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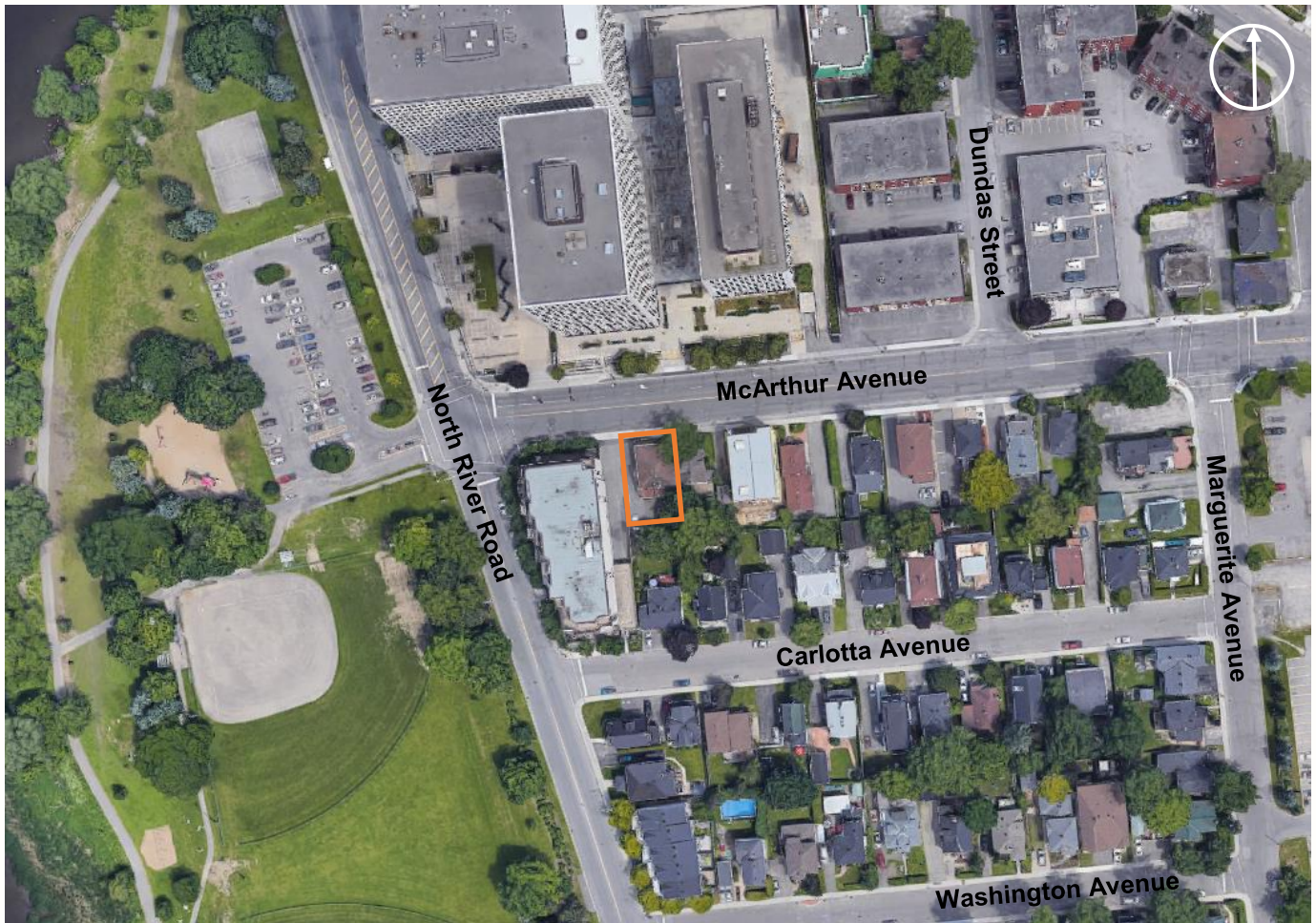
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# SITE PLAN CONTROL

## Proposed Low-Rise Apartment Building



PLANNING RATIONALE | 10 McArthur Avenue



### Prepared for:

2672915 Ontario Inc.  
2544 Bank Street  
Ottawa, ON K1T 1M9

### Prepared by:

Lloyd Phillips & Associates Ltd.  
File: 1881  
Date: July 30, 2019



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## 1. OVERVIEW

Lloyd Phillips & Associates Ltd. has been retained by 2672915 Ontario Inc. (the 'Owner') to prepare a planning rationale report for an application for Site Plan Control with respect to the development of a proposed low-rise residential apartment building located at 10 McArthur Avenue, in the City of Ottawa.

The site is a small, rectangular-shaped property with frontage on McArthur Avenue, which is a Traditional Mainstreet located in the Overbrook-McArthur neighbourhood. The existing two-storey building, which contains a former restaurant and second-storey residential use, will be demolished and replaced by the low-rise residential building.

The Owner is proposing to construct a new three-storey apartment building with ten dwelling units and two parking spaces in the rear. The proposed building will contain six one-bedroom units and four two-bedroom units. One of the one-bedroom units is a fully accessible, barrier-free unit. There will be a common rooftop terrace amenity area that is approximately 54 m<sup>2</sup> in size.

## 2. SUBJECT SITE & SURROUNDING CONTEXT

The site is in Ottawa's Overbrook-McArthur neighbourhood and is generally located east of the Rideau River, south of McArthur Avenue and west of the Vanier Parkway. The neighbourhood extends to the south in the area bounded by Donald Street to the north, St. Laurent Boulevard to the east, the Rideau River to the west, and Highway 417 to the south. The neighbourhood is characterized by a mix of residential and non-residential uses, in a variety of styles, ages and sizes.

Specifically, the site is located on the south side of McArthur Avenue, in the block bounded by North River Road to the west, Carlotta Avenue to the south and Marguerite Avenue to the east.

The following building types and land uses directly abut the site:

- < **North** High-rise government office complex; mid-rise apartment buildings
- < **South** Low-rise residential
- < **East** Low-rise residential apartment buildings; service commercial uses
- < **West** Low-rise residential condominium building (Les Terrasses Amélie); Riverain Park

The site currently contains a two-storey mixed use building with ground-floor restaurant use (former Toderic's) and upper residential. The site consists of the following specifications and legal description:

<b>Area</b>	406 m <sup>2</sup>
<b>Frontage</b>	14.5 m
<b>Depth</b>	28.0 m
<b>Legal Description</b>	Part of Lot 62, R-Plan 239, City of Ottawa
<b>PIN</b>	0424-90039





Figure 1. Site context demonstrating property addresses and boundaries (GeoOttawa, 2017)

Figure 2 below is an extract of the existing draft plan of survey. Figures 3 and 4 represent the subject site and immediately surrounding context. Figures 5 through 7 represent the existing site conditions and surrounding context at street level.

