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May 19, 2021

Ms. Kimberley Baldwin, MCIP RPP
Planner II
Planning, Infrastructure & Economic Development Department
City of Ottawa
110 Laurier Avenue West
Ottawa, ON
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Dear Ms. Baldwin:

**715 MIKINAK ROAD, WARD 13 - RIDEAU-ROCKCLIFFE
SITE PLAN APPLICATION
PLANNING RATIONALE**

Introduction

IBI Group has been retained to file a Site Plan Application on behalf of Ottawa Community Housing Corporation (OCH) for its property municipally known as 715 Mikinak Road in the City of Ottawa. This Planning Report is provided in support of the Site Plan Application and will provide a description of the site location and context, as well as the proposed development. Additionally, it provides a concluding planning opinion, having reviewed and analyzed the proposal with regard to the applicable planning policy framework. A Design Brief is provided separately, which addresses the applicable Official Plan Design Criteria, Community Design Plan Guidelines, response to initial Urban Design Review Panel comments and a Shadow Study.

Pre-Application Consultation

A pre-application consultation meeting was attended July 15th, 2020 by OCH staff, the consultant team and staff from various City departments. A list of Required Plans and Studies was provided, as well as feedback on the general proposal including design elements. A new Site Plan Control application, Manager Approval, subject to Public Consultation, under the 'complex' category is required; however, it is noted that the proponent is a not-for-profit housing operator and is exempt from the planning portion of application fees. The City of Ottawa Housing Services Department will cover the Engineering Design Review and Inspection Fee, as well as the Conservation Authority Fee.

An initial pre-consultation meeting with the Urban Design Review Panel was held on October 2nd, 2020 and comments from the Panel were received. Considerable efforts have been made to not only address the Panel's comments, but also encompass design expectations through the proposed scheme and layout of 715 Mikinak Road.

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Site Location and Context

A Plan of Subdivision application and associated rezoning was filed in 2015 for the redevelopment of the 131-hectare CDP parcel, which was named Wateridge Village. The subject property is legally described as Block 21 on registered Plan of Subdivision 4M-1581, within Phase 1B of Wateridge Village. Block 21, municipally known as 715 Mikinak Road, is bound by Hemlock Road to the north, Bareille-Snow Street to the west, Mikinak Road to the south, and Michael Stoqua Street to the east. The property has an area of approximately 12,185 square metres (1.2 hectares) and frontage of approximately 82 metres on Hemlock Road and Mikinak Road, and approximately 123 metres on Bareille-Snow Street and Michael Stoqua Street. The subject property is currently undeveloped.

The subject property is located on former Canadian Forces Base (CFB) Rockcliffe lands in the City of Ottawa. The former CFB Rockcliffe lands are located south of the Ottawa River and east of Ottawa's downtown core. The 131-hectare parcel of land is bound by Aviation Parkway to the west, the Sir George-Etienne Cartier Parkway to the north, Blair Road to the east and Montreal Road to the south. Adjacent land marks include the National Research Council to the east, the Montfort Hospital and the Montfort Woods environmentally sensitive lands to the southwest, the NCC Future National Cultural Institutional Lands to the northwest, Rockcliffe Airport and the Canada Aviation and Space Museum to the north.

Proposed Development

The proposed development consists of two seven-storey buildings (Buildings A & B) fronting onto Hemlock Road and Bareille- Snow Street, as well as a four-storey building (Building C) fronting onto Mikinak Road. The proposed use is primarily residential, with additional community-focussed non-residential space allocated on the ground floor of each building. The triad of buildings are arranged to provide an active street frontage on Hemlock Road and Mikinak Road. Building A is "L-shaped" and steps down to three-storeys on the Bareille-Snow Street frontage, whereas Building B is a block of seven-storeys and steps down to six-storeys as it approaches Mikinak Road and Building C. Buildings A and B both have a 2-meter step back at the fourth storey facing the street to provide building articulation and a visual base.

There are multiple outdoor amenity areas are provided to meet various functional needs of site users. The foundation of the landscape scheme is to create a space for healthy people, buildings, and ecology to exist together, creating a healthy community. The outdoor spaces include a community garden, BBQ area, junior playground, various courts, seating area, and ample planting at the base of the building facades. The parking lot medians are proposed to be bioswales and include plants and trees.

