

# PLANNING RATIONALE REPORT

## **994 BRONSON AVENUE**

PIN: 0410-30023

Lot 19, Registered Plan 91219 (BRONSON W.)

K1S 4H1

CITY OF OTTAWA

APPLICATION FOR SITE PLAN CONTROL

PREPARED BY

SMART LIVING CANADA

100 ARGYLE AVENUE, OTTAWA, ON, K2P 1B4

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# 1. INTRODUCTION

## 1.1 BACKGROUND

Smart Living Canada was retained by the owner to prepare a planning rationale in support of an application for the demolition of the existing structures on site, and the construction of a new residential complex at 994 Bronson Avenue.

This report will examine the existing conditions of the site along with the current community context of the Glebe - Dows Lake neighbourhood, examining its inherent qualities, and how they influence the proposed design. This report reviews the positive impacts of residential intensification and efficient design and construction which will further support the proposed construction on the site. The report will then proceed to explain the proposed development, through its original construction and occupation, to the current context and circumstances the lot addresses today. This report reviews the applicable land use planning policies outlined by the Provincial Policy Statement (PPS), Ottawa's Official Plan (OP), and the Ottawa Zoning By-law Provisions.

This report will indicate that the proposed development has taken into consideration all of the required policies to design and construct a residential building, and that the proposed construction, and all relevant applications will be vital to the success of the project.

## 1.2 DESIGN INTENT

The proposed development aims to demolish the existing two storey single detached house into a newly constructed three storey apartment residence. The apartment complex will house 13 units of comfortable living space, located in the mature neighbourhood on Bronson Avenue. The lot will provide ample space for urban intensification, while conserving the identity of the residential neighbourhood through the exterior use of materials, massing and scale.

In the Dows Lake neighbourhood, there can be found a variety of housing developments within clustered residential neighbourhoods, combining a variety of low density housing alongside mid to high-rise apartment complexes. The curvilinear avenues and roadways create a picturesque atmosphere, kept quiet and secluded from the arterial roadways that carry heavier traffic. This creates a region that encourages bicycle and pedestrian-friendly movement, private residential streets, and distinctive community character to the east of the Rideau Canal. It is the vision of Ottawa's urban planners to create communities with balanced development, using built form, greenspace and infrastructure to stimulate neighbourhood growth and activity, while maintaining the identity and character of the city's various neighbourhoods (*Urban Design and Compatibility 2.5.1*).

Numerous design principles that have been outlined by the Ottawa Official Plan have been considered by this project, to ensure that the whole building integrates into the community, and respects the established natural and built environment of the Glebe - Dows Lake neighbourhood.

With these guidelines for sustainable construction and design, the proposed construction will increase the amount of viable living space by optimizing the building envelope within the required setbacks outlined by the Ottawa Zoning Bylaws (Consolidation 2008-250), while maintaining an appropriate form of housing and land use for the neighbourhood. This new multi unit apartment residence will adhere to the prescribed community planning

guidelines and citywide urban development programmes to ensure a cohesion with the established mature neighbourhood.

## **2. PLANNING APPLICATIONS**

### **2.1 APPLICATION FOR SITE PLAN CONTROL**

The proposed development looks to achieve Site Plan Approval for the new construction and site demolition required at 994 Bronson Avenue. By the measures of Ottawa's Planning Act, a Site Plan Control would be required in this instance to ensure that the development meets the quality and standards approved by the City. The proposal looks to create 13 new dwelling units within the newly proposed 3 storey structure, after the removal of the existing single detached dwelling is approved.

### 3. EXISTING CONDITIONS

#### 3.1 COMMUNITY ATTRIBUTES

994 Bronson is located in the Capital Ward of downtown Ottawa, an area that comprises of the urban residential neighbourhoods of Heron Park, Old Ottawa South, Dows Lake and the Glebe. The region is bound by geographical and city infrastructure boundaries, encompassing the area between the Rideau River to the south, Dows Lake and Carleton University to the east, the Highway 417 to the north, and Riverside Drive to the east. The region is home to over 37 000 people, including areas of residential settlement, low density commercial uses, and expansive parkland.



**EXHIBIT A: MAP OF WARD 17 'CAPITAL' WARD - GLEBE DISTRICT**

The area known as the Glebe refers to the area once known as the Clergy Reserve lands, granted to the Presbyterian Church in the 1830s. Gradually over time, the area was developed into a residential neighbourhood after the 1870s. The primary use of the land was reserved for residential use, providing large sized lots along winding streets, with mature trees and ample room for landscaped exteriors. Wide avenues and mainstreets were maintained to ensure easy flow of traffic driving to and from the Ottawa city center, while the residential neighbourhoods beyond created cul-de-sacs, crescents and winding roads to generate privacy from heavy traffic and noise, and created uniquely shaped lots.

The majority of residential neighbourhood construction began during the 1920s and 30s, with newer commercial lots and high-rise developments being sparingly introduced after the 1970s. Most of the homes built during this time were intended for single family use, single detached or semi-detached houses, while row houses, duplexes and low-rise apartment buildings were introduced as residential infill over time. The arterial mainstreets tend to house



the more current developments and land uses, while the streetscapes between Bank Street, Bronson Avenue and Dows Lake are reserved for low density housing and larger estates.



**EXHIBIT B:** SITE CONTEXT ADJACENT PROPERTIES 993, 995 BRONSON AVE. E, 992 BRONSON AVE. W (GOOGLE STREETVIEW, 2016)

Ottawa census reveals that the primary demographic of the Glebe - Dows Lake region are single family units, or students and young professionals who live in shared living accommodations, such as rental duplexes, townhouses, or mid to high rise apartment dwellings. There are approximately 17 000 housing units to serve a population of around 37 000 citizens, primarily living in low density residential neighbourhoods.