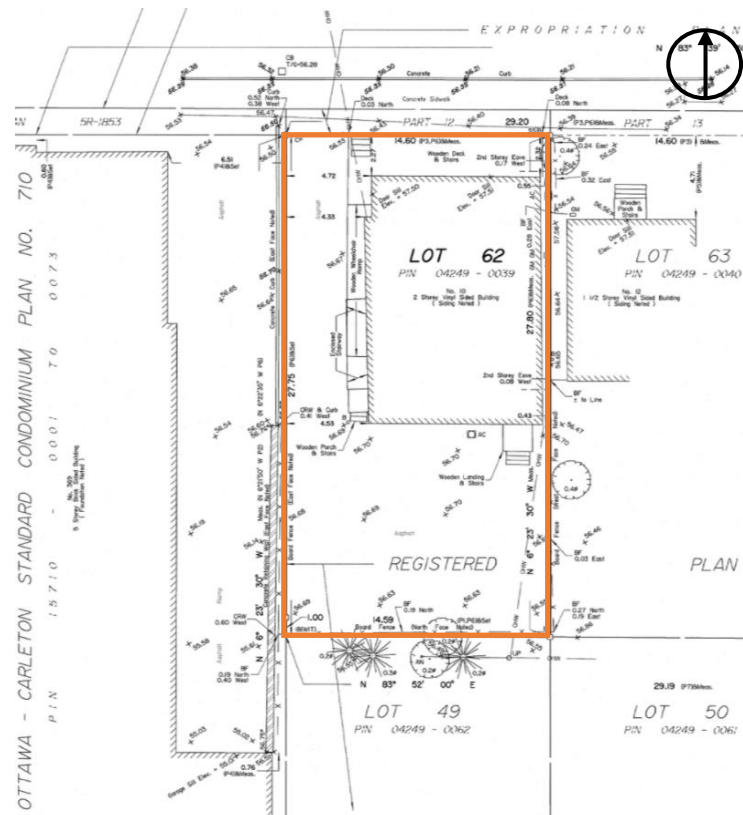


Figure 2. Extract of plan of survey







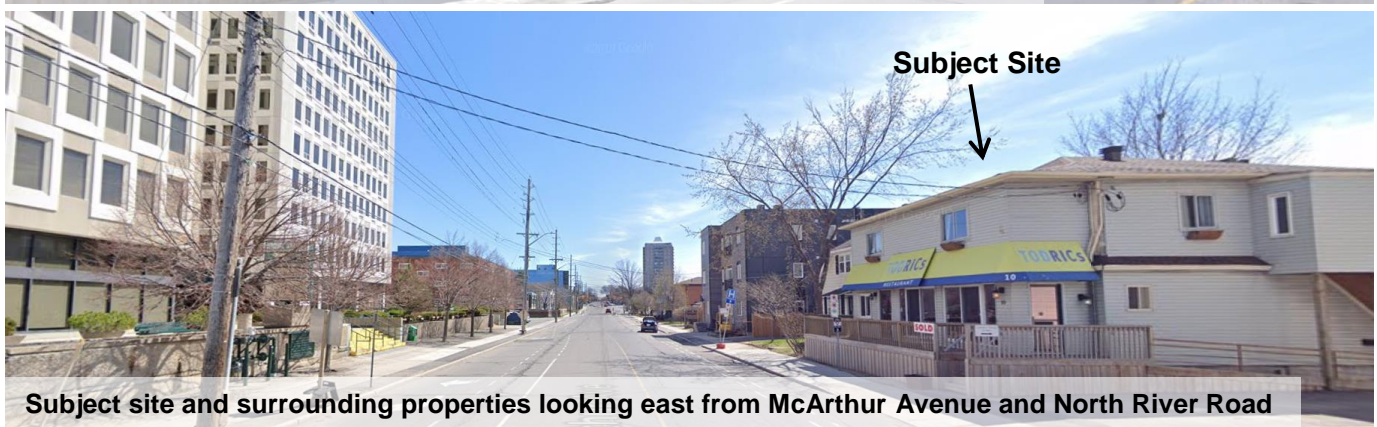
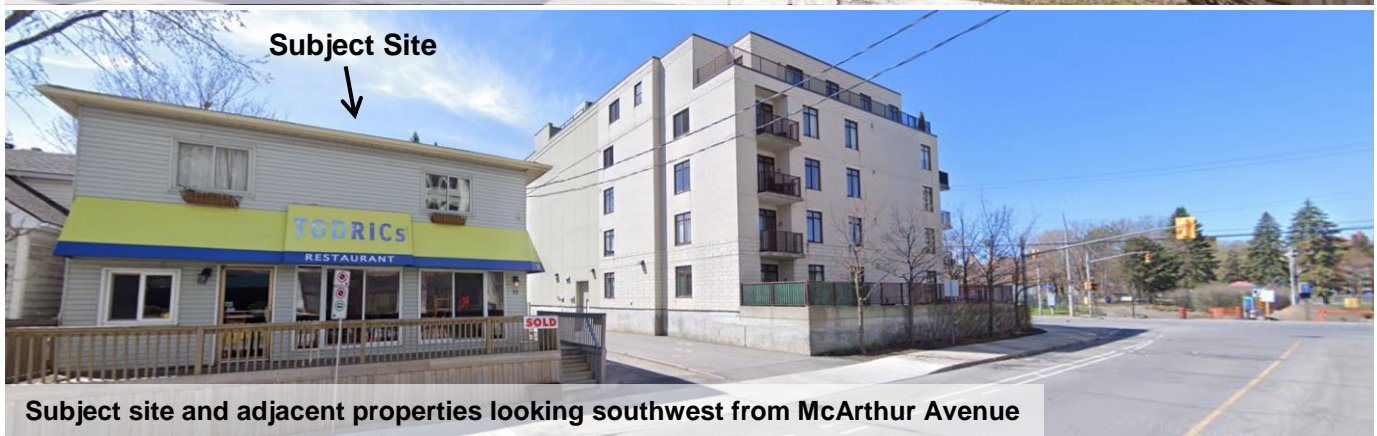
Figure 3. Aerial imagery of subject site and surrounding context, looking north (Google Maps)





Figure 4. Aerial imagery of subject site and surrounding context, looking southeast (Google Maps)





*Figure 5. Streetviews of site and surrounding properties (Google Maps, 2019)*





Figure 6. Streetviews of site and surrounding properties (Google Maps, 2019)



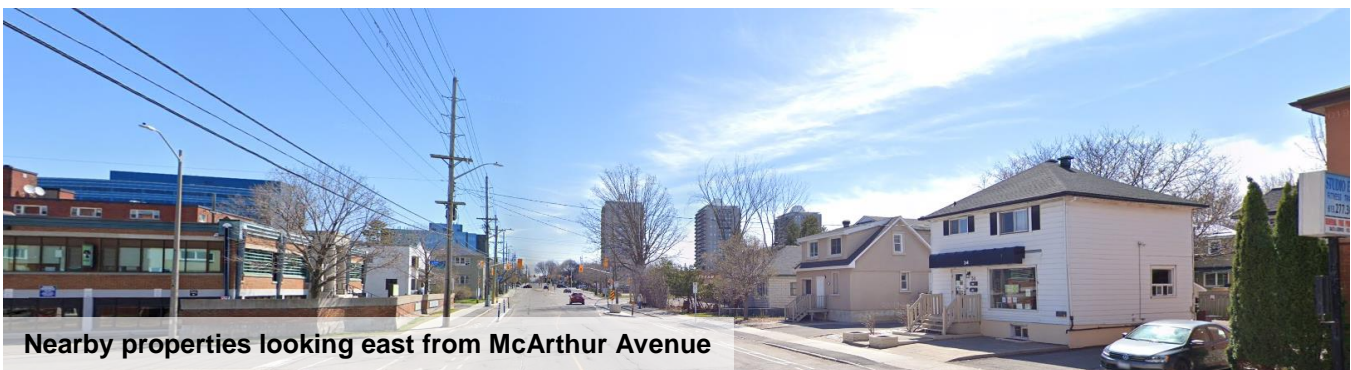
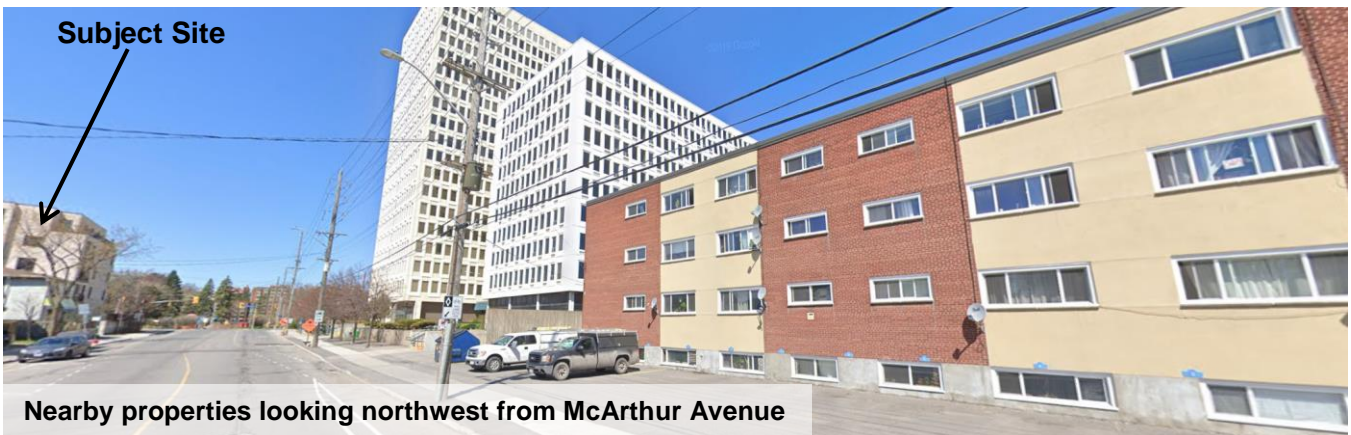
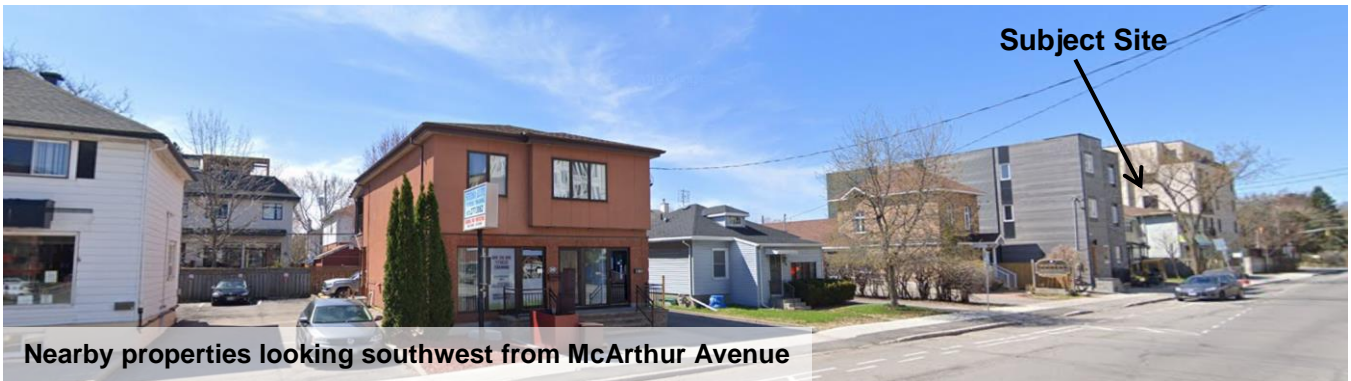
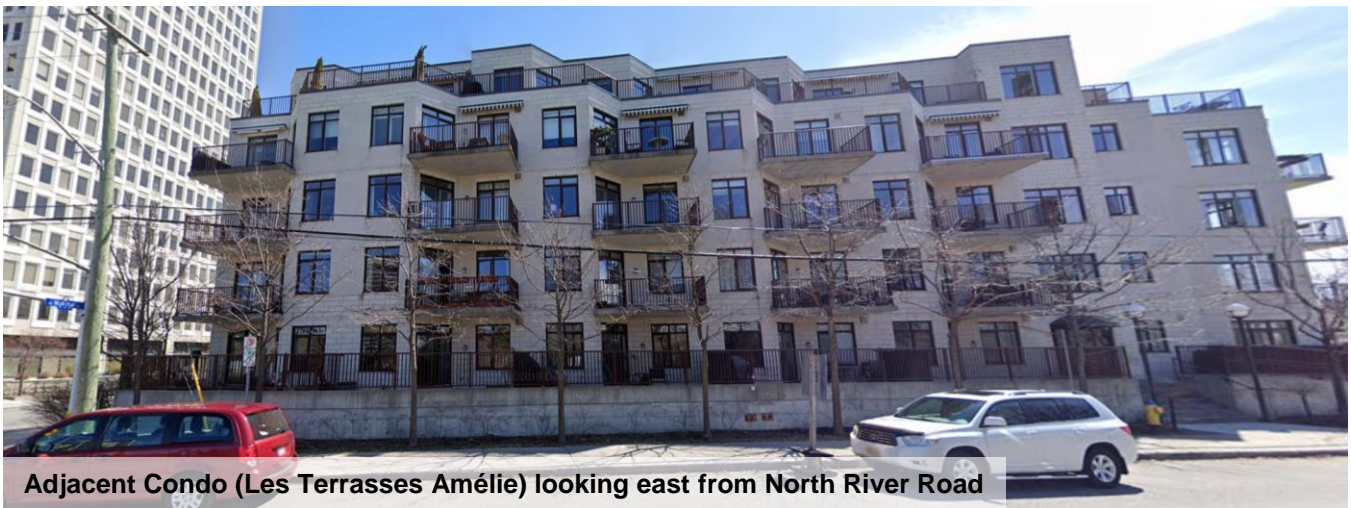