The project will meet the standards and performance criteria of LEED, WELL, Passive House PHIUS+ and Net Zero Carbon, without seeking formal certification. Recently, the Client has requested to determine the implications of certifying the project to CAGBC Net Zero Carbon. The analysis is in progress.

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Access and Circulation

Site-generated traffic will access the proposed development via two, two-way private approaches on Michael Stoqua Street. The internal drive aisle on site will be 6.7 metres wide.

Two waste collection/ recycling areas are provided on site. Each area has been designed to accommodate a front-loading Garbage Truck, and both are located along the western edge of the surface parking lot, well separated from the buildings' primary entrances. A swept path analysis was undertaken during the site plan development stage and confirmed that the curb radii at each of the site access driveways could be reduced to 5m, per the City's Traffic-Calming Guidelines.

The site is bound by two collector roads: Hemlock Road to the north, and Mikinak Road to the south. The site is also bound by two local roads: Barielle Snow Street to the west and Michael Stoqua Street to the east. Access to the regional road network is provided via existing signalized intersections on Montreal Road at Codd's Road and at Wanaki Road, as well as the Aviation Parkway to the west via Hemlock Road.

The Secondary Plan emphasizes a high-quality public realm that ensures pedestrian and cycling friendly streets by providing design directions for parking and driveways. The proposed site layout meets these objectives by providing a surface parking area within the interior of the block, separated on three sides from the public right-of-way, with the entrance on the side street. The proposed access points do not interrupt the active building frontages on Hemlock and Mikinak Road, as identified on Schedule E – Building Frontages.

Pedestrian & Cyclist Network

The Secondary Plan includes policies to achieve a mobility network that is organized around a regular grid of blocks that facilitates convenient pedestrian and cyclist connectivity within the area and to surrounding neighbourhoods. The mobility network of cycle tracks on Hemlock Rd and the multi-use pathway on Mikinak Rd are identified on Schedule D.

Presently, Montreal Road accommodates cyclists via on road, curbside bike lanes in both directions. In the broader area, multi-use paths exist west of the Aviation Parkway and north of the Sir-George-Etienne-Cartier Parkway. The Community Design Plan (CDP) indicates that proposed facilities on Mikinak Road will include a multi-use path on the south side, while Hemlock Road will provide grade-separated cycle tracks on both sides. All boundary streets will provide concrete sidewalks for pedestrians on both sides of the road.

Parking

A total of 122 parking spaces are proposed on the subject property: 96 residential spaces and 26 visitor spaces. A total of 6 barrier-free parking spaces are provided.

It is noted that the proposed number of parking spaces on the subject property do not meet the minimum residential and visitor parking requirements. A Minor Variance Application is anticipated and discussed in the section below.

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A total of 210 bicycle parking spaces are proposed on site, including 206 residential spaces (116 indoor and 90 outdoor spaces), as well as 4 outdoor commercial spaces. The proposed development therefore provides bicycle parking well in excess of the 137 spaces required in the Zoning By-law.

Applicant's Public Consultation Strategy

The subject Site Plan Application is subject to public consultation, as it includes a new residential building with more than fourteen units and is taller than 5 storeys. Public consultation relating to the subject Site Plan Control application will follow the City of Ottawa public consultation process and practices, including placing a large sign summarizing the proposal on the property to notify public that an application has been received. Additionally, registered community organizations who are enrolled with the city are provided notice of the application. The City also posts detailed information about all site plan proposals, including a copy of the Site Plan, on the City's Development Applications website. Contact information for the Planning File Lead is provided, and any person who would like to provide comments can do so through this link: <https://devapps.ottawa.ca/en/>

Additionally, a virtual community meeting in collaboration with the Ward Councillor Rawlson King and the Wateridge Village Community Association is intended to take place in the coming weeks, once a meeting date has been confirmed by the Councillor's office. An information meeting is also intended to be held with the Algonquin people to discuss the proposed development and design approach for the site.

Policy and Regulatory Context

City of Ottawa Official Plan

The subject lands are designated General Urban Area in the City of Ottawa Official Plan. The General Urban Area contemplates the development of a full range and choice of housing types, in combination with employment, retail, service, cultural, leisure, entertainment and institutional uses to facilitate the development of complete and sustainable communities. Throughout the General Urban Area, the City encourages small, locally-oriented convenience and services uses that complement the adjacent residential land uses, and which are of a size and scale consistent with the needs of nearby residential uses.

