

# 3026 SOLANDT ROAD

JANUARY 2020

## PLANNING RATIONALE

SITE PLAN CONTROL APPLICATION



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# 1 INTRODUCTION

WSP was retained by Colonnade BridgePort to prepare a Planning Rationale (the “Report”) in support of a Site Plan Control application for the property municipally known as 3026 Solandt Road (“the site”) in the City of Ottawa.

The proposed development consists of a 5-storey office building with a proposed gross floor area of approximately 8,230 m<sup>2</sup> (88,590 ft<sup>2</sup>) at the southeast corner of Solandt Road and March Road.

This Report is set up as follows:

- **Section 2** provides a description of the site location and community context;
- **Section 3** provides an explanation of the proposed development;
- **Section 4** outlines the policy and regulatory framework applicable to the site and a planning rationale for the proposed development; and
- **Section 5** summarizes the planning opinion.

A number of technical studies have been prepared in support of the application and have been submitted to the City and are available under separate cover.

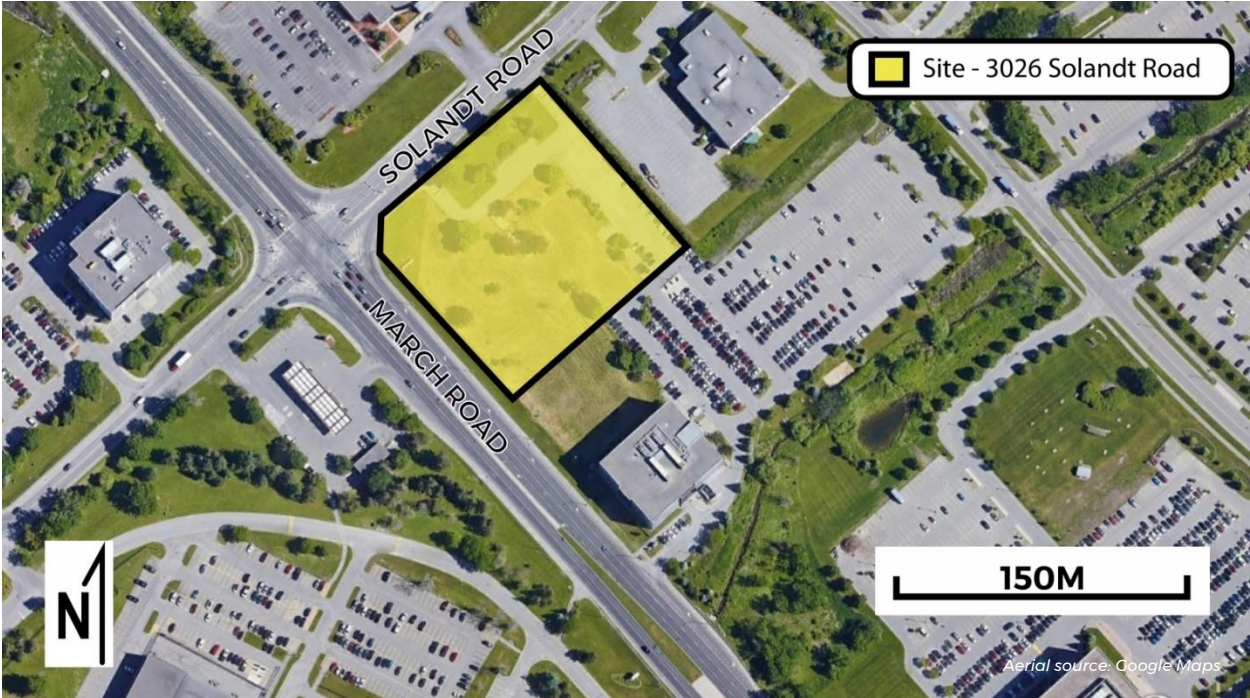
# 2 SITE LOCATION AND COMMUNITY CONTEXT

## 2.1 SITE LOCATION

The site is legally described as: MARCH CON 4 N PT LOT 7 RP; 5R3350 PT PARTS 1 5 6 in the City of Ottawa, as per GeoOttawa. It is municipally known as 3026 Solandt Road and is located in Ward 4 (Kanata North), at the southeast corner of Solandt Road and March Road. The proposed development has a lot area of approximately 1.7116 hectares (4.2294 acres).

The proposed development is adjacent to 450 March Road, an existing 5 storey office building, as illustrated in **Figure 2-1**. The registered property owner is Fiera Real Estate Core Fund GP Inc., who own the entire parcel of land, including 3000 and 3001 Solandt Road to the north. Both existing office buildings will be retained.

**Figure 2-1: Site Location**





A site visit was conducted on January 7, 2020. Photos contained herein are from WSP, unless otherwise stated.



Figure 2-2: Site looking south



Figure 2-3: Site looking southwest



Figure 2-4: Site looking southeast



Figure 2-5: Northwest of the site (March/Solandt Road intersection)

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## 2.2 COMMUNITY CONTEXT

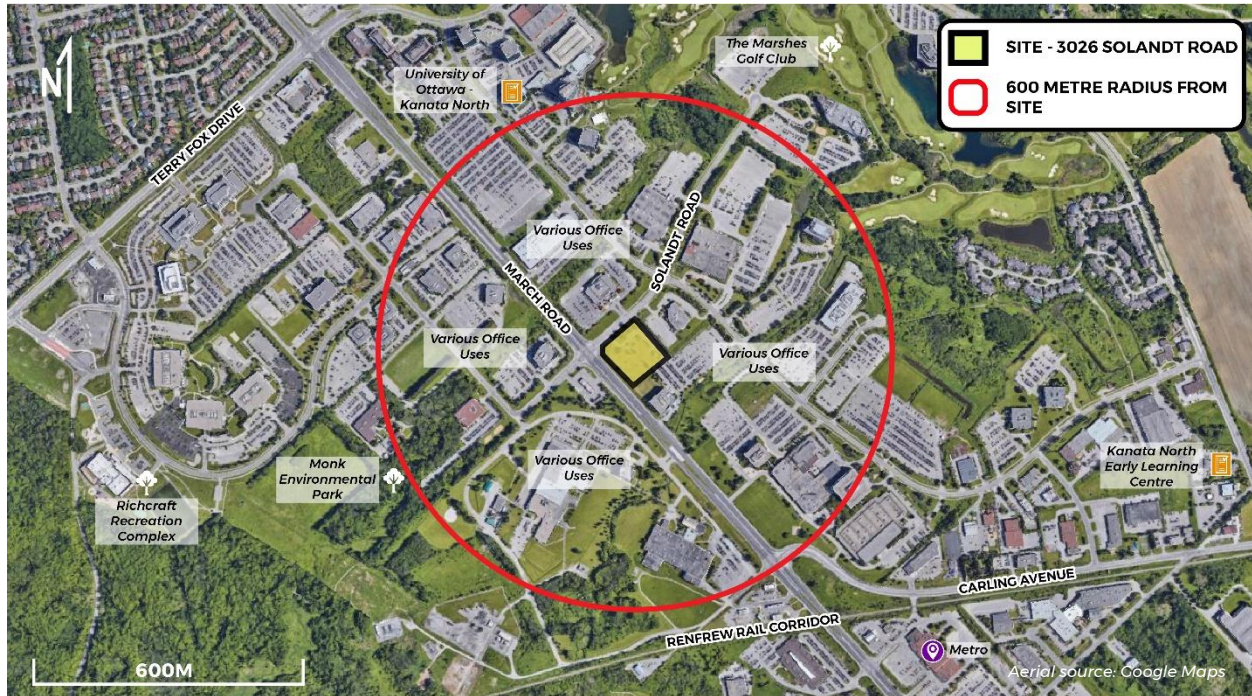
Land uses adjacent to the site are as follows:

- **North:** Offices
- **South:** Offices
- **East:** Offices
- **West:** Drive through restaurant and gas station



As identified in **Figure 2-6**, the site is in close proximity to office uses, which includes various engineering and software company offices. The site is also approximately 150 metres from the Renfrew Rail Corridor to the south.