Figure 7. Streetviews of site and surrounding properties (Google Maps, 2019)





### 3. PROPOSED DEVELOPMENT

The Owner is proposing to demolish the existing mixed-use commercial/residential building and construct a three-storey residential apartment building with 10 dwelling units. The proposal requires an application for Site Plan Control. No minor variances are required for the proposed development, and no amendments to the Official Plan or Zoning By-law are required.

The proposed low-rise apartment building will contain 10 dwelling units. Six of the apartments are one-bedroom units, one of which is fully accessible/barrier-free, and four are two-bedroom apartments.

There will be a common rooftop terrace amenity area that is approximately 54 m<sup>2</sup> in size. Access to the rooftop terrace will be controlled and restricted to ensure noise by-law compliance.

The site will make use of the existing drive aisle access on the west side of the property from McArthur Avenue, leading to two parking spaces in the rear yard. Garbage and recycling facilities will be fully enclosed within the ground floor garbage room and will be brought to the street by property maintenance crews. Snow will be stored on-site and removed as required.

The building features a flat roof with a mezzanine level, is rectangular in shape, and has a cantilever design with the second and third storeys overhanging the driveway along the west side of the building. This will allow for a larger floor plate on the second and third storeys while accommodating the drive aisle along the westerly property line.

The front yard will feature a grass area and low-rise planting, as well as a front entrance area with pavers. The paver walkway runs along the eastern property line to the rear of the building where there is a ramp to the barrier-free unit.

The rear yard will feature two parking spaces with a porous asphalt materiality. There is a small landscaped area with shrubs along the southern property line, as well as six bicycle parking spaces on a paver pad.

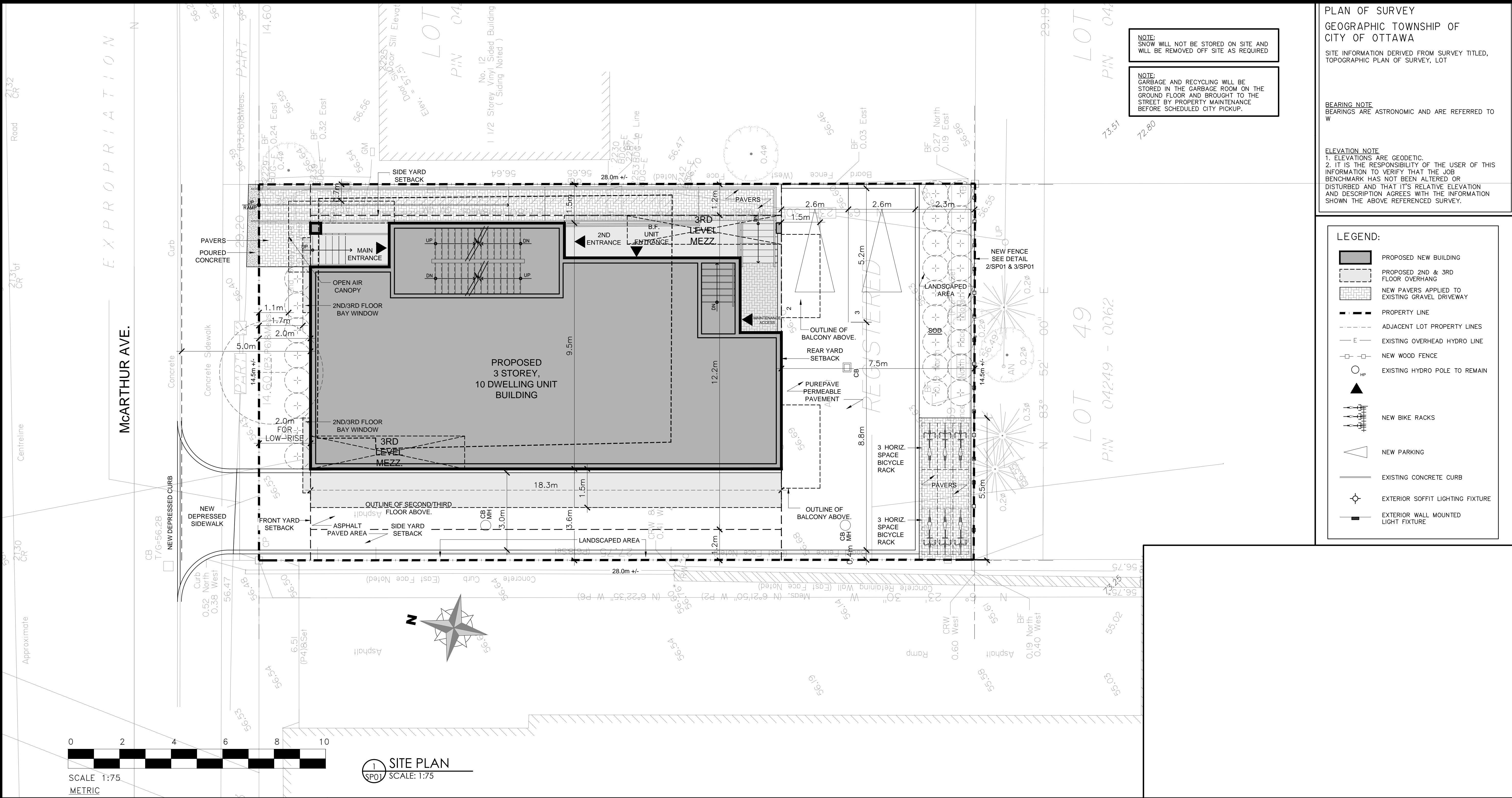
*Table 1. Development details*

Lot Area	406 m <sup>2</sup>	Gross Floor Area	791 m <sup>2</sup>
Front Yard Setback	2.0 m	Dwelling Units	10 units
Interior Side Yard Setback (West)	3.6 m	Private Amenity Area	19.36 m <sup>2</sup>
Interior Side Yard Setback (East)	1.2 m	Communal Amenity Area	56.40 m <sup>2</sup>
Rear Yard Setback	7.50 m	Vehicle Parking	2 spaces
Building Height	13.60 m	Bicycle Parking	6 spaces

Figure 8 presents the proposed Site Plan, Figure 9 presents the Landscape Plan prepared by Levstek Consultants Inc., and Figures 10 through 12 represent the proposed building elevations, prepared by P-Squared Concepts Inc. Renderings of the proposed design are presented in Figure 13.







CLIENT:

P<sup>2</sup> Concepts

739 REDGEWOOD AVE., UNIT 201  
OTTAWA, ONTARIO, K1V 6M8

PLAN OF SURVEY

GEOGRAPHIC TOWNSHIP OF  
CITY OF OTTAWA

SITE INFORMATION DERIVED FROM SURVEY TITLED,  
TOPOGRAPHIC PLAN OF SURVEY, LOT

BEARING NOTE  
BEARINGS ARE ASTRONOMIC AND ARE REFERRED TO  
W

ELEVATION NOTE  
1. ELEVATIONS ARE GEODETIC.  
2. IT IS THE RESPONSIBILITY OF THE USER OF THIS  
INFORMATION TO VERIFY THAT THE JOB  
BENCHMARK HAS NOT BEEN ALTERED OR  
DISTURBED AND THAT ITS RELATIVE ELEVATION  
AND DESCRIPTION AGREES WITH THE INFORMATION  
SHOWN THE ABOVE REFERENCED SURVEY.