Under the policies for Building Liveable Communities, the Official Plan speaks to affordable and appropriate housing for all residents being the building block of a healthy and liveable community. The Official Plan policies contribute to improving the supply of affordable housing in concert with other City initiatives to support the construction of affordable units. Specifically, the City will investigate means to increase the supply of affordable housing by engaging with the private sector, non-profit and cooperative housing providers in meeting the affordable housing target for low-income and moderate-income households, within the context of the City's Housing Strategy.

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Former CFB Community Design Plan

A Community Design Plan (CDP) was completed in 2015 for the former CFB Rockcliffe lands. The CDP identified an opportunity for approximately 9,800 residents and 2,600 jobs at build-out within 15-20 years. The CDP is a guiding document but the Secondary Plan and implemented zoning provide the detailed direction for future development.

Within the CDP, the subject lands are identified as the “Core” neighbourhood, which is a 9-block mid-rise mixed-use neighbourhood at the heart of the community. The Core neighbourhood is close to all major parks and is a place where residents and those people who work in the neighbourhood can gather and socialize. The South Community park is adjacent to the subject lands, on the south side of Mikinak Road and is 4.95 hectares.

The overarching development goal for the Core neighbourhood is to have a compact, pedestrian-oriented community with a variety of residential and employment uses. Building height is limited to 30 metres, and active street frontage on Hemlock and Mikinak Roads is emphasized. The minimum density target established for this area is 143 units per hectare. The subject property is approximately 1.2 hectare in size and proposed a total of 271 units. Permitted uses within mid-rise and high-rise mixed-use areas include residential uses such as apartments.

The CDP also speaks to affordable housing, noting that it will be dispersed throughout the community, will be mixed with other housing types and may include purpose-built rental housing, supportive housing, and not-for-profit subsidized housing. The City of Ottawa and Canada Lands Company have pursued partnerships with private, public, and non-governmental sectors to accommodate affordable housing projects in the new community. In addition to participation in multi-party partnerships to secure the resources necessary to sustain a range of affordable housing types, the City of Ottawa and Canada Lands Company will facilitate affordable housing by allowing alternative development standards on a case-by-case basis, such as reduced parking requirements.

The parking policies in the CDP indicate that, where practical, the reduction of minimum and maximum parking requirements will be encouraged. Furthermore, limited amounts of surface parking may be accommodated within the Core, either in a small number of surface parking spaces in the interior of blocks or associated with short stay parking for businesses and residential visitors, which demonstrate high urban design standards for screening, interior and exterior landscaping, and utilize permeable pavements for groundwater recharge.

The CDP includes policies for achieving sustainable development, which is consistent with the City’s goal of promoting environmental, economic and social sustainability. It is intended that affordable housing will incorporate innovative and flexible design to accommodate a wide range of residents and needs. Such design will provide for adaptive use and re-use over the long term.

Former Canadian Forces Base (CFB) Rockcliffe Secondary Plan

The purpose of the former CFB Rockcliffe Secondary Plan (the Secondary Plan) is to guide future growth and development on the former CFB Rockcliffe lands, by providing policy direction on land use, densities, building heights, open space, and mobility. The Secondary Plan is based on the CDP and translates key aspects of the CDP into statutory policy. The Secondary Plan includes Schedules A through F, that are read in conjunction with the policy direction.

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CFB Rockcliffe Secondary Plan – Schedules	715 Mikinak Road (Block 33 on Schedules)
Schedule A – Land Use	Mid-Rise Mixed-Use
Schedule B – Building Heights	30 meter maximum
Schedule C – Minimum Residential Density	143 units per net hectare minimum density
Schedule D – Mobility Network	Cycle tracks – Hemlock Rd; Multi-use Pathway – Mikinak Rd
Schedule E – Building Frontages	Building frontages – Hemlock Rd & Mikinak Rd
Schedule F – Public Rights-of-Way	Hemlock Road – 24 m ROW Mikinak Road – 26 m ROW

Planning Framework Summary

The policies of the Secondary Plan, along with the CDP and Official Plan policies, provide the framework to guide future development in the area. Implementation and interpretation are in accordance with the policies of the Official Plan for the City of Ottawa.