**Figure 2-6: Context**



## 2.2.1 SURROUNDING DEVELOPMENT ACTIVITY

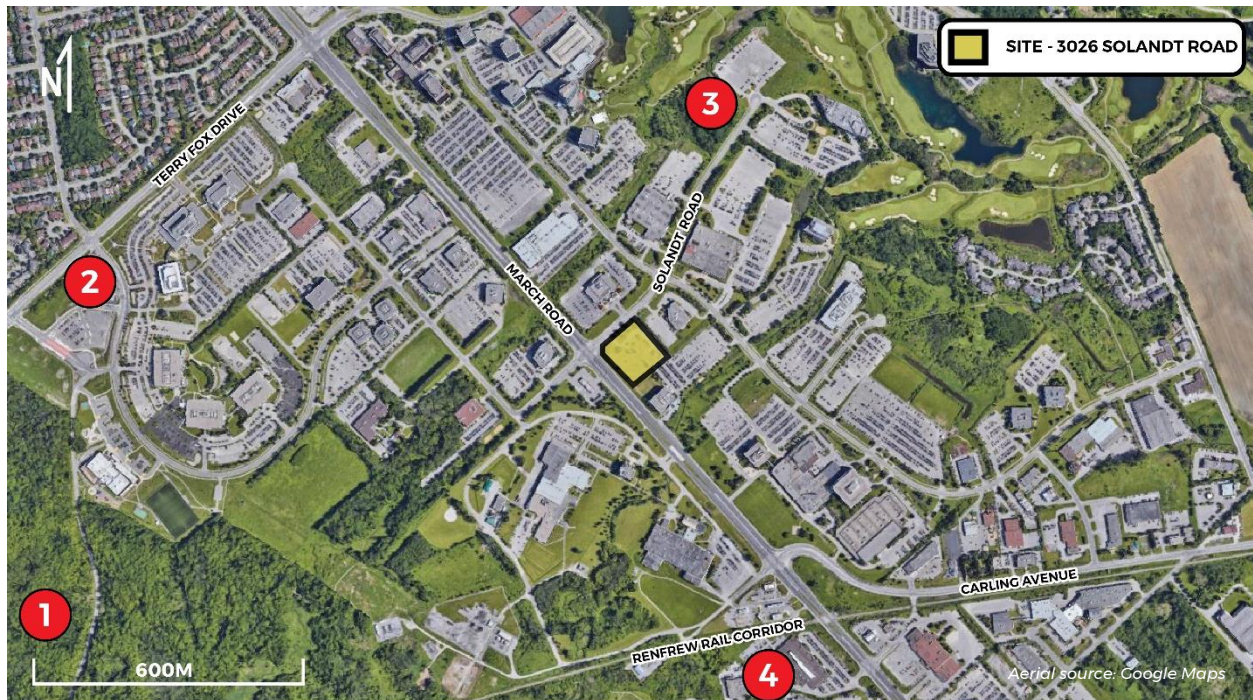
**Figure 2-7** outlines active development applications in the vicinity of the site. **Table 2-1** provides a summary of development applications within the vicinity of the site, including their address, development type, proposed building heights, number of units or gross floor area (GFA), and the status of the applications.

**Table 2-1: Development Activity within the Vicinity of the Site**

NO.	ADDRESS	LAND USE	MAX. HEIGHT (STOREYS)	NO. OF UNITS or GFA	DEVELOPMENT APPLICATION STATUS
1	300, 425, 535 and 605 Goulbourn Forced Rd.	Residential	N/A	3,248 residential units (3 phases)	Official Plan Amendment and Zoning By-law Amendment (ZBLA) application in effect.
2	5045 Innovation Dr.	Commercial	1	1,240 m <sup>2</sup>	ZBLA approved. Site Plan Agreement Registered and Final Legal Clearance Given.

NO.	ADDRESS	LAND USE	MAX. HEIGHT (STOREYS)	NO. OF UNITS or GFA	DEVELOPMENT APPLICATION STATUS
3	2505 and 2707 Solandt Rd.	Office	8	18,452 m <sup>2</sup>	Site Plan Application and ZBLA circulated.
4	56 Steacie Dr.	Office	2	1,628 m <sup>2</sup>	Site Plan Application approved. Receipt of agreement from owner pending.

**Figure 2-7: Development Activity within the Vicinity of the Site**



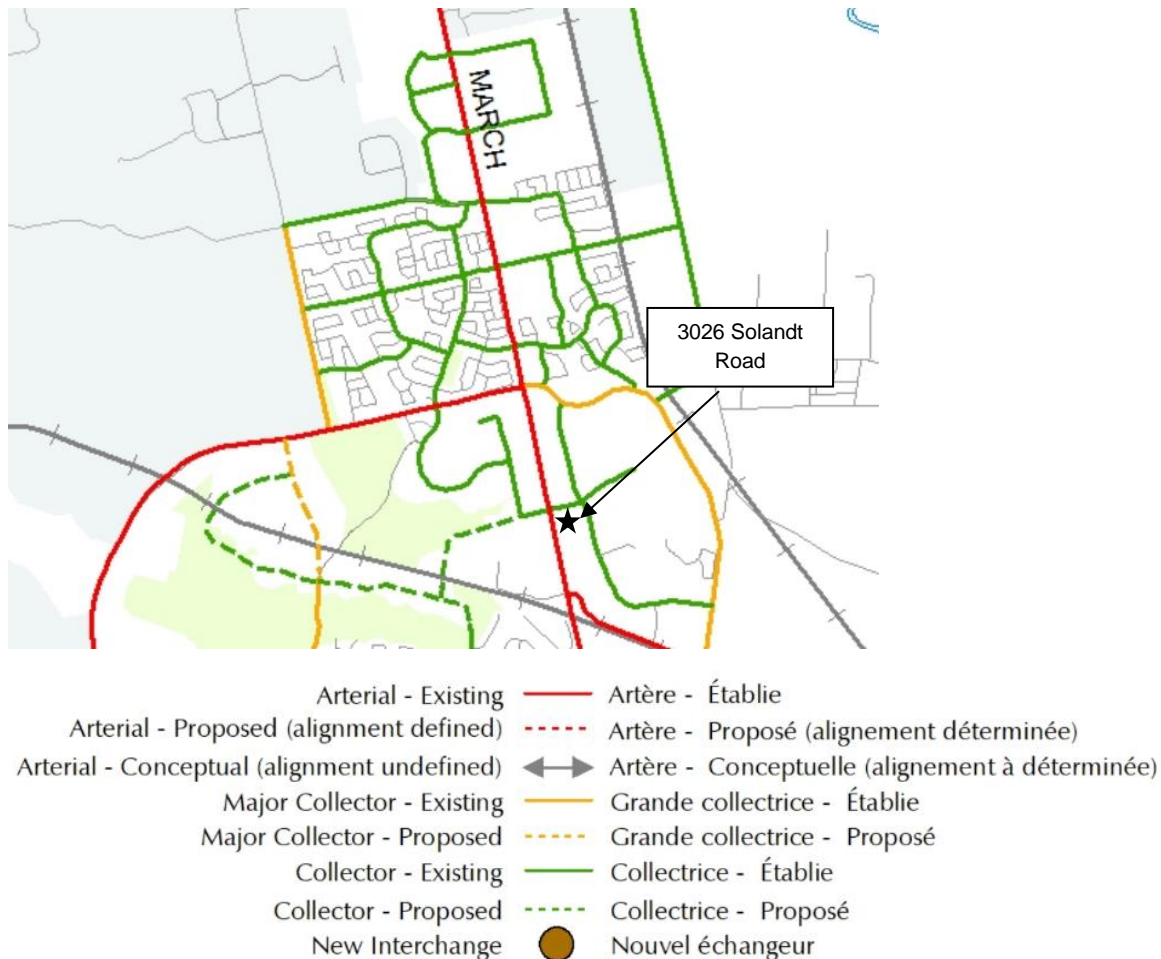


## 2.3 TRANSPORTATION NETWORK

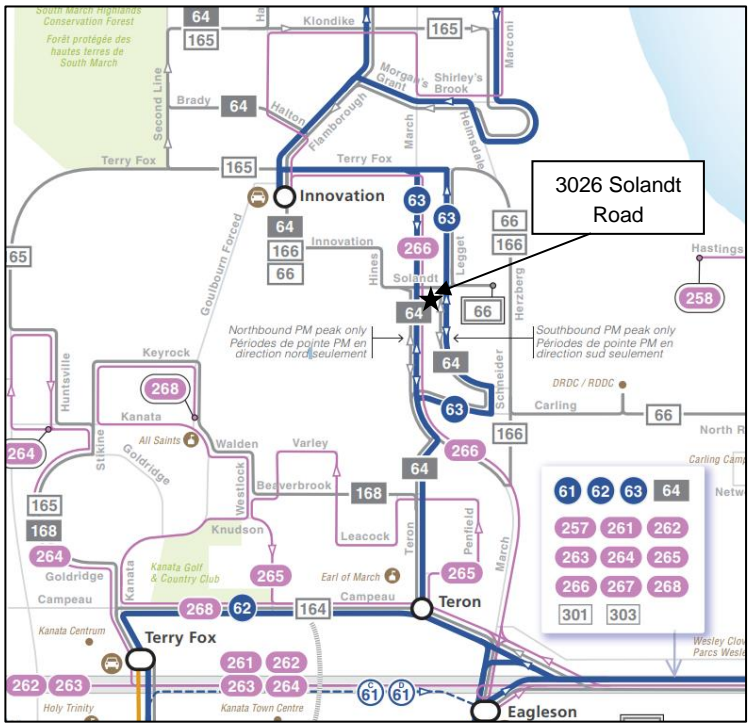
As per Schedule E (Urban Road Network) of the City of Ottawa Official Plan (2003, Website Consolidation), March Road is identified as an existing Arterial Road and Solandt Road is identified as an existing Collector Road, as illustrated in **Figure 2-8**. March Road abuts the site to the west and provides vehicular and transit access to Highway 417 (running east-west), which provides access to the greater Ottawa area. Solandt Road runs along the northern edge of the site. Both roads have sidewalks.

The site is well serviced by existing public transit, as illustrated in **Figure 2-9**. The site features a rapid transit route (Route 63) and a local route (Route 64).

