

# PLANNING RATIONALE

## 2950-2960 BANK STREET



Project No.: OCP-17-0565

Prepared for:

Firm Capital Properties Inc.  
163 Cartwright Avenue  
Toronto, ON  
K2G 1E8

Prepared by:

McIntosh Perry Consulting Engineers Ltd.  
115 Walgreen Road  
Carp, ON  
K0A 1A0

March 6, 2020

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## **1.0 OVERVIEW / INTRODUCTION**

This Planning Justification Report is submitted on behalf of Firm Capital Properties Inc. (c/o 2950-2960 BANK STREET RETAIL CENTER INC.) in support of a Site Plan Control application for the development of two additional commercial buildings on the subject lands.

The subject lands are known municipally as 2950-2960 Bank Street and legally as “LT LT 9 CON 4RF GLOUCESTER BEING PT OF THE S 1/2 AS IN N433559; GLOUCESTER.” The subject lands were previously subject to planning application no. D07-12-06-0017.

An initial Pre-Consultation meeting was held August 28, 2018 to discuss the proposal and a second Pre-Consultation meeting occurred July 19, 2019 where an updated Site Plan was presented and the City’s Drive-Through Design Guidelines were discussed. Results from both pre-consultation meetings are attached at Appendix A.

Submission requirements for this Site Plan Control application, as identified on the Applicant’s Study and Plan Identification List, include: a Site Servicing Plan; a Site Servicing Study / Brief; a Grade Control and Drainage Plan; a Geotechnical Study; an Erosion and Sediment Control Plan; a Stormwater Management Brief; this Planning Justification Report; a Site Plan; a Landscape Plan; a Survey Plan; Building Elevations; and a Phase 1 Environmental Site Assessment. These plans and studies are provided as part of the submission package.

This Planning Justification Report discusses the suitability of the current proposal in the context of applicable planning policy and zoning.

## **2.0 SITE CONTEXT**

The subject lands are located at 2950-2960 Bank Street in Ward 10 of the City of Ottawa, south of Hunt Club Road, and east of the Ottawa International Airport. The subject lands are located adjacent to and south of the Queensdale Avenue and Bank Street intersection. The approximately 5.87 hectare (58,673 square metre) lands have a frontage of roughly 195 metres on Bank Street and 265 metres on Queensdale Avenue.

An existing 13,022 square metre one-storey shopping centre is located on the subject lands towards the rear lot line, and is occupied by Farm Boy, Giant Tiger, Allstate Insurance, Monkey Around Play and Learn Centre, Kallisto Greek restaurant, and Kardish Health Food Centre. A 422 square metre, one-storey bank (TD Financial) is also located on the northeast corner of the subject lands. There is a total of 665 parking spaces existing on site.

Three driveways provide vehicular access to the subject lands: two provide access at Queensdale Avenue and one provides access from Bank Street. A public sidewalk is located adjacent to the subject lands along Queensdale Avenue. There is also a private walkway on site, along a portion of Bank Street.

A bus stop serviced by OC Transpo Bus Route 93 (Leitrim) is located between the two driveways on Queensdale Avenue.

Southwest of the existing shopping centre is a treed area that includes a portion of Sawmill Creek. The Sawmill Creek Corridor primary network is located on and adjacent to the subject lands, as noted by City Staff during Pre-Consultation. A map of the subject lands showing Natural Heritage Features, based on the Ministry of Natural Resources and Forestry Data, is attached as Appendix B.

As identified within Official Plan Schedule C (Primary Urban Cycling Network), a multi-use pathway is planned at the rear of the existing shopping centre and a spine bicycle pathway is planned for the portion of Bank Street adjacent to the subject lands.

The subject lands have full municipal servicing. An open ditch is located along Bank Street, adjacent to the subject lands and storm sewers are located within Queensdale Avenue.

Surrounding uses are outlined in Table 1.

*Table 1: Surrounding Uses*

Direction	Land Use
<b>North</b>	Residential Subdivision (Plan 326) [opposite Queensdale Avenue]
<b>East</b>	Global Auto Sales, Tim Hortons, AMJ Campbell Self Storage Ottawa [opposite Bank Street]
<b>South</b>	Development Reserve Zone, Fawn Meadow Park, Residential Subdivision under construction (Block 144 and 157, Plan 4M1290, VALECRAFT HOMES LIMITED)
<b>West</b>	117 Condominium Units (1602 Queensdale Ave, Block 15108)

Figures 1 and 2 illustrate the location of the subject lands in relation to the surrounding uses.

The following section discusses the development proposal.





*Figure 1: Site Location*



*Figure 2: Subject and Surrounding Lands*

### 3.0 DEVELOPMENT PROPOSAL

The proposal involves two freestanding retail buildings. “Proposed Retail Building ‘A’” has a gross floor area of approximately 692 square metres and “Proposed Retail Building ‘B’” has a gross floor area of approximately 975 square metres. Both buildings are located along the eastern lot line, nearest Bank Street, and will include drive-through facilities. Although building occupants are not yet known, building “A” has been designed to have five units, and building “B” has been designed with seven units.

The existing shopping centre and bank will remain unchanged; however, the parking areas that serve them will be reconfigured in order to accommodate the proposed buildings.

The proposed development will cause a reduction in the overall number of parking spaces provided, from 665 to 568 spaces. 547 standard spaces are provided together with 21 accessible spaces. The plaza sign located north of the Bank Street driveway is proposed to be relocated to the south side of the access driveway.

A new concrete walkway is proposed within the southeast portion of the subject lands, parallel to Bank Street.

Twenty-nine percent of the overall subject lands is landscaped area, and 16 percent of the parking lot is landscaped area.

The Site Plan, dated December 6, 2019 and prepared by KWC Architects Inc., is attached at Appendix C of this report.

Consideration of the proposal in the context of applicable planning policy and zoning is provided in the following sections.

### 4.0 PLANNING POLICY & REGULATORY FRAMEWORK

#### 4.1 Provincial Policy Statement

The 2014 Provincial Policy Statement (PPS) provides policy direction on land use planning matters of provincial interest. The proposed development is consistent with Provincial Policy Statement policies that promote intensification, economic development, and competitiveness. The proposal will provide new retail buildings to the existing Blossom Park Shopping Centre along with associated parking, which forms part of the local economy. The proposed development is supported by Policy 1.3.1:

Pol. 1.3.1 Planning authorities shall promote economic development and competitiveness by:

- a) providing for an appropriate mix and range of employment and institutional uses to meet long-term needs; [and]
- b) providing opportunities for a diversified economic base, including maintaining a range and choice of suitable sites for employment uses which support a wide range of economic activities and ancillary uses, and take into account the needs of existing and future businesses.

Policy 1.1.3.2 further supports the proposed development and reads: “Land use patterns within *settlement areas* shall be based on: c) Densities and a mix of land uses which: 1. efficiently use land and resources; 2. are appropriate for, and efficiently use, the *infrastructure* and *public service facilities* which are planned or available, and avoid the need for their unjustified and/or uneconomical expansion;...”

The proposal can be considered intensification in accordance with the PPS<sup>1</sup> and is therefore supported by Policy 1.1.3.2 (b): “Land use patterns within *settlement areas* shall be based on: ... a range of uses and opportunities for intensification and redevelopment in accordance with the criteria in Policy 1.1.3.3, where this can be accommodated.”

The proposal is consistent with matters of Provincial interest, as expressed in the 2014 Provincial Policy Statement. The proposal is also consistent with the 2020 Provincial Policy Statement, which comes into effect May 1, 2020.

## 4.2 City of Ottawa Official Plan

The City of Ottawa Official Plan provides policy direction on land use matters to manage municipal growth in a sustainable manner. The subject lands are designated General Urban Area per Schedule B (Urban Policy Plan) of Volume 1 of the City of Ottawa Official Plan and Arterial Mainstreet per Schedule A (Land Use) of Volume 2 (South Keys to Blossom Park, Bank Street Secondary Plan). The natural heritage feature towards the rear subject lands is designated Urban Natural Features on Schedule B of the Official Plan.

### 4.2.1 Volume 1 of the City of Ottawa Official Plan

Policy 3.6.1.1 of the Official Plan identifies uses permitted on lands designated General Urban Area and discusses the purpose of the designation:

“The General Urban Area designation permits many types of densities of housing as well as employment, retail uses, service, industrial, cultural, leisure, greenspace, entertainment and institutional uses. This will facilitate the development of complete and sustainable communities.”

The proposed retail buildings are considered intensification, as defined within the Official Plan<sup>2</sup>, and is in keeping with part of the preamble to Section 3.6.1: “...the City supports infill development and other intensification within the General Urban Area in a manner that enhances and complements the desirable characteristics and ensures the long-term vitality of the many existing communities that make up the city.”

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<sup>1</sup> The definition of intensification within the PPS is: “Intensification: means the development of a property, site or area at a higher density than currently exists through: a) *redevelopment*, including the reuse of *brownfield sites*; b) the development of vacant and/or underutilized lots within previously developed areas; c) infill development; and d) the expansion or conversion of existing buildings.”

<sup>2</sup> The definition of intensification within the City of Ottawa Official Plan is: “Intensification means that the density of development, measured in households or employment per hectare, increases.”



Policy 3.6.1 (7) reads: “The General Urban Area permits uses that may generate traffic, noise or other impacts that have the potential to create conflicts with the surrounding residential community. These types of uses are often large and serve or draw from broader areas. The City will ensure that anticipated impacts can be adequately mitigated or otherwise addressed. Such uses will be directed to:

- a. Locations on the Rapid Transit and Transit Priority network, or an arterial or major collector road with sufficient capacity to accommodate the anticipated traffic generated and where frequent, all-day transit service can be provided.”

Bank Street is designated Arterial – Existing within Schedule E (Urban Road Network) of the Official Plan.

The proposed development is subject to the City’s Traffic Impact Assessment Study Process.