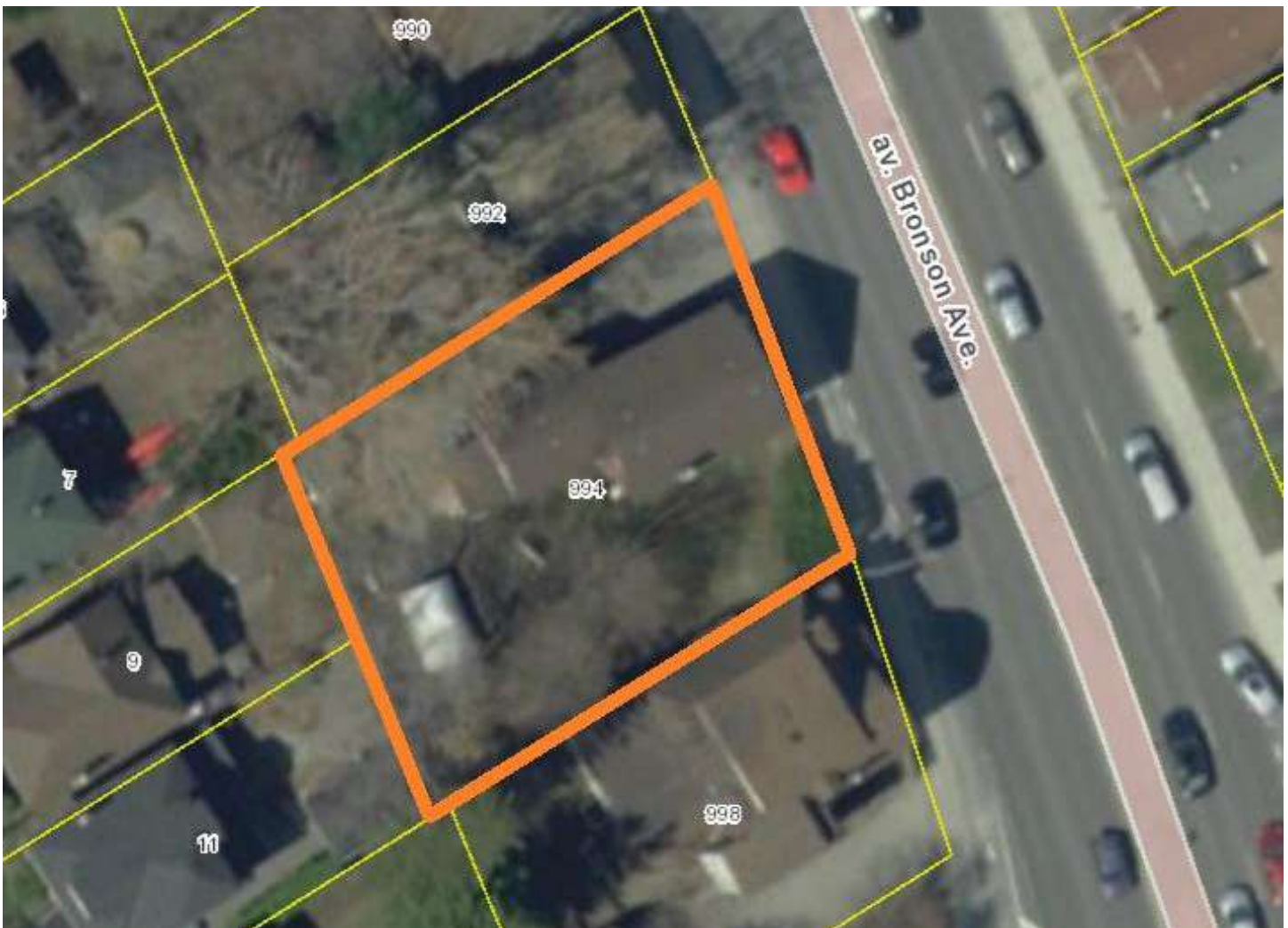
**EXHIBIT C:** SITE CONTEXT ADJACENT PROPERTIES 9, 11 LAKEVIEW TERR. & 998 BRONSON AVE. W (GOOGLE STREETVIEW 2016)

### 3.2 CURRENT SITE CONTEXT

The site at 994 Bronson Avenue is located on the western side of the roadway, between Holmwood Avenue and the Bronson Avenue Bridge that goes over the Rideau Canal. The closest major intersection is at Bronson Avenue and Holmwood Avenue, just a couple of lots to the north of the property. Bronson Avenue is a major arterial roadway that connects the south end of Ottawa to the downtown core and the highway.

The property parcel creates a rectilinear-shaped interior lot (No. 19, Plan 91219) that shares a rear property line with the residences facing onto Lakeview Terrace. The site has a road frontage of 22.98 meters, and extends a depth of

29.11 meters to the rear property line. To the north of the property on the same side of the street is 992 Bronson Avenue, and to the south is 998 Bronson Avenue, two lots of similar size and depth that have two storey single detached houses on the lot that predate 1928 (geoOttawa historical photos). To the west at the rear of the property are 9 and 11 Lakeview Terrace, a private residential road with comparably sized lots and residential structures of a similar age. To the east across the Bronson Avenue roadway are 991 and 993 Bronson Avenue, which also have mature homes of a similar age as well. a rectangular lot of similar size that is occupied by a two storey single detached single family residence. All of the immediately adjacent properties facing Bronson Avenue are of the same zoning (R4T), and are occupied by low density housing under 2 storeys. These houses all exhibit architectural styles of the late Victorian era (1870-1910), and have been either rented for rooming house purposes, or subdivided into apartments. The homes to the rear of 994 Bronson that face Lakeview are of a later construction era in the 1920s to the 1950s, exploring revivalist styles such as Edwardian and Tudor, constructing bungalow houses faced in brick and stucco.

