

KEY PLAN  
NOT TO SCALE

**SITE INFORMATION:**

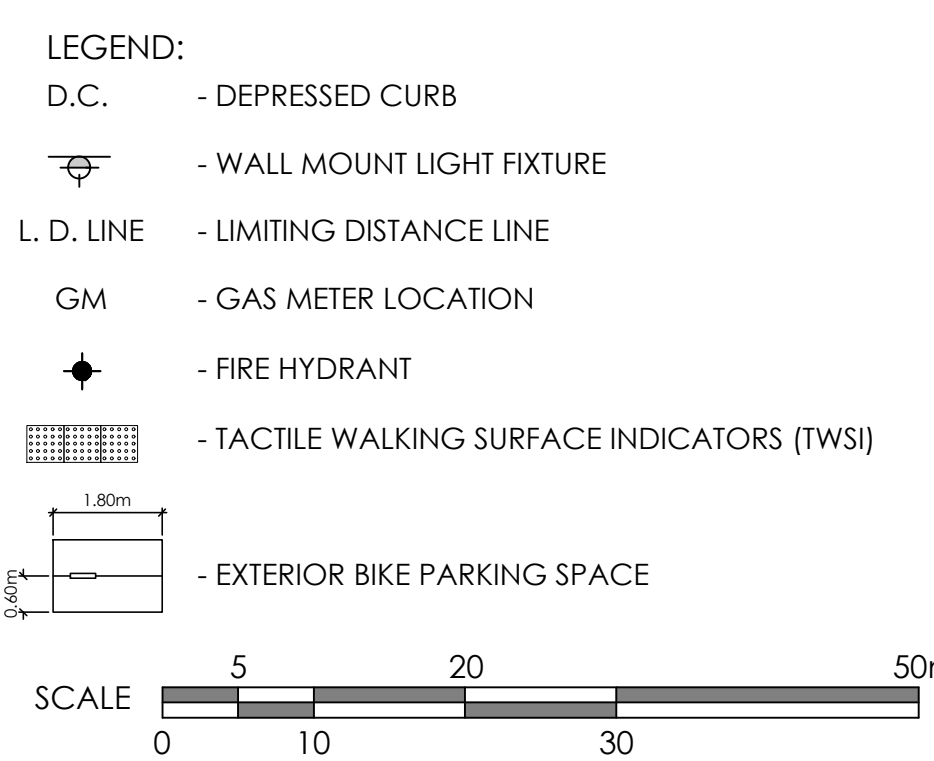
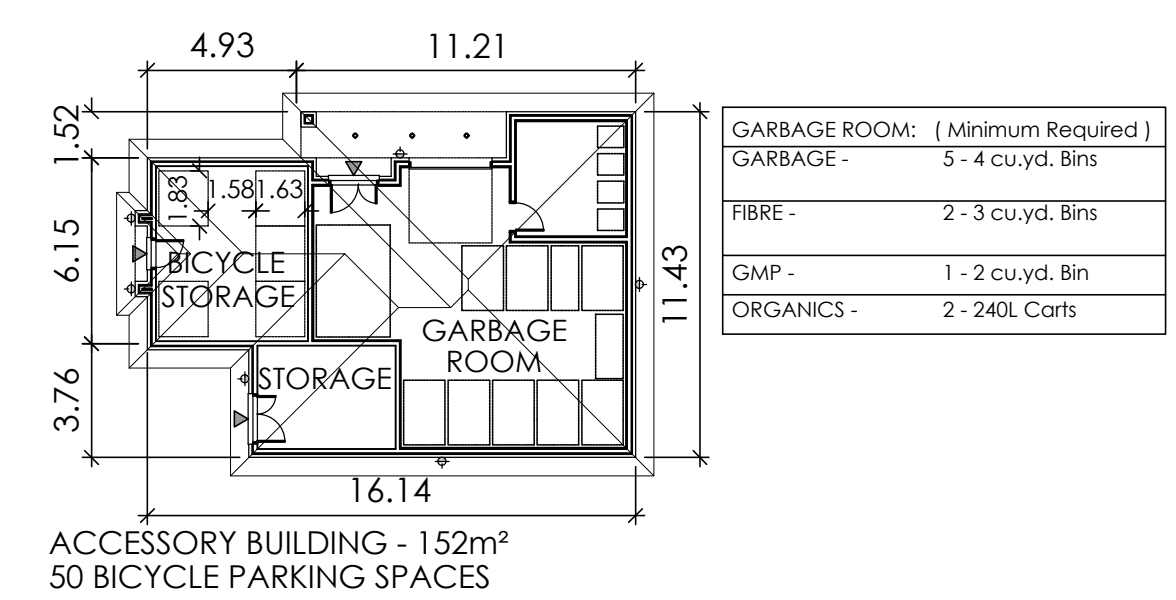
PROPOSED ZONING: R4Z [2704] - PERMITTED USES: - PLANNED UNIT DEVELOPMENT (SECTION 131)  
 - TOWNHOUSE DWELLING 90 UNITS  
 - STACKED DWELLING 94 UNITS