The proposal warrants consideration in relation to Sections 2.5.1 and 4.11 of the Official Plan, which provide urban design objectives to ensure new development is compatible with communities’ established characteristics. The proposal conforms to the design objectives within Section 2.5.1 as it respects the character of the area and builds upon the existing built form in such a way that contributes to the enhancement of Bank Street as a more pedestrian-friendly/multi-modal part of our urban fabric.

Policy 4.11 (5) establishes that “compatibility of new buildings with their surroundings will be achieved in part through the design of the portions of the structure adjacent to existing buildings and/or facing the public realm.” The building elevations enclosed with this application demonstrate that the design of the development fits with the existing character and planned function of the existing commercial/retail site. Further, the location of the proposed structures on the subject lands were arrived at in consultation with Staff who emphasized that the building should be loaded towards Bank Street as much as possible in keeping with the intent of Policy 4.11(6).

As mentioned above, the natural heritage feature towards the rear subject lands is designated Urban Natural Features on Schedule B of the Official Plan. Section 3.2.3 of the Official Plan addresses lands designated Urban Natural Features, which provide a “...valuable contribution to biodiversity and wildlife habitat in the area and are enjoyed by residents.” The intent of the designation is to “...preserve natural features that are currently managed for conservation or passive leisure uses.” Policy 3.2.3(5) indicates that “...development and site alteration will not be permitted within 30 metres of the boundary of a designated Urban Natural Feature unless an Environmental Impact Statement demonstrates that there will be no negative impacts on the natural features within the area of their ecological functions.” The proposed buildings are located more than 30 metres from the designation limit.

A multi-use pathway is planned at the rear of the existing shopping centre and a spine bicycle pathway is planned for the portion of Bank Street adjacent to the subject lands, according to Official Plan Schedule C (Primary Urban Cycling Network). Although this was discussed at Pre-Consultation, there is no indication that the City has current plans to develop this infrastructure.

The proposal conforms to the General Urban Area policies in the Official Plan.



#### **4.2.2 Volume 2A of the City of Ottawa Official Plan (South Keys to Blossom Park, Bank Street Secondary Plan)**

The South Keys to Blossom Park, Bank Street Secondary Plan is intended to develop into “...a more mixed-use, connected, attractive place for pedestrians and cyclists.” The proposed development is largely designated Arterial Mainstreet per the Secondary Plan, while the rear portion of the property (Sawmill Creek) is designated Greenspace.

Subsection 3.1.1 of the Secondary Plan provides direction for Bank Street and Adjacent Areas. Guiding Principles include:

1. Support the varying roles of Bank Street, which is a place of living, working, socializing, transportation and ultimately, is a street that is more comfortable for pedestrians and cyclists.
2. Recognize and protect land identified as an Area of Established Character.
3. Ensure a safe, identifiable network of sidewalks, pathways, greenways and cycling lanes that connect greenspaces, parks, transit stations and other key destinations along this corridor.

The design parameters for this area are “...characterized by mid-rise buildings with highly-transparent store frontages, landscaping, and ample pedestrian connections.” The Secondary Plan recognizes that the policies which apply to the Arterial Mainstreet designation of Volume 1 of the Official Plan are appropriate for this area. Schedule A – Land Use of this Secondary Plan “refines the areas to which the Arterial Mainstreet policies of the Official Plan apply.

The Greenspace designation within the Secondary Plan is generally commensurate with the lands designated Urban Natural Features within Volume 1 of the Official Plan and the Secondary Plan indicates that policies of the Urban Natural Features designation of Volume 1 of the Official Plan apply.

Further discussion on and review of the Arterial Mainstreet and Urban Natural Features Official Plan designations are included in the following two subsections.

#### **4.2.3 Arterial Mainstreet Official Plan (Volume 1) Designation**

Though the subject lands are largely designated General Urban Area per Schedule 2 of the Official Plan, Schedule A of the Secondary Plan noted above indicates that the Arterial Mainstreet policies also apply.

Per Section 3.6.3 of the Official Plan, the objective of the Mainstreet designation is “to encourage more dense and mixed-use development that supports, and is supported by, increased walking, cycling and transit use.” Arterial Mainstreets are characterised by “larger lots and builds, varied setbacks, and lower street-level densities than Traditional Mainstreets.” Arterial Mainstreets are also described as “automobile-oriented,” with the “predominant land use [being] single-purpose commercial.” The proposed buildings are in keeping with the intent of the Arterial Mainstreet designation.

In support of the proposal, “retail and service commercial uses” are permitted under the Arterial Mainstreet designation. In keeping with Policy 3.6.3(4), the proposal involves multi-modal access, improves the attractiveness, utility, and safety of the Bank Street right-of-way by enclosing/framing the streetscape,

enhances the pedestrian environment, reduces the visual impact of the existing surface parking area, and incorporates landscaping along the right-of-way.

Policy 3.6.3(8) confirms drive-through facilities are permitted, subject to their evaluation in relation to applicable guidelines. Section 4.4 of this report evaluates the proposed drive-through facilities relative to the City of Ottawa Urban Design Guidelines for Drive-Through Facilities.

### **4.3 South Keys to Blossom Park, Bank Street Community Design Plan**

The subject lands are largely designated Arterial Mainstreet, while the western portion is designated Greenspace within Figure 26: Land Use Map, of the South Keys to Blossom Park, Bank Street Community Design Plan (CDP). Sawmill Creek is also located at the rear of the existing shopping centre on Figure 26. Section 3.2.6(a) identifies “retail” as a permitted land use within the Arterial Mainstreet designation.

As noted in Section 3.2 of the CDP, the vision for Bank Street is to “be a comfortable, inviting place for all pedestrians, from children to seniors, and support cyclists via a well-connected and safe network of pathways and cycling lanes.” A vision for Sawmill Creek is also discussed: “Sawmill Creek will be restored and protected to a naturalized state with enough space in the corridor to support: aquatic, bird and mammalian habitats, surface water quality and native vegetation. In specific areas, the Creek will offer opportunities for active or passive recreation.”

The Arterial Mainstreet (Bank Street) land use designation within the CDP is intended to have a ‘look and feel’ characterized by mid-rise building that frame the street, is more compact, pedestrian-oriented and transit-friendly with ample pedestrian connections.”

The proposed development satisfies various design standards in Section 3.4.6, including:

- b. Maximum building height for properties in the Arterial Mainstreet area of Bank Street is 9 storeys, subject to building height transition requirements in the AM zone when abutting certain residential zones.
- c. To assist in creating a mainstreet look and feel to Bank Street, buildings should be sited close to the street and where possible at least 50% of the Bank Street frontage of the property should be occupied by building walls that include active entrances and windows at grade. Narrow lots may be exempt from this requirement.
- f. Sidewalks should connect the front doors of buildings to the existing or planned sidewalks along Bank Street. Although vehicular access may be granted on Queensdale Avenue, Kingsdale Avenue or Rosebella Avenue, depending on location, sidewalks shall connect the buildings to Bank Street.
- g. Where sidewalks do not exist, sidewalk construction will be required along Bank Street adjacent to all properties undergoing redevelopment as a condition of development approval.

The proposed development generally satisfies CDP design direction.

#### 4.4 City of Ottawa Urban Design Guidelines for Drive-Through Facilities

The purpose of the City of Ottawa Urban Design Guidelines for Drive-Through Facilities is to “provide urban design guidance at the planning application stage in order to assess, promote and achieve appropriate development of drive-through facilities.” The proposed development conforms to applicable objectives of the Urban Design Guidelines for Drive-Through Facilities that were noted during the July 19, 2019 Pre-Consultation meeting:

- To promote compatible development that fits well with, and improves, its existing or planned context;
- To enhance public streets and contribute to a high quality public space;
- To create efficient stacking movements on site; [and]
- To create a safe and comfortable pedestrian environment on site.

During Pre-Consultation, City Staff also discussed that the proposed development should be considered in relation to the following three Guidelines:

Applicable Guideline (per Urban Design Guidelines for Drive-Through Facilities)	Explanation
<b><i>Guideline 8: Landscape the area in front of blank walls that face public streets and use projections, recesses, arcades, awnings, colour and texture to reduce the visual size of any unglazed walls.</i></b>	<p>For visual screening, proposed landscape areas have been planned in the front yard of the proposed retail buildings, adjacent to Bank Street (see the Site Plan attached as Appendix C).</p> <p>Additional landscaping is proposed on the south side of “Proposed Retail Building ‘A,’” nearest the driveway entrance from Bank Street.</p> <p>Various types of materials are used for the exterior of the proposed buildings (refer to “Elevation Notes” table in Building Elevations attached at Appendix D for a list of materials).</p>
<b><i>Guideline 9: Coordinate architectural detail and character within an overall design concept for all building sides and components.</i></b>	<p>Architectural details and character are consistent on all sides of and coordinated between the two proposed buildings. Architectural details and character are also notably consistent between the existing drive-through building (TD Financial) and the proposed retail buildings.</p> <p>Building Elevations (Appendix D) illustrate coordination of architectural details and character between buildings.</p>

**Guideline 22: Avoid locating the stacking lane between the building and the public street.**

In the Site Plan submitted with this application, stacking lanes for the proposed retail buildings have been relocated to the interior side of the building, nearest the parking lot.

The stacking lanes are no longer between the building and Bank Street.

The proposed development conforms with Guidelines 8, 9, and 22 of the Urban Design Guidelines for Drive-Through Facilities. A more comprehensive review of conformity with the Guidelines is included at Appendix E.

#### 4.5 City of Ottawa Zoning By-law No. 2008-250

The subject lands are zoned Arterial Mainstreet (AM1 H(30)), AM1 Subzone, within the City of Ottawa Zoning By-law 2008-250. The purpose of the AM zoning district, as identified in the preamble to Section 185 and 186, is to:

- (1) accommodate a broad range of uses including retail, service commercial, offices, residential and institutional uses in mixed-use buildings or side by side in separate buildings in areas designated Arterial Mainstreet in the Official Plan; and
- (2) impose development standards that will promote intensification while ensuring that they are compatible with the surrounding uses.