As per the Secondary Plan, a Plan of Subdivision for the entire Former CFB Rockcliffe CDP area was required to establish the public streets, blocks, areas for stormwater management infrastructure, public parkland and open space which are generally in accordance with the configuration depicted in the Secondary Plan Schedules. Public streets, parks and open spaces have been dedicated to the City as a condition of the subdivision approval.

Establishing a high level of urban design is critical in making new development enjoyable places to live, work and socialize. To ensure urban design is not overlooked as communities develop and change over time, the City has identified design priority areas. In design priority areas, all private and public developments, and community partnerships within and adjacent to the public realm will be reviewed for their contribution to an enhanced pedestrian environment and their response to the distinct character and unique opportunities of the area. Although the subject lands are not located within a defined design priority area, the Secondary Plan includes policies that require Site Plan Control applications for lands located within Mixed-Use designations on Schedule A – Land Use in the Secondary Plan, to be subject to review by the City's Urban Design Review Panel. This requirement will be addressed through the subject Site Plan Control application for the subject lands.

Comprehensive Zoning By-law 2008-250

The subject property is zoned General Mixed-Use, Subzone 31 – GM31 H(30) in the City of Ottawa Comprehensive Zoning By-law 2008-250. The purpose of the GM – General Mixed-Use Zone is to:

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1. Allow residential, commercial and institutional uses, or mixed-use development in the General Urban Area and in the Upper Town, Lowertown and Sandy Hill West Character Areas of the Central Area designations of the Official Plan;
2. Limit commercial uses to individual occupancies or in groupings in well defined areas such that they do not affect the development of the designated Traditional and Arterial Mainstreets as viable mixed-use areas;
3. Permit uses that are often large and serve or draw from broader areas than the surrounding community and which may generate traffic, noise or other impacts provided the anticipated impacts are adequately mitigated or otherwise addressed; and
4. Impose development standards that will ensure that the uses are compatible and complement surrounding land uses.

In the GM zone, there are many permitted non-residential uses that would meet the policy goals for a pedestrian-oriented, compact, mixed-use development, including but not limited to, convenience store, day care, office, personal service business, post office, restaurant, retail food store, retail store and a service and repair shop.

There are several permitted residential uses including apartment dwelling low-rise and mid-rise, which includes building up to 9 storeys in height. As per the H(30) height suffix, which adds a single regulation to the zone for height, the maximum building height is restricted to less than 30 metres, and therefore, an Apartment High rise, which is defined as 10 or more storeys, is a prohibited use.

The applicable GM31 H(30) zoning permits mixed-use buildings with reduced setbacks to define the street edge and to provide animation at the ground floor with active retail uses, glazing and entrances to the street. The permitted building envelope includes a maximum height of 30 metres with additional requirements for step backs at certain floor levels, and a maximum floor plate at and above the 7th storey, for residential uses.

Parking is provided in the interior of the property, with limited driveway access points on Michael Stoqua Street to limit disruption of pedestrian and cycle paths along Hemlock and Mikinak Road frontages. It is understood that underground parking is not feasible due to a high-water table, and it is cost prohibitive. Therefore, it is anticipated that a reduced number of surface parking spaces will be provided. The zoning does not require parking for non-residential uses of less than 200 m² GFA.

Minor Variance Application

The subject property is located within Area X on Schedule 1A: Areas for Minimum Parking Requirements, which requires 0.5 spaces per unit above 12 units, per building, and an additional 0.1 spaces per unit above 12 units, per lot, for visitor parking. A total of 144 parking spaces are required for the proposed 271 apartment units.

It is anticipated that additional variances will be required to address the proposed driveway widths of 6.7 metres and 7.6 metres, respectively, to provide safe ingress/egress for vehicles and garbage trucks, whereas the Zoning provided for a maximum of 6.0 metres for two-way driveways.

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Additionally, a 2-metre step back is not provided on the corner side of Building A facing Barrielle-Snow Street at and above the fourth storey, as required by Zoning.

