from sensitive land uses to maintain the long-term operational and economic viability of the planned uses and function of these areas.”*

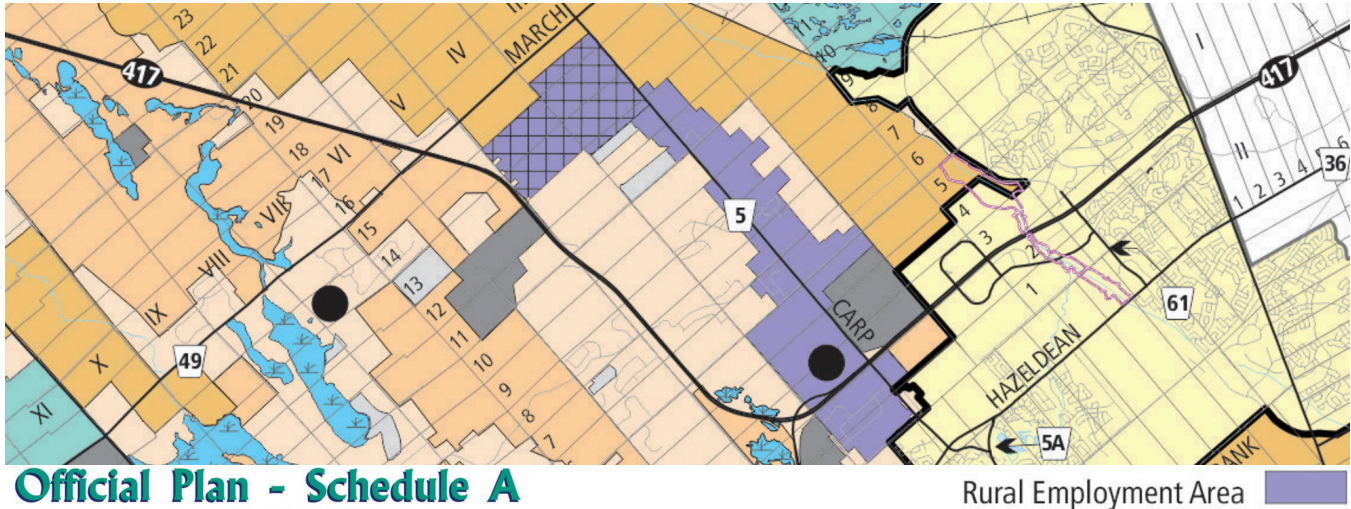
Section 1.3.2.3 states that *“within employment areas planned for industrial or manufacturing uses, planning authorities shall prohibit residential uses and prohibit or limit other sensitive land uses that are not ancillary to the primary employment uses in order to maintain land use compatibility. Employment areas planned for industrial or manufacturing uses should include an appropriate transition to adjacent non-employment areas.”*

In the above PPS policies, there is a significant emphasis on efficient development that is safe, uses available infrastructure, and provides for the needs of the community in both the short- and long-term, and including employment-related uses within designated Employment Areas. The proposed low-rise warehouse and potential partial workshop building will introduce a new permitted use onto the existing vacant lot within the rural area. This site is within an existing and previously established light industrial subdivision which was designed for the proposed type of use. Carp Road presently contains a range and mix of uses including commercial, light industrial, warehouse, outdoor storage, automobile sales and servicing, heavy equipment sales, and others. The proposed use is compatible with the existing and planned function of the immediate area. The site is within the Carp Road Corridor area and designated Rural Employment Area in the existing Official Plan, with the New Official Plan designating it as Rural Industrial and Logistics. Within both existing and future designations, the proposed warehouse use is permitted and appropriate, and further supported by PPS policies by retaining lands intended for employment-related uses that are separated from potentially incompatible uses (i.e. residential). This site and the others within the existing subdivision are serviced by private water and septic servicing in the form of on-site well and septic, which is supported by City policies. Details of the proposed servicing are enclosed within the Site Servicing and Stormwater Management Report as well as the Hydrogeological and Terrain Study prepared by Kollaard Associates Engineers which are submitted alongside this Planning Rationale. The use of municipal roads within the existing light industrial subdivision is also an appropriate and efficient use of resources. The proposed development represents a form development that fits and functions well within the City’s rural area and Rural Employment specific lands.