NOTE: FOR THE PURPOSE OF ZONING INTERPRETATION - FRONT LOT LINE ABUTS BRIAN COBURN BLVD.

SITE AREA: 26,051.7 m<sup>2</sup>  
 TOTAL BUILDING AREA: 8,357.3 m<sup>2</sup>

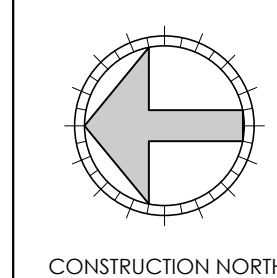
PROPOSED ZONING:	R4Z[2704]	PROVIDED:
LOT AREA (MIN.):	1,400.0 m <sup>2</sup>	26,051.70 m <sup>2</sup> (2.60 ha)
LOT AREA - BACK TO BACK UNIT (MIN.):	84.0 m <sup>2</sup>	92.0 m <sup>2</sup>
LOT WIDTH (MIN.):	18.0 m	BRIAN COBURN BLVD. 131.94 m
FRONT YARD (MIN.):	3.0 m	4.88 m
CORNER SIDE YARD (MIN.):	3.0 m	Block 2 - 5.34 m
		Block 17 - 4.10 m
INTERIOR SIDE YARD (MIN.):	n/a	
REAR YARD (MIN.):	n/a	
COULOIR ROAD (MIN.):	3.0 m	4.14 m
BUILDING SPACING:		
BETWEEN BUILDING & PRIVATE WAY	1.8 m	2.57 m
BETWEEN GARAGE & PRIVATE WAY	5.2 m	5.94 m
BETWEEN CARPORT/GARAGE & SIDEWALK	6.2 m	6.20 m
BETWEEN BUILDINGS	1.2 m	3.80 m
MINIMUM LANDSCAPED AREA:	30.0%	45.4% (11,833.4m <sup>2</sup> )
BUILDING HEIGHT (TOWNHOUSE) (MAX.):	11.0 m	10.6 m
BUILDING HEIGHT (STACKED UNITS) (MAX.):	15.0 m	9.5 m
PORCH STAIR TO LOT LINE (SECTION 65)	0.60 m	1.14 m
MINIMUM LANDSCAPING for PARKING LOTS (SECTION 110):	15.0% of PARKING LOT AREA:	PARKING LOT AREA: 4,058.0m <sup>2</sup> LANDSCAPED AREA: 1,857.0m <sup>2</sup> 45.7%
TOTAL AMENITY AREA REQUIRED:		
- STACKED DWELLING 6.0m <sup>2</sup> x 96 = 576.0m <sup>2</sup>		- PRIVATE AMENITY AREA - (BALCONIES & PATIOS) 6.5m <sup>2</sup> x 96 = 624.0m <sup>2</sup>
		- COMMUNAL AMENITY AREA - 1,130.0m <sup>2</sup>
COMMUNAL AMENITY AREA REQ'D. (MIN.):	50% of 576 m <sup>2</sup> = 288.0m <sup>2</sup>	TOTAL AMENITY AREA PROVIDED: 1,754.0m <sup>2</sup>
ACCESSORY BUILDING BUILDING HEIGHT (MAX.):	SECTION 131 4.5 m	PROVIDED: 4.47 m
FLOOR AREA (MAX.):	200.0 m <sup>2</sup>	152.00 m <sup>2</sup>
BACK TO BACK PARKING:		
PARKING PROVIDED:	2 Spaces / UNIT - 1 Driveway / 1 Garage	
TERRACE FLATS PARKING:		
PARKING REQUIRED:	1.2 Spaces / (96) d.u. + 0.2 / (96) d.u. (Visitor) = 115.2 + 19.2 = 134.4 Spaces	
PARKING PROVIDED:	116 Spaces + 19 Visitor Spaces = 135 Spaces	
BICYCLE PARKING REQUIRED:	96 (0.5 / (96) d.u.) = 48.0 Spaces	