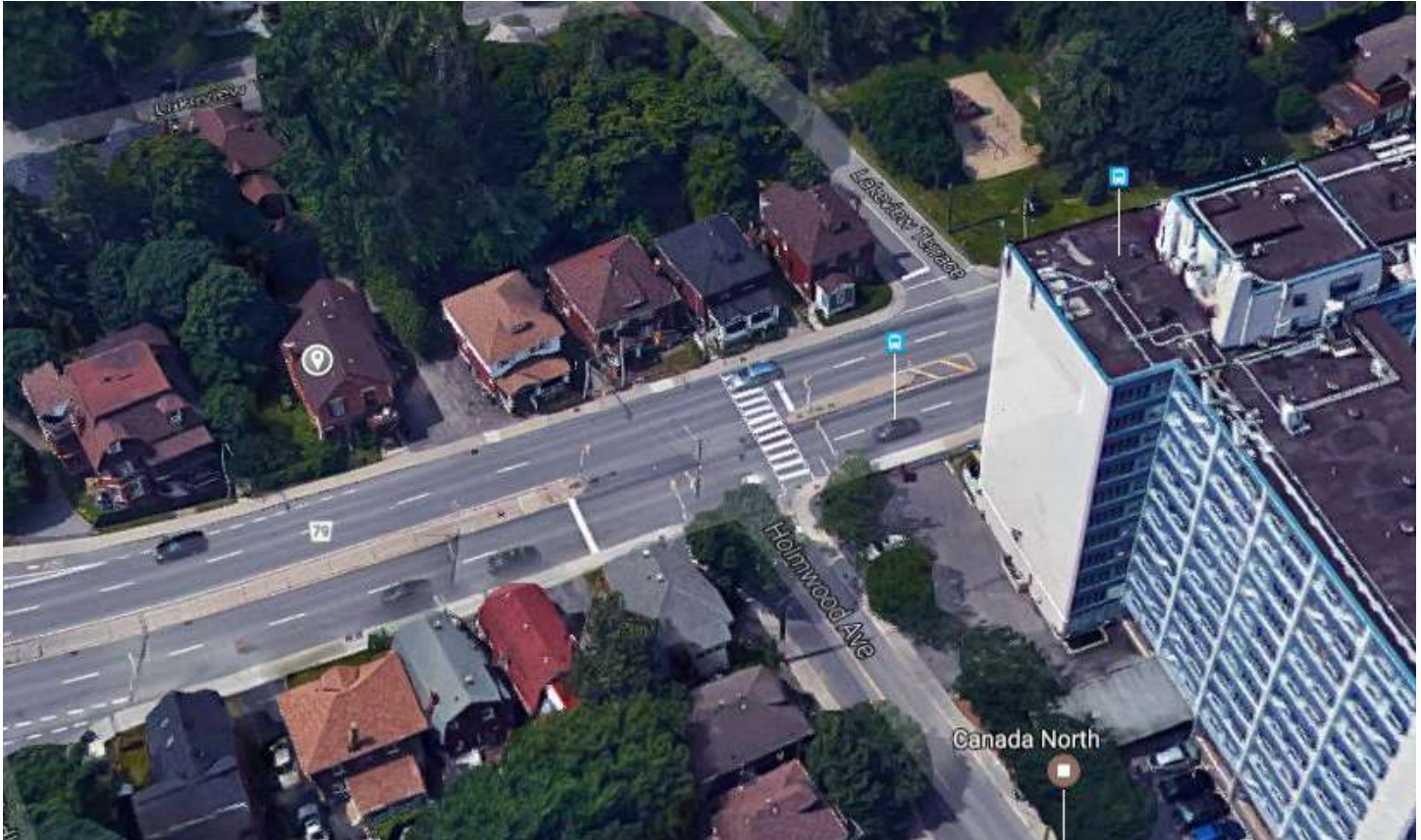


**EXHIBIT D: 994 BRONSON AVE. W - CURRENT SITE CONTEXT AERIAL VIEW, GEO OTTAWA 2014**

The site at 994 Bronson is currently occupied by a 2 storey single detached residence, faced in red brick, with a pitched gable roof. The approximate date of construction is the early 1900s, designed as a standard two storey craftsman-style home, that has since been renovated to house rental apartment units. The residence is surrounded by ample landscaped space, with mature trees in the rear yard, and an asphalt driveway along the northern property



line that extends the depth of the lot. There are no secondary uses on this site, and all residential functions are contained within the single structure.



**EXHIBIT E:** 994 BRONSON AVE. - CURRENT SITE CONTEXT BIRDSEYE VIEW (WEST), (GOOGLE EARTH 2016)

The existing residence on site matches the established pattern of land use and location within the lot, set back from the roadway 2.05 meters, pressing the entrance landing and stairs right up to the public sidewalk. The rear yards are deep and landscaped, allowing for the retention of mature deciduous trees, while the front yard offers little or no vegetation to act as a screen from the noise generated by the passing traffic. The use of natural vegetation and the preservation of mature trees and shrubbery are integral to maintaining the privacy of the older established residential neighbourhood as it travels away from the main arterial roadway.



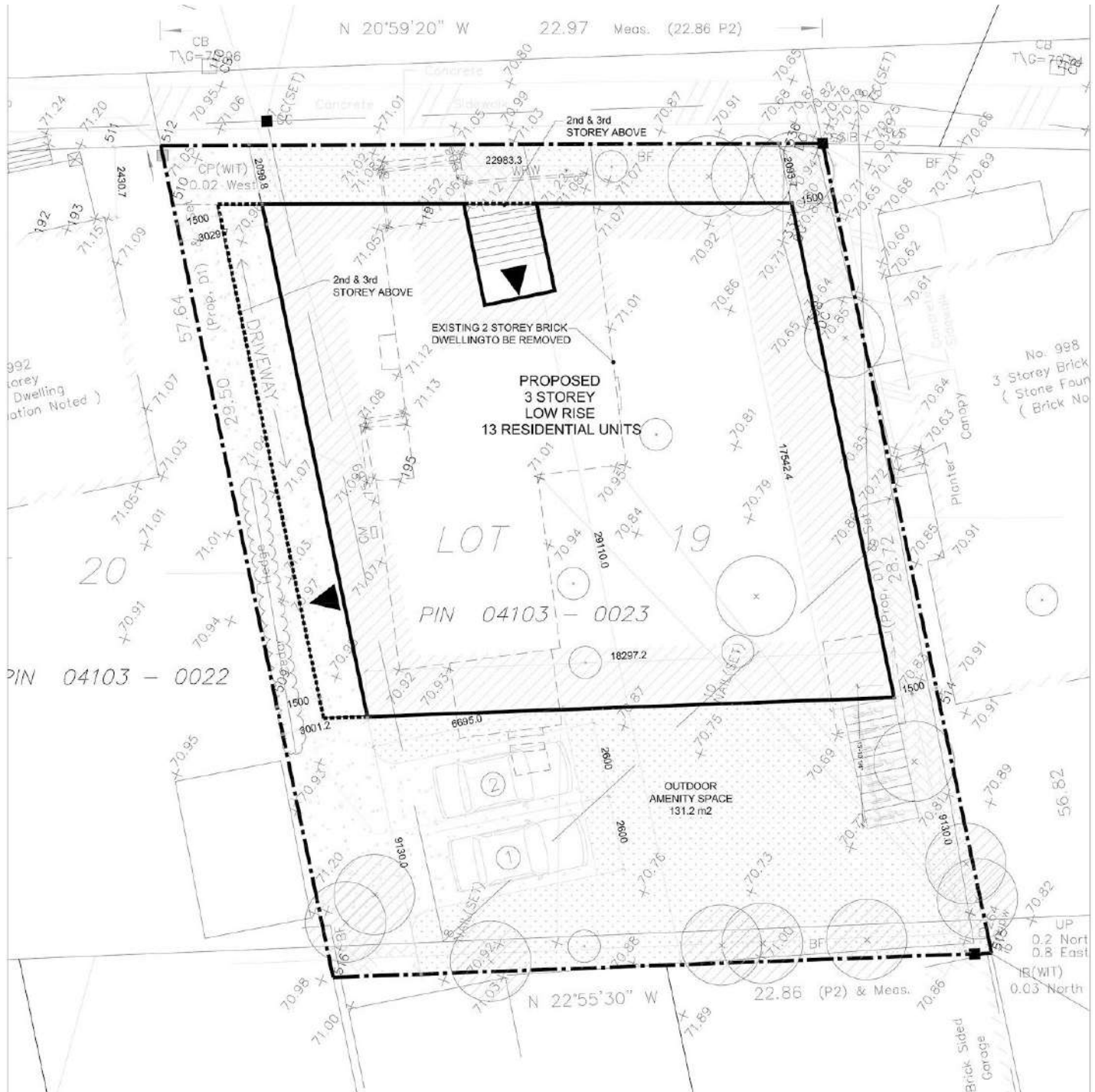
**EXHIBIT F:** 994 BRONSON AVE. - CURRENT SITE CONTEXT STREET VIEW, (GOOGLE EARTH 2016)



## 4. DEVELOPMENT PROPOSAL

### 4.1 DESIGN SUMMARY

The proposed construction at the site of 994 Bronson Avenue looks to remove the existing structure on site to construct an all new three storey apartment complex that will house 13 residential units. The residence will aim to assimilate into the existing mature neighbourhood through the use of familiar scale, height, and exterior cladding, while affording the residents a more contemporary form of living in the established mature Glebe community.



