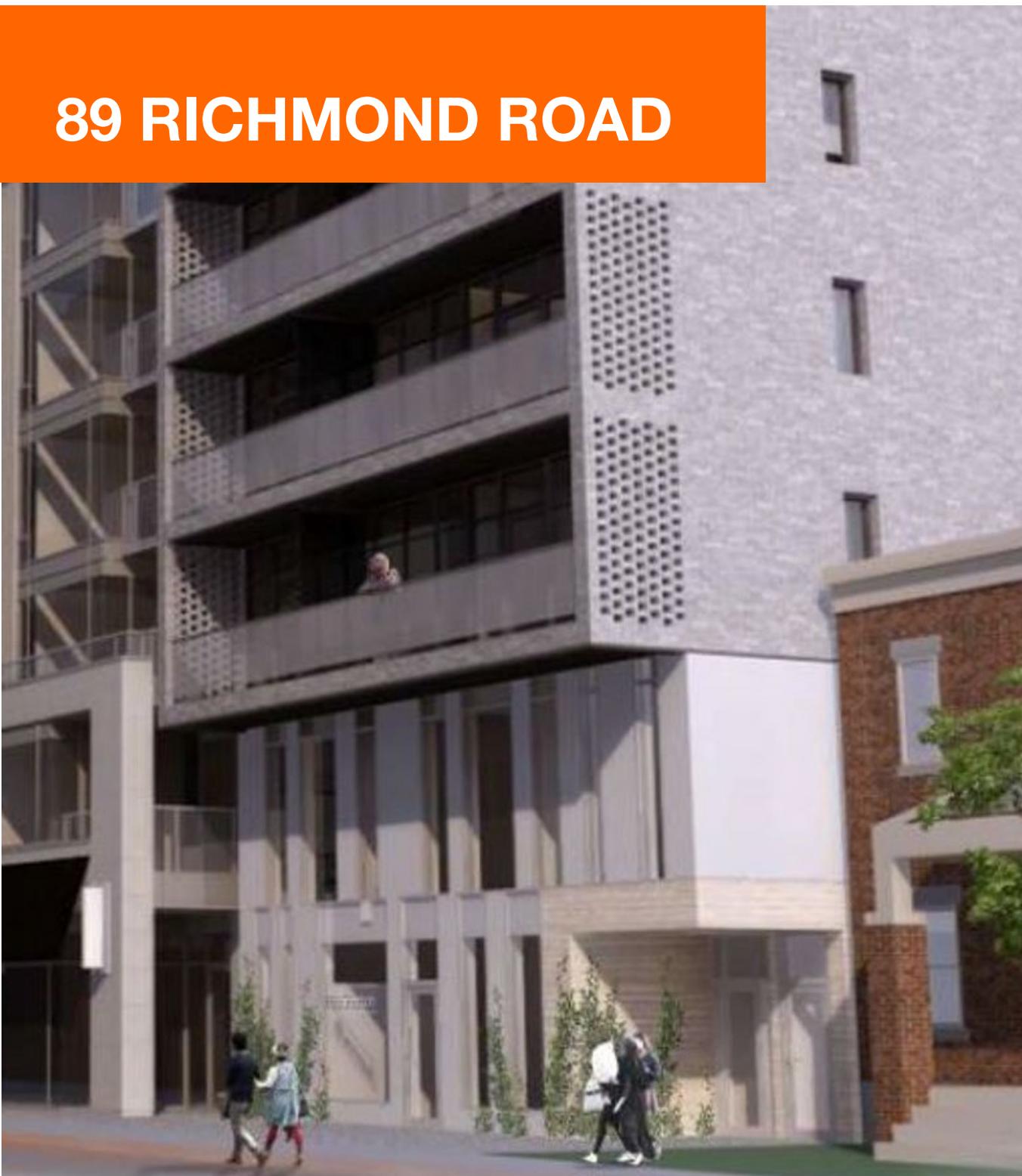


FOTENN

89 RICHMOND ROAD





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Fotenn Consultants Inc., acting as agents for SAAISH Incorporated, is pleased to submit the enclosed Official Plan Amendment, Zoning By-law Amendment and Site Plan Control Applications to facilitate the proposed development on the lands municipally known as 89 Richmond Road in the City of Ottawa.

The property owners are proposing to construct a six (6) storey mixed-use building on the subject property. The basement, first, and second floors will be comprised of a purpose built 497m² spa and health centre, while the third through sixth floor will contain 14 residential dwelling units. The residential unit mix includes one- and two-bedroom units.