BICYCLE PARKING PROVIDED:	50 Interior Spaces + 8 Exterior Spaces = 58 Spaces	
STREET BACK TO BACK TOWNHOMES:		
BLOCK No.:	BUILDING AREA: GROSS FLOOR AREA: No. UNITS:	
BLOCK 1 = BACK TO BACK TOWNHOMES	544.3 m <sup>2</sup> 1,281.1 m <sup>2</sup> 10 UNITS	
BLOCK 2 = BACK TO BACK TOWNHOMES	436.0 m <sup>2</sup> 1,026.6 m <sup>2</sup> 8 UNITS	
BLOCK 3 = BACK TO BACK TOWNHOMES	436.0 m <sup>2</sup> 1,026.6 m <sup>2</sup> 8 UNITS	
BLOCK 4 = BACK TO BACK TOWNHOMES	436.0 m <sup>2</sup> 1,026.6 m <sup>2</sup> 8 UNITS	
BLOCK 5 = BACK TO BACK TOWNHOMES	436.0 m <sup>2</sup> 1,026.6 m <sup>2</sup> 8 UNITS	
BLOCK 6 = BACK TO BACK TOWNHOMES	436.0 m <sup>2</sup> 1,026.6 m <sup>2</sup> 8 UNITS	
BLOCK 7 = BACK TO BACK TOWNHOMES	438.5 m <sup>2</sup> 1,026.6 m <sup>2</sup> 8 UNITS	
BLOCK 8 = BACK TO BACK TOWNHOMES	436.0 m <sup>2</sup> 1,026.6 m <sup>2</sup> 8 UNITS	
BLOCK 9 = BACK TO BACK TOWNHOMES	436.0 m <sup>2</sup> 1,026.6 m <sup>2</sup> 8 UNITS	
BLOCK 10 = BACK TO BACK TOWNHOMES	436.0 m <sup>2</sup> 1,026.6 m <sup>2</sup> 8 UNITS	
BLOCK 11 = BACK TO BACK TOWNHOMES	438.5 m <sup>2</sup> 1,026.6 m <sup>2</sup> 8 UNITS	
TOTAL =	4,909.3 m <sup>2</sup> 11,547.1 m <sup>2</sup> 90 UNITS	
TERRACE FLATS:		
BLOCK No.:	BUILDING AREA: GROSS FLOOR AREA: No. UNITS:	
BLOCK 12 = TERRACE FLATS	412.0 m <sup>2</sup> 1,219.0 m <sup>2</sup> 12 UNITS	
BLOCK 13 = TERRACE FLATS	412.0 m <sup>2</sup> 1,219.0 m <sup>2</sup> 12 UNITS	
BLOCK 14 = TERRACE FLATS	412.0 m <sup>2</sup> 1,219.0 m <sup>2</sup> 12 UNITS	
BLOCK 15 = TERRACE FLATS	412.0 m <sup>2</sup> 1,219.0 m <sup>2</sup> 12 UNITS	
BLOCK 16 = TERRACE FLATS	412.0 m <sup>2</sup> 1,219.0 m <sup>2</sup> 12 UNITS	
BLOCK 17 = TERRACE FLATS	412.0 m <sup>2</sup> 1,219.0 m <sup>2</sup> 12 UNITS	
BLOCK 18 = TERRACE FLATS	412.0 m <sup>2</sup> 1,219.0 m <sup>2</sup> 12 UNITS	
BLOCK 19 = TERRACE FLATS	412.0 m <sup>2</sup> 1,219.0 m <sup>2</sup> 12 UNITS	
BICYCLE / GARBAGE =	152.0 m <sup>2</sup> 152.0 m <sup>2</sup>	
TOTAL =	3,448.0 m <sup>2</sup> 9,904.0 m <sup>2</sup> 96 UNITS	
SNOW STORAGE:	SNOW STORAGE WILL BE OFF SITE.	