Section 185 of the By-law lists “Retail Store” as a permitted use.

The maximum floor space index for the proposed development is 2, per Section 185, Table 185 (h)(i)(2) of the By-law. Within Section 186(1), further provisions apply to lands located in the AM1 Subzone: “(a) No greater than 50% of the maximum permitted floor space index may be used for non-residential uses.” Based on the above-noted provisions of the By-law, a floor space index no greater than 1 is permitted on the subject lands and therefore, the proposed floor space index of 0.26 complies with the By-law.

The building height for “Proposed Retail Building ‘A’” and “Proposed Retail Building ‘B’” is approximately 6.25 metres, which is compliant with the maximum building height of 30 metres per Section 185, Table 185 (f)(iv) of the By-law.

The proposed 568 total parking spaces are compliant with the requirements for the subject lands (designated Area C on Schedule 1A), as per Section 101, Table 101, Row N83 of the By-law.

Thirty-eight bicycle parking spaces are provided, which complies with the minimum requirement of 30 spaces as per Section 111, Table 111A, item (f) of the By-law.

The three loading spaces existing on the subject lands comply with the minimum requirement of two loading spaces outlined in Section 113, Table 113A, item (c) of the By-law.



Attached at Appendix F is a zoning table with applicable provisions for the proposed development, as outlined in the By-law.

The proposed development is compliant with the City of Ottawa Zoning By-law 2008-250.

## **5.0 CONCLUSION**

The development is consistent with the Provincial Policy Statement and conforms to the City of Ottawa Official Plan. The proposed commercial/retail buildings are permitted under the Zoning By-law and the proposal complies with other applicable zoning provisions.

The proposal is an appropriate use of the subject property, is in the public interest, and represents good planning.

Respectfully submitted,

Prepared By:



Sarah Butt, M.PL  
Junior Planner

Reviewed By:



Benjamin Clare, MCIP RPP  
Senior Land Use Planner

**APPENDIX A**  
Pre-Consultation Meeting Results

## Pre-consultation Notes

**Meeting: August 28, 2018 – 4106E – 110 Laurier Avenue West – 11am**

2950 & 2960 Bank Street

### **City Attendees:**

Kelby Lodoen Unseth  
Planner  
Christopher Moise  
Architect, Urban Design

Natasha Baird  
Project Manager  
Adam Baker  
Project Manager

### **Property Overview:**

The subject property is located at 2950 & 2960 Bank Street, which is an existing shopping centre facing Bank Street, zoned AM1 H(30) (Arterial Mainstreet). The intent of the AM zoning district is to:

- 1) Accommodate a broad range of uses including retail, service commercial, offices, residential and institutional uses in mixed-use buildings or side by side in separate buildings in areas designated Arterial Mainstreet in the Official Plan; and
- 2) Impose development standards that will promote intensification while ensuring that they are compatible with the surrounding uses.

The property is also under Subzone 1, which also states:

- a) No greater than 50% of the maximum permitted floor space index may be used for the non-residential uses; and
- b) The provisions of subsection 186(1)(a) above to not apply to community centre, community health and resource centre, day care and library.

The zoning also includes a height restriction of 30 metres.

Additionally, the property is within the South Keys to Blossom Park, Bank Street Secondary Plan and new development must align with the policies of that document.

### **Discussion:**

An overview discussion outlined the intent of the project, as well as the current layout of the property. The project would add a new pad site that is approximately 16,000 square feet, which could be divided into as many as 10 bays. The development would be placed on land currently designated for parking. Discussion occurred on the placement of the structure, and how a more pedestrian friendly area could be created. Questions were posed as to what the rationale for the placement of the structure is, and if the building is oriented differently could the visual aspect or pedestrian realm be improved upon. Further comments on the design, materials, and placement of the structure may be made once plans have been submitted to the City for review.

Confirmation will also be required with a submission that the site will comply with the Subzone 1 requirement that no greater than 50% of the maximum permitted floor space index may be used for non-residential uses.

## Pre-consultation Notes

**Meeting: August 28, 2018 – 4106E – 110 Laurier Avenue West – 11am**

2950 & 2960 Bank Street

Other notes:

- Please consult with the Ward Councillor and community groups prior to submission of an application.
- Please ensure that the Parking, Queuing and Loading Provisions are following and appropriate vehicle and bicycle parking is provided on-site (<https://ottawa.ca/en/part-4-parking-queuing-and-loading-provisions-sections-100-114#bicycle-parking-space-rates-and-provisions-sec-111>).
- Please ensure that the Landscaping Provisions for Parking Lots is followed (<https://ottawa.ca/en/part-4-parking-queuing-and-loading-provisions-sections-100-114#section-110-landscaping-provisions-parking-lots>).
- Please also review the accessible design standards for parking requirements, etc. ([https://documents.ottawa.ca/sites/default/files/documents/accessibility\\_design\\_standards\\_en.pdf](https://documents.ottawa.ca/sites/default/files/documents/accessibility_design_standards_en.pdf)).

### Transportation:

The transportation Impact Assessment (TIA) is required which will analyse the driveways to the site and the main intersections on boundary streets. On-site circulation of pedestrians, cyclists, and cars needs to be discussed in the report, in addition to documenting the MMLOS at major intersections.

### Parks:

The proposal is to construct a +/- 16,000 sq. ft. retail/commercial building in the southeast corner of the site. We recommend taking cash-in-lieu of parkland dedication (CIL) for the proposed development as a condition of site plan approval. The parkland dedication rate for commercial uses is 2% of the gross land area. For the proposed expansion, the parkland dedication calculation would be based on the following formula:

$$\text{Parkland (sq. m)} = ((P - E) \times A \times 2\%) / P$$

P = Total Floor Area of Proposed and Existing Development in sq. m

E = Total Floor Area of Existing Development in sq. m

A = Gross Land Area of the Site in sq. m

If the applicant would like to provide the total floor area of the existing development, we could provide an estimate of the parkland dedication area requirement. The final amount for the CIL would be determined through the site plan application process.

### Rideau Valley Conservation Authority:

It is understood that the proposed structure will be placed where there are currently parking spots. Provided that there is no alteration to the remaining parking areas (ie: retrofit of storm sewers, catchbasins, etc. then the Conservation Authority would defer stormwater comments to the City as rooftop drainage is considered to be clean. If however, there is any proposed change to existing storm sewers and catchbasins within the remaining parking areas, then water quality



## Pre-consultation Notes

**Meeting: August 28, 2018 – 4106E – 110 Laurier Avenue West – 11am**

2950 & 2960 Bank Street

needs to be taken into consideration. The appropriate water quality target for Sawmill Creek is 80% TSS removal.

**Engineering notes by Natasha Baird (attached).**

I have included the study and plan identification list. For any questions, please feel free to contact me at the information below.

Please feel free to contact me should you have any questions with the information provided.

Regards,

A handwritten signature in blue ink that reads "Kelly Lodoen Unseth".

**Kelby Lodoen Unseth** MCIP, RPP

Planner II | Urbaniste II

Development Review (South Services) | Examen des projets d'aménagement (services sud)

Planning, Infrastructure and Economic Development | Services de planification, d'infrastructure et de développement économique

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## Meeting Notes

Meeting: July 19, 2019 – 4102E – 110 Laurier Avenue West – 10am

2950 & 2960 Bank Street

### **City Attendees:**

Kelby Lodoen Unseth

Planner

Christopher Moise

Architect, Urban Design

### **Property Overview:**

The subject property is located at 2950 & 2960 Bank Street, which is an existing shopping centre facing Bank Street, zoned AM1 H(30) (Arterial Mainstreet). The intent of the AM zoning district is to:

- 1) Accommodate a broad range of uses including retail, service commercial, offices, residential and institutional uses in mixed-use buildings or side by side in separate buildings in areas designated Arterial Mainstreet in the Official Plan; and
- 2) Impose development standards that will promote intensification while ensuring that they are compatible with the surrounding uses.

The property is also under Subzone 1, which also states:

- a) No greater than 50% of the maximum permitted floor space index may be used for the non-residential uses; and
- b) The provisions of subsection 186(1)(a) above do not apply to community centre, community health and resource centre, day care and library.

The zoning also includes a height restriction of 30 metres.

Additionally, the property is within the South Keys to Blossom Park, Bank Street Secondary Plan and new development must align with the policies of that document.

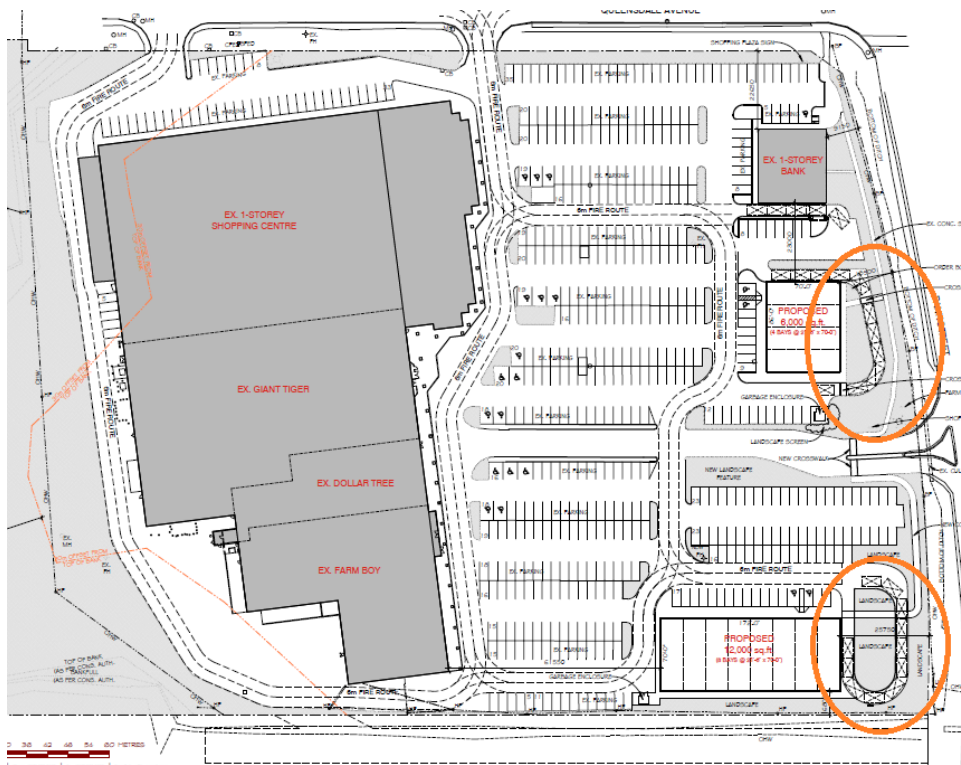
### **Discussion:**

The most recent version of the proposed site plan was provided and discussed. Concerns were brought up over the layout and location of the two proposed drive-through facilities, and the reasoning for their placement.