1.1 Property History

The existing building on the subject property was significantly damaged by fire in December 2017. Prior to the fire on-site, the building most recently contained a massage therapy clinic and a 2-bedroom apartment. A review of historic aerial photography from the GeoOttawa database indicates the existing building has been on the subject property for approximately 50 years.

1.2 Purpose of Applications

In order to proceed with the development as envisioned, two (2) planning applications will be required.

1.2.1 Minor Zoning By-law Amendment (ZBLA)

Although the proposed development is generally compliant with the provisions of the Traditional Mainstreet Zone, a Minor Zoning By-law Amendment is required to address non-compliance in the following areas:

- / **Height:** The proposed building is 19.07 metres in height, whereas the site-specific zoning on the subject property regulates height to a maximum 15 metres.
- / **Step-backs:** The front wall of the building does not step-back after the fourth floor (15 metres), whereas the zoning provisions on the Subject Property state that where building height is greater than four storeys or 15 metres, at and above the fourth storey or 15 metres, a building must be setback a minimum of 2 metres more than the provided setback from the front lot.
- / **Rear-Yard Setback:** The proposal includes a 6-metre rear-yard setback for the first four (4) storeys, whereas the zoning provisions for the subject property require a 7.5 metre rear-yard setback.
- / **Parking:** The proposed building does not provide on-site parking, whereas based on the residential unit count the building requires one (1) resident parking space. At 497 square metres, the commercial portion of the proposed building does not trigger parking requirements under the provisions of the Zoning By-law.

1.2.2 Official Plan Amendment (OPA)

A site-specific OPA to the Richmond Road/Westboro Secondary Plan is required for the proposed six (6) storey building to amend a specific policy in Section 1.3.4: Sector 4 – East Village Secondary Plan which limits building heights to a maximum of (4) storeys on lots with a depth less than 45 metres.

1.2.3 Site Plan Control (SPC) Application

In addition to the ZBLA, a concurrent Site Plan Control application is also submitted as part of the application package.

2.0 SURROUNDING AREA AND SITE CONTEXT

2.1 Subject Property

The subject property is located in the southwest portion of Kitchissippi Ward in the established community of Westboro. The property is an interior lot consisting of an area of 372m² (4,000 square feet) with 12.2 metres of frontage along Richmond Road and a lot depth of 30 metres. The subject property is occupied by a now vacant two (2) storey mixed-use building recently damaged by fire, that previously contained a commercial and residential use and accompanying rear-yard surface parking lot.



Figure 1 Subject Site (Yellow).

2.2 Surrounding Area

The surrounding community is characterized by a mix of land uses, including residential, institutional, and commercial. Richmond Road is designated as an Arterial Roadway on Schedule E: Urban Road Network, a Transit Priority Corridor on Schedule D: Rapid Transit Network, and a Traditional Mainstreet on Schedule B: Urban Policy Plan of the Official Plan.

The abutting street network provides access to a range of daily goods and services, employment opportunities, various residential communities, and access to the central downtown and business district by means of active, public, and private modes of transportation.

To the North:

The neighbourhood to the north of the subject property is predominantly characterized by low-rise residential dwellings with key transportation routes including the Scott Street Corridor, Sir John A Macdonald Parkway and the Island Park Bridge to Quebec in close proximity. To the northeast of the subject property is Tunney's Pasture; an important employment hub for the community and the region as a whole. The property immediately north of the subject property is a single-detached low-rise residential property.

To the East:

The established Richmond/Wellington Traditional Mainstreet corridor is located east of the Subject Property. The neighbourhood to the east of the subject property contains low to medium density residential buildings within established neighbourhoods with higher density, medium-rise buildings located predominantly along the Richmond/Wellington corridor. The area east of the subject property also contains ample parks and community amenities. Immediately east of the subject property includes low-rise residential and commercial buildings, the Island Park/Richmond Road intersection, and the recently approved 12-storey Residences at Island Park Drive building at 1451 Wellington Street West.

To the South:

To the south of 89 Richmond Road is a residential community that is primarily composed of a range of detached and semi-detached low-rise dwellings. Approximately one (1) kilometre south of the subject property is access and egress to Highway 417. The Byron Avenue linear Park and Multi-use pathway is also 250 metres south of the subject property. Located on the south side of Richmond Road directly across from the subject property is an eight (8) storey mixed-use building.

To the West:

Directly west of the Subject Property are two mid-rise mixed-use buildings of six (6) and seven (7) storeys. The established Richmond Road Traditional Mainstreet corridor, including a range of commercial and mixed uses, is located further west of the Subject Property.