NOTE:  
 - SITE PLAN TO BE READ IN CONJUNCTION WITH:  
 - SITE SERVICING PLAN PREPARED BY STANTEC.  
 - LANDSCAPING PLAN PREPARED BY NAK DESIGN STRATEGIES.  
 - BOUNDARIES DERIVED FROM: PLAN OF SURVEY OF PART OF LOTS 2 AND 3 CONVESSION 3 (OTTAWA FRONT). PLAN PREPARED BY STANTEC NOV. 11, 2020.



**GENERAL NOTES:**

- THE CONTRACTOR IS RESPONSIBLE FOR CHECKING AND VERIFYING ALL DIMENSIONS. ANY DISCREPANCY MUST BE REPORTED TO M. DAVID BLAKELY ARCHITECT INC.
- ALL WORK AND MATERIALS TO BE IN COMPLIANCE WITH ALL CODES, REGULATIONS, AND BY-LAWS.
- ADDITIONAL DRAWINGS MAY BE ISSUED FOR CLARIFICATION TO ASSIST THE PROPER EXECUTION OF WORK. SUCH DRAWINGS WILL HAVE THE SAME MEANING AND INTENT AS IF THEY WERE INCLUDED WITH THE PLANS IN CONTRACT DOCUMENTS.
- DO NOT SCALE DRAWINGS.
- THIS DRAWING SHALL NOT BE USED FOR PERMIT OR CONSTRUCTION UNLESS THE DRAWING BEARS THE ARCHITECT'S SEAL AND SIGNATURE.
- THIS REPRODUCTION SHALL NOT BE ALTERED.



NO.	DATE	DESCRIPTION	INIT.	NO.	DATE	DESCRIPTION	INIT.	NO.	DATE	DESCRIPTION	INIT.
10.	27/08/20	REVISED B/B TOWN LAYOUT	SM	20.	14/05/21	GATEWAY COLUMNS ADDED	SM	30.			
9.	25/08/20	REVISED BLOCK SEPARATIONS	SM	19.	14/04/21	GENERAL REVISIONS	SM	29.			
8.	11/08/20	REVISED BLOCK LAYOUT	SM	18.	08/04/21	MOVED BLOCKS 8-11 EAST 0.13m	SM	28.			
7.	16/07/20	ADDED ACCESS PATHWAYS	SM	17.	07/04/21	REV. AMENITY AREA/GENERAL	SM	27.			
6.	30/06/20	FOR REVIEW	SM	16.	26/03/21	AS PER CITY COMMENTS	SM	26.			
5.	26/03/20	REMOVED TRANSFORMER LOCATIONS	SM	15.	10/12/20	REV. SIDEWALK @ COULOIR ROAD	SM	25.			
4.	06/03/20	FOR REVIEW	SM	14.	02/12/20	REV. BLKS. 18&19 PATHWAY	SM	24.	14/09/21	TRANSFORMER LOCATIONS ADDED	SM
3.	26/02/20	FOR REVIEW	SM	13.	20/11/20	GENERAL REVISIONS	SM	23.	11/08/21	RELOCATED BLOCKS 18 & 19	SM
2.	26/09/19	FOR REVIEW	SM	12.	05/11/20	REV. BLKS. 1-7 & ACC. BLDG.	SM	22.	12/07/21	FUTURE GATEWAY FEATURE LOCATION	SM
1.	19/09/19	FOR REVIEW	SM	11.	03/09/20	REVISED B/B TOWN LAYOUT	SM	21.	28/06/21	AS PER CITY COMMENTS	SM

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PROJECT: **186 UNIT TERRACE FLATS & B/B TOWNHOMES TRAILSEDGE BLOCK 193 & 194 OTTAWA, ONT.**

DRAWING TITLE: **SITE PLAN**

CLIENT: **RICH CRAFT Group Of Companies**

DATE: **SEPT., 2019.** SCALE: **1:500** SHEET NO.: **SP-1**

DRAWN BY: **SBM** CHECKED: **MDB**

D07-12-20-0184