## Meeting Notes

Meeting: July 19, 2019 – 4102E – 110 Laurier Avenue West – 10am

2950 & 2960 Bank Street



The design guideline document for drive-through facilities was referenced when discussing the proposed drive-through facilities. The objective of the guideline document is to:

- promote compatible development that fits well with, and improves, its existing or planned context;
- enhance public streets and contribute to a high-quality public space;
- create efficient stacking movements on site;
- create a safe and comfortable pedestrian environment on site.

The following policies relate to the current site plan concept:

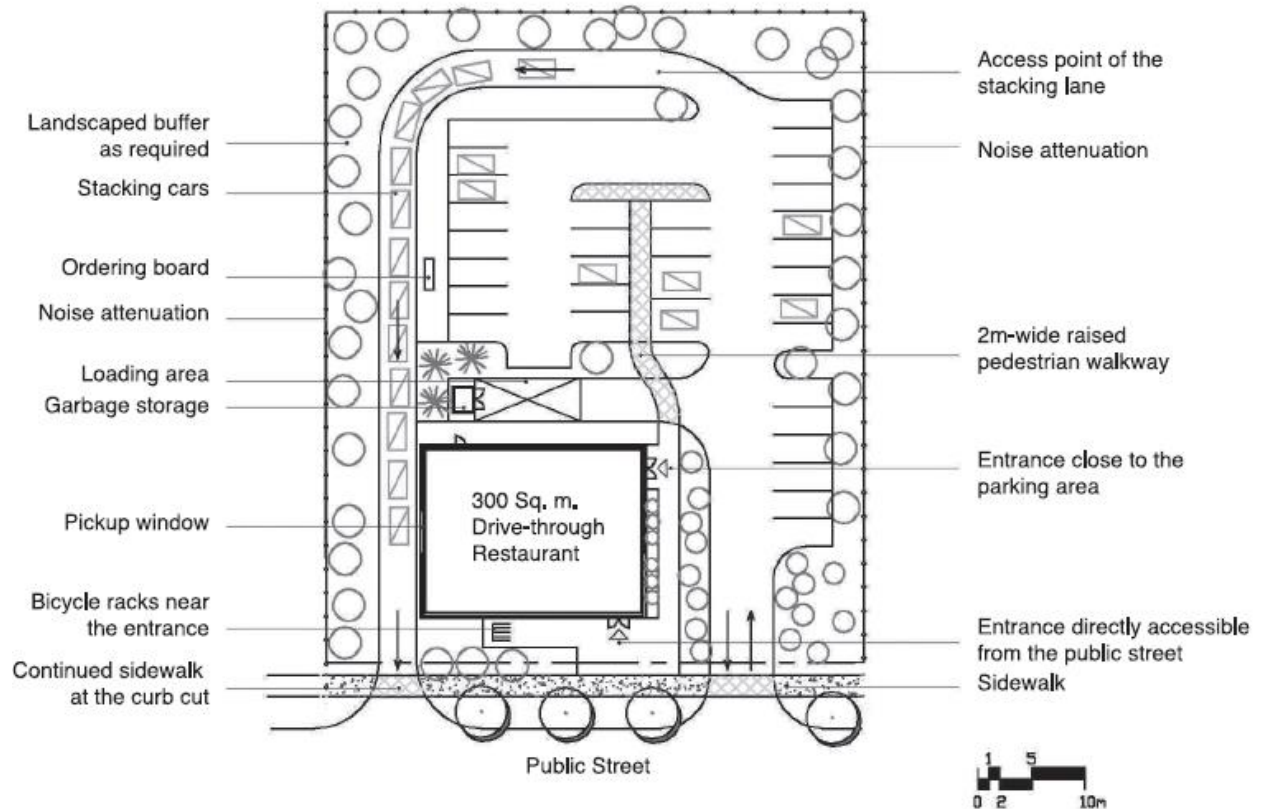
- **Guideline 8:** Landscape the area in front of blank walls that face public streets and use projections, recesses, arcades, awnings, colour and texture to reduce the visual size of any unglazed walls.
- **Guideline 9:** Coordinate architectural detail and character within an overall design concept for all building sides and components.
- **Guideline 22:** Avoid locating the stacking lane between the building and the public street.

## Meeting Notes

Meeting: July 19, 2019 – 4102E – 110 Laurier Avenue West – 10am

2950 & 2960 Bank Street

Example from guidelines:



Urban Design Guidelines for Drive-Through Facilities:

<https://documents.ottawa.ca/sites/default/files/documents/con029023.pdf>

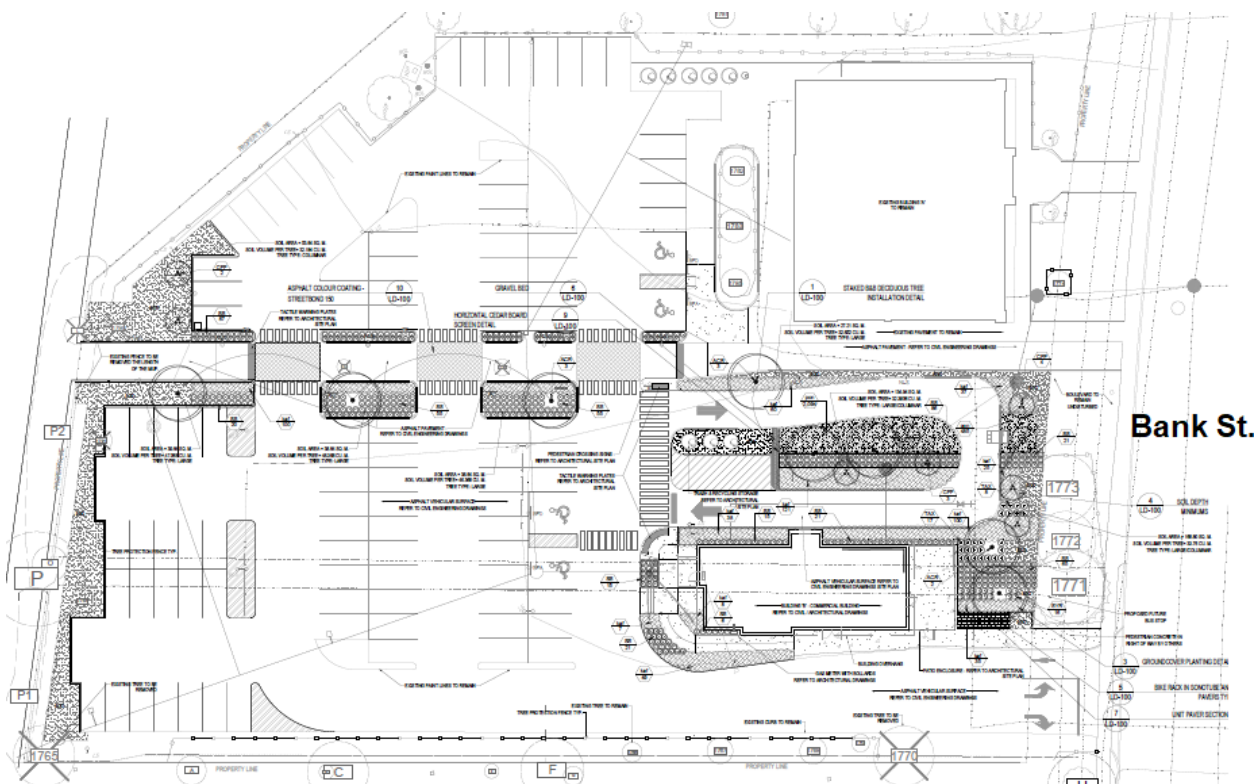
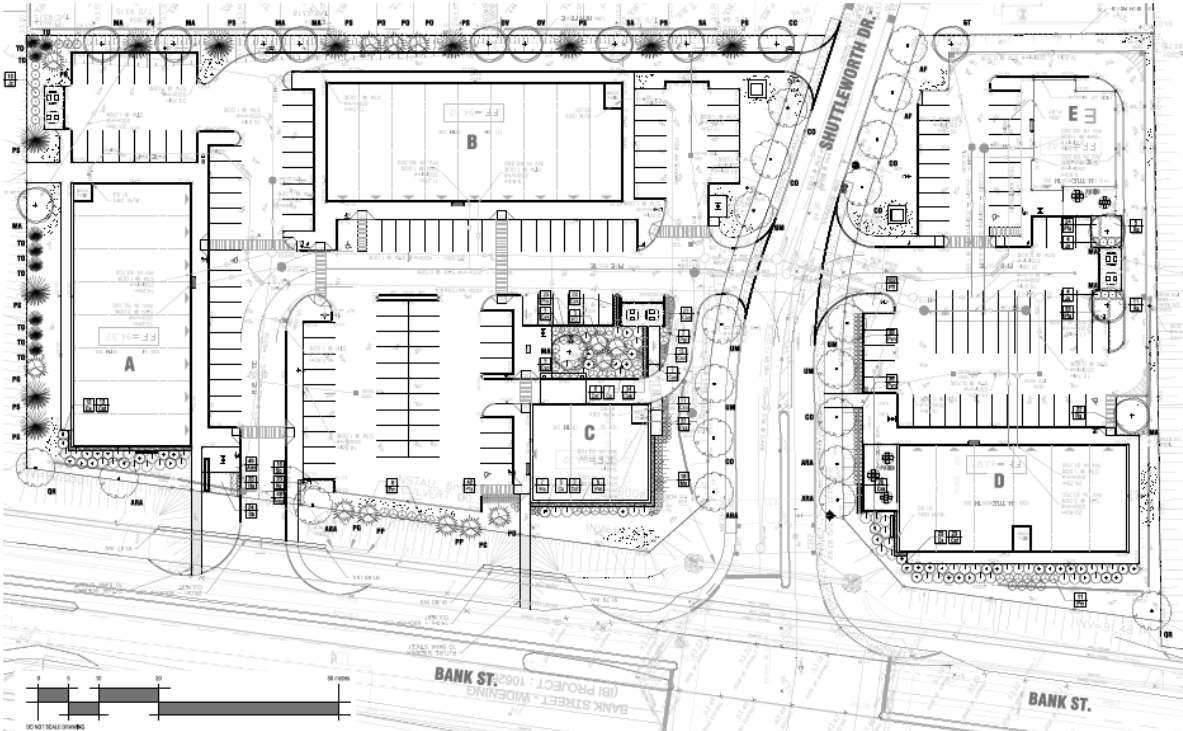