**EXHIBIT G: 994 BRONSON AVENUE, PROPOSED SITE PLAN, SOMA STUDIOS 2016**

## 4.2 PROPOSED OCCUPANCY

The residence is intended for the use of rental tenants within the thirteen units over three floors. The lot would look to accommodate outdoor parking for two vehicles, while developing the rest of the site to house a building within the prescribed setbacks of the R4T Zone (*Ottawa Bylaws Consolidation 2008-250*).

## 4.3 BUILDING FEATURES

This all new construction will create an apartment complex that contributes to the overall character and appearance of the Bronson Avenue streetscape, identifying key elements of construction and land use that will benefit the area by creating affordable residential units within the established neighbourhood

The three storey apartment complex will follow the rectilinear form of the lot itself, angling the exterior walls to align with the north and south property lines, and creating a rhombus-shaped building footprint. This will create dynamic, unique interiors while squaring the facade of the building to the roadside. The levels at the basement and grade set the building in from the northern interior property line by three meters to accommodate enough width for a driveway accessing the rear yard amenity space and parking spaces. The floors above the ground floor cantilever over this driveway, maximizing the interior living space above, while remaining within the prescribed 1.5 meter setback at the interior side yard. The building is also aligned to gather as much natural light from the east-west

The building has an elevated basement slab, allowing for larger windows to the basement at the facade, and eliminating the necessity for window wells around the building footprint. This will create a greater amount of natural light for the proposed basement residential units, while also elevating the ground floor units above the eye level of passing pedestrians and traffic for additional privacy and noise reduction. The front entrance recessed away from the public roadway, creating an outdoor stair and porch landing. This creates a symmetrical alignment to the front facade, which divides the building exterior into three proportionate vertical sections. The depth of the front entrance away from the public sidewalk generates visual interest, and recalls a similar appearance of other historic residences built during the Edwardian or Victorian era of Ottawa architecture.

The height of the building encourages a human scale found consistently within the Glebe, which will not overshadow the existing structures on any of the adjacent properties. The flat roof will be available to the use of the tenants as a shared outdoor space. The roof will allow for a sizeable deck that still ensures a comfortable setback from the ledge of the roof, to reduce noise transfer and afford privacy.

The lot will continue to preserve the existing mature trees and shrubbery on site where possible. The rear yard will retain the existing deciduous trees along the rear property line to maintain a natural privacy screen with the adjacent R1 Zone behind the structure. This will prevent noise transfer, generate natural screens, and create partial shade for the residential units facing the rear yard. The depth of the rear setback at 9.13 meters will provide ample room for outdoor amenity space at grade, while preserving the existing vegetation from the proposed site excavation.

The site will provide two outdoor parking spaces in the rear yard for vehicular access, and an area designated for 7 bicycle parking spaces which are hidden from view at the roadside. All tenant storage facilities and residential garbage storage containers will be housed within the building to prevent odors from carrying, and providing a clear and organized exterior.

## 4.4 MATERIALITY

The newly constructed structure will be clad in materials that assimilate with the architectural culture and atmosphere of the Glebe, using common or traditional materials to relate the building to the fabric of the neighbourhood. The central front entrance will be made a visual focal point, using metal panel to clad the recessed entranceway all the way to the roofline. The building will be horizontally clad in various materials to create visual interest which proportions the building according to the number of storeys.

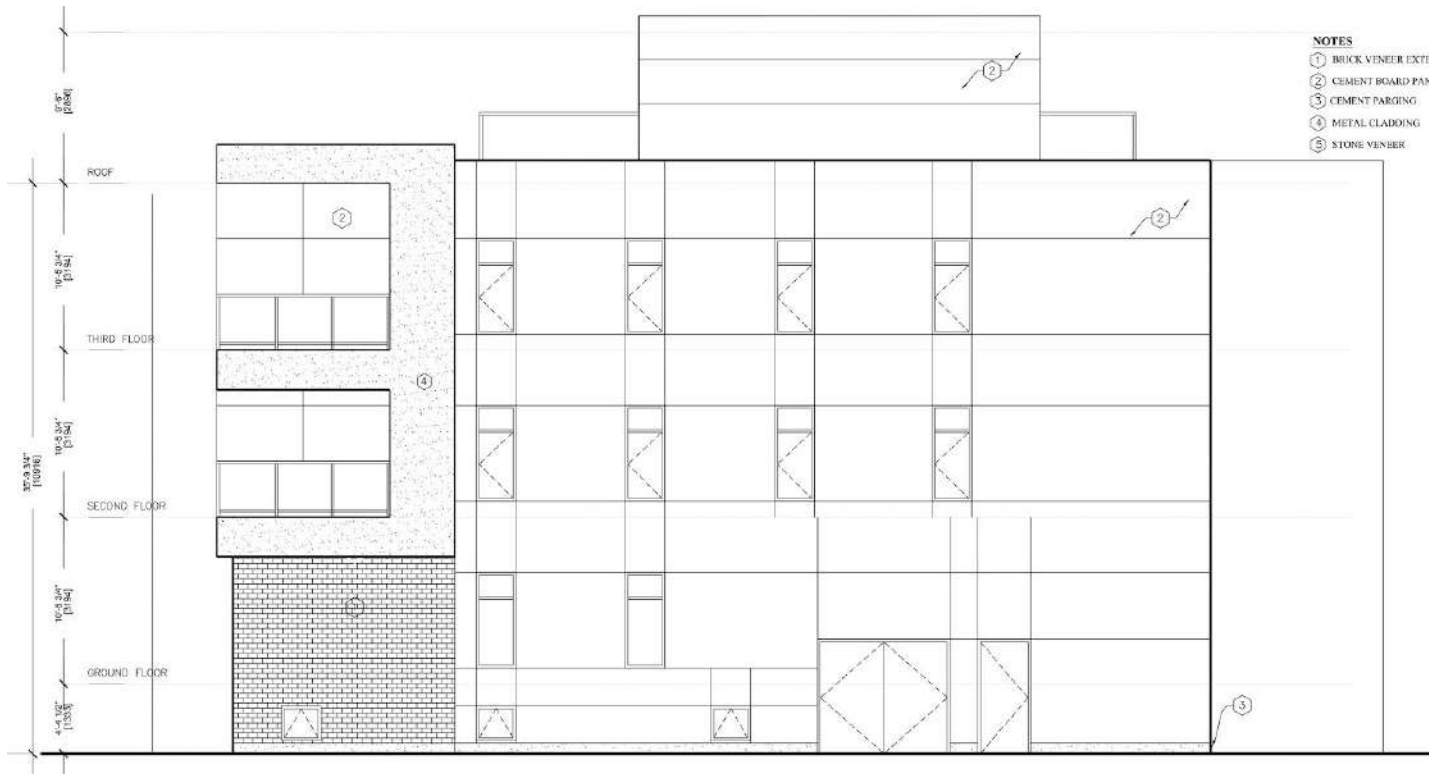
The ground floor at the front facade will be clad in red brick masonry from grade to the first storey, which will extend around the northern facade under the cantilevered floor to a depth of roughly ten feet. This will give the impression from the street view that the brick wraps around the entire exterior of the ground floor, in order to relate the new construction to the appearance of the adjacent brick clad houses. The second and third floors at the front facade will be clad in a combination of exterior metal panels, and exterior cement board siding, emphasizing the roadside facing windows. The exterior cladding for the rest of the building at the interior facades and rear elevation will be clad in exterior cement board paneling, for economizing the exterior, while using a unitary cladding system that is insulated and sustainable. Accent trim and juliet balconies will contribute visual interest to the exterior, breaking up the massing and flat plane of the exterior.