3.2 Official Plan – 2003

The City of Ottawa’s Official Plan guides development within the City’s urban and rural areas until 2031. The City’s New Official Plan has been drafted and approved by Council in September 2021, however, is presently before the Ministry of Municipal Affairs and Housing for adoption and implementation. As such, the existing Official Plan policies apply, though consideration must be given to the newly approved Official Plan (refer to Section 3.3 below).

The site is designated Rural Employment Area per Schedule A – Rural Policy Plan. The Rural Employment Areas *“are intended to support and encourage clustering of primarily industrial uses not suitable in the Urban Area or General Rural Area. The existing rural employment areas provide for a full range of industry sectors, which includes, but is not limited to, manufacturing, minimum (aggregates), construction, agriculture, transportation as well as other professional and business services and ancillary retail. Rural Employment Areas add to the diversity of opportunities for economic development and those seeking large sites and proximity to the urban area”*.



Official Plan - Schedule A Rural Policy Plan

Rural Employment Area

Figure 10. Extract of Official Plan - Schedule A, Rural Policy Plan

Per Section 3.7.5, Policy 2, uses permitted within rural employment areas include:

- a. New heavy and light industrial uses, such as steel and concrete fabrication, farm equipment and supply centres, machine and vehicle sales service and repair, construction yards, building products yards, landscape contractors, nurseries;
- b. New transportation, warehouse and storage operations; these uses are encouraged to locate on sites in close proximity to Arterial roads and Highway interchanges...

Per Policy 3, “development will be subject to Site Plan Control and particular attention will be given to the physical design of the building and site including signage, buffering, landscaping and fencing. In particular, the City shall require that suitable screening and landscaping is provided for any new external storage areas for goods, material and equipment that abut a highway or arterial road”. Development must be supportable on individual well and septic.

The site falls within the Carp Road Corridor Rural Employment Area, which “plays an important role in the well-being of the local economy... In order to remain successful in the future, this area must maintain the ability to allow for a diverse range of uses while developing an appealing environment for those new industries willing to expand within or relocate to the corridor”. A discussion of the Carp Road Corridor Community Design Plan is outlined in Section 3.2.1 below.

Section 2.5.1 Urban Design and Compatibility sets out criteria for compatible development, which enhances an established community through good design and innovation, and “fits well” within its physical context and “works well” with the existing and planned function of the neighbourhood. Per Section 2.5.1: “[t]he Design Objectives of this Plan are qualitative statements of how the City wants to influence the built environment as the city matures and evolves. These Design Objectives are broadly stated and are to be applied within all land use designations, either at the citywide level or on a site-specific basis. Design Principles further describe how the City hopes to achieve each of the Design Objectives, but may not be achieved or be achievable in all cases; these objectives include:

1. To enhance the sense of community by creating and maintaining places with their own distinct identity;
2. To define quality public and private spaces through development;

3. *To create places that are safe, accessible and are easy to get to, and move through;*
4. *To ensure that new development respects the character of existing areas;*
5. *To consider adaptability and diversity by creating places that can adapt and evolve easily over time and that are characterized by variety and choice. [OMB decision #2649, September 21, 2006];*
6. *To understand and respect natural processes and features in development design;*
7. *To maximize energy-efficiency and promote sustainable design to reduce the resource consumption, energy use, and carbon footprint of the built environment.”*

Section 4.11 Urban Design and Compatibility states that, “[a]t the city-wide scale, issues of compatibility are addressed in the Official Plan through the appropriate designation of land and associated policies that direct where and how certain categories of land use should be permitted to develop. [...] It is recognized that because land use designations such as [...] Employment Area contain broad use permissions, it will be necessary for the zoning by-law to establish more specific permitted use lists and development regulations within areas and on individual sites in a manner that achieves compatibility among proximate uses and built forms.

“At the scale of neighbourhoods or individual properties, issues such as noise, spillover of light, accommodation of parking and access, shadowing, and micro-climatic conditions are prominent considerations when assessing the relationships between new and existing development. Often, to arrive at compatibility of scale and use will demand a careful design response, one that appropriately addresses the impact generated by infill or intensification. Consequently, the issue of ‘context’ is a dominant theme of this Plan where it speaks to compatibility and design.”

The Official Plan emphasizes a need for context appropriate and efficient development within the City overall as well as the Rural Employment Area in specific, which is compatible with the surrounding context and supports employment and industrial type uses within the Rural Area. The proposed development of new, low-rise warehouse building along the Carp Road Corridor Rural Employment Area which features a range and mix of light industrial type uses is appropriate and compatible with the surrounding context. The existing area has a broad range and mix of building types including low-rise light industrial, commercial, service, automobile sales, and other similar types of uses.