## Meeting Notes

Meeting: July 19, 2019 – 4102E – 110 Laurier Avenue West – 10am

2950 & 2960 Bank Street

Examples of retail buildings with multiple frontages and drive-through layouts:



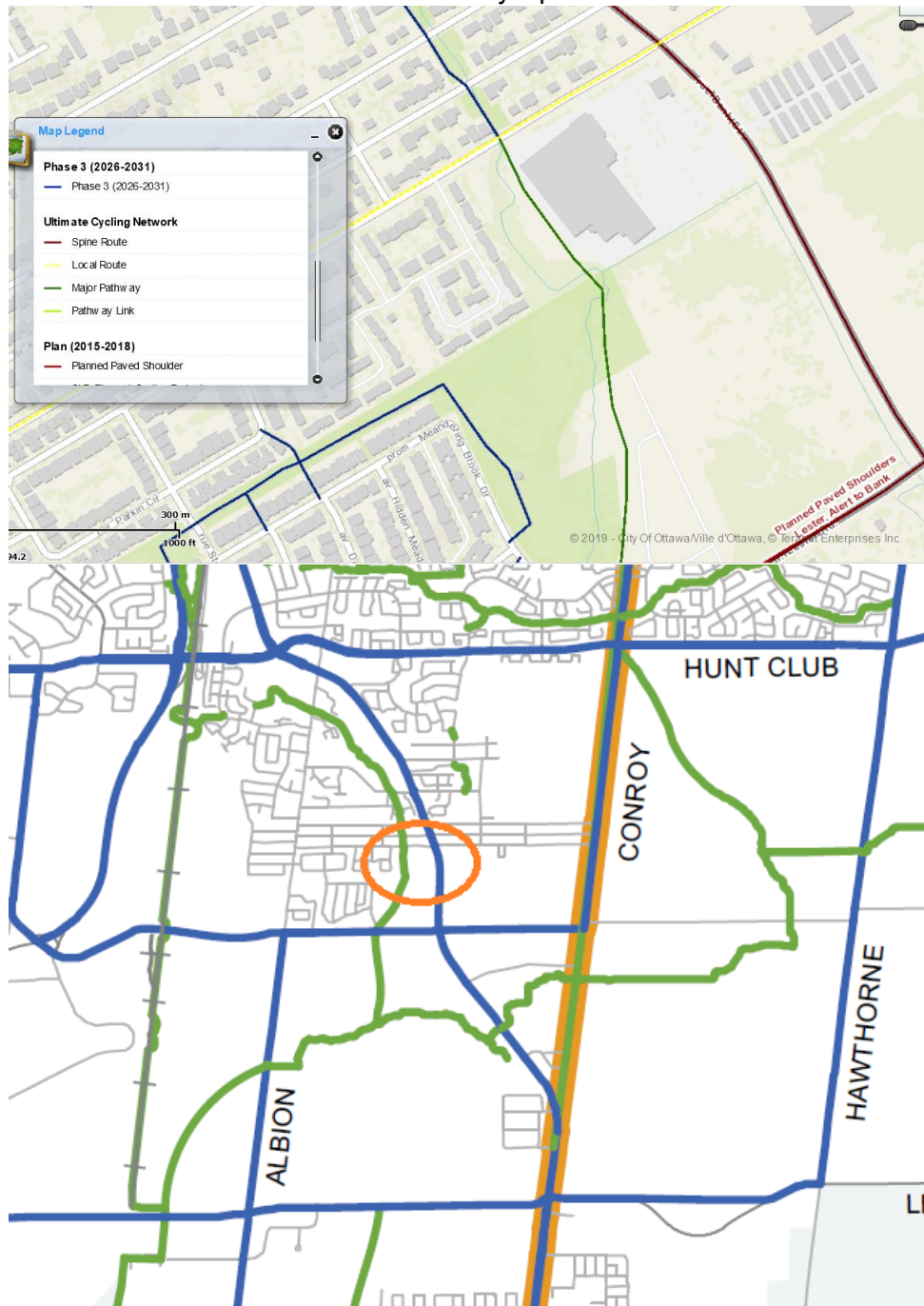
## Meeting Notes

### Meeting: July 19, 2019 – 4102E – 110 Laurier Avenue West – 10am

#### 2950 & 2960 Bank Street

In both of these examples (above) the sites face Bank Street. Specifically, for the 2<sup>nd</sup> example, the site was constrained with existing development but the amount of drive-through length fronting Bank Street was reduced, with landscaping placed for visual screening.

Another item for consideration is the City's plans to utilize the Sawmill Creek corridor.





## Meeting Notes

Meeting: July 19, 2019 – 4102E – 110 Laurier Avenue West – 10am

2950 & 2960 Bank Street

### PRIMARY NETWORK

Spine Route   
Major Pathway 

I would encourage the property owner to consider how their site may tie into this pathway in the future, however at this time I am unable to provide any assurances of the timeline for the construction the pathway project.

These notes should be received in addition to prior notes/details provided from the earlier pre-application consultation meeting.

Please feel free to contact me should you have any questions with the information provided.

Regards,



**Kelby Lodoen Unseth** MCIP, RPP

Planner II | Urbaniste II

Development Review (South Services) | Examen des projets d'aménagement (services sud)

Planning, Infrastructure and Economic Development | Services de planification, d'infrastructure et de développement économique

City of Ottawa | Ville d'Ottawa

☎ 613.580.2424 ext./poste 12852

[ottawa.ca/planning](http://ottawa.ca/planning) / [ottawa.ca/urbanisme](http://ottawa.ca/urbanisme)

**APPENDIX B**  
Natural Heritage Features





## Legend

- Assessment Parcel
- Woodland
- Conservation Reserve
- Provincial Park
- Natural Heritage System
- Ecoregion

### Wetland

- Provincially Significant Wetland Evaluated
- Non - Provincially Significant Wetland Evaluated
- Unvalued Wetland

### Area of Natural Heritage & Scientific Interest (ANSI)

- Provincially Significant Life Science ANSI
- Provincially Significant Earth Science ANSI

### Greenbelt Plan

- Boundary
- River Valley Connections

### Land Use Designations

- Protected Countryside
- Towns and Villages
- Hamlets
- Urban River Valley
- Specialty Crop Area

### Niagara Escarpment Plan (NEP)

- Boundary
- Parks and Open Space System

### Land Use Designations

- Escarpment Natural Area
- Escarpment Protection Area
- Escarpment Rural Area
- Mineral Resource Extraction Area
- Escarpment Recreation Area
- Urban Area
- Minor Urban Centre

### Oak Ridges Moraine Conservation Plan (ORM)

- Boundary

### Land Use Designations

- Natural Core Area
- Natural Linkage Area
- Countryside Area
- Rural Settlement
- Palgrave Estates Residential Community
- Settlement Area

0.3 0 0.16 0.3 Kilometers



This map should not be relied on as a precise indicator of routes or locations, nor as a guide to navigation. The Ontario Ministry of Natural Resources and Forestry (OMNRF) shall not be liable in any way for the use of, or reliance upon, this map or any information on this map.

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**APPENDIX C**

Site Plan

Dated December 9, 2019, prepared by KWC

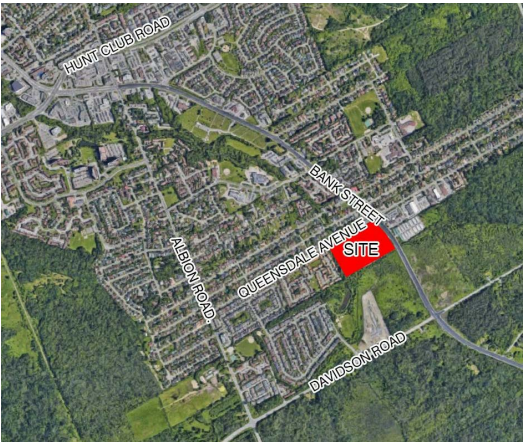


Municipality: City of Ottawa  
Municipal Address: 2950 Bank Street  
Registered Owner: Blossom Park Retail Centre Inc.  
Lot Area: 58,674 sq.m. (631,561 sq.ft.) (14.5 acres)  
Zoning Analysis  
Ottawa  
Zoning By-law: 2008-250  
Zone: AM1 H(30)  
Proposed Use: Retail Store; Drive-Through Facility; Restaurant (Shopping Centre)

Building Areas	Gross (out-to-out)	
	Sq.m.	Sq.ft.
Existing Shopping Centre	13,022	140,167
Existing Bank Building	422	4,542
Existing Development	13,444	144,709
Proposed Building 'A'	692	7,450
Proposed Building 'B'	975	10,500
Proposed Development	1,668	17,950
Grand Total	15,112	162,659