LEGEND:

- PROPOSED NEW BUILDING
- PROPOSED 2ND & 3RD  
FLOOR OVERHANG
- NEW PAVERS APPLIED TO  
EXISTING GRAVEL DRIVEWAY
- PROPERTY LINE
- ADJACENT LOT PROPERTY LINES
- EXISTING OVERHEAD HYDRO LINE
- NEW WOOD FENCE
- EXISTING HYDRO POLE TO REMAIN
- NEW BIKE RACKS
- NEW PARKING
- EXISTING CONCRETE CURB
- EXTERIOR SOFFIT LIGHTING FIXTURE
- EXTERIOR WALL MOUNTED  
LIGHT FIXTURE

11			
10			
09			
08			
07			
06			
05			
04			
03	FOR SITE PLAN CONTROL	PE	JULY 10 2019
02	FOR FINAL REVIEW	PE	JULY 08 2019
01	FOR COORDINATION	PE	JUNE 10 2019
No.	REVISIONS	BY	DATE

NOT AUTHENTIC UNLESS SIGNED AND DATED

CONSULTING ENGINEERS

PLANNER 1827 WOODWARD DR., SUITE 109, OTTAWA, ON, K2E 0P9

LANDSCAPE LEVISTEK CONSULTANTS LTD.  
5871 HUGH CRES., OTTAWA, ON, K0A 2W0

CIVIL LRL ASSOCIATES LTD.  
5430 CANOTEK RD., OTTAWA, ON, K1J 9G2

GEOTECHNICAL PATERSON GROUP INC.  
154 COLONNADE RD., OTTAWA, ON, K2E 7J5

DESIGNED BY: P.E. DRAWN BY: APPROVED BY:

PROJECT

10 McArthur Avenue

DRAWING TITLE

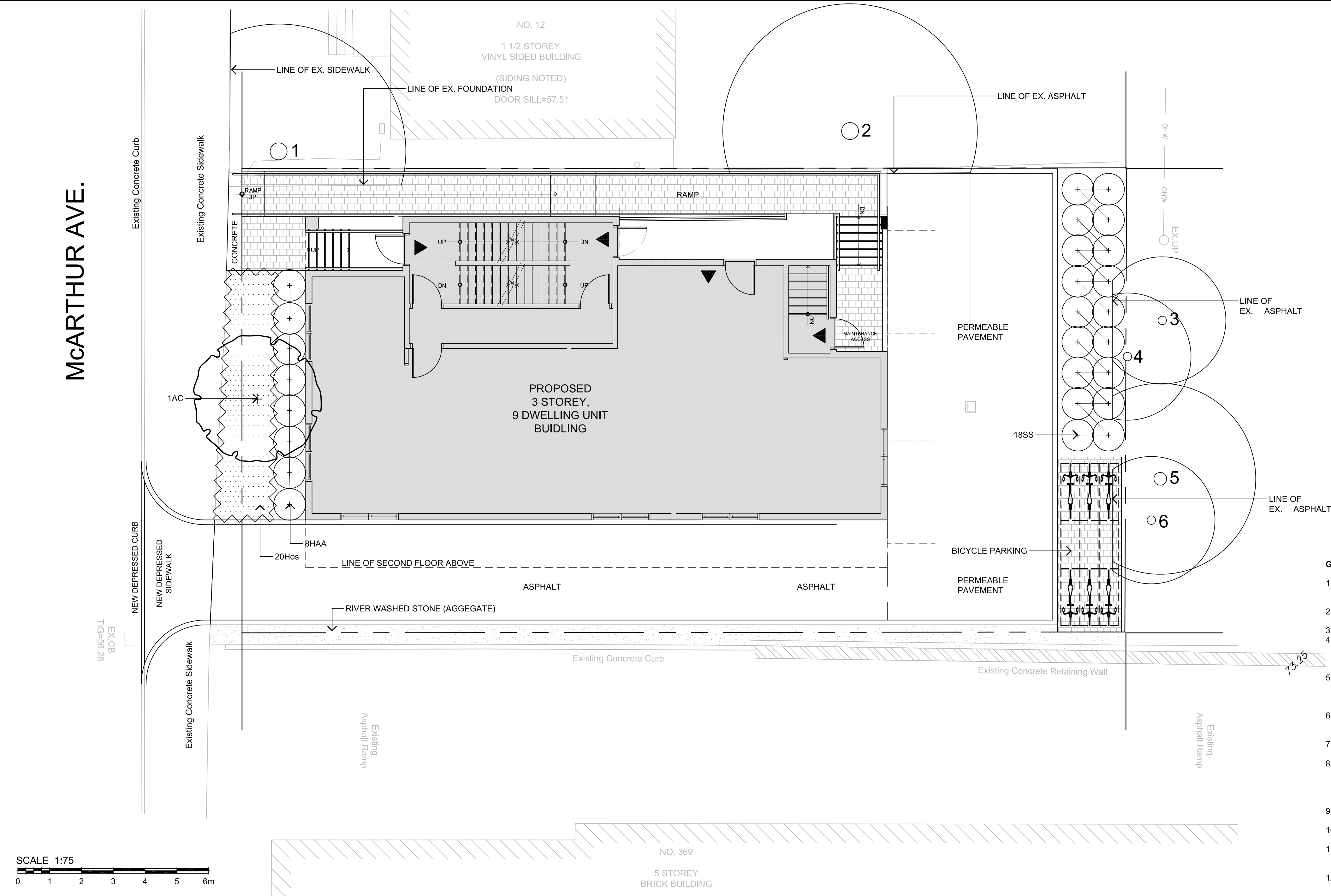
SITE PLAN

PROJECT NO.  
0318

DATE  
MAY 31, 2019

SP-01





- LEGEND / SYMBOL
- EXISTING TREE TO BE RETAINED (Refer to Existing Tree Inventory)
  - CRZ (Critical Root Zone)
  - PROPOSED DECIDUOUS TREE
  - PROPOSED SHRUBS
  - PROPOSED PERENNIALS / GRASSES
  - PROPOSED UNIT PAVERS
  - PROPOSED RIVER WASHED STONE

- GENERAL NOTES
- This drawing shall be read in conjunction with all related Architectural, Engineering and related Drawings and Documents.
  - Refer to Engineering Drawings for Grading and Servicing.
  - Refer to Architectural Drawings for Site layout.
  - Contractor shall provide the location(s) of all services/utilities by consulting Municipal Authorities and Utility companies and shall be responsible for adequate protection from damage during construction.
  - Plant material shall be No.1 Grade and shall comply with the Metric Guide Specifications For Nursery Stock (latest edition), published by the Canadian Nursery Trades Association.
  - Plant Material locations are **Schematic / Approximate only**. Contractor shall stake out locations on site prior to work.
  - Contractor shall guarantee all plant material for a period of one (1) full year from the date of final acceptance.
  - Perennials** and **Ornamental Grasses** shall be planted in a continuous prepared bed of 450mm depth planting soil mix covered over with 75mm depth composted pine mulch to finish grade. (**NO WEED CONTROL FABRIC**), as specified.
  - Reinstate all areas damaged or disturbed beyond the limit of Work.
  - Plant Material substitutions shall not be permitted without written approval from the Consultant.
  - Contractor shall advise Consultant a minimum of **48hrs.** prior to proceeding landscape work and any required Field Reviews.
  - THIS PLAN HAS BEEN PREPARED FOR MUNICIPAL SITE PLAN APPROVAL ONLY AND MAY NOT BE USED FOR ANY OTHER PURPOSE.**

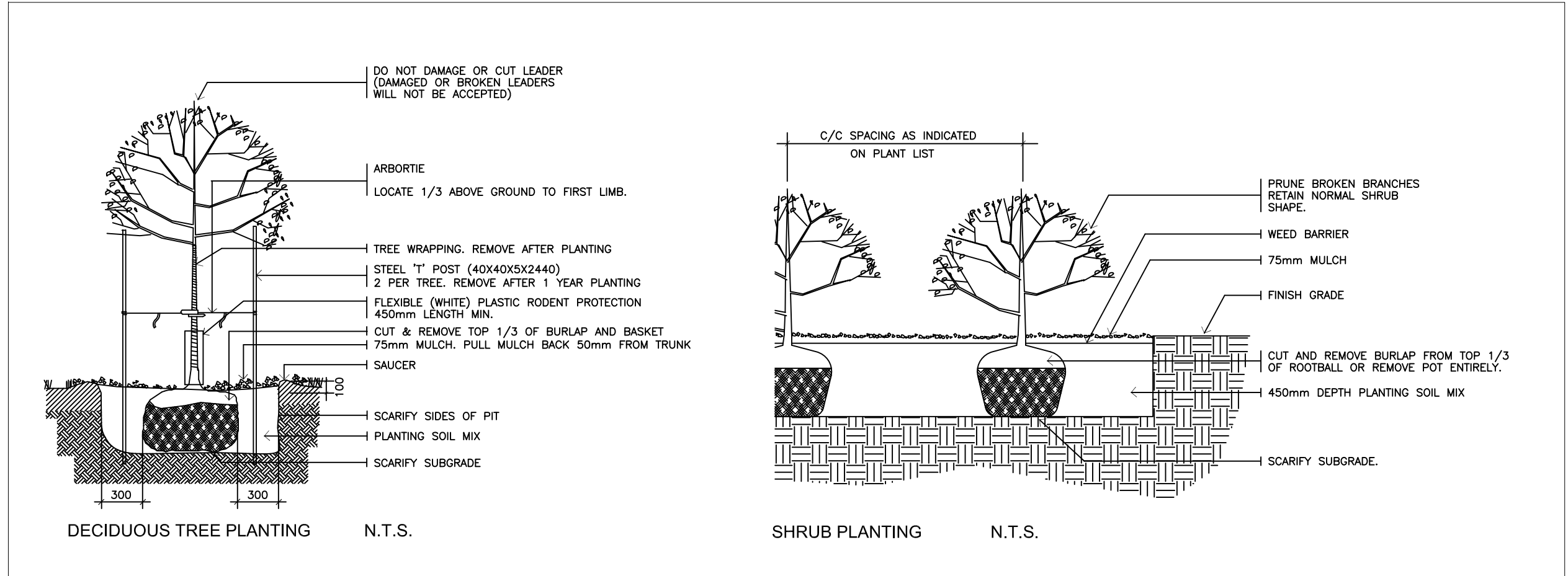
2.	JULY 10/19	ISSUE FOR SITE PLAN APPROVAL
1.	JULY 9/19	ISSUE FOR COORDINATION
no.	date	revision
North Arrow		
Stamp		

SCHEDULE OF EXISTING TREES  
(Inventory conducted June 14, 2019)

CODE	SPECIES	SIZE (dia.in cm)	CONDITION / TREATMENT / REMARKS
1	WHITE ELM	40	good / retain / preserve / adjacent property
2	NORWAY MAPLE	40	good / retain / preserve / adjacent property
3	WHITE SPRUCE	20	good / retain / preserve / adjacent property
4	CHOKECHERRY	20 (clump)	good / retain / preserve / shared property
5	WHITE SPRUCE	30	good / retain / preserve / adjacent property
6	WHITE SPRUCE	20	good / retain / preserve / adjacent property

PLANT MATERIAL SCHEDULE

CODE	BOTANICAL NAME	COMMON NAME	QTY.	SIZE	REMARKS
DECIDUOUS TREES					
AC	AMELANCHIER CANADENSIS	SHADBLow SERVICEBERRY	1	50mm cal	B&B, single stem
DECIDUOUS SHRUBS					
HAA	HYDRANGEA ARBORESCENS	ANNABELLE HYDRANGEA	8	50cm ht	potted, 100cm o/c
SS	SORBARIA SORBIFOLIA	FALSE SPIREA	18	50cm ht	potted, 100cm o/c
PERENNIALS					
Hos	HOSTA 'Big Daddy'	'Big Daddy' HOSTA	20	15 cm pot	plant 75cm o/c



1  
L1.01 PLANTING DETAILS NTS

Figure 9. Landscape Plan prepared by Levstek Consultants Inc.