The compatibility criteria outlined in Section 2.5.1 and Section 4.11 of the Official Plan are used to evaluate the compatibility of development proposals, based on land uses and built form and their sensitivity to the context of the surrounding neighbourhood. The proposed warehouse use is proposed within an existing light industrial park and is highly compatible within its surroundings. The proposed one-storey design is typical for the immediately surrounding area and allows for added employment and light industrial opportunities within an area designated for this type of use. There are no adjacent residential uses and therefore the proposed development maintains the policy for separation of potentially incompatible uses within the area. The development will be adequately buffered from surrounding properties with landscape buffering, and parking will be concealed at the rear of the building. The proposed development is serviced by private well and septic which is permitted given there is no municipal servicing to the site. The proposed warehouse building conforms to the policies of the City of Ottawa Official Plan, as amended.

3.2.1 Carp Road Corridor Community Design Plan

The site is within the Carp Road Corridor Community Design Plan and is designated “Light Industrial Area” within the Carp Road Corridor Rural Employment Area per Schedule 1. Per Section 2.3 Light Industrial, the intention is to “concentrate industrial development within existing industrial parks, and

include uses such as construction, manufacturing, transportation and warehousing. Permitted uses include light manufacturing plants, distribution, recycling and assembly plants, warehouses, service and repair shops, research, design and testing facilities, storage uses, transportation depts, contractor or construction related uses, compatible public and institutional uses”. Policy 2 indicates that the uses should minimize interference with existing or proposed adjoining uses, including landscape buffering. Development should take place via plan of subdivision in industrial parks (Policy 3). The proposed warehouse and partial workshop use within the existing light industrial subdivision is a permitted and compatible use within the Carp Road Corridor CDP.

3.3 New Official Plan – 2021

The City of Ottawa’s New Official Plan was approved by Council on October 26th, 2021 and is proceeding to the Ministry of Municipal Affairs and Housing for adoption and implementation. Though not yet adopted, the direction of the New Official Plan should be reviewed and considered for development proposals.

Per Schedule B9 – Rural Transect of the new Official Plan, the subject site is designated Rural Industrial and Logistics. The intention of this designation is to permit a range of industrial activity to make best use of rural locations and maintain clusters of industrial uses to reduce incompatibilities with the rural area. Per Section 9.3, the Rural and Industrial Logistics areas permits the following uses:

- (a) *Heavy and light industrial uses, such as value-added processing, fabrication, manufacturing, equipment and supply centres, machine and vehicle sales and servicing, landscape and construction yards, nurseries;*
- (b) *Transportation, distribution, warehouse and large-scale storage operations...*

Development within this designation shall be supported by individual private wells and sewage systems. Further, the site is within the Carp Road Corridor per Volume 2C – Area-Specific Policies and is designated Light Industrial Area per Schedule 8A. Permitted uses within the Light Industrial Area include light manufacturing operations, warehouse, distribution, storage, recycling, assembly, service and repair, research and development, transportation depots, showrooms, public services, and institutional uses. Commercial uses are prohibited within this designation.

The proposed development of a one-storey warehouse building, and potential workshop, is a permitted and compatible use within the Rural Industrial and Logistics designation as well as the Light Industrial Area per the Carp Road Corridor. The site is within an existing light industrial subdivision business park, and the proposed use and site design is compatible with its surroundings as well as the existing and planned function of the neighbourhood. The proposed development meets the policy intent and direction of the New Official Plan.

3.4 Zoning By-law

The site is zoned Rural General Industrial, Subzone 4 – RG4, in the City of Ottawa’s Zoning By-law 2008-250, as demonstrated in Figure 11 below. The RG zone permits a variety of uses including automobile service station, body shop, and dealerships, heavy equipment and vehicle sales, rental and servicing, light industrial uses, parking lot, service and repair shop, storage yard, warehouse, and others. The purpose of the Rural General Industrial zone is to:

- (1) *permit the development of light industrial uses in areas mainly designated as General Rural Area, Village and Carp Road Corridor Rural Employment in the Official Plan;*

- (2) accommodate a range of light industrial uses and limited service commercial uses for the travelling public; and,
- (3) regulate development in a manner that respects adjacent land uses and will have a minimal impact on the surrounding rural area.