2.3 Transportation Network + Road Network

2.3.1 Road Network

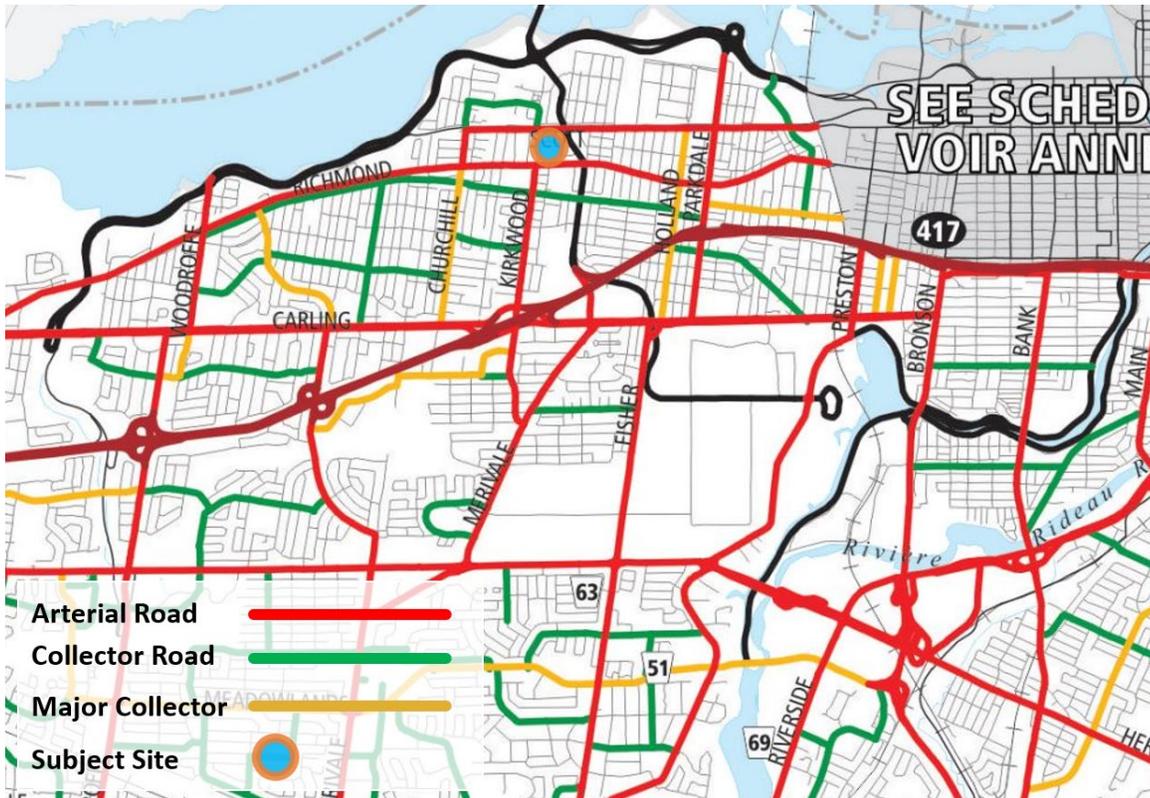


Figure 3: Extract from Official Plan Schedule E- Urban Road Network

Richmond Road is identified as an Arterial Roadway on Official Plan Schedule F (Central Area/Inner City Road Network) and a Traditional Mainstreet on Schedule B of the Official Plan (Urban Land Use Designations). Arterial Roadways are roads within the City that carry higher volumes of traffic to local and regional destinations. These roadways function as major public and infrastructure corridors that are intended to accommodate not only vehicular traffic but also pedestrians, public utilities, cyclists and public transit as well. Due to their ability to accommodate increased capacity, Arterial Roadways are generally best suited for increased activity stimulated by residential and commercial intensification.

The subject property is well serviced with respect to the existing road network. In addition to Richmond Road, the subject property is also located within close proximity of Island Park Drive offering connectivity to major collectors and key destinations, two arterial roads (Scott Street and Kirkwood Avenue), and as mentioned previously the 417 Queensway, a provincially-owned 400-series freeway, located just south of the subject property

Richmond Road is also designated a cycling route in the Official Plan and provides connection via the Island Park bike lane to the Scott Street and Ottawa River Multi-Use Pathway and the greater Ottawa Cycling network.

2.3.2 Public Transportation

The Subject Property is well served by public transit options. 89 Richmond Road is located within 600 metres of the Westboro Bus Rapid Transit (BRT) Station along the Confederation Line and within one (1) kilometre of the soon to open Tunney's Pasture Light Rail Transit (LRT) Station. Additionally, the subject site is served by multiple local bus routes including the 11, 16, and 153, with stops at key destination and transfer points en route.