Contractor shall check and verify all dimensions on site and report all errors and/or omissions to the Consultant.		
Work to be done in accordance with all applicable codes and by-laws.		
Do not scale Drawing.		
This Drawing shall not be used for construction until signed by the Consultant.		
Copyright reserved. This Drawing is the exclusive property of Levstek Consultants Inc. and shall not be used without consent.		
Consultant		
LEVSTEK CONSULTANTS INC. LANDSCAPE ARCHITECTS		
1871 Hugh Crescent, Ottawa, Ontario, K6A 2W0 613-826-0518		
Client		
Project		
10 McARTHUR AVENUE OTTAWA ONTARIO		
Drawing Title		
LANDSCAPE PLAN		
Drawn	Date	Drawing No.
MGB	MAY 2019	L1.01
Scale	Project No.	
1:75	1055	



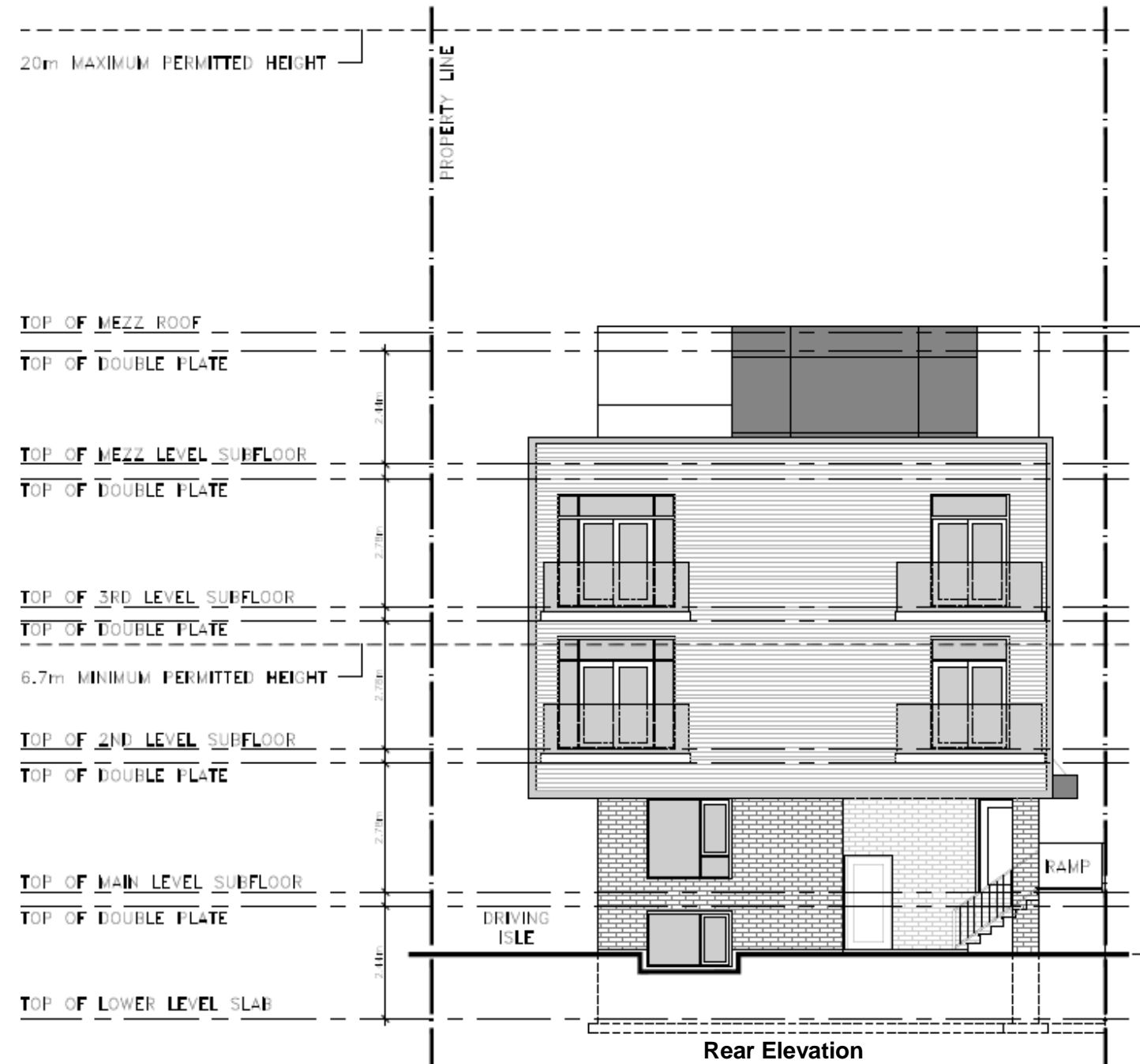
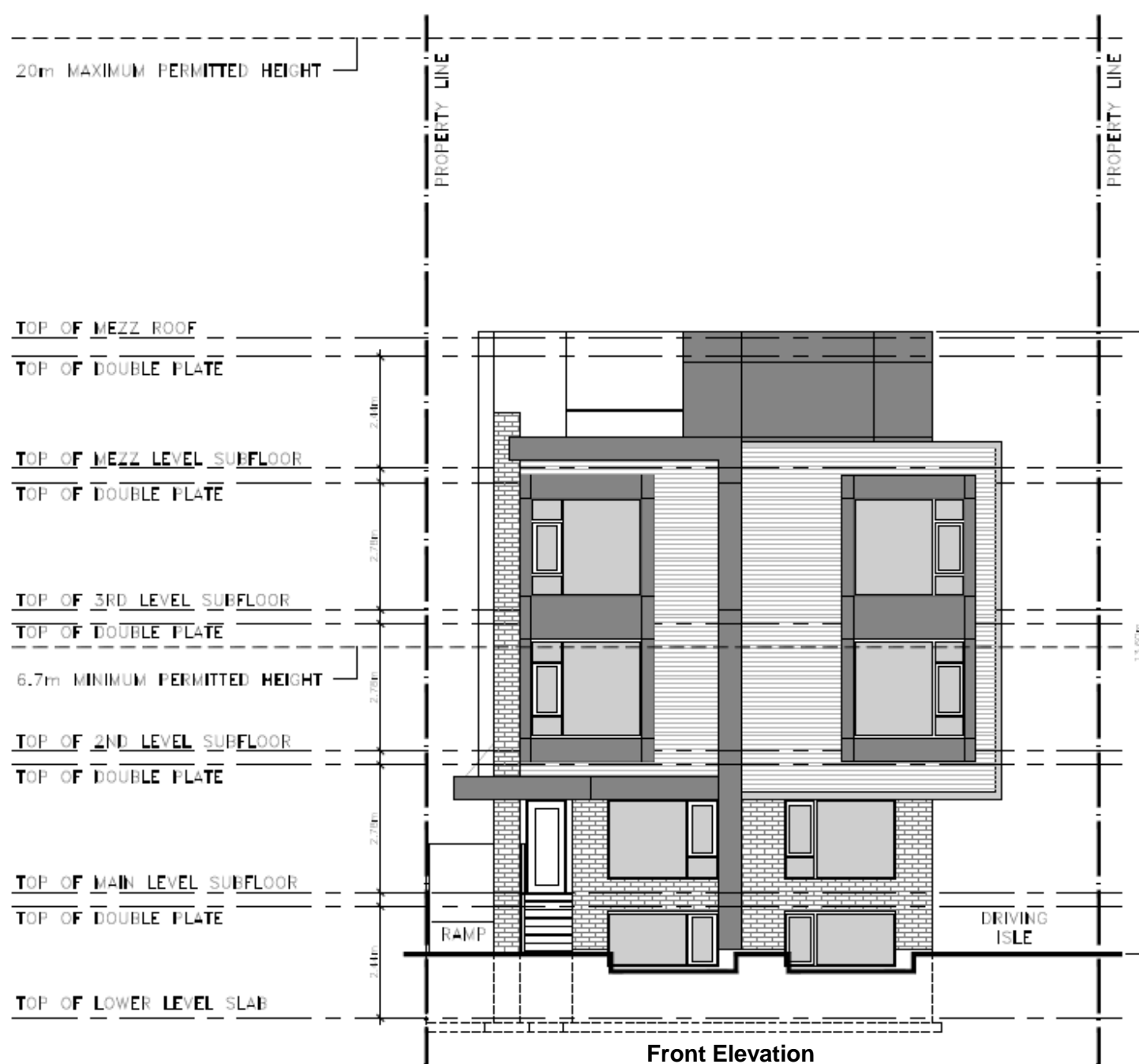


Figure 10. Front and rear elevations prepared by P-Squared Concepts Inc.