Development Standards		
	Required	Provided
Minimum Lot Area	No min.	58,674 sq.m.
Minimum Lot Width	No min.	194.6m
Minimum Required Yard		
Front Yard (min.)	No min.	6m
Corner Side Yard (min.)	No min.	14.5m
Interior Side Yard (min.)	No min.	5m
Rear Yard (min.)	7.5m	69.3m
Maximum Building Height	30m	10m
Maximum Floor Space Index	2**	0.26

Parking, Loading, Queuing		
Parking Spaces (2.6m x 5.2m)	Required: 544	Area C Schedule 1A
Shopping Centre (3.6 spaces/100 sq.m.)	Provided: 547	
Accessible Parking	Required: 13	(6 Type 'A'; 3.4m wide; 7 Type 'B'; 2.6m wide)
Bicycle Parking (0.6m x 1.8m)	Required: 21	(17 Existing; plus 2 Type 'A' & 2 Type 'B')
Shopping Centre: 1/500 sq.m.)	Provided: 38	
Loading Spaces (3.5m x 7.0m)	Required: 2	(Table 113A)
	Provided: 3	(Existing)



SITE PLAN LEGEND	
ICON	DESCRIPTION
[Symbol]	Existing Concrete
[Symbol]	Proposed Concrete
[Symbol]	Existing Landscape Area
[Symbol]	Proposed Landscape Area
[Symbol]	Existing Paver Walkway
[Symbol]	Parking Lines to be Repainted
[Symbol]	Watercourse
[Symbol]	Existing Buildings
[Symbol]	Proposed Buildings
[Symbol]	Existing Concrete Curb
[Symbol]	Proposed Concrete Curb
[Symbol]	Overhead Wires
[Symbol]	Property Lines
[Symbol]	Road Lanes
[Symbol]	Fence
[Symbol]	Signage
[Symbol]	BF Parking Space
[Symbol]	6m Wide Fire Route (12m centreline radius on all turns, TYP.)
[Symbol]	Principal Entrance Door
[Symbol]	Exterior Door ("OH" indicates Overhead Door)

SITE PLAN NOTES	
Note #	Note Text
(E)B	Existing Bolard
(E)BR	Existing Bike Rack
(E)CB	Existing Catch Basin
(E)CB/MH	Existing Catch Basin / Manhole
(E)CLF	Existing Chainlink Fence
(E)CP	Existing Concrete Pad
(E)CSW	Existing Concrete Sidewalk
(E)CV	Existing Culvert
(E)DS	Existing Directional Sign
(E)FDC	Existing Fire Department Connection
(E)FH	Existing Fire Hydrant
(E)GM	Existing Gas Meter
(E)LS	Existing Light Standard
(E)MH	Existing Manhole
(E)SS	Existing Stop Sign
(E)T	Existing Tree to remain
(E)TC	Existing Trash Compactor
(E)TCB	Existing Traffic Control Box
(E)TF	Existing Transformer
(E)TR	Existing Traffic Light Pole
(E)UP	Existing Utility Pole
(E)WC	Existing Well Cap
(E)WV	Existing Water Valve
BR	Bike Rack, See Landscape Plan
BRK1	Brick Veneer Williamsburg MKII
BRK2	Brick Veneer 'Sierra Sandstone'
BSC	Brick Soldier Course, 'Sierra Sandstone'
CC	Concrete Curb
CL	Clear Vision Glass
CSW	Concrete Sidewalk
CW	Crosswalk, Painted Lines
DTO	Drive-Thru Order Board
DTW	Drive-Thru Window
EIFS	EIFS Cornice, Colour - TBD
EIFS	Exterior Insulating Finish System: Colour - TBD
FH	Fire Hydrant
GB	In-Ground Garbage Bins
LI	Landscape Island
LS	Light Standard
MF	Metal Cap Flashing, Colour Matching EIFS
SB	Illuminated Sign Box
SGL	Spandrel Glass, Colour - TBD
SS	Stop Sign

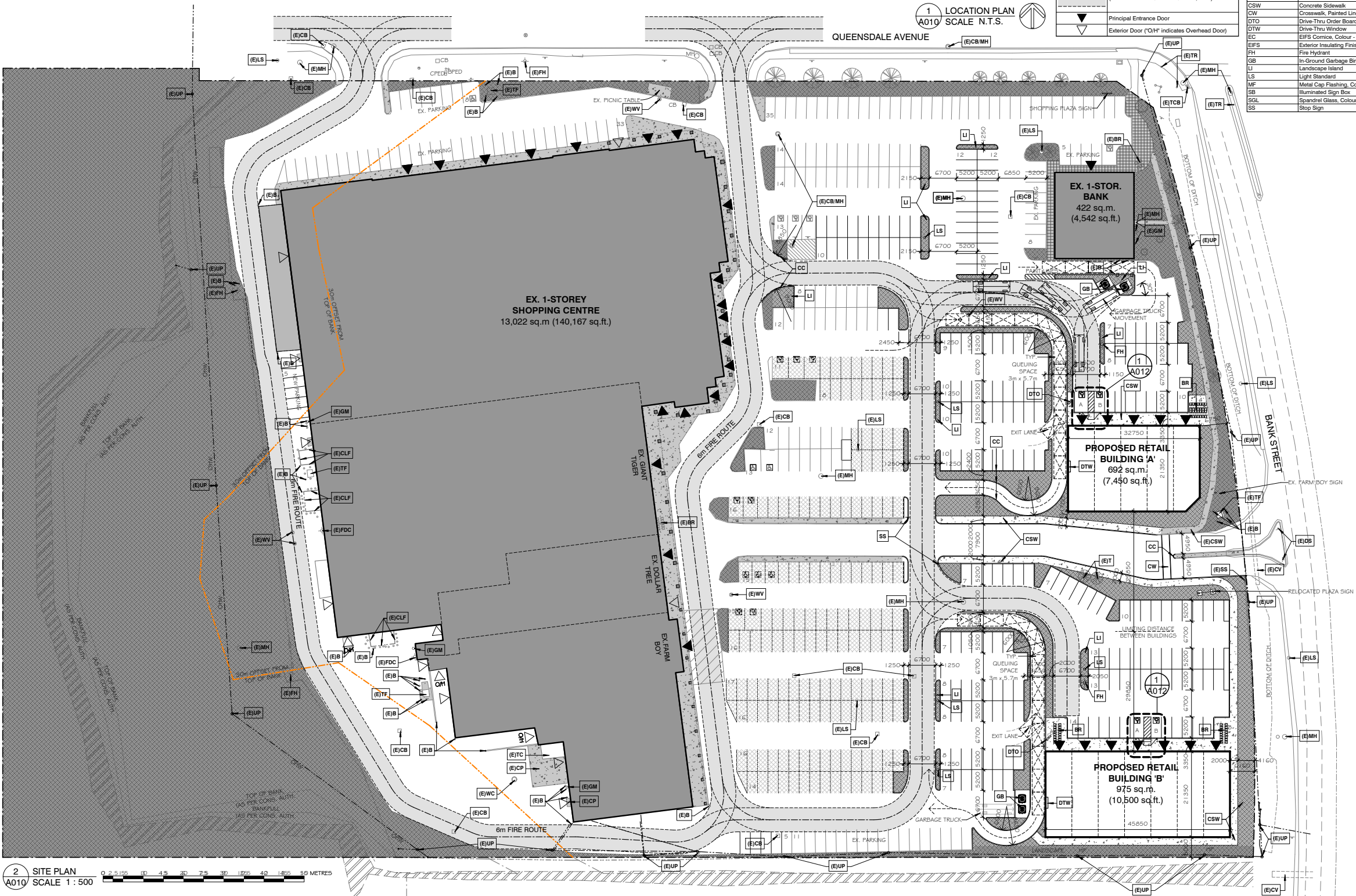
NOTES:

Contractor shall check and verify all dimensions on site and report any discrepancies to the Architect before proceeding.

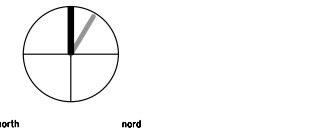
GENERAL SITE PLAN NOTES:

- Exterior site lighting shall be directed onto the site away from adjacent properties. See Electrical Drawings.
- Read this drawing in conjunction with the Landscape Drawings, Civil Engineering Drawings and Electrical Drawings.

plot scale 1:1



0	ISSUED FOR COORDINATION	09 DEC 2019
no.	revision	date



KWC  
383 Parkdale Avenue, Suite 201  
Ottawa Ontario Canada K1Y 4R4  
KWC ARCHITECTS INC.  
PHONE (613) 238-2117  
FAX (613) 238-6595  
E MAIL kwc@kwc-arch.com

detail no.	1	détail no.
sheet no.	A1	feuille no.

project  
NEW FREESTAND  
RETAIL BUILDINGS  
BLOSSOM PARK  
SHOPPING CENTRE  
2950-2960 BANK STREET  
OTTAWA, ON.

designed by conv' par	KWC	approved by approved par	
drawn by dessiné par	AK	project no. no. du projet	1849
date	09 DECEMBER 2019	scale	as noted
drawing / dessin			

SITE PLAN

revision révision	sheet no. no. de la feuille	A010
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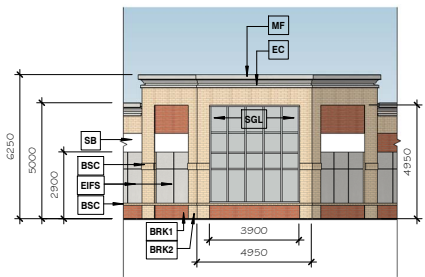