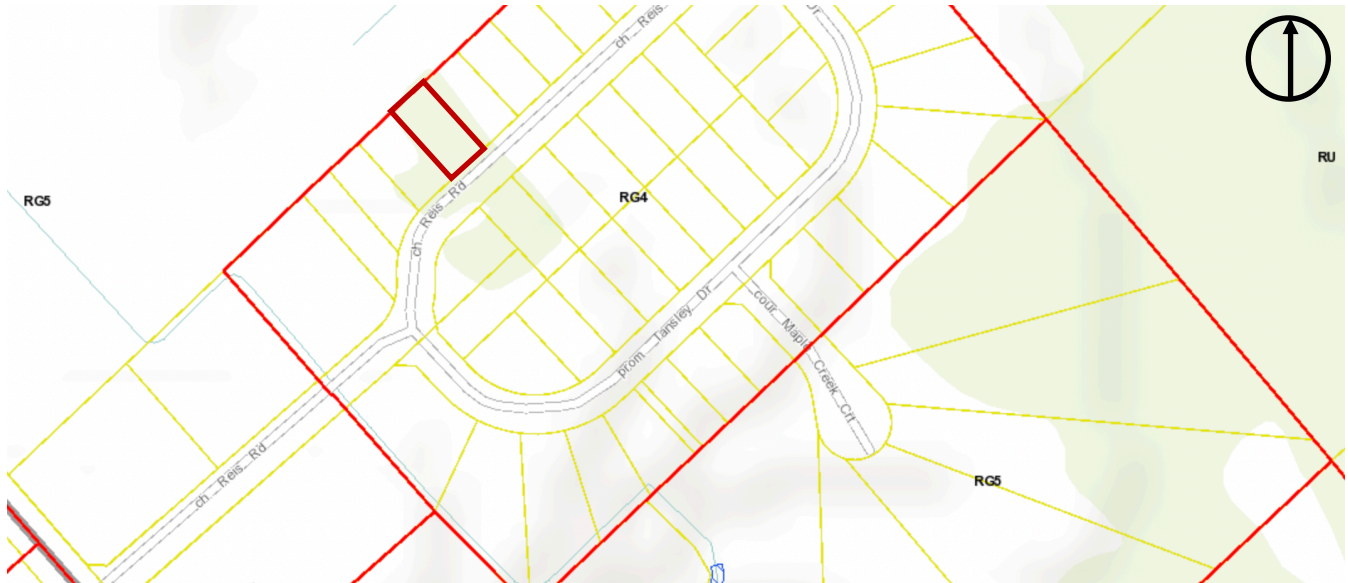


Figure 11. Extract of zoning showing RG4 zoning with subject site shown in red (GeoOttawa, 2019)

The applicable provisions of the RG4 zone are indicated in Table 1 below.

Table 2. RG4 zoning provisions

RG4		
Provisions	Permitted/Required	Provided
(a) Minimum lot width	30 m	30.25 m
(b) Minimum lot area	1800 m ²	1818.04 m ²
(c) Minimum front yard setback (m)	12 m	16 m
(d) Minimum rear yard setback (m)	(i) abutting an RG, RH or RC zone 7.5 m	13.62 m
(e) Minimum interior side yard setback (m)	(i) Abutting an RG, RH or RC zone 4.5 m	4.5 m (n) / 10.31 m (s)
(f) Minimum corner side yard setback (m)	12 m	-
(g) Maximum principal building height (m)	15 m	9.4 m
(h) Maximum lot coverage (%)	50 m	25.6 %
(i) Outdoor storage	(a) outside storage is not permitted within any required front yard or corner side yard (b) outside storage must be screened from abutting residential uses or zones and public streets by an opaque screen at least 1.8 m in height from finished grade	N/A
Parking		
Minimum parking rate (light industrial/warehouse) – 0.8/100 m ² GFA up to 5000 m ²	4 spaces	5 spaces

The RG4 subzone sets out the following additional provisions:

- (a) the automobile service station may not retail fuel;*
- (b) retail store is limited to the sale of agricultural, construction, gardening or landscaping-related products, equipment or supplies; and to the sale of goods, products, equipment, supplies or services manufactured, processed, assembled, packaged or delivered by a permitted use, or the sale of previously prepared materials used in the manufacturing, processing, assembly, packaging or delivery of finished parts, finished products or services.*
- (c) the following uses are prohibited:*
 - gas bar*
 - kennel*
- (d) the following uses are also permitted:*
 - research and development centre*
 - technology industry*
- (e) the following conditional uses are also permitted subject to the use being located on the same lot as a primary permitted use:*
 - office*

The proposed warehouse and partial workshop use are permitted uses in the zoning by-law. The development complies fully with the zoning provisions for the RG zone as well as the additional provisions set out in the RG4 subzone.