**Figure 2-8: Schedule E (Urban Road Network), City of Ottawa Official Plan**



**Figure 2-9: OC Transpo Network Map (October 2019)**



- Rapid • Rapide**  
 Station-to-station bus service  
 Operating 7 days/week in all time periods  
 Service d'autobus de station à station  
 Service offert en tout temps, tous les jours de la semaine
- Frequent • Fréquent**  
 Service every 15 minutes or less on weekdays  
 Operating 7 days/week in all time periods  
 Service aux 15 minutes ou moins en semaine  
 Service offert en tout temps, tous les jours de la semaine
- Local**  
 Custom routing to local destinations  
 Réseau local adapté aux besoins des usagers
- Connexion**  
 Convenient connection to the O-Train  
 Weekday peak-periods only  
 Correspondance pratique à l'O-Train  
 Service offert aux heures de pointe, les jours de semaine seulement

### 3 THE PROPOSED DEVELOPMENT

The proposed development consists of a 5-storey office building with a proposed gross floor area of approx. 8,230 m<sup>2</sup> (88,590 ft<sup>2</sup>) at the southeast corner of the Solandt and March Road intersections, as illustrated in the site plan shown in **Figure 3-1**. The proposed development has a total building area of approx. 1,910 m<sup>2</sup> (20,560 ft<sup>2</sup>).

A total of 355 parking spaces are proposed for the site. Of those parking spaces, 9 will be barrier free parking spaces meeting the Type A and Type B requirements outlined in the Accessibility for Ontarians with Disabilities Act (AODA).

The site currently has two access points from Solandt Road. The easterly access is proposed to have both ingress and egress while the westerly access is proposed as right-in only. Access from March Road is not proposed.

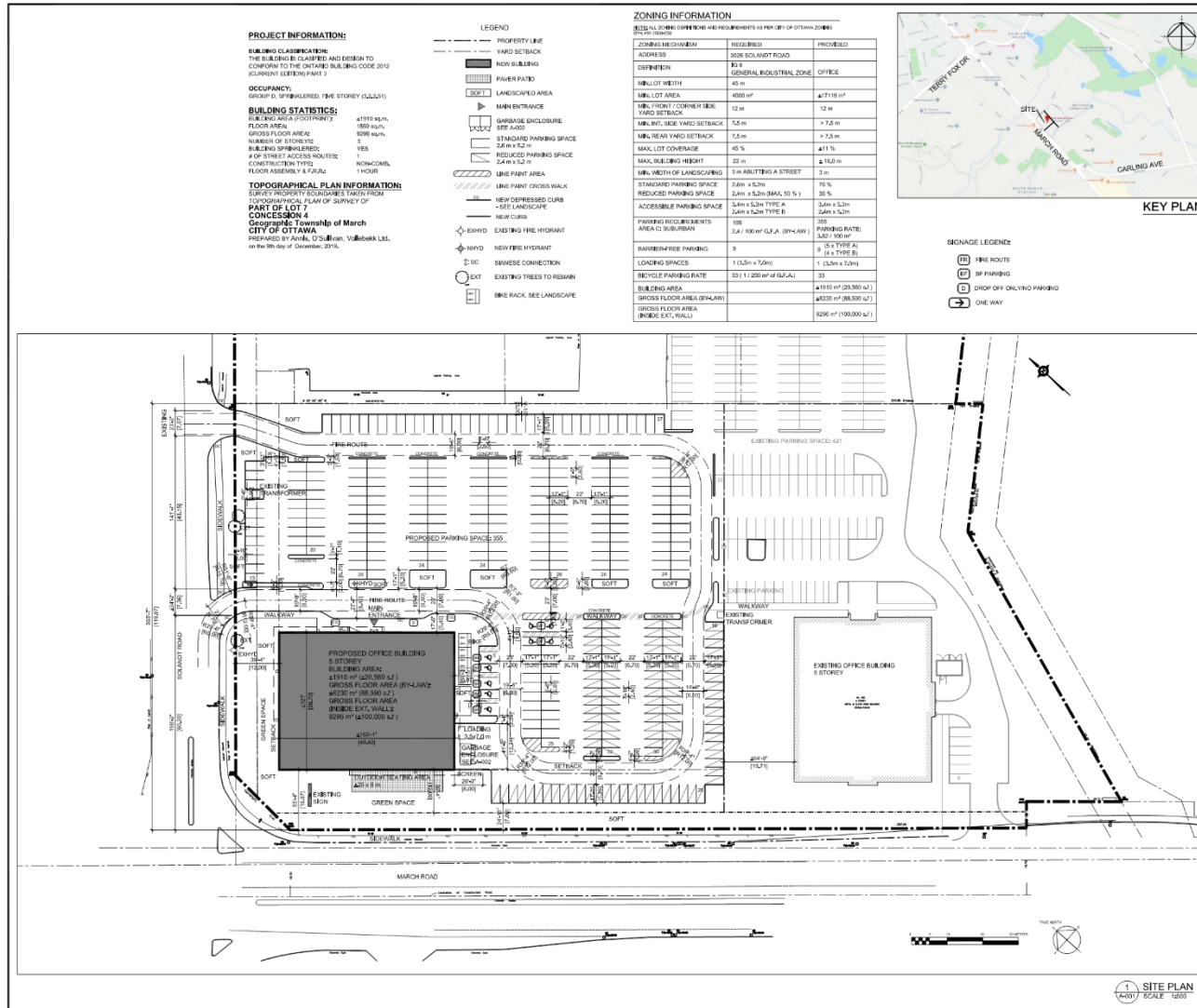
The site had a single-storey office building that was demolished in 2014.

The site will be in close proximity to an existing 5-storey office building that contains 421 existing parking spaces. The site is not being considered one lot for zoning purposes and there is currently no intended severance at this time.

Municipal services currently exist on the site. The estimated date of construction is December 2020.

A public consultation strategy is proposed which will include an information session and outreach to the Ward Councillor (Ward 4 – Kanata North).

Figure 3-1: 3026 Solandt Road – Site Plan (dated December 2019, revised January 17, 2020)



**COLONNADE BRIDGEPORT**  
 Property Investment & Management

4		
3		
2		
1	ISSUED FOR SITE PLAN APPLICATION	17 JAN, 2020
NO.	revision	date

**N45 ARCHITECTURE INC.**  
 71 Bank Street, 7th Floor - Ottawa, Ontario, K1P 5N2  
 tel: 613.224.0955 fax: 613.224.9811

**project**  
**SOLANDT ROAD OFFICE BUILDING**  
 3026 SOLANDT ROAD, KANATA, ONTARIO

**construction north**  
 true north

**scale**  
 AS SHOWN

**drawing title**  
**SITE PLAN**

scale	drawn by
AS SHOWN	J.J.
date	checked by
01/17/2020	RAM
project number	drawing number
19-458	A-001

CONTRACTOR TO VERIFY ALL DIMENSIONS AND NOTIFY THE ARCHITECT OF ANY DISCREPANCIES BEFORE COMMENCEMENT. DO NOT SCALE DRAWINGS.

revision



## 4 POLICY AND REGULATORY FRAMEWORK

This section describes the provincial, and local policy framework that is relevant or applicable to the proposed development of the site.

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### 4.1 PROVINCIAL POLICY STATEMENT (2014)

The Provincial Policy Statement (2014) (PPS) provides policy direction on matters of provincial interest related to land use planning and development. As a key part of Ontario's policy-led planning system, the PPS sets the policy foundation for regulating development and use of land.

The PPS seeks to strike a balance between the province's economic, social and environmental interests through the following:

- Promoting cost-effective development patterns which stimulate economic growth;
- Protecting resources for their economic use and/or environmental benefits; and
- Directing development away from areas where there is a risk to public health and safety or of property damage.

Part IV: Vision for Ontario's Land Use Planning System identifies that healthy, liveable and sustained communities are sustained by promoting efficient development, cost effective land use patterns and development standards, accommodating an appropriate range of mix of uses to meet long term needs, and focusing growth within settlement areas and away from significant or sensitive resources and areas which may pose a risk to public health and safety.

Section 1.1, Managing and Directing Land Use to Achieve Efficient and Resilient Development and Land Use Patterns, includes policies to sustain healthy, liveable, resilient and safe communities. Policy 1.1.1 states that healthy, liveable and safe communities are sustained by promoting efficient development patterns and land use patterns, accommodating an appropriate range and mix of uses to meet long term needs, and promoting cost-effective development patterns and standards, among other considerations.

Policy 1.1.3.2 (a) states that land use patterns within settlement areas shall be based on densities and a mix of land uses which:

1. "efficiently use land and resources;
2. are appropriate for, and efficiently use, the infrastructure and public service facilities which are planned or available, and avoid the need for their unjustified and/or uneconomical expansion;
3. minimize negative impacts to air quality and climate change;
4. support active transportation; and

5. are transit-supportive, where transit is planned, exists or may be developed”.

Further, Policy 1.1.3.2 (b) states that land use patterns within settlement areas shall be based on a range of uses and opportunities for intensification and redevelopment in accordance with the criteria in Policy 1.1.3.3, where this can be accommodated.

Policy 1.1.3.3 states that planning authorities shall identify appropriate locations and promote opportunities for intensification and redevelopment where this can be accommodated taking into account existing building stock or areas, including brownfield sites, and the availability of suitable existing or planned infrastructure and public service facilities required to accommodate projected needs.

Policy 1.1.3.4 states that appropriate development standards should be promoted which facilitate intensification, redevelopment and compact form, while avoiding or mitigating risks to public health and safety.

Policy 1.3.1 states planning authorities shall promote economic development and competitiveness by:

- a. providing for an appropriate mix and range of employment and institutional uses to meet long-term needs;
- b. providing opportunities for a diversified economic base, including maintaining a range and choice of suitable sites for employment uses which support a wide range of economic activities and ancillary uses, and take into account the needs of existing and future businesses;
- c. encouraging compact, mixed-use development that incorporates compatible employment uses to support liveable and resilient communities; and
- d. ensuring the necessary infrastructure is provided to support current and projected needs.