715 Mikinak Road GM31 H(30)		Required Parking		Provided Parking	
Apartment Dwelling, Mid-rise	Building A – 115 units	Residential	Visitor	Residential	Visitor
	Building B – 114 units	0.5/unit above 12 units per building	0.2/unit above 12 units per lot	0.35/unit above 12 units per building	0.1/unit above 12 units per lot
	Building C – 42 units				
	Total units: 271	118	26	96	26
Total parking:		144		122	

It is anticipated that the Minor Variance Application will be filed following receipt of the Site Plan review comments from the initial submission.

Urban Design Review Panel

The subject property is located in a Design Priority Area and is therefore subject to evaluation by the Urban Design Review Panel. A pre-consultation meeting was held with the Panel on October 2, 2020. Comments were received, and a number of revisions were made to the site plan in an effort to address the Panel's central observations and comments.

We will submit for the Formal Urban Design Review Meeting in advance of the June 24, 2021 submission deadline for the July 9, 2021 meeting date and look forward to presenting the revised proposal to the Panel.

Supporting Studies

A number of studies and plans were identified as required materials for a complete Site Plan application submission through the Pre-Consultation Meeting process. A summary is provided below, and full copies of each report are included with the Site Plan submission. Please reference each report directly for further details.

Stormwater Management and Servicing Study

A Design Brief has been prepared by IBI Group to present a detailed servicing scheme to support the proposed development of the subject property, including a recommended servicing plan for the major municipal infrastructure. This report will also include sections on water supply, wastewater disposal, stormwater management, as well as erosion and sediment control. The proposed design philosophy and criteria are in accordance with the policies set out by the City of Ottawa.

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Noise Study

IBI Group was retained to prepare an environmental Noise Study in support of the proposed development to determine the impacts of traffic and stationary noise for the proposed residential development. The Study concludes that based on the results and recommendations provided, the proposed development can be designed appropriately to address noise impacts provided the recommendations are implemented. Recommendations for noise control measures include window glazing sound isolation standards, central air-conditioning to allow the windows to remain closed, as well as the inclusion of noise warning clauses related to sound levels.

Transportation Impact Assessment

Following the completion of the TIA Steps 1, 2 & 3 submitted to City staff, IBI Group has prepared a Transportation Impact Assessment – Step 4: Analysis in support of the proposed development. Consistent with the Wateridge Phase 2A/2B TIA, refinements to the existing 'blended rate' mode share were applied to better represent the travel characteristics based on the site density and its location within the Community Core. The expected demographic profile of residents on this site further supports the use of higher non-auto mode share proportions.

Even though the vehicular parking proposed on site falls short of the minimum requirements specified in the Zoning By-law by providing just 122 out of the 144 spaces required, given the reduced likelihood of its residents to own private automobiles and its location within the Wateridge Village Community Core, no further review of parking will be necessary for the purposes of this study.

Based on the findings of this study, it is the overall opinion of IBI Group that the proposed development will integrate well with and can be safely accommodated by the adjacent transportation network.

Geotechnical Study

A Geotechnical Investigation was conducted by Paterson Group to determine the subsurface soil and groundwater conditions by means of boreholes, that have provided geotechnical recommendations for the design of the proposed development based on the results and other available soil information. From a geotechnical perspective, the investigation concludes that the subject site is suitable for the proposed development. Additional details and recommendations are provided in the Geotechnical Report.

Phase I & II Environmental Site Assessment

Lopers & Associates was retained to complete a Phase I Environmental Site Assessment (Phase I ESA) for the subject property, to research the past and current use of the site and study area, as well as to identify any environmental concerns that have potentially impacted the property. As per historical research, the site was undeveloped prior to the 1950's when initial development was competed as part of CFB Rockcliffe. The north portion of the Phase One Property was formerly occupied by a retail fuel outlet (Canex/Shell), while the remainder of the property was occupied by residential (military) buildings and Rights-of-Way. Decommissioning/demolition of the Property's structures began in the 1990's and the Property was vacant by 2008.

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The presence of a former retail fuel outlet was identified on the north portion of the Phase One Property. This former retail fuel outlet is a significant potentially contaminating activity (PCA) which represents area of potential environmental concern #1 (APEC #1) for the Property. Backfilling with fill of unknown environmental quality is a significant PCA which represents APEC #2 for the Property.