This portion of Richmond Road is Classified as a Transit Priority Corridor on Schedule D of the Official Plan. This designation indicates opportunities for increased transit services along Richmond Road and therefore the ability for this area to potentially accommodate more residential and commercial growth.

Schedule D of the Official Plan also forecasts supplementary intensive rail to run along Carling Avenue in the future, with a planned station at the intersection of Kirkwood and Carling, about 1.3 kilometres south of the subject property.

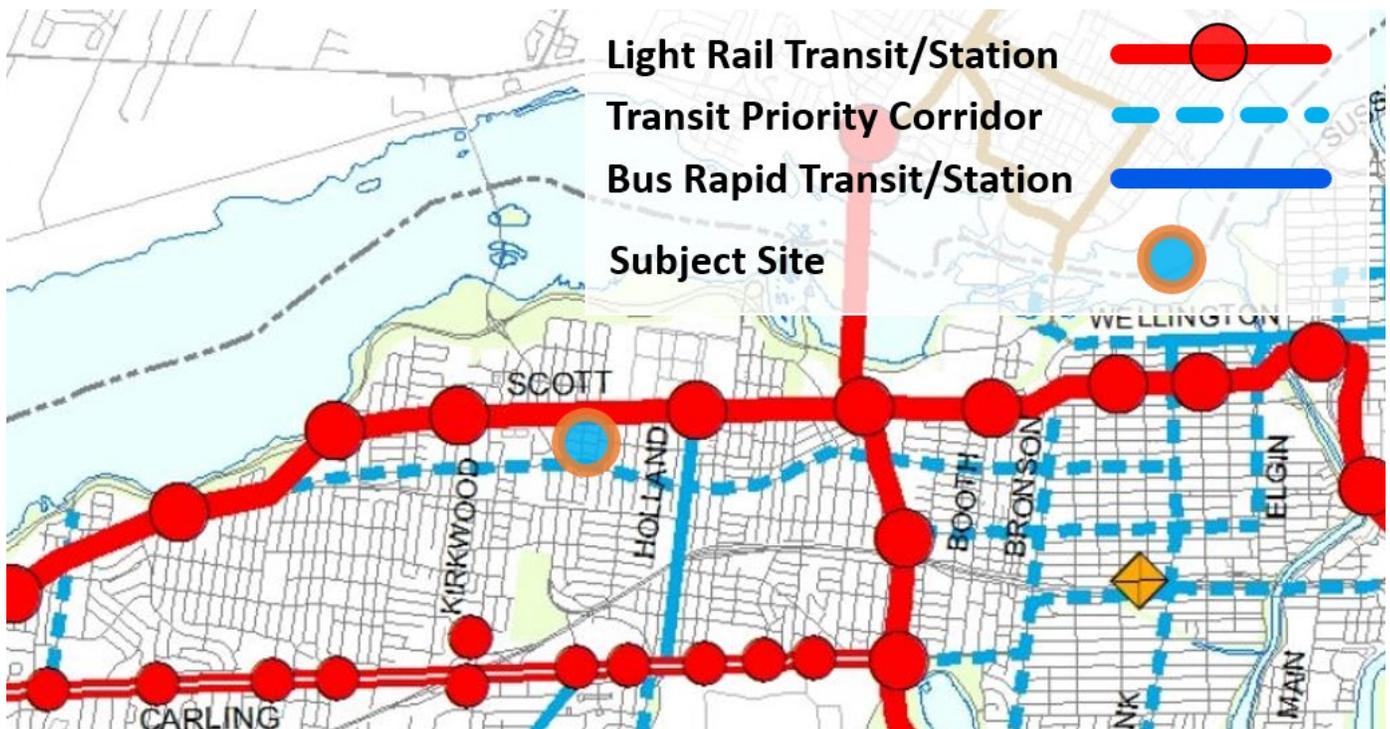


Figure 4: Extract from Official Plan Schedule D (Rapid Transit Network) illustrating to subject property's proximity to light rail transit and bus rapid transit.

3.0 PROPOSED DEVELOPMENT

3.1 Building and Site Design

The proposed development consists of a six (6) storey (19.07 metre) mixed-use building fronting onto Richmond Road containing 497 square metres of commercial area, 14 dwelling units, and 243 square metres of amenity space. The proposal does not provide vehicle parking spaces, but includes 17 bicycle parking spaces located within the basement of the building.