Further, Policy 1.7.1 (a) states that long-term economic prosperity should be supported by promoting opportunities for economic development and community investment-readiness.

The Ministry of Municipal Affairs and Housing released a Proposed Provincial Policy Statement (July 2019). The following summary of proposed changes are relevant to this application while it is in draft form:

- Protect the environment and public safety by encouraging transit-oriented development and ensuring municipalities prepare for the impacts of a changing climate while continuing to protect important natural features; and
- Support certainty and economic growth by giving more flexibility to municipalities to ensure areas designated for employment are planned to reflect local needs and to help facilitate the conditions for economic investment, and to ensure that local infrastructure

investments are efficiently used, and that protections for transportation and energy corridors for future needs are in place.

**The proposed development is consistent with the 2014 PPS, as it represents intensified development within a settlement area that contributes to economic growth and utilizes land efficiently. It is adjacent to an existing office building and will utilize existing infrastructure to complement the character of the area at an appropriate scale. The proposed development will also enhance opportunities for economic prosperity and competitiveness through further employment uses to meet long-term economic needs.**

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## **4.2 CITY OF OTTAWA OFFICIAL PLAN (2003, WEBSITE CONSOLIDATION)**

The City of Ottawa Official Plan (2003, Website Consolidation) (OP) provides a comprehensive vision and policy framework for managing growth and development to the year 2036. The OP contains policies that address matters of provincial interest as described in the PPS, and “is not a tool to limit growth but rather to anticipate change, manage it and maintain options” (Section 1.1).

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### **4.2.1 BUILDING A SUSTAINABLE CITY**

The OP seeks to achieve Council’s vision of a sustainable, resilient and liveable city. This vision is also articulated in the City’s Strategic Plan, which identifies goals including the provision of housing options that are green, healthy, and meet the needs of the whole community. The City also strives for sustainability in connectivity and mobility by making walking, cycling, and transit residents’ first choices for transportation.

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### **4.2.2 STRATEGIC DIRECTIONS**

**Section 2.1 – Patterns of Growth** indicates that Ottawa’s growth will be managed in ways that create complete communities with a good balance of facilities and services to meet people’s everyday needs, including schools, community facilities, parks, a variety of housing options, and places to work and shop.

Growth is to be directed to urban areas where services already exist. Growth in existing designated urban areas is to be directed to areas where it can be accommodated in compact and mixed-use development, and served with quality transit, walking, and cycling facilities. Further, infill and redevelopment will be compatible with the existing context or planned function of the area and contribute to the housing, employment, or services in the area. Designated Mixed-use Centres will also be compact, liveable, and pedestrian-oriented with a vibrant mix of residential uses, and social, cultural and economic activity.

**Section 2.2 – Managing Growth** indicates that the majority of the City’s growth will be directed to areas designated within the urban boundary of the OP. This strategy has the least impact on agricultural land and protected environmental areas and allows for a pattern and density of development that supports transit, cycling and walking.

**Section 2.2.2 – Managing Growth Within the Urban Area** indicates that intensification is supported throughout the urban area where there are opportunities to accommodate more jobs and housing and increase transit use.

Employment intensification means intensification of a property, building or area that results in a net increase in jobs and/or gross floor area and may occur by infilling of vacant or underutilized land within urban employment lands (Section 2.2.2, Policy 2 c).

**Section 2.2.3 – City-wide Employment Area Policies** states this Plan manages economic growth and diversity by protecting some lands primarily for employment use so that they remain affordable for employment purposes and so that they can develop over time without conflict from competing land uses.

The following City-wide Employment Area Policies are applicable:

1. Employment Areas corresponding to clusters of business and economic activities in the Provincial Policy Statement include: Urban Employment Areas designated on Schedule B of this Plan.
2. In all urban communities outside the Central Area, areas will be planned for a range of employment opportunities.
3. Major Office Development, consisting of buildings over 10,000 m<sup>2</sup> gross leasable area, will play a fundamental role in supporting and increasing the ridership of the Rapid Transit and Transit Priority Network and shall be located in areas adjacent to the Rapid Transit and Transit Priority Network. Except where currently permitted by zoning, the City will only permit new Major Office Development in the following locations:
  - a. Central Area;
  - b. Mixed Use Centres;
  - c. Town Centres;
  - d. On land fronting Traditional Mainstreets or Arterial Mainstreets that are Transit Priority corridors or where the development is within 800 metres walking distance of an existing or planned Rapid Transit station;
  - e. Urban Employment Areas where the majority of the site is within 800 metres walking distance of an existing or planned Rapid Transit station or 400 metres walking distance of a Transit Priority corridor (Official Plan Amendment 180).
  - f. Kanata North and Kanata South Urban Employment Areas once a secondary plan for the Area or Areas has been adopted and identifies the preferred location for Major Office Uses (Official Plan Amendment 180).

The proposed office development is not over 10,000 m<sup>2</sup>, as indicated in Section 2.2.3, Policy 6 of the Official Plan. Policy 6 (f) states that the City will permit Major Office Development, consisting of buildings over 10,000 m<sup>2</sup> of gross leasable area, once a Secondary Plan for the Area has been adopted. Since the development is under 10,000 m<sup>2</sup> of gross leasable floor area and it is permitted in the Zoning By-law, this policy does not apply.

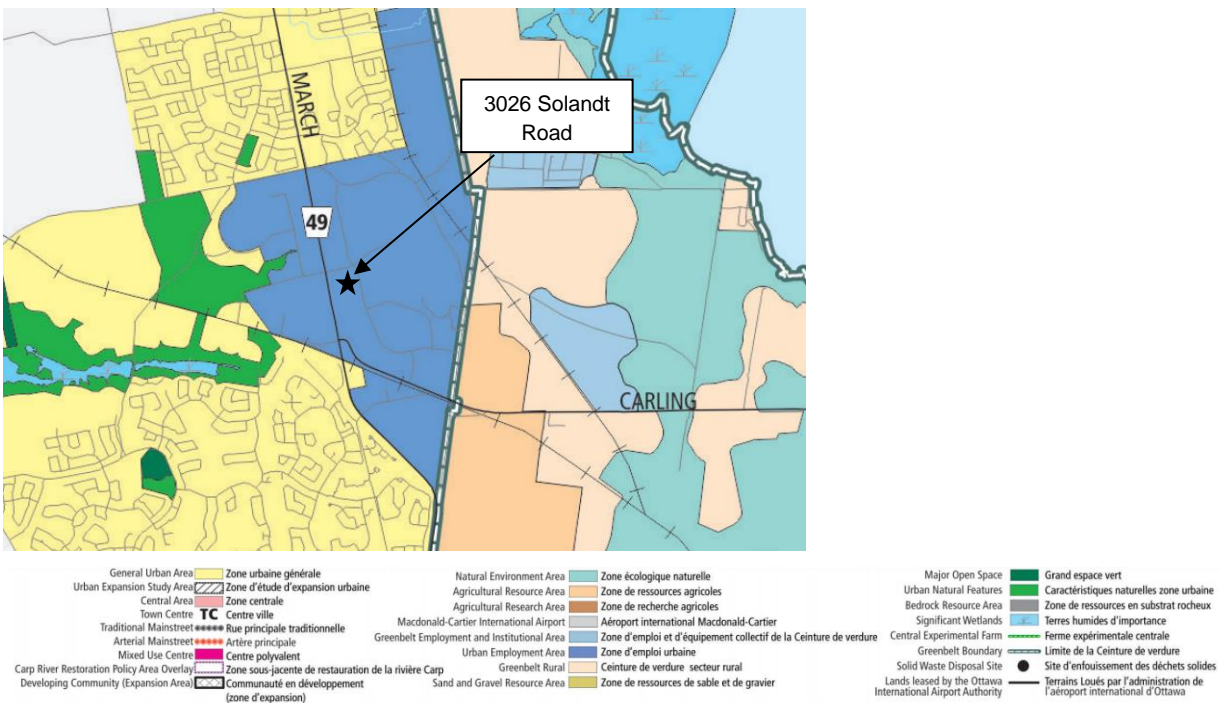
**Section 2.5.1 – Urban Design and Compatibility** encourages good urban design and quality and innovative architecture as it can help create lively community places with distinctive character. In order for a development to be compatible, it does not necessarily have to be the same or similar to existing buildings in the vicinity, but has to enhance an established community and coexist with existing development without causing undue impact on the surrounding properties. The design objectives of the proposed development are addressed in detail in **Section 4.2.4** of this Report.

The proposed development supports the strategic directions of the OP by directing economic growth to urban areas and supporting intensification in the urban area.

### 4.2.3 LAND USE DESIGNATION

The site is located within the City’s urban boundary and is designated as Urban Employment Area on Schedule B Urban Policy Plan, as illustrated in **Figure 4-1**.

**Figure 4-1: City of Ottawa Official Plan, Schedule B - Urban Policy Plan (Excerpt)**



## **SECTION 3.6.5 – URBAN EMPLOYMENT AREA**

Employment-related land uses are permitted in almost every urban designation in the Official Plan. Lands designated as Urban Employment Areas on Schedule B of this Plan are distributed throughout the urban fabric, with at least one in each urban community outside the Greenbelt boundary.