The architectural style reminisces about the architectural vernacular of the Glebe in new materials and complementary forms. The unique shape of the building creates a dynamic angled view of the front facade, while remaining square to the sidewalk. The mass of this building does not overpower the adjacent properties, and is not out of proportion or height with the streetscape. The proposed three storey apartment building will be a welcome addition to the Bronson Avenue residential neighbourhood.

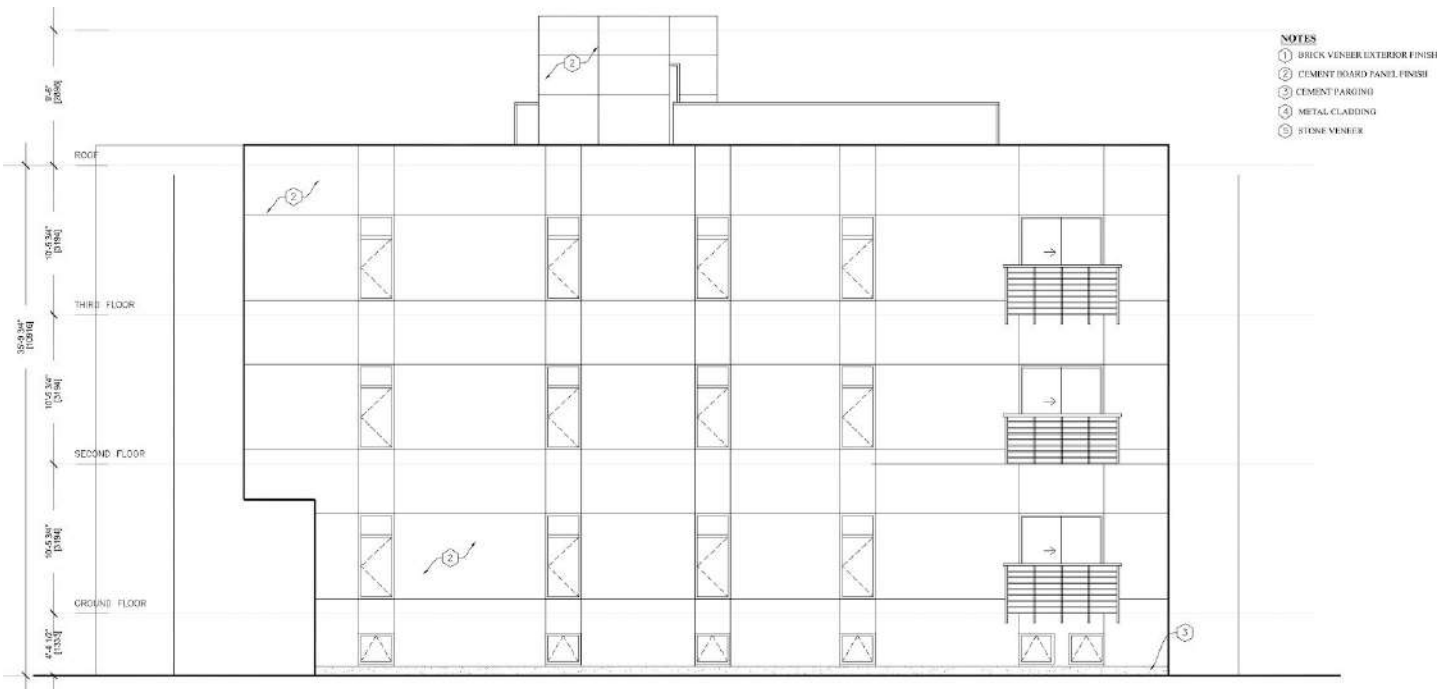


**EXHIBIT H: 994 BRONSON AVENUE - PROPOSED FRONT ELEVATION (EAST), SOMA STUDIOS 2017**





**EXHIBIT I: 994 BRONSON AVENUE - INTERIOR SIDE ELEVATION (NORTH), SOMA STUDIOS 2017**



**EXHIBIT J: 994 BRONSON AVENUE - REAR ELEVATION (WEST), SOMA STUDIOS 2017**

## 5. LEGISLATION FRAMEWORK

The following paragraphs are the policies and guidelines that this project and all Ontario development must adhere to. The governing bodies of each Planning Act set forth the regulations for sustainable forms of construction by enforcing sensible land use, and projects of varying density and intent, to ensure a safe, quality living environment. Each subsection will express the policy in direct phrasing or paraphrasing, which has aided in guiding and informing the choices made regarding the proposal for new construction at the site of 994 Bronson Avenue. All policies pertaining to the project will be fulfilled, and in instances where the project does not meet the requirements, applications for amendments and procedures for approval will be sought to complete the project with the full consent of the City of Ottawa.