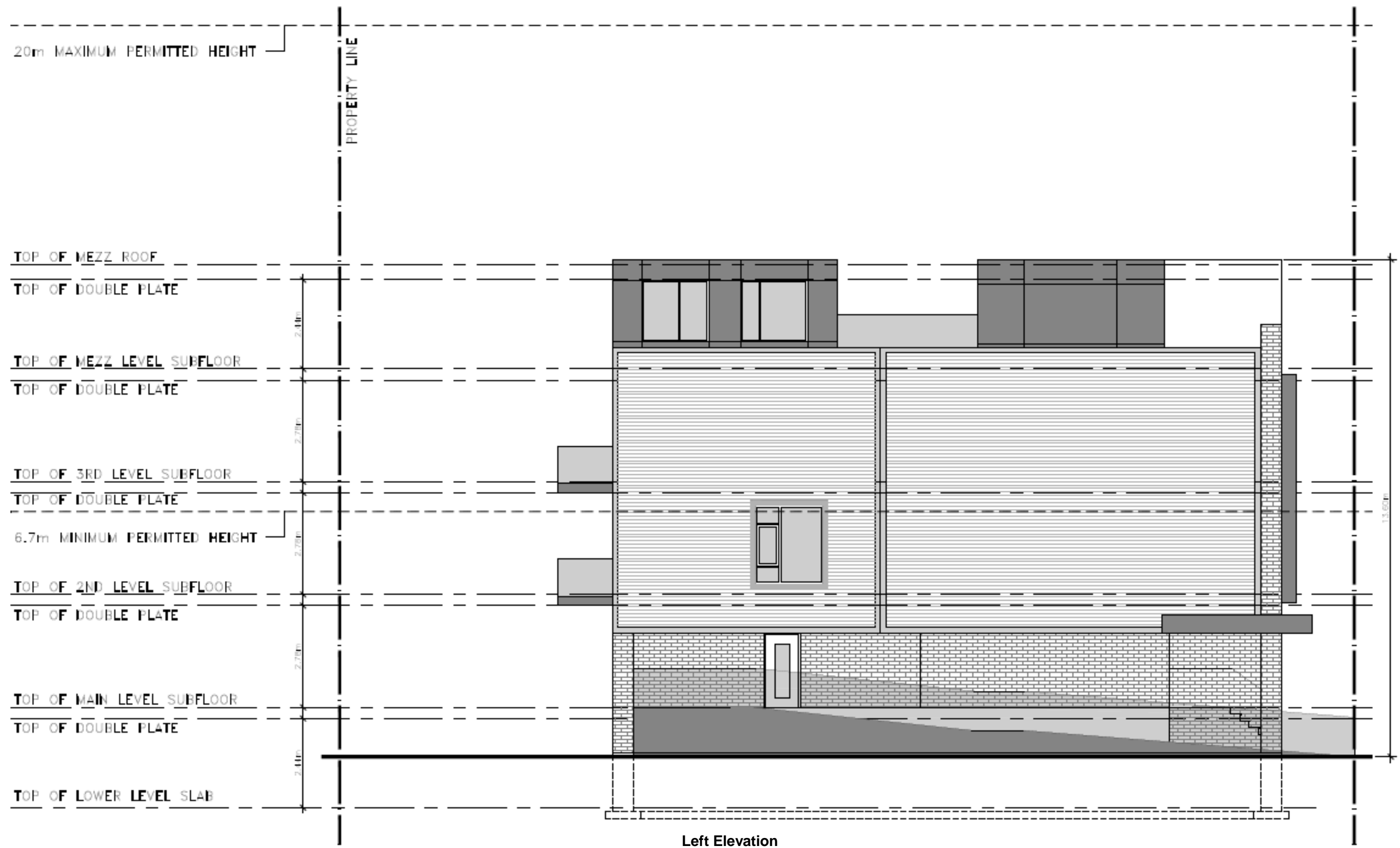


Figure 11. Side elevation prepared by P-Squared Concepts Inc.





Figure 12. Side elevation prepared by P-Squared Concepts Inc.



FRONT-RIGHT CORNER



FRONT-LEFT CORNER

Figure 13. Renderings prepared by P-Squared Concepts Inc.





## 4. PROVINCIAL POLICY STATEMENT 2014

The Provincial Policy Statement 2014 (PPS) provides policy direction on planning matters for the Province of Ontario. Decisions affecting all planning matters shall be consistent with the Provincial Planning Policies. The proposed application for Site Plan Control is consistent with the applicable policies of the PPS, as demonstrated below.

**Section 1.1.1** of the PPS states that healthy, livable, and safe communities are sustained by:

- a) “promoting efficient development and land use patterns which sustain the financial well-being of the Province and municipalities over the long term;
- b) accommodating an appropriate range and mix of residential [...] to meet long-term needs;
- c) avoiding development and land use patterns which may cause environmental or public health and safety concerns;
- e) promoting cost-effective development patterns and standards to minimize land consumption and servicing costs;”

**Section 1.1.3.2** of the PPS states that:

“Land use patterns within settlement areas shall be based on:

- a) densities and a mix of land uses which:
  - 1. efficiently use land and resources;
  - 2. are appropriate for, and efficiently use, the infrastructure and public service facilities which are planned or available, and avoid the need for their unjustified and/or economical expansion...
- b) a range of uses and opportunities for intensification and redevelopment...”

**Section 1.4.1** of the PPS states that:

To provide for an appropriate range and mix of housing types and densities required to meet projected requirements of current and future residents of the regional market area, planning authorities shall:

- a) “maintain at all times the ability to accommodate residential growth for a minimum of 10 years through residential intensification and redevelopment and, if necessary, lands which are designated and available for residential development; and
- b) maintain at all times where new development is to occur, land with servicing capacity sufficient to provide at least a three-year supply of residential units available through lands suitably zoned to facilitate residential intensification and redevelopment, and land in draft approved and registered plans”.

The above-noted PPS policies emphasize efficient development that is safe, respects the natural environment, uses available infrastructure, and provides for community needs in both the short- and long-term. The proposed development is an appropriate form of residential infill that capitalizes on existing services on an urban site.

The proposed development represents a form of intensification that fits and functions well within the City’s Urban Area. The use of existing municipal roads and services an appropriate and efficient use of resources. The proposal supports the intensification policies outlined in the PPS through the provision of added density to the urban area, while contributing to a diversification of housing options in an efficient use of land. The design of the proposed development is appropriate for the surrounding context and adjacent dwellings, including the nearby low-rise residential apartment buildings and condominium buildings, and maintains a comfortable scale at the streetscape and public realm. Overall, the proposed three-storey apartment building is consistent with the policies and objectives of the PPS.



## 5. OFFICIAL PLAN

The site is designated Traditional Mainstreet in the City of Ottawa's Official Plan, as shown in the below extract of Schedule B – Urban Policy Area. The Traditional Mainstreet designation is intended to encourage more dense and mixed-use development that both supports and is supported by increase walking, cycling and transit use. Traditional Mainstreets are typically characterized as having a tightly knit urban fabric, with buildings that are small-scale with narrow frontages close to the street.

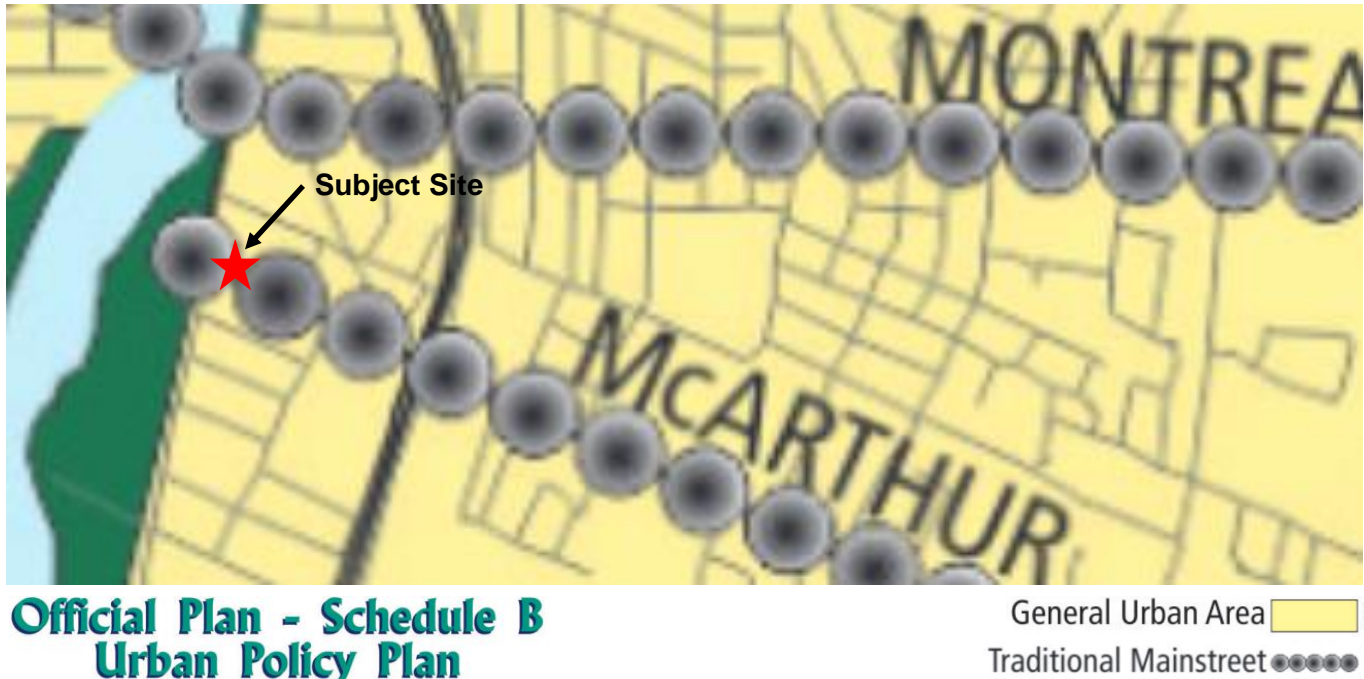


Figure 14. Extract from City of Ottawa Official Plan, Schedule B - Urban Area

**Section 2.1 Patterns of Growth** states that “[g]rowth will be directed towards key locations with a mix of housing, shopping, recreation and employment – locations that are easily accessible by transit and that encourage walking and cycling because destinations are conveniently grouped together... Future development... will be compact and efficient from a servicing point of view”. Designated Mainstreets will be “compact, liveable, and pedestrian-oriented with a vibrant mix of residential uses, and social, cultural and economic activity”.