SECTION 4: SUPPORTING STUDIES & REPORTS

4.1 Hydrogeological Study and Terrain Analysis

A Hydrogeological Study was prepared by Kollaard Associates dated August 13, 2021 to evaluate the water quality and quantity for the well to supply water for the proposed light industrial development. The report sets out that the water meets all Ontario Drinking Water Standards health and aesthetic parameters, however, is considered hard by water treatment standards. It is indicated that treatment via water softening is effective to reduce scale formation. The report also sets out the required sewage design and indicates that there is sufficient space at the site to accommodate a sewage system, as well as separation between the well. The report concludes that the well should supply adequate water (quantity and quality) for the proposed development with suitable treatment and wellhead protection. Please refer to the enclosed report for additional details and information.

4.2 Site Servicing and Stormwater Management Report

A Site Servicing and Stormwater Management Report was prepared by Kollaard Associates dated August 13, 2021 to address the serviceability of and design for the light industrial building in regards to water and sanitary demands. The report sets out that the development is within the Reis Road Business Park and the allowable runoff rate is governed by design assumptions within the approved Engineering Report for the subdivision. The report is summarized as follows:

- *Stormwater management will be provided in keeping with the approved engineering report for the Reis Business Park;*
- *Quantity control measures are not required;*
- *A normal level of quality control will be achieved by vegetative filtration, followed by through a sand filter;*
- *Discharge will be conveyed to the roadside ditch;*
- *Sanitary sewage will be disposed of in an onsite Class 4 sewage system with a level IV treatment unit;*
- *The site will be serviced by a drilled cased well; and,*
- *Erosion and sedimentation shall be controlled during construction.*

Please refer to the enclosed report for full details, findings and recommendations.

4.3 Geotechnical Investigation

A Geotechnical Investigation was prepared by St. Lawrence Testing & Inspection Co. Ltd. dated March 31, 2021. A subsurface investigation was completed for the property including drilling and sampling. Recommendations for the foundations, slab on grade design and parking areas are set out in the report. Refer to the enclosed letter submitted with this application package for full details.

SECTION 5: CONCLUSION

The subject site is a vacant approximately 0.45-acre rectangular lot within an existing light industrial subdivision development, located on the northwest side of Reis Road, and just northeast of Carp Road. The site is currently undeveloped with an internal gravel driveway accessed from Reis Road roughly in the centre of the property, bordered by mixed natural grasses and shrubbery. The site is located within a light industrial and employment area of Carp Road which contains a broad range of rural industrial and commercial type uses including warehouses, outdoor storage, vehicle and machinery sales and service, among others. The site is located outside of the urban boundary, within the rural area, and is designated Rural Employment Area within the Official Plan. To facilitate development of the site, an Application for Site Plan Control is required.

The proposal is to construct a new one-storey warehouse building and associated parking and landscaping. The proposed warehouse and potential mechanical workshop use are permitted per Official Plan policies (including current and new) as well as site zoning provisions. The site will be privately serviced by on-site well and septic systems, as there is presently no municipal servicing within the existing light industrial subdivision. The site will contain 5 parking spaces, four garage bays, a well and septic, and landscape buffering.

The existing vacant lot has the capacity to support this type of development which introduces an appropriate size and scale of warehouse and workshop use within an existing light industrial subdivision park. The proposed development respects the existing surrounding character which is comprised of a variety of light industrial uses including, but not limited to service, commercial, warehouse, automobile sales and maintenance, and machinery sales. The addition of new warehouse building provides opportunity for employment-related and light industrial services, which is desirable within the City's rural employment areas and within close proximity to Carp Road and Highway 417.

Overall, the proposal is consistent with the Provincial Policy Statement 2020, conforms to the intent of the City of Ottawa's Official Plan, including the newly approved Official Plan (as of fall 2021), and conforms to the City of Ottawa's Zoning By-law 2008-250.

The proposed development represents good land use planning and is recommended for approval.

Respectfully submitted,



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Owner + Senior Planner