### 5.1 ONTARIO PROVINCIAL POLICY STATEMENT

The Provincial Policy Statement (P.P.S.) is a provincial wide policy that guides land use planning and development toward sustainable and efficient forms of construction. The goal of this policy is to improve the quality of life for Ontarians, by protecting the province's natural resources, considers the importance of public safety, and the quality of built environments. The O.P.P. is issued under Section 3 of the Planning Act, and came into effect on April 30, 2014. The following guidelines are integral to the design of 994 Bronson, in regards to effective urban planning and design:

#### Part V: Policies

##### 1.1 Managing and Directing Land Use to Achieve Efficient and Resilient Development and Land Use Patterns

1.1.1 Healthy, liveable and safe communities are sustained by:

- a) promoting efficient development and land use patterns which sustain the financial well-being of the Province and municipalities over the long term;
- b) accommodating an appropriate range and mix of residential (including second units, affordable housing and housing for older persons), employment
- c) avoiding development and land use patterns which may cause environmental or public health and safety concerns;
- e) promoting **cost-effective development patterns** and standards to minimize land consumption and servicing costs;

1.1.3.1 Settlement areas shall be the focus of growth and development, and their **vitality and regeneration** shall be promoted.

1.1.3.2 Land use patterns within settlement areas shall be based on: a) densities and a mix of land uses which:

- 1. efficiently use land and resources;
- 2. are appropriate for, and efficiently use, the infrastructure and public service facilities which are planned or available, and avoid the need for their unjustified and/or uneconomical expansion;
- 3. minimize negative impacts to air quality and climate change, and promote energy efficiency;
- 4. support **active transportation**;
- 5. are transit-supportive, where transit is planned, exists or may be developed; and
- 6. are freight-supportive; and b) a range of uses and opportunities for intensification and redevelopment in accordance with the criteria in policy 1.1.3.3, where this can be accommodated.

1.1.3.3 Planning authorities shall identify appropriate locations and promote opportunities for **intensification and redevelopment** where this can be accommodated taking into account existing building stock or areas, including brownfield sites, and the availability of suitable existing or planned infrastructure and public service facilities required to accommodate projected needs. Intensification and redevelopment shall be directed in accordance with the policies of Section 2: Wise Use and Management of Resources and Section 3: Protecting Public Health and Safety.

**1.1.3.4** Appropriate development standards should be promoted which facilitate intensification, redevelopment and compact form, while avoiding or mitigating risks to public health and safety.

## **1.2 Coordination**

### **1.2.6 Land Use Compatibility**

**1.2.6.1** Major facilities and sensitive land uses should be planned to ensure they are appropriately designed, buffered and/or separated from each other to prevent or mitigate adverse effects from odour, noise and other contaminants, minimize risk to public health and safety, and to ensure the long-term viability of major facilities.

### **1.4.1 Housing**

To provide for an appropriate range and mix of housing types and densities required to meet projected requirements of current and future residents of the regional market area, planning authorities shall:

a) maintain at all times the ability to accommodate residential growth for a minimum of 10 years through residential intensification and redevelopment and, if necessary, lands which are designated and available for residential development; and

b) maintain at all times where new development is to occur, land with servicing capacity sufficient to provide at least a three-year supply of residential units available through lands suitably zoned to facilitate residential intensification and redevelopment, and land in draft approved and registered plans.

**1.4.3** Planning authorities shall provide for an appropriate range and mix of housing types and densities to meet projected requirements of current and future residents of the regional market area by:

a) establishing and implementing minimum targets for the provision of housing which is affordable to low and moderate income households. However, where planning is conducted by an upper-tier municipality, the upper-tier municipality in consultation with the lower-tier municipalities may identify a higher target(s) which shall represent the minimum target(s) for these lower-tier municipalities;

b) permitting and facilitating:

1. all forms of housing required to meet the social, health and well-being requirements of current and future residents, including special needs requirements; and

2. all forms of residential intensification, including second units, and redevelopment in accordance with policy 1.1.3.3;

e) establishing development standards for residential intensification, redevelopment and new residential development which minimize the cost of housing and facilitate compact form, while maintaining appropriate levels of public health and safety.

## **1.7 Long-Term Economic Prosperity**

**1.7.1** Long-term economic prosperity should be supported by:

b) optimizing the long-term availability and use of land, resources, infrastructure, electricity generation facilities and transmission and distribution systems, and public service facilities;

c) maintaining and, where possible, enhancing the vitality and viability of downtowns and mainstreets;

d) encouraging a sense of place, by promoting well-designed built form and cultural planning, and by conserving features that help define character, including built heritage resources and cultural heritage landscapes;

The proposed development at 994 Bronson Avenue will observe the aforementioned planning guidelines outlined by the P.P.S. The proposed construction of a three storey apartment complex will contribute to the controlled revitalization of the Bronson Avenue streetscape, creating an optimal housing opportunity in a burgeoning Ottawa residential neighbourhood. The construction on the site will minimize the negative impacts on environmental and natural resources, by updating the existing municipal services to the building, and employing more sustainable and environmentally friendly construction materials, providing the building envelope to be more energy efficient, and contributing the living quality of the residents. Introducing new construction to this area of Bronson Avenue in the Glebe will bring vitality to the urban residential neighbourhood, complementing the existing forms and housing patterns, while exploring new materials and architectural volumes that will enhance the dwelling's roadside presence. As the nearby campus for Carleton University increases its enrollment numbers, demands for affordable housing and convenient location will increase, which this project aims to assist. This will aid in the long term prosperity of the



neighbourhood, encouraging other neighbouring rental units to improve upon their living standards, and provide ample accommodation to an increasing local population.

## 5.2 CITY OF OTTAWA'S OFFICIAL PLAN

The Official Plan (O.P.) is the prescribed policy framework for all introduced development in the City of Ottawa up to the year 2021. It is a legal document that is defined by the P.P.S. under the Ontario Planning Act, thereby guiding new construction to include more sustainable and efficient forms of design. It is also the framework that directs many other municipal activities, such as road and park construction, community design programs, creation of comprehensive zoning by-laws, and the review of development applications. The site at 994 Bronson is governed by both the general Official Plan of Volume 1. The following policies are relevant and integral to the completion of the proposed development phase:

### Section 2: Strategic Directions

#### 2.2 Managing Growth

- The City will manage growth by directing it to the urban area where services already exist or where they can be provided efficiently.
- Growth in the urban area will be directed to areas where it can be accommodated in compact and mixed-use development, and served with quality transit, walking and cycling facilities.

About 90 per cent of the growth in population, jobs and housing will be accommodated within areas designated within the urban boundary in this Plan. These are areas where services are already available or can be readily provided through the logical extension of existing services. Concentrating growth within the designated urban area also allows for a pattern and density of development that supports transit, cycling and walking as viable and attractive alternatives to the private automobile. This is the most cost-effective pattern for the provision of municipal services and infrastructure.

This Plan requires that intensification proposals have full regard for the existing built context and a full understanding of the impacts the proposal will have on both the immediate and wider surroundings. Consequently, this Plan calls for excellence in urban design and architecture, both in the public and private realms.