**APPENDIX D**

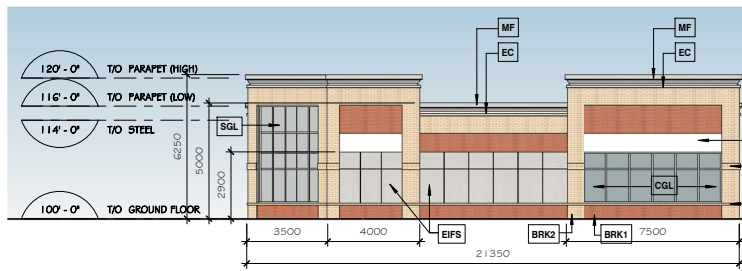
Elevation Drawings

Dated November 5, 2019, prepared by KWC

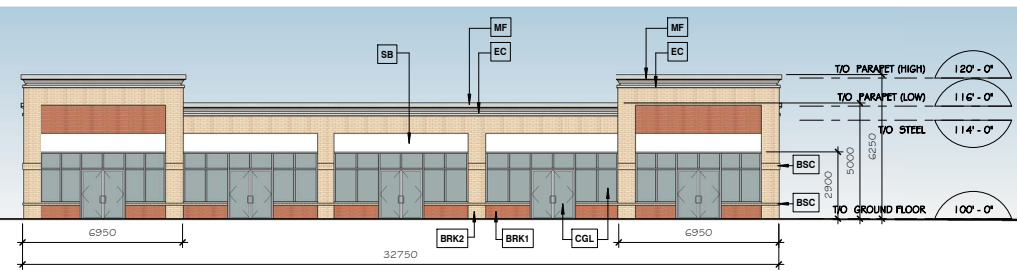
plot scale 1:1



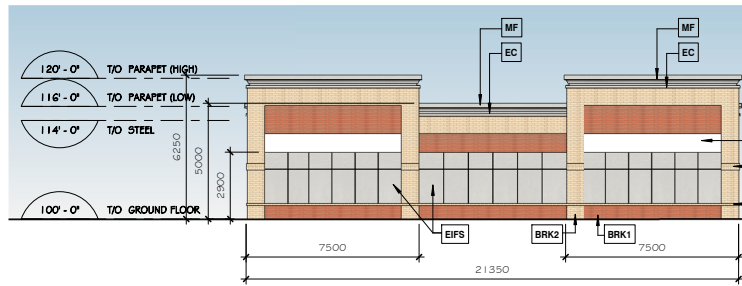
2a SOUTHEAST ELEVATION - BUILDING 'A'  
A013 SCALE 1:150



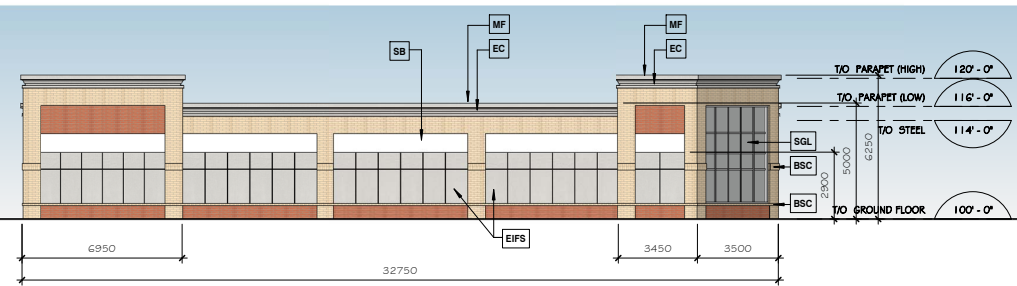
2 EAST ELEVATION - BUILDING 'A'  
A013 SCALE 1:150



1 NORTH ELEVATION - BUILDING 'A'  
A013 SCALE 1:150



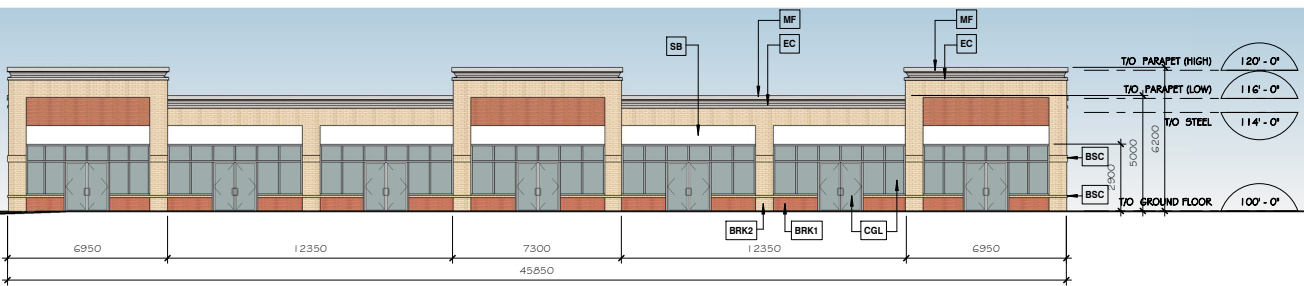
4 WEST ELEVATION - BUILDING 'A'  
A013 SCALE 1:150



3 SOUTH ELEVATION - BUILDING 'A'  
A013 SCALE 1:150



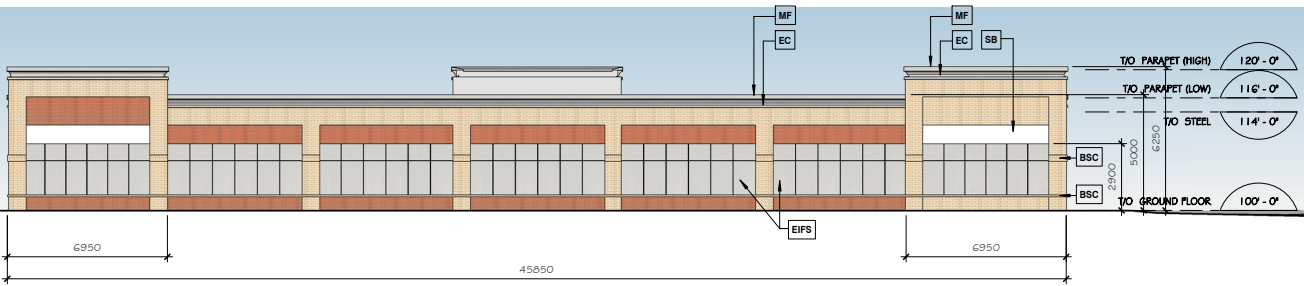
6 EAST ELEVATION - BUILDING 'B'  
A013 SCALE 1:150



5 NORTH ELEVATION - BUILDING 'B'  
A013 SCALE 1:150



8 WEST ELEVATION - BUILDING 'B'  
A013 SCALE 1:150



7 SOUTH ELEVATION - BUILDING 'B'  
A013 SCALE 1:150



9 BANK ST. STREETSCAPE ELEVATION  
A013 SCALE 1:250

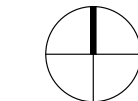
## NOTES:

Contractor shall check and verify all dimensions on site and report any discrepancies to the Architect before proceeding.

### Elevation Notes

Note #	Note Text
BRK1	Brick Veneer, Williamsburg MKII
BRK2	Brick Veneer, Sierra Sandstone
BSC	Brick Soldier Course, 'Sierra Sandstone'
CGL	Clear Vision Glass
EC	EIFS Cornice, Colour - TBD
EIFS	Exterior Insulating Finish System, Colour - TBD
MF	Metal Cap Flashing, Colour Matching EIFS
SB	Illuminated Sign Box
SGL	Spandrel Glass, Colour - TBD

0	ISSUED FOR COORDINATION	06 NOV 2019
no.	revision	date



north nord

**KWC**  
383 Parkdale Avenue, Suite 201  
Ottawa Ontario Canada K1Y 4R4  
**KWC ARCHITECTS INC.**  
PHONE (613) 238-2117  
FAX (613) 238-6595  
E MAIL kwc@kwc-arch.com

detail no.	1	détail no.
sheet no.	A1	feuille no.

project  
projet  
**NEW FREESTAND  
RETAIL BUILDINGS  
BLOSSOM PARK  
SHOPPING CENTRE**  
2950-2960 BANK STREET  
OTTAWA, ON.

designed by conçu par	KWC	approved by approuvé par	
drawn by dessiné par	AK	project no. no. du projet	1849
date	05 NOVEMBER 2019	scale	as noted

drawing / dessin

## BUILDING ELEVATIONS

revision  
révision

sheet no.  
no. de la feuille

**A013**



**APPENDIX E**

Urban Design Guidelines for Drive-Through Facilities  
Review of Conformity

City of Ottawa Urban Design Guidelines for Drive-Through Facilities		Comply	Non-Comply	N/A or TBC	Comment
<b>Streetscape and Built Form</b>					
1	Respond to the positive elements of the context through such means as building height, setbacks, building orientation and architectural styles (Figures 1 and 2)	●			Three pads in front provide some continuity in framing the right of way, while also providing good visibility to uses within the existing plaza/mall.
2	Locate buildings close to the street to help define the street edge.	●			Proposed buildings are at the street.
3	Provide ample landscaping, in combination with building orientation, to enhance the streetscape and define the street edge when setting buildings back from the street is unavoidable (Figure 3).	●			The proposed landscape areas effectively frame the new buildings.
4	Provide weather protection at the main building entrance, for areas close to public transit stops, bicycle parking, walkways, and in places with pedestrian amenities.		●		<i>Awnings are not proposed.</i>
5	Locate public amenities close to the building entrances (Figure 4).	●			There are no public amenities, per se, but bicycle parking racks are located where building entrances will likely be chosen. The buildings are located in close proximity to the public right-of-way; however, sidewalks (walkways) are within the subject lands.
6	Locate interior uses such as seating areas, employee rooms, offices, waiting areas and lobbies, which have the potential for clear windows, along street-facing walls (Figure 5).			●	Interior elements will be decided upon once tenants have been identified, etc.
7	Make the majority of the pedestrian level façade facing the street highly transparent with clear glass windows and doors that animate public streets and maximize views in and out of the building.	●			Clear glass windows employed for functional facades. Simulated fenestration incorporated along Bank Street and at rear of buildings.
8	Landscape the area in front of blank walls that face public streets and use projections, recesses, arcades, awnings, colour and texture to reduce the visual size of any unglazed walls.	●			Landscaping areas are shown adjacent to walls that are likely to be blank.
9	Coordinate architectural detail and character within an overall design concept for all building sides and components (Figure 6).	●			Proposal is compatible with existing built form.
<b>Pedestrians and Cyclists</b>					
10	Provide an unobstructed 2.0 metre wide sidewalk in the public right-of-way, across private access driveways. Ensure little or no change in elevation (Figure 7).			●	There is no public sidewalk. Walkways are private and are secondary to parking lot vehicle circulation.
11	Distinguish walkways from driving surfaces by using varied paving treatments and by raising walkways to curb level (Figure 8).	●			Walkways are either concrete or painted sidewalks. To be determined if raising the grades of select sidewalks is practicable from a drainage standpoint.
12	Provide customer entrance doors that are close to parking areas (Figure 9).	●			Parking areas are in close proximity to the front façade of the proposed buildings.
13	Provide customer entrance doors clearly visible from public streets and directly accessible from the public sidewalk (Figure 9).	●			Customer entrances will be visible to southbound traffic.
14	Provide an unobstructed 2.0 metre wide pedestrian walkway between the public sidewalk (and/or parking areas) and building entrances.	●			There is no public sidewalk. However, there is a private walkway that runs adjacent and parallel to Bank Street and walkway connections exist from that walkway to the front façade of the proposed buildings.
15	Use landscaping or similar means to delineate pedestrian walkways and pedestrian access to the buildings.	●			Landscaping is paired with walkways throughout the site where possible.