Urban Employment Areas are designated to enable a variety of functions, which includes prestigious uses (usually office or combinations of office/clean industrial) with a signature address and a desire to locate among other similar uses.

Policy 1 states, “Urban Employment Areas are designated on Schedule B and are intended to be established clusters of business and economic activity capable of accommodating more than 2,000 jobs and with a broad spectrum of job densities. Urban Employment Areas shall be distributed throughout the urban area to help provide access to jobs throughout the city. Established Urban Employment Areas have capacities ranging from 2,000 to 20,000+ jobs.”

Policy 2 states in Urban Employment Areas, the Zoning By-law will:

- a. Permit traditional industrial uses such as manufacturing, warehousing, distribution, storage, communications, construction;
- b. Permit uses that store most products outdoors and require large land areas devoted to external storage, sale or service of goods or for vehicle sales and service; and
- c. Permit office uses and similar uses at similar densities, including, research & development and emergency services.

**The proposed development conforms to the Urban Employment Area policies of the OP, as it provides an office use of similar scale and purpose to the existing character of the area.**

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## **4.2.4 URBAN DESIGN**

The City of Ottawa has a framework in place to guide urban design in accordance with a series of policies and guidelines documents. The following sections identify the urban design policies and guidelines which are applicable to the site.

## **SECTION 2.5.1 – URBAN DESIGN AND COMPATIBILITY**

Compatible development is defined in the OP as “development that, although it is not necessarily the same as or similar to existing buildings in the vicinity, nonetheless enhances an established community and coexists with existing development without causing undue adverse impact on surrounding properties. It ‘fits well’ within its physical context and ‘works well’ among those functions that surround it” (Section 2.5.1).



The proposed development supports the seven (7) urban design objectives and associated principles set out in Section 2.5.1, as demonstrated below. It should be noted that the OP specifies “proponents are free to respond in creative ways to the Design Objectives and Principles and are not limited only to those suggested by the Design Considerations.”

**1. To enhance the sense of community by creating and maintaining places with their own distinct identity.**

- The proposed development will enhance the sense of community by providing for employment uses on a currently vacant portion of a lot.

**2. To define quality public and private spaces through development.**

- The proposed development will contribute to the overall coherency of the urban fabric and to the vitality of the surrounding area by providing employment uses that fits within the current employment area.

**3. To create places that are safe, accessible and are easy to get to, and move through.**

- The proposed development will be accessed from Solandt Road, an existing Collector Road, via March Road, an existing Arterial Road. A pedestrian walkway will also link the site to Solandt Road. The site is also within 200 metres of the future transit station at Solandt Road and March Road, providing greater access to the site through an alternative form of transportation that meets the City’s transportation objectives.

**4. To ensure that new development respects the character of existing areas.**

- The site is within the Kanata North Business Improvement Area. The character of the existing employment area in Kanata North is characterized by a variety of employment uses. The proposed development will infill a vacant portion of a lot with office uses that will further stimulate economic growth and provide a use that serves and complements the area.

**5. To consider adaptability and diversity by creating places that can adapt and evolve easily over time and that are characterized by variety and choice.**

- The proposed development will achieve a more compact urban form and will be adaptable to future circumstances in response to changing social, economic and technological conditions by acknowledging current best practices and creating an appropriate built form for the area.

**6. To understand and respect natural processes and features in development design.**

- The proposed development respects the natural processes and features in development design by using the existing available infrastructure on the site (i.e. stormwater). It also makes an effort to retain healthy trees on the site.

**7. To maximize energy-efficiency and promote sustainable design to reduce the resource consumption, energy use, and carbon footprint of the built environment.**

- The proposed development will consider sustainable goals and objectives to construct an efficient building for operational purposes. At a minimum, it will meet 2020 OBC requirements.

In addition to the Design Objectives described above, development proposals are evaluated against the compatibility criteria set out in Section 4.11 Urban Design and Compatibility of the OP.

**Table 4-1** provides an evaluation of the proposed development against the compatibility criteria set out in Policy 4.11.2.

**Table 4-1: Evaluation of Proposed Development - City of Ottawa Compatibility Criteria**

<b>Policies</b>	<b>Measure of Compatibility</b>
<b>Views</b>	The proposed development is of an appropriate scale, while meeting as-of-right setbacks, and will not impact or obstruct views.
<b>Building Design</b>	The proposed development design fits with the existing desirable character and planned function of the surrounding area by providing an employment use that complements the City's goals and objectives for the area. Existing office buildings in proximity to the proposed development are 5 storeys, which is the same number of storeys for the proposed development.
<b>Massing and Scale</b>	The zoning for the site directs building design and massing that the proposed development must comply with. The maximum height permitted in the Zoning By-law for the site is 22 metres. The proposed development would be approximately 19 metres tall, which is below the maximum permitted. Accordingly, the proposed development is of an appropriate massing and scale.
<b>High-Rise Buildings</b>	n/a
<b>Outdoor Amenity Areas</b>	The proposed development will include an outdoor seating area of approx. 150 m <sup>2</sup> for the office employees.
<b>Public Art</b>	n/a
<b>Design Priority Areas</b>	The site is not within a Design Priority Area.
<b>First Nations Peoples Design Interests</b>	n/a

**The proposed development meets the intent of the City of Ottawa's urban design objectives and compatibility criteria, as established in Sections 2.5.1 and 4.11 of the OP.**

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## 4.2.5 WALKING, CYCLING, TRANSIT, ROADS AND PARKING LOTS

To promote increased transit usage, private and/or public proponents of any development or redevelopment within 600 metres of a transit station or major transit stop along the rapid transit network shown on Schedule D will:

- a. Ensure that convenient and direct access between the proposed development and the transit station is provided or maintained; rapid-transit stations and where possible, transit stops are integrated into the development; and that in such cases, extended hours of public access through the buildings and quality linkages from stations and building entrances to sidewalks on nearby streets are provided;
- b. Locate any proposed high-density employment and residential development close to transit stations;
- c. Provide a pedestrian-friendly, weather-protected (where possible) environment between the access point(s) of the rapid-transit station or major transit stop and the principal entrances to adjacent buildings;
- d. Minimize walking distances from buildings to stations/major transit stops; and
- e. Provide adequate, secure and highly visible bicycle parking at rapid-transit stations/major transit stops (Section 4.3, Policy 3).

The proposed development is located within 200 metres of the future Solandt Road and March Road bus rapid transit station, as illustrated **Figure 4-2** and on Schedule D of the OP, as illustrated in **Figure 4-3**.

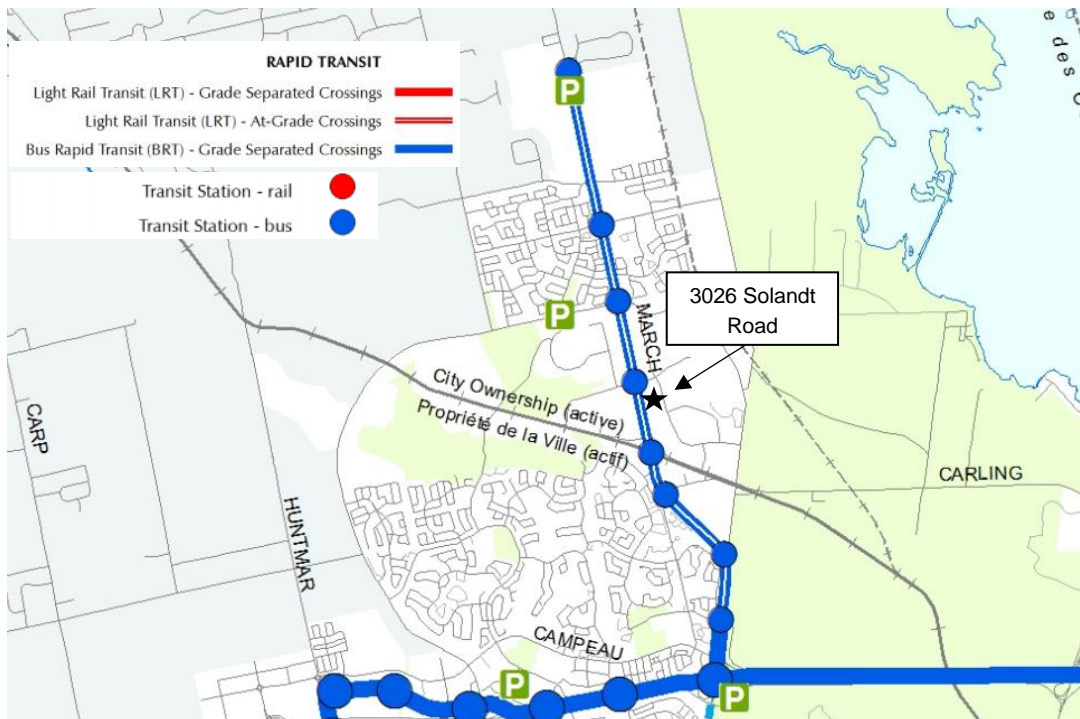
Further, as per Policy 5, the City may reduce parking requirements for uses located within 600 metres of a rapid-transit station and for uses where the need for on-site parking can be balanced with efforts to reduce reliance on the automobile. This is further discussed in the Zoning By-law section (Section 4.3) of this Report.