### 2.5: Building Liveable Communities

#### 2.5.1 Urban Design and Compatibility

##### Urban Design

Community design generally deals with patterns and locations of land use, relative densities, street networks, and the allocation of community services and facilities. Urban design is more concerned with the details relating to how buildings, landscapes and adjacent public spaces look and function together.

##### Compatibility

In support of lively and complete mixed-use communities, the City's growth management strategy includes intensification of development in the urban area over the next 20 years and concentrating rural development in Villages. Introducing new development in existing areas that have developed over a long period of time requires a sensitive approach and a respect for a communities established characteristics.

Compatible development means development that, although it is not necessarily the same as or similar to existing buildings in the vicinity, nonetheless enhances an established community and coexists with existing development without causing undue adverse impact on surrounding properties.

##### Design Objective and Principles

The Design Objectives of this Plan are qualitative statements of how the City wants to influence the built environment as the city matures and evolves. Design should:

- Integrate new development to complement and enliven the surroundings.
- Allow the built form to evolve through architectural style and innovation.
- Allow for varying stages of maturity in different areas of the city, and recognize that buildings and site development will exhibit different characteristics as they evolve over time.

- Orient development to maximize opportunities for passive solar gain, natural ventilation, and use energy efficient development forms and building measures.
- Utilize advanced water conservation and efficiency measures.

### Section 3: Designations and Land Use

#### 3.6.1 General Urban Area

The General Urban Area designation permits the development of a full range and choice of housing types to meet the needs of all ages, incomes and life circumstances. The zoning by-law will continue to regulate the location, scale and type of land use in accordance with the provisions of this Plan.

#### Policies

1. General Urban Area areas are designated on Schedule B. The General Urban Area designation permits all types and densities of housing.
2. The evaluation of development applications, studies, other plans and public works undertaken by the City in the General Urban Area will be in accordance with Section 2.5.1 and Section 4.11.
3. When considering a proposal for residential intensification through infill or redevelopment in the General Urban Area, the City will:
  - Recognize the importance of new development relating to existing community character so that it enhances and builds upon desirable established patterns and built form;
  - Apply the policies of Section 2.5.1 and Section 4.11;
  - Consider its contribution to the maintenance and achievement of a balance of housing types and tenures to provide a full range of housing for a variety of demographic profiles throughout the General Urban Area;

The proposed demolition of the existing single detached house is critical to the development of the proposed three storey apartment building. While the existing house itself is of a considerable age, the property is not of historic significance, and the building does not express defining or unique attributes that are imperative to preserve. The newly constructed building will incorporate the outlined design objectives as the Official Plan prescribes, ensuring the new development incorporates itself effectively into the fabric of the established community. It is the aim of the proposed development to balance the desire to introduce modern construction and materials to the site, while adhering to the overall appearance and housing patterns of the area. As the definition of “compatibility” suggests, the construction will coexist within the established community, while exhibiting a unique individuality. The streetscape pattern will be maintained in accordance with the Ottawa Zoning Bylaws, keeping the landscaped front yard depth from the roadway, the generous setback from the rear property line, and the width of the asphalt driveway at the northern interior side property line. The spacing between the adjacent houses will also be maintained, to ensure the continuity and scale of the streetscape.

The intention of the design is to establish a visual separation from the adjacent properties through the use of exterior material and building volume. This will define the difference between the houses of an earlier construction age, and the newly renovated site.

## **5.3 OTTAWA ZONING BY-LAW PROVISIONS**

The proposed development at 994 Bronson Avenue is designated under the R4T Zone. All relevant zoning bylaws and setbacks are included below, addressing each by-law associated with the site’s newly proposed development phase:

#### Adequate Municipal Services (Section 56)

56. (1) No land can be used or the intensity of any use of land expanded or any building placed, erected, altered, enlarged, or used within of the City of Ottawa unless the land is serviced by municipal water, sewerage and drainage systems that have adequate capacity. (By-law 2010-307)

Due to the increase in potential tenants in the new structure at 994 Bronson, the need for services from the City of Ottawa have been increased. All sewage, drainage and plumbing systems into the proposed building have been

designed to these specifications, and will provide a necessary system to the residents that is adequate to its proposed capacity.

#### Minimum Parking Space Rates (Section 101)

1. Off-street motor vehicle parking must be provided for any land use at the rate set out in Table 101 below. In addition, visitor parking must be provided in accordance with Section 102.
10. In the case of an apartment building, mid – high rise and **low rise** and stacked dwelling, where a dwelling unit has a driveway accessing its own required parking space, additional required parking may be located in tandem in the driveway. (*Subject to By-law 2014-292*)

#### Amenity Space (Section 137)

137. (1) Amenity area must be provided for a residential use that is a permitted use in the zone in which it is located, in accordance with Table 137.
- (2) Amenity area must be located on the same lot as the use for which it is provided.
- (3) Amenity area provided outdoors must not be located in a required front or corner side yard.
- (4) Where amenity area is located outside at grade, it may be included in the calculation of landscaped area requirements.
- (5) Minimum required communal amenity area may only be included as part of a required landscaped buffer where it is aggregated into areas of 54m<sup>2</sup> or more.

The existing site conditions will be maintained at grade for a private entranceway in the northern interior side yard, which will provide a 3 meter wide driveway to the proposed two parking spaces located in the rear yard. The proposed occupancy does not require a parking space for the first twelve units of residential space, however the site is large enough to accommodate the required single parking space, and an additional second space. This will not negatively impact or interfere with the adjacent properties, nor will it diminish the size of the landscaped rear yard for tenant amenity.

The amenity area is located within the rear yard at grade, retaining the existing trees and shrubbery where available, and provides a sizeable area of 270.9 square meters. The minimum required amenity area is 150 square meters.

**Table 137 - Amenity Area (By-law 2014-189) (OMB Order File #PL150797 issued July 25, 2016 – By-law 2015-228)**

Column	Land Use	Total Amenity Area	Communal Amenity Area	Layout Of Communal Amenity Area
3)	Low rise Apartment	15m <sup>2</sup> per dwelling, up to 8 units, + 6m <sup>2</sup> per unit in excess of 8	100% of the required amenity area required for the first 8 units	Communal amenity area required for the first 8 units must: -be located at grade and in the rear yard; -be landscaped; -consist of at least 80% soft landscaping; and -abut the rear lot line, unless the lot has access to a rear lane





**EXHIBIT K: ZONING MAP 994 BRONSON AVENUE, GEO OTTAWA 2014**

## R4 - Residential Fourth Density Zone (Sec. 161-162)

### Purpose of the Zone

The purpose of the R4- Residential Fourth Density Zone is to:

- 1) allow a wide mix of residential building forms ranging from detached to low rise apartment dwellings, in some cases limited to four units, and in no case more than four storeys, in areas designated as General Urban Area in the Official Plan;
- 2) allow a number of other residential uses to provide additional housing choices within the fourth density residential areas;

### 161. In the R4 Zone:

**Permitted Uses** The following uses are permitted uses subject to:

- a) the provisions of subsection 161 (2) to (15);
- b) a maximum of three guest bedrooms in a bed and breakfast;
- c) a maximum of ten residents is permitted in a group home; and

**apartment dwelling, low rise**

### Zone Provisions

- 8) Thirty percent of the lot area must be provided as landscaped area for a lot containing an apartment dwelling, low rise, stacked dwelling, or retirement home, or a planned unit development that contains any one or more of these dwelling types.
- 9) The maximum height of any permitted use may not exceed that which is specified in Column VI of Table 162A, and in no case, may be greater than a maximum four storeys.