**Section 2.2.2 Managing Growth Within the Urban Area** states that: “[...] the policy direction of this Plan is to promote an efficient land-use pattern within the urban area through intensification [...]”.

The applicable policies to the proposed development include:

1. “Residential intensification means intensification of a property, building or area that results in a net increase in residential units or accommodation and includes:
  - b. The development of vacant or underutilized lots within previously developed areas, being defined as adjacent areas that were developed four or more years prior to new intensification; and,
  - c. Infill development.
3. All intensification will occur in accordance with the provisions of Section 2.5.1, Urban Design and Compatibility, and 4.11, Urban Design and Compatibility, and with Section 4.6.1, Heritage Buildings and Areas.”



**Section 2.5.1 Urban Design and Compatibility** states the following: “[t]he Design Objectives of this Plan are qualitative statements of how the City wants to influence the built environment as the city matures and evolves. These Design Objectives are broadly stated, and are to be applied within all land use designations, either at the citywide level or on a site-specific basis. Design Principles further describe how the City hopes to achieve each of the Design Objectives, but may not be achieved or be achievable in all cases; these objectives include:”

1. To enhance the sense of community by creating and maintaining places with their own distinct identity;
2. To define quality public and private spaces through development;
3. To create places that are safe, accessible and are easy to get to, and move through;
4. To ensure that new development respects the character of existing areas;
5. To consider adaptability and diversity by creating places that can adapt and evolve easily over time and that are characterized by variety and choice. [OMB decision #2649, September 21, 2006];
6. To understand and respect natural processes and features in development design;
7. To maximize energy-efficiency and promote sustainable design to reduce the resource consumption, energy use, and carbon footprint of the built environment.”

**Section 3.6.3 Mainstreets** sets out policies relating to the development of Traditional and Arterial Mainstreets. Policy 3.6.3.1 states that Traditional Mainstreets “are planned as compact, mixed-use, pedestrian-oriented streets that provide access by foot, cycle, transit and automobile. A broad range of uses are permitted including retail and service commercial uses, offices, residential and institutional uses, which may be in mixed buildings or individual buildings (Section 3.6.3.5). Policy 3.6.3.7 states “Traditional and Arterial Mainstreets, or portions thereof, represent important areas for the preparation of Community Design Plans in accordance with the policies of Section 2.5.5. Further, Policy 3.6.3.10 encourages redevelopment and infill “in order to optimize the use of land through intensification, in a building format that encloses and defines the street edge with active frontages that provide direct pedestrian access to the sidewalk”.

**Section 4.11 Urban Design and Compatibility** states that, “[a]t the city-wide scale, issues of compatibility are addressed in the Official Plan through the appropriate designation of land and associated policies that direct where and how certain categories of land use should be permitted to develop. [...] It is recognized that because land use designations such as [...] Mainstreets contain broad use permissions, it will be necessary for the zoning by-law to establish more specific permitted use lists and development regulations within areas and on individual sites in a manner that achieves compatibility among proximate uses and built forms.

“At the scale of neighbourhoods or individual properties, issues such as noise, spillover of light, accommodation of parking and access, shadowing, and micro-climatic conditions are prominent considerations when assessing the relationships between new and existing development. Often, to arrive at compatibility of scale and use will demand a careful design response, one that appropriately addresses the impact generated by infill or intensification. Consequently, the issue of ‘context’ is a dominant theme of this Plan where it speaks to compatibility and design.

“Infill development may occur virtually anywhere in the city. Infill generally occurs on a single lot or a consolidated number of small lots, on sites that are vacant or underdeveloped. The resulting



development may be similar in use and size with adjacent uses, in which case it is generally straightforward to design the infill to be compatible with-or fit well with-its surroundings.”

On December 11, 2013, City Council adopted **Official Plan Amendment 150 (OPA 150)** to implement the completion of the five-year review of the Official Plan. OPA 150 is under appeal to the OMB and therefore not technically enforceable by the City. OPA 150 has no implications on the proposal.

The Official Plan emphasizes a need for context appropriate and efficient residential intensification as well as pedestrian and cycling-friendly development in areas designated as Traditional Mainstreet, and encourages a range of housing types and tenures. The compatibility criteria outlined in Section 2.5.1 and Section 4.11 of the Official Plan are used to evaluate the compatibility of development proposals, based on land uses and built form and their sensitivity to the context of the surrounding neighbourhood. Section 4.11, Urban Design and Compatibility sets out the policies and the criteria for assessing compatibility of development.

The proposed development of new, low-rise apartment building, which is intended to be purpose-built rental, offers an opportunity for increased housing options while maintaining an appropriate land use and development pattern for the surrounding context. The existing context and neighbourhood has a broad range and mix of building types and uses including low-rise single detached residential, mid-high rise office, low-rise service commercial, and low-to mid-rise condominium buildings. The proposed design of a low-rise apartment building capitalizes on the redevelopment of a small site in the urban area while providing intensification along a Traditional Mainstreet. The building is setback appropriately from the street and will help to animate the streetscape while providing opportunity for municipal roadway improvements. The proposed dwelling will contribute positively to the diverse neighbourhood fabric along McArthur Avenue. The design has thoughtfully considered the context within the existing streetscape, and features appropriate massing, scale, and materiality.

As demonstrated in the site plan and elevations, the proposed development will be compatible with the surrounding neighbourhood. The two provided parking spaces support a wider variety of rental opportunity while still encouraging active transportation and transit use, with the provision of bicycle parking facilities. The front yard landscaping will animate the streetscape, and the rear yard drive treatment will be permeable so as to support improved stormwater management. There will be appropriate landscape buffering and fencing along the rear of the property adjacent to the existing low-rise residential area. Finally, the development offers ample communal and private amenity area for the future residents.

Given the surrounding diverse neighbourhood context along this Traditional Mainstreet, the proposed development is compatible with the surrounding streetscape and will help to further animate the street by replacing a presently unused restaurant building. The proposal provides for a more efficient use of the land by creating a higher density residential accommodation on a small infill lot. The site is fully serviced by municipal water and sanitary services.

The proposed development conforms to the policies of the City of Ottawa Official Plan 2003, as amended.





### 5.1 Montreal Road District Secondary Plan

The subject site falls within the West Sector of the Montreal Road District Secondary Plan. The main goals of the Montreal Road District as set out in the Secondary Plan are to:

1. “Foster development and redevelopment along the Traditional Mainstreets and Arterial Mainstreet which complements and improves upon the positive qualities of the existing character of the District.
2. Improve streetscaping along Montreal Road, McArthur Avenue, St. Laurent Boulevard and the Vanier Parkway.
3. Improve pedestrian, cycling and transit facilities throughout the District.
4. Provide open spaces or other areas for public functions, publicly accessible landscaped open spaces and public art that reflects the history of the District”.

Policies related to the development include:

11. “Development and redevelopment on lots with frontage along Montreal Road, North River Road, McArthur Avenue and the Vanier Parkway will have building setbacks that provide a minimum with of 5.0 metres for sidewalks and related boulevards, which may include a combination of private and public property...
15. To maintain a Traditional Mainstreet building form fronting on Montreal Road and McArthur Avenue, the maximum building height along the Traditional Mainstreets will be six storeys, unless otherwise specified in this Plan”.

Per **Section 1.3.3 McArthur Avenue**, lots with frontage on McArthur Avenue are subject to the following policy:

1. “Development and redevelopment will reduce the number of vehicular accesses and will relocate these accesses to improve pedestrian connections and safety. No net increase to the number of vehicular accesses will be permitted on McArthur Avenue”.

The proposal meets the intent of the Secondary Plan by establishing a compact, well-designed infill residential development that improves the streetscape and makes use of the existing vehicular access. Further, the three-storey building does not exceed the prescribed the height limit, and the proposed building setback meets the 5.0 metre requirement for sidewalk and boulevard improvements.

The proposed development conforms to the policies of the Montreal Road District Secondary Plan.



## 6. ZONING BY-LAW 2008-250

The subject site is zoned TM, Traditional Mainstreet, in the City of Ottawa Zoning By-law 2008-250. A range and mix of residential and commercial uses are permitted in the TM zone. The proposed low-rise apartment building is a permitted use in the TM zone.