**The proposed development provides a higher density 5-storey office building of appropriate density conveniently located in close proximity to a future bus rapid transit station. Motor vehicle and bicycle parking are provided at a level appropriate for a suburban employment use. A pedestrian walkway is proposed that links the proposed development to Solandt Road. The site will also be well lit to allow for safe pedestrian routes in and around the building. The layout of the site has been configured to anticipate future intensification and alternative transportation options once the March/Solandt Road future bus rapid transit station is constructed.**

Figure 4-2: 3026 Solandt Road within 200 metre-buffer of future transit station. Source: GeoOttawa



Figure 4-3: City of Ottawa Official Plan, Schedule D – Rapid Transit and Transit Priority Road Network (Excerpt)



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#### 4.2.6 ANNEX 1 – ROAD CLASSIFICATION AND RIGHTS-OF-WAY

As per Section 2.0 of the OP, the City may acquire land for rights-of-way or the widening of rights-of-way through conditions of approval for a subdivision, severance, site plan, condominium or minor variance.

Section 7, Annex 1, Table 1 of the OP sets forth the right-of-way (ROW) widths that the City may acquire for roads, as indicated in **Table 4-2**.

**Table 4-2: Annex 1 – Right-of-Way Protection**

Road	From	To	ROW to be protected	Classification	Sector
March	Terry Fox	Richardson	44.5 m	Arterial	Urban
Solandt	Entire Length		24 m	Collector	Urban

**The proposed development will account for the rights-of-way to be protected, where necessary.**

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#### 4.2.7 TRANSIT-ORIENTED DEVELOPMENT GUIDELINES (2007)

The City encourages proponents of new development or redevelopment in close proximity to existing and proposed future transit stations to take into consideration and to demonstrate how the City’s Transit Oriented Development Guidelines have been addressed (Policy 3, Section 4.3).

The Transit-Oriented Development Guidelines were approved by Council on September 26, 2007. According to the Guidelines, “Transit-Oriented Development (TOD) is a mix of moderate to high-density transit-supportive land uses located within an easy walk of a rapid transit stop or station that is oriented and designed to facilitate transit use”.

The purpose of these Guidelines is to provide guidance to assess, promote and achieve appropriate Transit-Oriented Development within the City of Ottawa. These guidelines are applied throughout the City for all development within a 600 m walking distance of a rapid transit stop or station, in conjunction with the policies of the City of Ottawa Official Plan and other applicable regulations such as the City of Ottawa’s Zoning By-law.

The proposed development meets the following guidelines presented in **Table 4-3**, as outlined in the City’s Transit-Oriented Development Guidelines in anticipate of the future bus rapid transit station at March Road and Solandt Road.

**Table 4-3: Applicable Transit-Oriented Development Guidelines**

Guideline	Measure of Compatibility
<p><b>Guideline 1:</b> Provide transit supportive land uses within a 600 metres walking distance of a rapid transit stop or station.</p>	<p>The proposed development would be within 200 metres walking distance of the future Solandt Road and March Road transit station. The layout of the site has been configured to be transitional in order to anticipate future intensification and the potential for a greater modal split once the March/Solandt Road future bus rapid transit station is constructed.</p>
<p><b>Guideline 2:</b> Discourage non transit-supportive land uses that are oriented primarily to the automobile and not the pedestrian, cyclist or transit user.</p>	<p>The proposed development has taken into consideration its location in proximity to a proposed future bus rapid transit station at Solandt Road and March Road. The site has been configured in a way to allow additional intensification to occur on the site.</p>
<p><b>Guideline 16:</b> Design pedestrian connections that are convenient, comfortable, safe, easily navigable, continuous and barrier-free and that lead directly to transit.</p>	<p>The proposed development has been arranged to provide connections that meet the needs of pedestrians. A pedestrian walkway is proposed to link the proposed development to Solandt Road.</p>
<p><b>Guideline 32:</b> Provide no more than the required number of vehicle parking spaces, as per the Zoning By-law.</p>	<p>The proposed development provides more than the minimum requirement currently, but the layout is configured so that parking may be removed in the future to accommodate greater intensification and utilize the March/Solandt future bus rapid transit station in close proximity to the site.</p>

**The proposed development has taken into consideration the applicable Transit-Oriented Development Guidelines and incorporated these design elements wherever possible.**

#### **4.2.8 URBAN DESIGN GUIDELINES FOR DEVELOPMENT ALONG ARTERIAL MAINSTREETS**

As previously noted in **Figure 2-8**, March Road is an Arterial Mainstreet as identified in Schedule E (Urban Road Network), the City of Ottawa’s Official Plan.

The City of Ottawa’s Urban Design Guidelines for Development Along Arterial Mainstreets was approved by City Council in 2006. These guidelines are to be applied throughout the City for all streets identified as an Arterial Mainstreet within the Official Plan.



The Guidelines contain the following objectives:

- To foster compatible development that will contribute to the recognized or planned character of the streets;
- To promote a comfortable pedestrian environment and create attractive streetscapes;
- To achieve a high-quality built form and establish a strong street edge along Arterial Mainstreets;
- To facilitate a gradual transition to more intensive forms of development on Arterial Mainstreets;
- To accommodate a broad range of uses including retail, services, commercial, office, institutional and higher density residential; and
- To enhance connections that link development sites to public transit, roads and pedestrian walkways.

The Arterial Mainstreet design guidelines applicable to the proposed development are addressed in **Table 4-4**.

**Table 4-4: Applicable Arterial Mainstreet Development Guidelines**

<b>Guideline</b>	<b>Measure of Compatibility</b>
<b>Guideline 1:</b> Locate new buildings along the public street edge.	The proposed development will be located along the public street edge.
<b>Guideline 2:</b> Provide or restore a 2.0-metre-wide unobstructed concrete sidewalk. Locate the sidewalk to match the approved streetscape design plans for the area.	There is an existing sidewalk on March Road.
<b>Guideline 6:</b> Set new buildings 0 to 3.0 metres back from the front property line, and 0 to 3.0 metres back from the side property line for corner sites, in order to define the street edge and provide space for pedestrian activities and landscaping.	The proposed development is set back further than 3 metres from the March Road property line. However, the front of the property is along Solandt Road, which is a collector road.
<b>Guideline 7:</b> Design new development to be compatible with the general physical character of adjacent neighbourhoods. Protect the positive elements of the existing fabric including significant buildings, existing trees, pedestrian routes, public facilities and pedestrian amenities.	The proposed development will be compatible with the office use character along March Road and will provide a height appropriate to the existing buildings. Existing site elements will be retained.
<b>Guideline 8:</b> Provide significant architectural or landscape features at the corner on corner sites	Significant soft landscaping and green space is provided at the corner of the site adjacent to the March/Solandt Road corner.

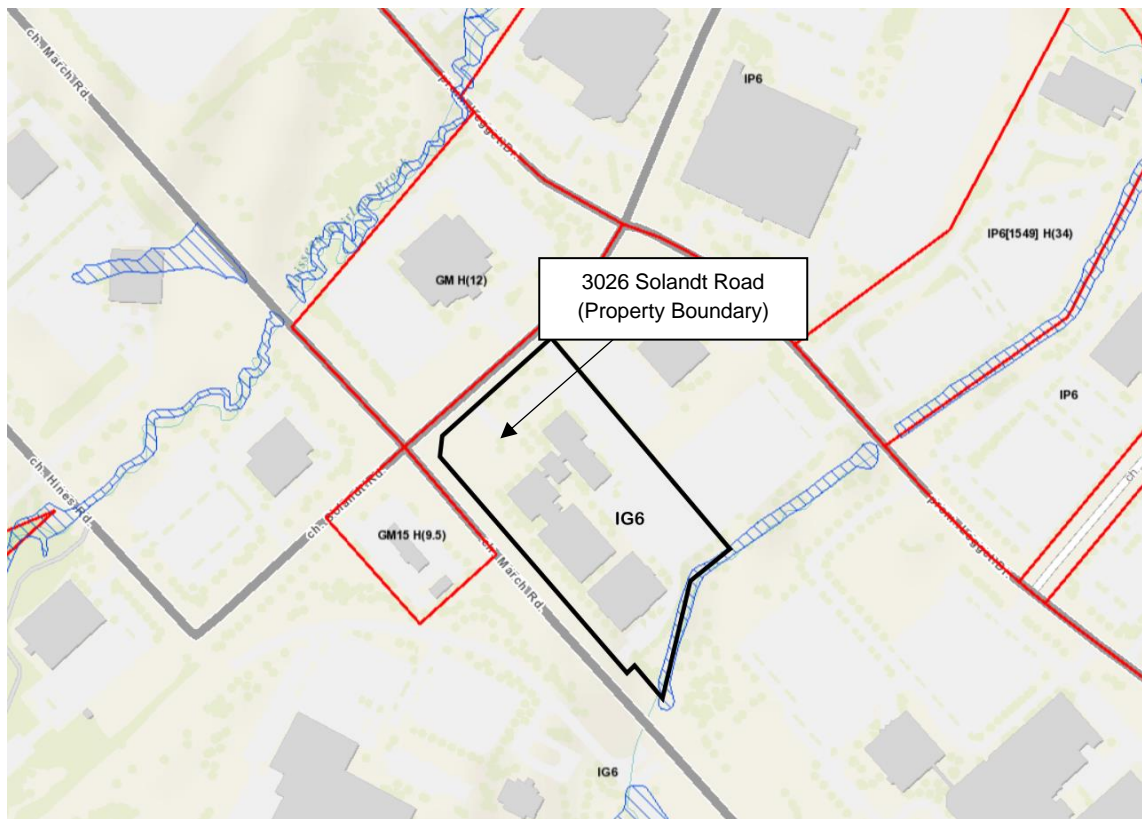
Guideline	Measure of Compatibility
<p>where there is no building, to emphasize the public streets and enhance the streetscape.</p>	
<p><b>Guideline 12:</b> Design the built form in relation to the adjacent properties to create coherent streetscapes.</p>	<p>The proposed development provides a height similar to the heights of the buildings in close proximity to the site.</p>
<p><b>Guideline 21:</b> Provide unobstructed pedestrian walkways that are a minimum of 2.0 metres wide along any façade with a customer entrance, along any façade adjacent to parking areas, and between the primary entrance and the public sidewalk. Provide additional width where doors swing out and car bumpers can potentially interfere with the walkway. Make all other on-site pedestrian walkways at least 1.5 metres wide.</p>	<p>A pedestrian walkway is proposed to link the proposed development to Solandt Road, connecting to the sidewalk.</p>
<p><b>Guideline 26:</b> Link access drives and parking lots of adjacent properties in order to allow for the circulation of vehicles between sites.</p>	<p>Existing access on Solandt Road will be utilized for the proposed development that will link the proposed development's parking lot to the road. The proposed development can also be accessed from an access on March Road south of the existing 5-storey office building adjacent to the proposed development. This allows for vehicle circulation between sites, if necessary.</p>
<p><b>Guideline 30:</b> Provide a consistent width of landscape and pedestrian areas across the front of the site.</p>	<p>The proposed development provides consistent soft landscaping around the outer periphery of the site. It also provides pedestrian access across the front of the site via a pedestrian walkway.</p>
<p><b>Guideline 36:</b> Provide a minimum 3.0 metre wide landscape area along the edge of a site where parking areas, drive lanes or stacking lanes are adjacent to a public street. Use trees, shrubs and low walls to screen cars from view while allowing eye level visibility into the site.</p>	<p>The proposed development provides 7.6 metres of soft landscaping abutting March Road to buffer the parking area from the road.</p>
<p><b>Guideline 50:</b> Enclose all utility equipment within buildings or screen them from both the arterial mainstreet and private properties to the rear. These include utility boxes, garbage and recycling container storage, loading docks and ramps and air conditioner compressors.</p>	<p>The proposed development provides screening from March Road for the garbage enclosure and loading space.</p>