Based on the identification of PCAs and APECs at the Phase One Property, the Phase I Environmental Site Assessment recommended that a Phase Two Environmental Site Assessment be completed to assess the soil and/or groundwater quality in the vicinity of the APECs.

The Phase II ESA concludes that all of the soil and groundwater results for the Phase Two Property are in compliance with the applicable site condition standards as of the certification date of June 23, 2020.

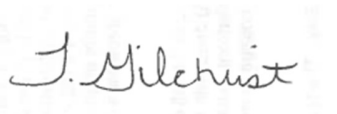
A record of site condition (RSC) was filed with the Ministry of Environment, Conservation and Parks (MECP) in 2020 for the Phase Two Property, which designated the Property for residential land use, as such, further RSC filings are not required for the proposed land use.

Conclusion

It is our professional opinion that the proposed development of the subject property, to be reviewed through the Site Plan Control process, conforms with and implements the policies of the City of Ottawa Official Plan, and generally complies with the intent and provisions of the application GM31 H(30) Zone, as per Zoning By-law 2008-250, with the exception of parking, driveway widths and a corner side step back at the fourth storey and above, for which a future Minor Variance Application will be submitted. The development, as proposed, provides a built form and site layout that is consistent with the principals of good urban design, which will be compatible with the existing and future uses on adjacent lands and will contribute to the achievement of a complete community. The development, as proposed, represents good planning.

Sincerely,

IBI Group



Tess Gilchrist, MCIP RPP
Senior Planner

I hereby certify that this Planning Justification Report was prepared under the direction of a Registered Professional Planner, within the meaning of the Ontario Professional Planners Institute Act, 1994.

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Appendix 1 – Zoning Table, City of Ottawa Zoning By-law 2008-250

Zoning Mechanisms		Required	Provided
715 Mikinak Road			
GM31 H(30)			
Table 187 – GM Zone Provisions			
Minimum Lot Area		No minimum	12,185 sq m
Minimum Lot Width		No minimum	92m
Maximum Building Height		30 metres	Building A – 24 m/ 7 storeys
			Building B – 24 m/ 7 storeys
			Building C – 14 m/ 4 storeys
Minimum width of landscaped area	Abutting a street	3 m	3 m (Mikinak Road only)
	Abutting a residential or institutional zone	3 m	N/A
	Other cases	No minimum	N/A
Table 188H – GM31 Subzone Provisions			
Minimum front (Hemlock Road) and corner side (Barielle Snow and Michael Stoqua Streets) yard setbacks	1. for a building with residential land use at grade	5 m	N/A
	2. all other cases	0 m	<u>Building A</u> – Hemlock Rd: 2 m <u>Building A & B</u> – Barielle Snow St: 5 m <u>Building C</u> – Michael Stoqua St: 5 m
Maximum setback	1. despite row (i)1 above, for those lots east of Codd's road where they abut Hemlock Road	2 m	Hemlock Road: 2 m

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	2. all other cases	n/a	N/A
Minimum rear yard setback (Mikinak Road)	1. a building with a residential land use at grade or where the rear yard abuts a residential zone	7.5 m	N/A
	2. where the rear yard abuts a park	5 m	N/A
	3. where the rear yard abuts a street	3 m	<u>Building C</u> – Mikinak Street: 3 m
	4. all other cases	No minimum	Not applicable.
Maximum floor space index		No maximum	Not applicable.
Additional GM 31 Subzone Provisions			
Where the building contains more than four storeys but less than 13 storeys, at and above the fourth storey a building must be setback a minimum of an additional 2 metres more than the provided setback from the front and corner side lot lines;		<p>An additional 2 m setback is provided on Buildings A & B at the fourth storey and above facing Hemlock Rd (Bldg A) and Barielle Snow Street (Bldg B).</p> <p>An additional 2 m setback is not provided on the corner side of Building A facing Barielle Snow Street at and above the fourth storey*.</p>	
<p>For buildings over 20 metres in height or that are greater than six storeys, whichever is less:</p> <p>when the portion of the building over 20 metres in height or at or above the seventh storey, whichever is less contains only residential uses, the maximum building area of each floor plate over 20 metres in height or at or above the seventh storey, whichever is less, is 750 square metres of gross floor area;</p>		The seventh storey residential portion of Buildings A & B above 20 m is less than 750 m ² per building.	
The minimum separation distance between portions of a building above four storeys is 23 metres.		<p>Between Buildings A & B: 25.59 m</p> <p>Between Buildings B & C: Not applicable.</p>	
Where non-residential uses are located on the first storey of a building, the area of the		51% - 58% varies	