City of Ottawa Urban Design Guidelines for Drive-Through Facilities		Comply	Non-Comply	N/A or TBC	Comment
16	Locate required bicycle parking close to the building entrance in a manner that does not impede pedestrian movement.	●			Bicycle parking racks are located where building entrances will likely be chosen.
<b>Vehicles and Parking</b>					
17	Locate surface parking areas and stacking lanes at the side or rear of buildings. (Figures 10).	●			Surface parking is located towards the sides or rear of the buildings and stacking lanes are located at the rear of the proposed buildings.
18	Minimize the number and width of driveways from the public street while ensuring they meet the requirements of the Private Approach By-law (Figure 11).	●			No new driveways are proposed.
19	Locate vehicular access points to the sites as far away as possible from street intersections. Locate vehicle access points to corner sites on the secondary street			●	No new access points are proposed.
20	Locate stacking lanes away from adjacent sensitive uses, such as residential and outdoor amenity areas, to reduce the impacts of noise and pollution that could be caused by stacking cars on such uses. Use landscaping and fencing to help buffer potential impacts.	●			The lands to the south are those most likely to be impacted by a drive-through. Those lands are zoned DR - Development Reserve Zone and it does not appear as though there is an active development application.
21	Locate the start point to the stacking lane at the rear of the site so that queued vehicles do not block traffic along the public streets or the movement of other vehicles on site (Figure 13).	●			Stacking lane entrances are not in close proximity to either point of access.
22	Avoid locating the stacking lane between the building and the public street.	●			Stacking lanes are behind the proposed buildings.
23	Provide escape lanes and the appropriate number of queuing spaces as required by the Zoning By-law to create efficient stacking lanes and to minimize on-site conflicts (Figures 13 and 14).	●			The proposed uses are not yet known; however, a total of eleven queuing spaces are shown at each drive through. Escape lanes are shown.
24	Separate stacking lanes from parking areas and driveways using landscaped islands, decorative pavement, pervious islands and painted lines (Figures 15 and Figure 16).	●			Stacking lanes are separated from parking areas and driveways.
25	Design the on-site circulation to minimize the conflicts between pedestrians and vehicles.	●			On-site circulation has been designed to minimize conflicts between pedestrians and vehicles.
26	Provide separate stacking lanes when two drive-through uses exist on the same site.	●			Separate stacking lanes are provided for the drive-throughs.
27	Provide only the minimum number of parking spaces required by the Zoning By-law.	●			Parking provided is approximately equal to the number of parking required (proposed surplus of approximately 0.2%)
<b>Landscape and Environment</b>					
28	Plant street trees between 7.0 to 10.0 metres apart along public streets. Plant trees in permeable surface areas, with approximately 10.0 square metres of soil area per tree (Figure 17).	●			Street trees appear to be placed approximately 7 metres apart along Bank Street.
29	Select trees, shrubs and other vegetation considering their tolerance to urban conditions, such as road salt and heat. Give preference to native species of the region that are of equal suitability.	●			Please refer to Landscape Plan.
30	Provide a minimum 3.0 metre wide landscape area along the edge of a site where parking areas, drive lanes or stacking lanes are adjacent to a public street. Use trees, shrubs and low walls to screen cars from view while allowing eye level visibility into the site (Figures 18 and 19).	●			3.0 metre wide landscape area provided along the front property line in the vicinity of the proposed drive throughs.

City of Ottawa Urban Design Guidelines for Drive-Through Facilities		Comply	Non-Comply	N/A or TBC	Comment
31	Provide a minimum 2.5 metre wide landscape area along the site's side and rear yards in order to provide screening and enhance site environmental benefits (Figure 20).	●			Landscaped areas provided are greater than 2.5 metres wide.
32	Provide a minimum 3.0 metre wide landscape area, which may include a solid wall or fence in addition to planting, at the edges of sites that are adjacent to residential or institutional properties.			●	The lands to the south are those most likely to be impacted by a drive-through. Those lands are zoned DR - Development Reserve Zone and it does not appear as though there is an active development application.
33	Protect and feature heritage, specimen and mature trees on the site by minimizing grade changes and preserving permeable surfaces.			●	It is not anticipated that natural heritage (incl. mature trees) will be impacted by the proposed development.
34	Use green building technologies such as green roofs, drip irrigation, and other Leadership in Energy and Environmental Design (LEED) approaches.			●	Methods to be considered during detailed design. Not relevant to drive throughs.
35	Use sodded areas and shrub beds to collect, store and filter stormwater in order to improve groundwater recharge (Figures 20 and 21).			●	Stormwater management methods to be considered during detailed design.
36	Divide large parking areas into smaller and well-defined sections using soft and hard landscaping in order minimize the amount of paved area (Figure 22).	●			Extensive paved areas are proposed owing to on-site parking requirements; however, landscaping islands are proposed throughout, as well as large landscaped areas towards Bank Street.
<b>Signs</b>					
37	Locate and design ground-mounted and wall-mounted signs to complement the character and scale of the area. Integrate landscape features with ground-mounted signs (Figure			●	Detailed signage not yet proposed.
38	Use pavement markings and directional signs to enhance clarity of movement patterns on site.			●	Pavement markings and directional signage to be proposed later.
39	Design buildings to include defined spaces to accommodate signs that respect building scale, architectural features, signage uniformity and established streetscape design objectives.			●	Detailed signage not yet proposed.
40	Restrict temporary and portable signs. Prohibit billboards, revolving signs and roof signs on private property.	●			Existing billboard to be removed.
41	Design sign illumination to be task-oriented and avoid glare/light spillover toward adjacent land uses.			●	Detailed signage not yet proposed.
<b>Servicing and Utilities</b>					
42	Design garbage enclosures that are external to the building with the same materials as the building and ensure that the wall height is sufficient to completely conceal garbage dumpsters (Figure 24).			●	To be considered. Not relevant to drive throughs.
43	Enclose all utility equipment within buildings or screen them from both public streets and private properties to the rear. These include utility boxes, garbage and recycling container storage, loading docks and ramps, and air conditioner compressors.			●	To be considered. Not relevant to drive throughs.
44	Locate noise-generating areas, including ordering board speakers, outdoor loading areas and garbage storage, away from sensitive uses such as residential areas and schools.			●	Ordering boards to be designed to minimize impact on adjacent lands/uses.
45	Buffer potential noise impacts with building structures, landscaped berms or attenuation fencing (minimum 1.8 metre in height) in front with landscaping.			●	Fencing to be considered.

City of Ottawa Urban Design Guidelines for Drive-Through Facilities		Comply	Non-Comply	N/A or TBC	Comment
46	Design lighting so that there is no light spillage, glare or light cast over adjacent uses. Direct and/or shield lighting sources away from adjacent residential properties and provide screening as necessary.			●	Site lighting to be considered during detailed design phase.
47	Use efficient white light sources to reduce energy costs and to create a natural colour balance for safety and security.			●	Site lighting to be considered during detailed design phase.
48	Plan the site to include areas for temporary snow storage without conflicting with site circulation, landscaping and utility boxes.			●	Approach for snow storage to be considered.
49	Provide views and clear sightlines between the site and surrounding uses to ensure sufficient safety and comfort levels (Figure 25).	●			The buildings have been designed with sightlines in mind and do not introduce safety concerns.

**APPENDIX F**

**Zoning Table**

Per City of Ottawa Zoning By-law 2008-250



Development Standard	Required	Provided
<b>Minimum Lot Area</b>	No min.	58,674 square metres
<b>Minimum Lot Width</b>	No min.	194.6 metres
<b>Minimum Required Yard</b>		
<b>Front Yard (min.)</b>	No min.	6 metres
<b>Corner Side Yard (min.)</b>	No min.	14.5 metres
<b>Interior Side Yard (min.)</b>	No min.	5 metres (not abutting residential zone)
<b>Rear Yard (min.)</b>	7.5 metres	69.3 metres (abutting residential zone)
<b>Maximum Building Height</b>	30 metres	10 metres (approximately – existing building)
<b>Maximum Floor Space Index</b>	2	0.26

Parking, Loading, Queuing	Required	Provided
<b>Parking Spaces</b>		
<b>Shopping Centre</b>	544 spaces	547 spaces
<b>Accessible Parking</b>	13 spaces	21 spaces
<b>Bicycle Parking</b>	30 spaces	38 spaces
<b>Loading Spaces</b>	2 spaces	3 spaces (existing)