The proposed development has taken into consideration the applicable Urban Design Guidelines for Arterial Mainstreets and incorporated these design elements wherever possible.

### 4.3 CITY OF OTTAWA COMPREHENSIVE ZONING BY-LAW 2008-250 (CONSOLIDATION JUNE 27, 2018)

Under the City of Ottawa Comprehensive Zoning By-law 2008-250 (Consolidation June 27, 2018), the site is currently zoned **General Industrial, Subzone 6 (IG6)**, as illustrated in **Figure 4-4**.

Figure 4-4: Site Zoning (geoOttawa, 2019)



#### 4.3.1 ZONING PROVISIONS

##### GENERAL INDUSTRIAL (IG) ZONE

The purpose of the IG – General Industrial Zone is to:

- Permit a wide range of low to moderate impact, light industrial uses in accordance with the Employment Area designation of the Official Plan or, the General Urban Area designation where applicable;

- Allow in certain Employment Areas or General Urban Areas, a variety of complementary uses such as recreational, health and fitness uses and service commercial (e.g. convenience store, personal service business, restaurant, automobile service station and gas bar), occupying small sites as individual occupancies or in groupings as part of a small plaza, to serve the employees of the Employment or General Urban Area, the general public in the immediate vicinity, and passing traffic;
- Prohibit retail uses in areas designated as Employment Area but allow limited sample and showroom space that is secondary and subordinate to the primary use of buildings for the manufacturing or warehousing of the product; and
- Provide development standards that would ensure that the industrial uses would not impact on the adjacent non-industrial areas.

The IG Zone permits a wide range of uses, such as:

- Light industrial uses;
- Office;
- Research and development centre; and
- Technology industry.

Additionally, Subzone 6 permits a place of assembly and prohibits the following uses:

- animal care establishment
- animal hospital
- automobile body shop
- automobile dealership
- automobile rental establishment
- automobile service station
- car wash
- convenience store
- crematorium
- gas bar
- heavy equipment and vehicle sales, rental and servicing
- hotel
- instructional facility, unless it is limited to computer training
- personal brewing facility
- personal service business
- restaurant
- truck transport terminal

Based on the site plan prepared by N45 Architecture Inc., dated December 2019, **Table 4-5** provides a detailed compliance chart of how the proposed development meets the Zoning By-law provisions for the General Industrial, Subzone 6 (IG6) Zone.

**Table 4-5: Zoning Compliance for General Industrial Zone, Subzone 6**

Zoning Provision	Requirement	Compliance (Yes or No)
Minimum lot area – Sec. 200 (6), Table 200C (i)	4,000 m <sup>2</sup>	Yes – Approx. 17,116 m <sup>2</sup>

Zoning Provision	Requirement	Compliance (Yes or No)
Minimum lot width – Sec. 200 (6), Table 200C (ii)	45 m	Yes – 119.97 m (Solandt Road)
Minimum front yard and corner side yard – Sec. 200 (6), Table 200C, (iii)	12 m	Yes – 12 m
Minimum interior side yard and rear yard – Sec. 200 (6), Table 200C (iv)	7.5 m	Yes – Greater than 7.5 m
Maximum lot coverage – Sec. 200 (6), Table 200C (v)	45%	Yes – Approx. 11%
Maximum building height – Sec. 199 (3), Table 199 (h) (ii)	22 m	Yes – Approx. 19 m
Minimum width of landscaped area – Sec. 199 (3), Table 199 (i) (ii)	Abutting a street: 3 m	Yes – 3 m

#### 4.3.2 PARKING AND LOADING SPACE PROVISIONS

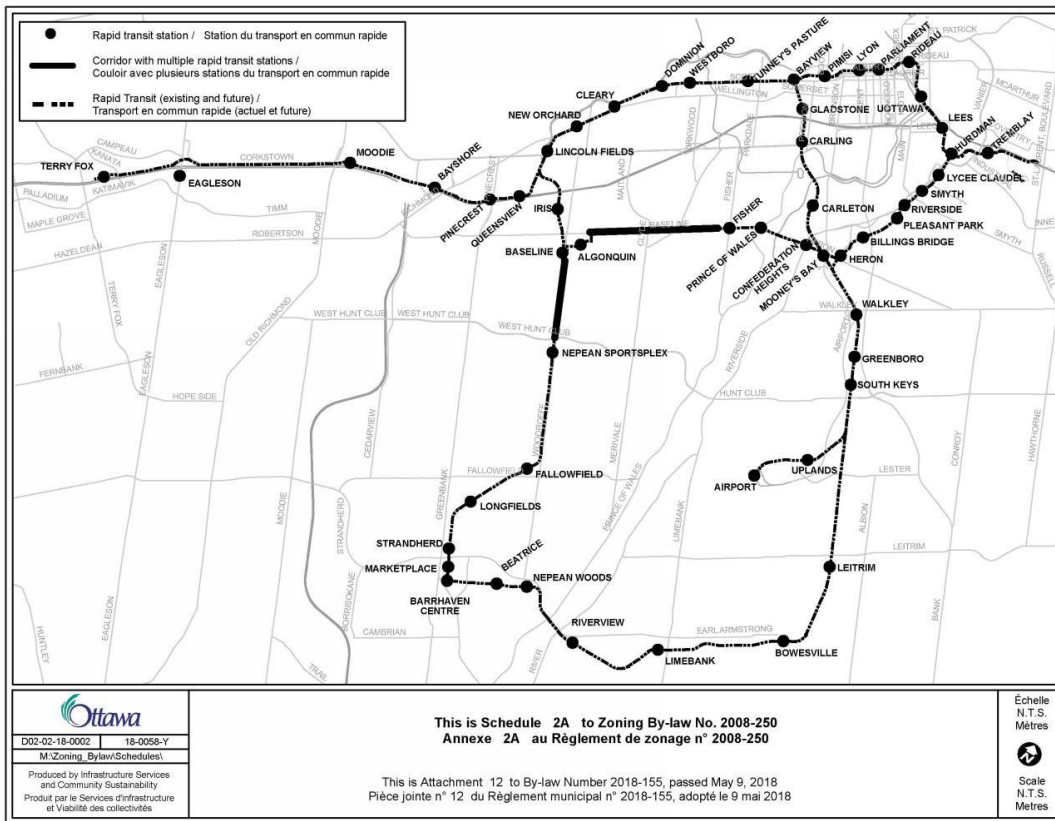
In addition, the Zoning By-law contains provisions for parking as set out in **Table 4-6**. The site is within Area C on Zoning By-law Schedule 1A, as illustrated in **Figure 4-6**.

Please note that, alternative parking provisions apply to uses located within close proximity to a rapid transit station as shown in Schedule 2A and 2B of the Zoning By-law, as indicated in Subsection 101(5) and Section 103 of the City of Ottawa’s Zoning By-law.