The purpose of the TM – Traditional Mainstreet zone is to:

- (1) *accommodate a broad range of uses including retail, service commercial, office, residential and institutional uses, including mixed-use buildings but excluding auto-related uses, in areas designated Traditional Mainstreet in the Official Plan;*
- (2) *foster and promote compact, mixed-use, pedestrian-oriented development that provide for access by foot, cycle, transit and automobile;*
- (3) *recognize the function of Business Improvement Areas as primary business or shopping areas; and*
- (4) *impose development standards that will ensure that street continuity, scale and character is maintained, and that the uses are compatible and complement surrounding land uses.*

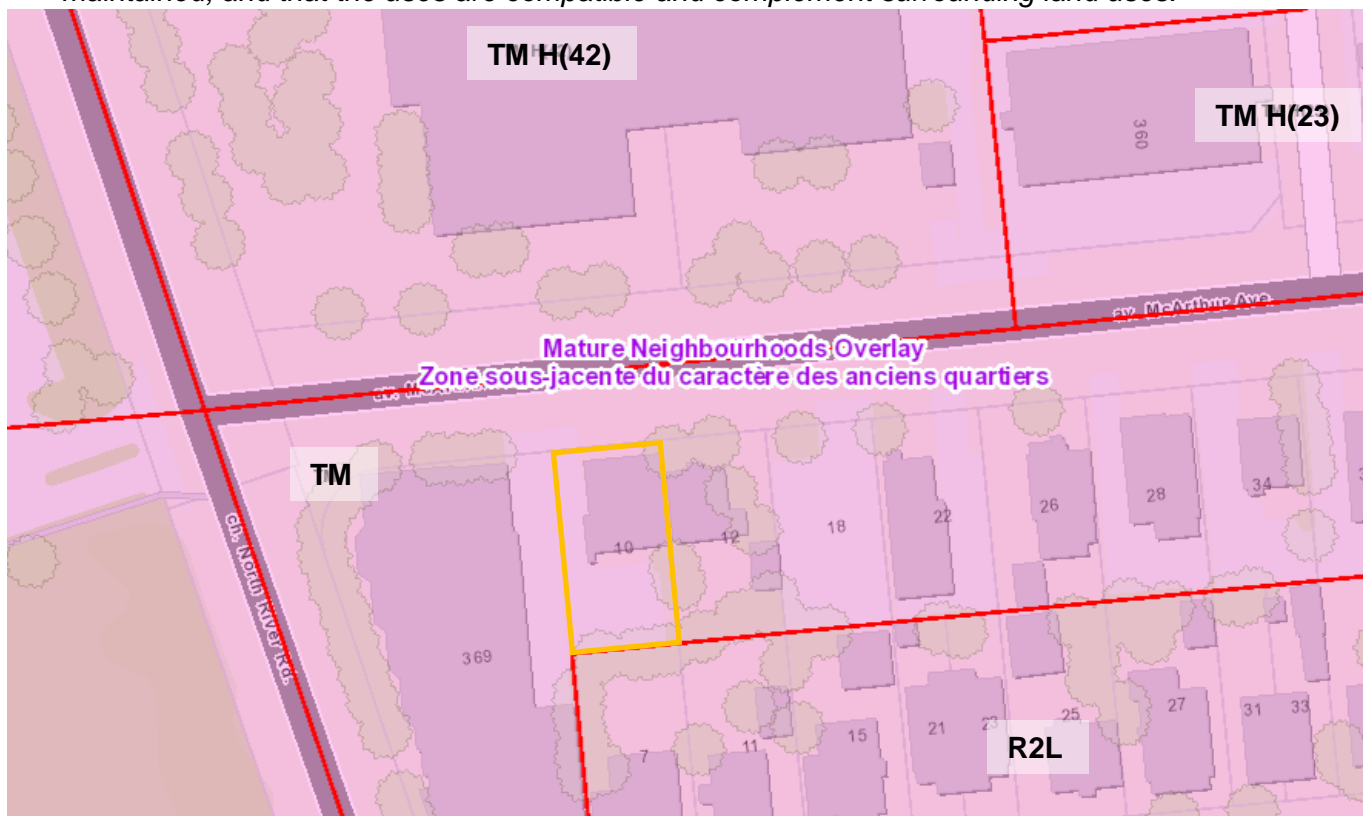


Figure 15. Excerpt of zoning (GeoOttawa, 2017)

The TM zone permits a variety of non-residential uses, as well as the following residential uses:

- |                                |                              |
|--------------------------------|------------------------------|
| < apartment dwelling, low rise | < group home                 |
| < apartment dwelling, mid rise | < retirement home            |
| < bed and breakfast            | < retirement home, converted |
| < dwelling units               | < rooming house              |



Table 2. Performance standards of the TM zone

Zoning Mechanisms		Provisions	
		Required	Provided
(a) Min. lot area (m <sup>2</sup> )		No minimum	406
(b) Min. lot width (m)		No minimum	14.6
(c) Max. front yard setback (m)		2.0	2.0
(d) Min. interior side yard setback (m) (residential use building)		1.2	1.2
(f) Min. rear yard setback (m) (abutting residential zone)		7.5	7.5
(g) Building height (m)	(i) min.	6.7 m for 20 m from lot line	13.6
	(ii) max.	20 metres, not more than 6 storeys	
(i) Min. width of landscaped area (m)	(ii) abutting residential zone	3; may be reduced to 1 m with 1.4 m opaque fence	2.30
s.101: Min. parking space rate (residential)		0	2
s.102: Min. visitor parking space rate (residential)		0	0
s.111: Min. bicycle parking spaces	0.5 / unit	5	6
s.137: Min. amenity area	Total (m <sup>2</sup> ), 6 m <sup>2</sup> per dwelling unit	60	75.76
	Min. communal (m <sup>2</sup> ), 50% of required total	30	56.4
s.197(8)(a): Min. driveway width (m)		3.0	3.0

The proposed low-rise apartment building adheres to all provisions as set out in the Zoning By-law, and no minor variances are required for this development.

## 7. SUPPORTING MATERIAL

The following supporting studies and reports for the development are included with this Site Plan Control Application and summarized as follows:

### 7.1 Stormwater Management Report and Servicing Brief

A Stormwater Management Report and Servicing Brief was prepared by LRL Associates Ltd., dated March 2019. The report concludes the following:

- The stormwater release rates from the development will meet the pre-development allowable release rate of 6.00 L/s onto McArthur Avenue;
- Stormwater quantity control objectives will be met through on-site stormwater ponding and storage;
- The anticipated maximum water demand is 0.841L/s;
- The maximum required fire flow is 58.3 L/s;
- There are two existing fire hydrants, both within the 90m radius;
- The development will be serviced with a new 50mm watermain connected to the existing 150mm watermain on McArthur Avenue
- The anticipated sanitary flow is 0.28 L/s; and,
- The building will be serviced by a new 150mm sanitary service connection to the existing 900mm diameter sanitary sewer on McArthur Avenue.

### 7.2 Geotechnical Investigation

A Geotechnical Investigation was prepared by Paterson Group Inc., dated April 10, 2019. The objectives of the investigation were to determine the subsoil and groundwater conditions at the site via boreholes, and to provide geotechnical recommendations for the design of the proposed development including construction considerations which may affect the design. The report sets out the following recommendations:



- Observe all bearing surfaces prior to the placement of concrete;
- Sample and test the concrete and fill materials used;
- Periodically observe the condition of unsupported excavation side slopes in excess of 3m in height, if applicable;
- Observe all subgrades prior to backfilling;
- Complete field density tests to determine the level of compaction and achieved; and,
- Sample and test the bituminous concrete including mix design reviews.

### **7.3 Environmental Noise Control Study**

An Environmental Noise Control Study was prepared by Paterson Group Inc., dated May 2, 2019. The objectives of the study were to:

- Determine the primary noise sources impacting the site and compare projected sound levels to provincial and City guidelines; and,
- Review the projected noise levels and offer recommendations regarding warning classes, construction materials or alternative sound barriers.

The report recommends that all units on the northern, western and eastern elevations are to be designed with the installation of a central air conditioning unit, and a warning clause included. The report outlines noise control measures to be implemented and indicates that concrete panels or brick veneer and double pane windows are suitable for the proposed noise attenuation based on industry standards.

### **7.4 Phase I & Phase II Environmental Site Assessments**

A Phase I Environmental Site Assessment (ESA) was prepared by Paterson Group Inc., dated March 27, 2019 in order to research the past and current use of the subject site and area, and to identify any environmental concerns with the potential to have impacted the property. The Phase I report indicates that several potentially contaminating activities were identified within the study area, but did not represent areas of potential environmental concern, with the exception of one former retail fuel outlet immediately to the west. Upon site visit, no potentially contaminating activities were identified with the current use of the site or lands in the study area. A Phase II Environmental Site Assessment was determined to be required. The Phase I report recommends that prior to demolition of the existing building, a designated substance survey must be conducted based on the potential for asbestos and/or lead paints.

A Phase II Environmental Site Assessment (ESA) was prepared by Paterson Group Inc., dated April 26, 2019. The purpose of the Phase II ESA was to address the potentially contaminating activities identified during the Phase I ESA. Following the subsurface investigation, the Phase II ESA concluded that no further environmental investigation is required.

## **8. CONCLUSION**

The subject site is a small lot within the McArthur-Overbrook neighbourhood, which is presently occupied by a two-storey mixed-use former restaurant building with second-storey residential. The site is currently underdeveloped and offers an excellent opportunity for residential intensification.

The proposed Application for Site Plan Control is to permit the development of a new three-storey residential apartment building with ten dwelling units. The proposed building will include parking in the rear yard for two vehicles, six bicycle parking spaces, private balconies on some upper-units, and a common rooftop amenity area. There will be a mix of one- and two-bedroom units, with one barrier-free unit.



The existing lot and municipal servicing have the capacity to support this type of density and residential intensification. Careful consideration has been placed into the design of the building, including height, materiality, amenity space, parking, and landscaping. The proposed development respects the existing neighbourhood character, provides adequate parking to accommodate the new residents, and is of massing, scale, and density that is appropriate and permitted within the neighbourhood. The addition of new residential apartment units provides opportunity for a diversity of housing choice, which is desirable within the City's urban area.

It is important to note that Lloyd Phillips & Associates as well as the Owners met with the adjacent Condominium Association representatives for Les Terrasses Amélie (located at 369 North River Road) to discuss the proposal and address any potential concerns or feedback. A meeting was held was also held with Councillor Mathieu Fleury to discuss the proposal, gather feedback and respond to questions.

Overall, the proposal is consistent with the Provincial Policy Statement 2014, conforms to the policies of the City of Ottawa Official Plan, including OPA 150, conforms to the Montreal Road District Secondary Plan, and complies with the City of Ottawa's Zoning By-law 2008-250.

The proposed development represents good land use planning and is recommended for approval.

Respectfully submitted,

Lloyd Phillips & Associates Ltd.



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Planner