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wall of the first storey facing the street must have a minimum of 50 per cent of the façade consisting of transparent glazing.			
With the exception of Planned Unit Developments and dwelling units within an apartment, the principal entrance door is required to face the front or corner side lot line.	Provided.		
The minimum building height is two storeys.	Provided.		
The maximum driveway width for parking lots and parking garages with 20 or more parking spaces is 6 metres.	Two driveways, 6.7 m and 7.6 m wide, respectively, are proposed on Michael Stoqua Street to provide safe ingress/egress for vehicles and garbage trucks. *		
Where a lot is abutting Hemlock Road, the façade facing Hemlock Road must include at least one active entrance per occupancy serving each residential or non-residential use occupying any part of the ground floor;	An entrance is provided for the non-residential use in Building A, and an entrance is provided to the residential portion of the building via common lobby.		
despite item h) in Table 187, no landscaped area is required abutting a street for buildings with no required front or corner side yard setback; (By-law 2015-369)	All provided setbacks will be landscaped. Building C: 3m setback will be landscaped.		
Part 4 – Parking, Queuing and Loading Provisions			
Sections 101 Minimum Parking Space Rates & 102 Minimum Visitor Parking Space Rates			
Residential Parking: No parking required for first 12 units <u>per building</u> Section 101(3)(a) 0.5 space per unit above 12 units	Required		Provided
	Building A – 115 units	52	
	Building B – 114 units	51	
	Building C – 42 units	15	
	Total: 271 units	118	96*
Visitor parking No parking required for first 12 units <u>on a lot</u> Section 102(2) 0.1 space per unit above 12 units	Building A – 115 units		
	Building B – 114 units		
	Building C – 42 units		

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	Total: 271 units	26	26
		144	122
Section 110 Landscaping Provisions for Parking Lots			
Minimum of 15% of parking lot area must be provided as perimeter or interior landscaped area	3845.3 m ² x 15% = 576.80 m ²		1963 m ²
Location of Landscape Buffer abutting a street for a parking lot containing more than 100 spaces	3 m		3m
Location of Landscape Buffer not abutting a street for a parking lot containing more than 100 spaces	3 m		4.5m–12.8m Varies
All outdoor loading and refuse collection areas contained in a parking lot must be located at least 9 m from a lot line abutting a public street, and screened from view by an opaque screen with a minimum height of 2 m.	Provided.		39m
Section 111 Bicycle Parking Space Rates and Provisions			
Building 1 – 115 units	0.5 per unit	58	88
Building 2 – 114 units	0.5 per unit	57	72
Building 3 – 42 unit	0.5 per unit	21	46
		136	206
Non-residential rates vary from 1 per 250 m ² GFA to 1 per 500 m ² GFA	239 m ²	1	4
Minimum Bicycle Parking Space Dimensions	Width	Length	
Horizontal	0.6 m	1.8m	Provided
Vertical	0.5 m	1.5 m	N/A
Minimum access aisle width 1.5m			Provided
Where four or more spaces are provided in a common area, each space must contain a rack that is securely anchored to the ground.			Provided
Maximum of 50 % may be vertical.			N/A
Where requirement exceeds 50 spaces, a minimum of 25% must be located within a building, secure area or lockers.			
Section 137 – Amenity Area			
Building 1 – 114 units	6 m ² per dwelling unit	684 m ²	703 m ²
Building 2 – 115 units	6 m ² per dwelling unit	690 m ²	793 m ²
Building 3 – 42 unit	6 m ² per dwelling unit	252 m ²	264 m ²
	Total	1626 m ²	1760 m ²
50% of the required total amenity area is to be communal, aggregated into areas up to 54 m ² GFA, and where more	Total Communal	813 m ²	1553 m ²

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than one aggregated area is provided, at least one must be a minimum of 54 m ²			
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***Provisions for which a Minor Variance Application is required**