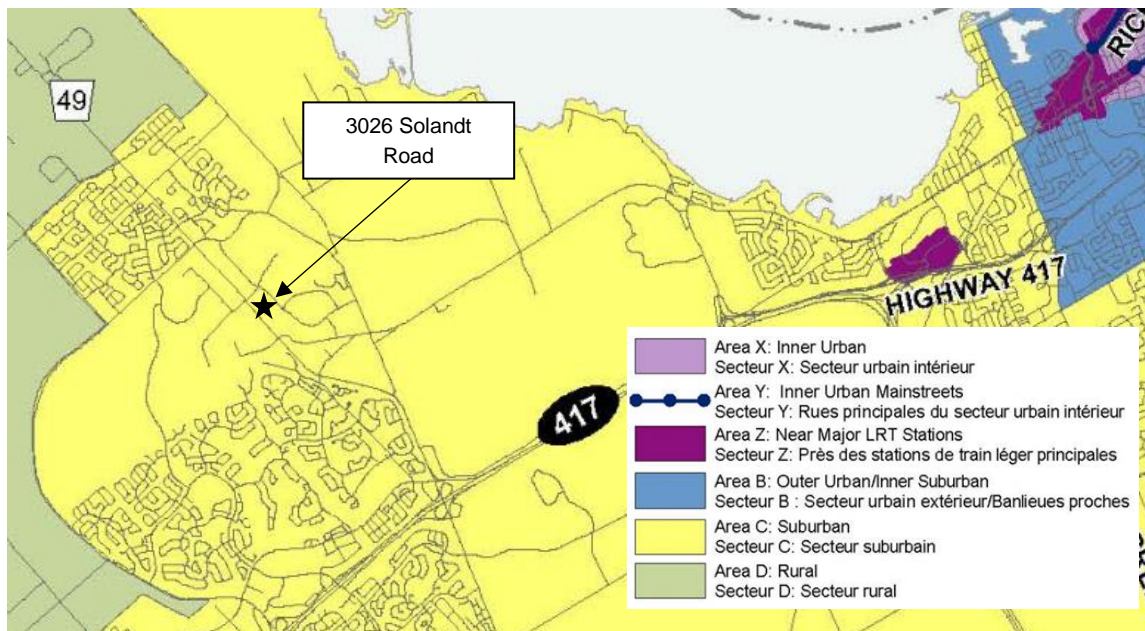
While the site is within 200 metres of the future March/Solandt Road bus rapid transit station, as was noted in **Figure 4-2**, this future station is not indicated on Schedule 2A, as indicated in **Figure 4-5**. Therefore, the alternative parking provisions in the Zoning By-law do not currently apply.



**Figure 4-5: Schedule 2A of the City of Ottawa's Zoning By-law**



**Figure 4-6: 3026 Solandt Road (Schedule 1A of the City of Ottawa Zoning By-law, via geoOttawa)**



**Table 4-6: Parking and Loading Space Provisions**

Zoning Provision		Requirement	Calculation (if applicable)	Compliance (Yes or No)
<b>Minimum parking space rate for office use (Table 101, Row N59)</b>		2.4 per 100 m <sup>2</sup> of gross floor area	8,230 m <sup>2</sup> / 100 * 2.4 = 198 spaces	Yes – 355 spaces
<b>Dimension requirements for a motor vehicle parking space – Sec. 106 (1)</b>	<b>(a) Width</b>	Minimum width of 2.6 m; and Maximum 3.1 m	n/a	Yes – 70 percent of spaces provide a width of 2.6 m
	<b>(b) Length</b>	5.2 m	n/a	
<b>Reduced dimension requirements for a motor vehicle parking space – Sec. 106 (3) (b)</b>		Up to 50% of the required and provided parking spaces may be reduced to a minimum width of 2.4 metres:  (i) Where 50 or more spaces are required for a broadcasting studio, heavy industrial use, light industrial use, office, post secondary educational institution, production studio, research and development centre and technology industry.	n/a	Yes – 30 % of spaces are reduced to a width of 2.4 m
<b>Barrier free parking space requirements for large organizations – Accessibility for Ontarians with Disabilities Act (AODA) – Regulation 80.36 (1), 4</b>		Where there are between 201 and 1,000 parking spaces: 2 parking spaces and an additional 2 per cent of parking spaces for the use of persons with disabilities.  i. Where an even number of parking spaces for the use of persons with disabilities are provided, an equal number of parking spaces for Type A and Type B parking spaces must be provided.  ii. Where an odd number of parking spaces for the use of persons with disabilities are provided, the number of parking spaces must be divided equally between Type A and Type B parking spaces, but the additional parking space, the	355 spaces * 0.02 = 7 spaces + 2 spaces = 9 spaces	Yes – 9 spaces provided (5 Type A and 4 Type B)

Zoning Provision	Requirement	Calculation (if applicable)	Compliance (Yes or No)
	odd-numbered space, may be a Type B parking space.		
<b>Type A parking space width – AODA, Regulation 80.34, 1</b>	A wider parking space which has a minimum width of 3.4 metres and signage that identifies the space as “van accessible”.	n/a	Yes – 3.4 m
<b>Type B parking space width – AODA, Regulation 80.34, 2</b>	A standard parking space which has a minimum width of 2,400 mm	n/a	Yes – 2.4 m
<b>Access aisle width – AODA, Regulation 80.35, 1 and 2</b>	<p>Access aisles must be provided for all parking spaces for the use of persons with disabilities in off-street parking facilities</p> <p>Access aisles may be shared by two parking spaces for the use of persons with disabilities in an off-street parking facility and must meet the following requirements:</p> <ol style="list-style-type: none"> <li>1. They must have a minimum width of 1.5 m.</li> <li>2. They must extend the full length of the parking space.</li> <li>3. They must be marked with high tonal contrast diagonal lines, which discourages parking in them, where the surface is asphalt, concrete or some other hard surface.</li> </ol>	n/a	Yes – 1.5 m
<b>Minimum driveway width – Sec. 107 (1) (a) (ii)</b>	In the case of a parking lot, 6.7 metres for a double traffic lane.	n/a	Yes – 6.7 m
<b>Minimum aisle width – Sec. 107, Table 107</b>	6.7 m for 90 degrees	n/a	Yes – 6.7 m
<b>Minimum number of bicycle parking spaces for office use – Sec. 111 – Table 111A (e)</b>	1 per 250 m <sup>2</sup> of gross floor area	8,230 m <sup>2</sup> / 250 * 1 = 33 spaces	Yes – 33 spaces

Zoning Provision		Requirement	Calculation (if applicable)	Compliance (Yes or No)
Minimum bicycle parking space dimensions – Sec. 111, Table 111B	(a) Horizontal	Width: 0.6 m Length: 1.8 m	n/a	Yes
	(b) Vertical	Width: 0.5 m Length: 1.5 m	n/a	Yes
Bicycle parking spaces may be located in landscaped area – Sec. 111 (7)		Max. of 50% or 15 spaces, whichever is greater	$33 / 2 = 16.5$	Yes – Bicycle parking not located in landscaped area
Minimum bicycle parking aisle width - Sec. 111 (9)		1.5 m	n/a	Yes – 6.97 m
Minimum number of vehicle loading spaces for office use – Sec. 113, Table 113A (b)		5,000-9,999 m <sup>2</sup> = 1 space	n/a	Yes – 1 loading space is provided
Minimum width of driveway accessing loading space – Sec. 113, Table 113B (a) (ii)		Double traffic lane – 6 m	n/a	Yes – 6.7 m
Minimum width in metres of aisle accessing loading space, by angle of loading space – Sec. 113, Table 113B (b) (i)		45 degrees or less = 5 m	n/a	Yes – 12.71 m
Minimum loading space dimensions – Sec. 113, Table 113B	(b) Width	3.5 m	n/a	Yes – 3.5 m
	(c) Length	(ii) Other cases: 7 m	n/a	Yes – 7 m

The proposed development complies with the general intent of the Zoning By-law and the proposed office development is a permitted use. The proposed development provides a compatible use and meets the appropriate requirements, as regulated in the Zoning By-law.

## 5 SUMMARY OF OPINION

It is the professional opinion of WSP that the proposed 5-storey office building at 3026 Solandt Road represents good land use planning and is appropriate for the site for the following reasons:

- The proposed development supports and is consistent with the 2014 Provincial Policy Statement.
- The proposed development conforms to the strategic directions and policies of the Official Plan by directing economic growth to urban areas and supporting intensification in the urban area.
- The proposed development meets several principles under the Urban Design Objectives and Principles of Section 2.5.1 (Urban Design and Compatibility) of the OP, as well as the Compatibility criteria of Section 4.11.
- The proposed development has taken into consideration the applicable Transit-Oriented Development Guidelines, and Urban Design Guidelines for Development Along Arterial Mainstreets, and incorporated these design elements wherever possible and applicable.
- The proposed development complies with the general intent and purpose of the Zoning By-law.

In conclusion, the Site Plan Control approval being sought to support the proposed development at 3026 Solandt Road represents good planning, is in the public interest, and contributes to the economic growth of the area by providing for new employment opportunities.

Please feel free to contact me at [Nadia.De-Santi@wsp.com](mailto:Nadia.De-Santi@wsp.com) or 613-690-1114 if you have any questions or require additional information.

Yours truly,

**WSP**



Nadia De Santi, MCIP, RPP  
Senior Project Manager



3026 SOLANDT ROAD  
JANUARY 2020

PLANNING RATIONALE  
SITE PLAN CONTROL APPLICATION