### Alternative Setbacks for Urban Areas

(OMB Order File No: PL150797, issued July 25, 2016 - By-law 2015-228)

- (11) Despite the minimum rear yard setback provision in column VII of Table 156A, the minimum required rear yard setback on through lots or interior lots where the rear lot line abuts R1, R2, R3, and R4 zones, and where the minimum front yard setback is greater than 4.5 metres in Area A on Schedule 342 are as follows:
  - a) for any lot with a lot depth:
    - (iii) greater than 25 metres except for a lot containing a Planned Unit Development: a distance equal to 30 per cent of the lot depth which must comprise at least 25 per cent of the area of the lot,

### Alternative Projections into Required Yards Provisions for Urban Areas

(OMB Order File No: PL150797, issued July 25, 2016 - By-law 2015-228)

- (15) In Area A on Schedule 342
  - a) despite Subsection (6) of Table 65 a balcony may not project into a required rear yard on lots 30 metres or less in depth,
  - b) despite Subsection (7) of Table 65 a bay window may project to a maximum of 0.5 metres into a required rear yard on lots 30 metres or less in depth.

### Alternative Projections Above the Height Limit Provisions for Urban Areas

(OMB Order File No: PL150797, issued July 25, 2016 - By-law 2015-228)

- (16) In Area A on Schedule 342
  - a) A parapet must not project more than 0.3 metres above the maximum building height.

994 BRONSON AVENUE W, LOT 19, PART OF REGISTERED PLAN 91219							
REQUIRED SITE DIMENSIONS R4 (TABLE 162A)			EXISTING SITE DIMENSIONS R4T (SECTION 161-162)			PROPOSED BUILDING MEASUREMENTS FOR SINGLE DETACHED DWELLING	
ZONING		R4 T	ZONING		R4 T	ZONING	R4 T
PRINCIPAL DWELLING		LOW RISE APARTMENT DWELLING	EXISTING PRINCIPAL DWELLING		SINGLE DETACHED DWELLING	PRINCIPAL DWELLING	LOW RISE APARTMENT DWELLING
MINIMUM LOT WIDTH		15m	EXISTING LOT WIDTH		22.98 m	PROPOSED LOT WIDTH	22.98 m
MINIMUM LOT DEPTH		NO MINIMUM	EXISTING LOT DEPTH		29.11 m	PROPOSED LOT DEPTH	29.11 m
MINIMUM LOT AREA		450 sq. m	EXISTING LOT AREA		655.85 m sq.	PROPOSED LOT AREA	655.85 m sq.
FRONT YARD SETBACK		MINIMUM 3 m	EXISTING FRONT YARD SETBACK		2.09 m	PROPOSED FRONT YARD SETBACK	2.09 m
INTERIOR YARD SETBACKS	MAXIMUM	NO MAXIMUM	EXISTING INTERIOR YARD SETBACKS		N/A	PROPOSED INTERIOR YARD SETBACKS	1.5 m (SOUTH); 3 m at grade, 1.5 m above grade (NORTH)
	MINIMUM	For exterior walls equal than or less than 11m in height, 1.5m					
CORNER YARD SETBACK		N/A	CORNER YARD SETBACK		N/A	CORNER YARD SETBACK	N/A
REAR YARD SETBACK	MINIMUM	For lots greater than 25 m, up to and including 32 m, a distance equal to 30% of the total lot depth, and a minimum of 25% of the total lot area (min. 8.73 m depth, and min. 163.96 m <sup>2</sup> area)	EXISTING REAR YARD SETBACK		N/A	PROPOSED REAR YARD SETBACK	9.13 m depth (31.4% of lot depth); area = 270.9 m sq. 41.3% of total lot area
BUILDING HEIGHT	MAXIMUM	14.5 m (Bylaw 2014-289)	BUILDING HEIGHT		APPROX. 6.09 m	BUILDING HEIGHT	10.91 m

As indicated by the zoning bylaw table above, the proposed development remains well within the prescribed setbacks and height limitations of the zone, which negates the necessity of applying for minor variance.

The three storey apartment dwelling adheres to the minimum front and interior yard setbacks, and provides an enlarged rear yard for tenant use and parking. The dimensions of the lot accommodate the proposed building type, as well as all the proposed uses at the exterior.

## **6. SUMMARY AND CONCLUSION**

### **6.1 PROPOSAL SUMMARY**

The scope of the proposed site demolition and new construction looks to maintain the existing scale and housing pattern of the neighbourhood as much as possible, while optimizing the occupied land use to create more functional, livable space. The proposed three storey apartment complex is encouraged and supported by the City's desire to provide various forms of residential development, while reducing the negative impact on vacant land use and natural resources within the central core of Ottawa. The approved setbacks, height limitations, and access to services all allow for the controlled development of the site with a comfortable interior area for quality living. The use of materials, massing, human scale and natural landscaping will provide a residence that promotes safe and healthy living within the Glebe neighbourhood, while also remaining respectful of the architectural character and environmental quality in the region.

The proposed renovation is consistent with the prescribed guidelines of the City of Ottawa Official Plan, and the Ottawa Zoning Bylaws, creating an appropriate low rise apartment residence for the neighbourhood, while creating a unique housing opportunity that will invigorate the streetscape. The proposed construction will comply with all relevant planning amendments, to perpetuate appropriate land uses and provide comfortable and controlled growth for an established residential neighbourhood. The presence of vibrant residential villages will encourage public interaction and community involvement, while promoting active transportation, as well as pedestrian travel.

### **6.2 CONCLUSION**

This submission for planning rationale is an appropriate and sustainable course of action for construction of a three storey low rise apartment dwelling at 994 Bronson Avenue. The redevelopment of the existing property will aim to combine the desires of the owner with the prescribed guidelines and bylaws of the City of Ottawa, to contribute to its vision for intensified urban land use, in a responsible and sustainable manner.