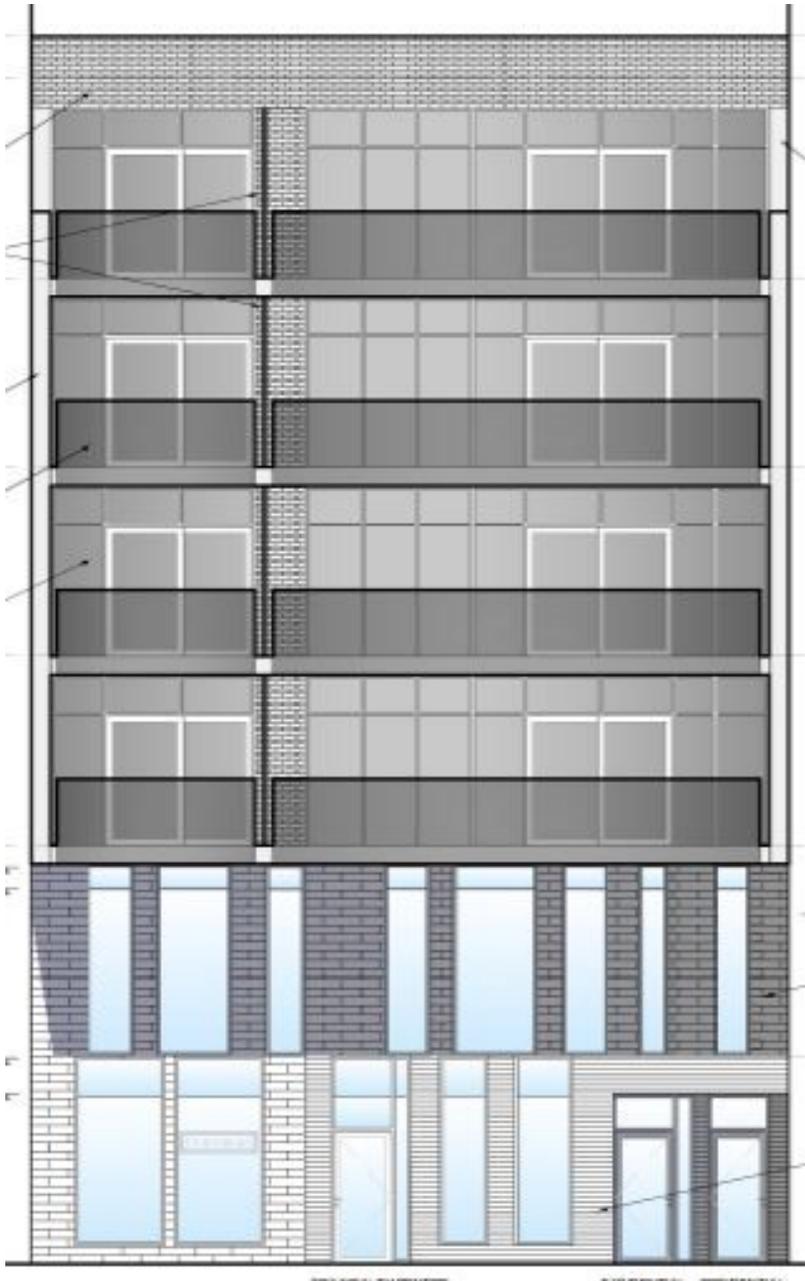


Figure 6 Conceptual Building Elevation from Richmond Road.

3.1.1 Building Design

The form of the proposed building reinforces the distinction between the commercial and residential components of the building through the use of complimenting but distinct material and colour. The commercial portion of the building is meant to have a more light and dynamic reading, and this is achieved by using more asymmetrical and varied window placement and widths while using a buff coloured concrete block, while the residential portion above utilizes an earthen grey clay brick, and more regularized openings.

The residential portion of the building is given further distinction from the lower portion by framing the balconies with dove-coted brick. These dove cotes offer residents a level of privacy and produce a warm lantern like effect at night when the lighting of the balconies is emitted through the gaps in the brick.



Figure 7 Conceptual Building Rendering outlined in white.

3.1.2 Interface with the Public Realm

Along Richmond Road, the proposed development is situated close to the front property line to create a pedestrian-friendly environment and contribute to the established continuous street frontage typical of a

Traditional Mainstreet. The building's main entrance along Richmond Road is recessed and sheltered with plantings.

The orientation of the building, the proposed setbacks, building height, and the entrance and window treatments are all in keeping with the existing community character. With direct access from Richmond Road and ample floor to ceiling heights, the at-grade commercial space has been located and designed to be a positive and complementary contribution to the street.

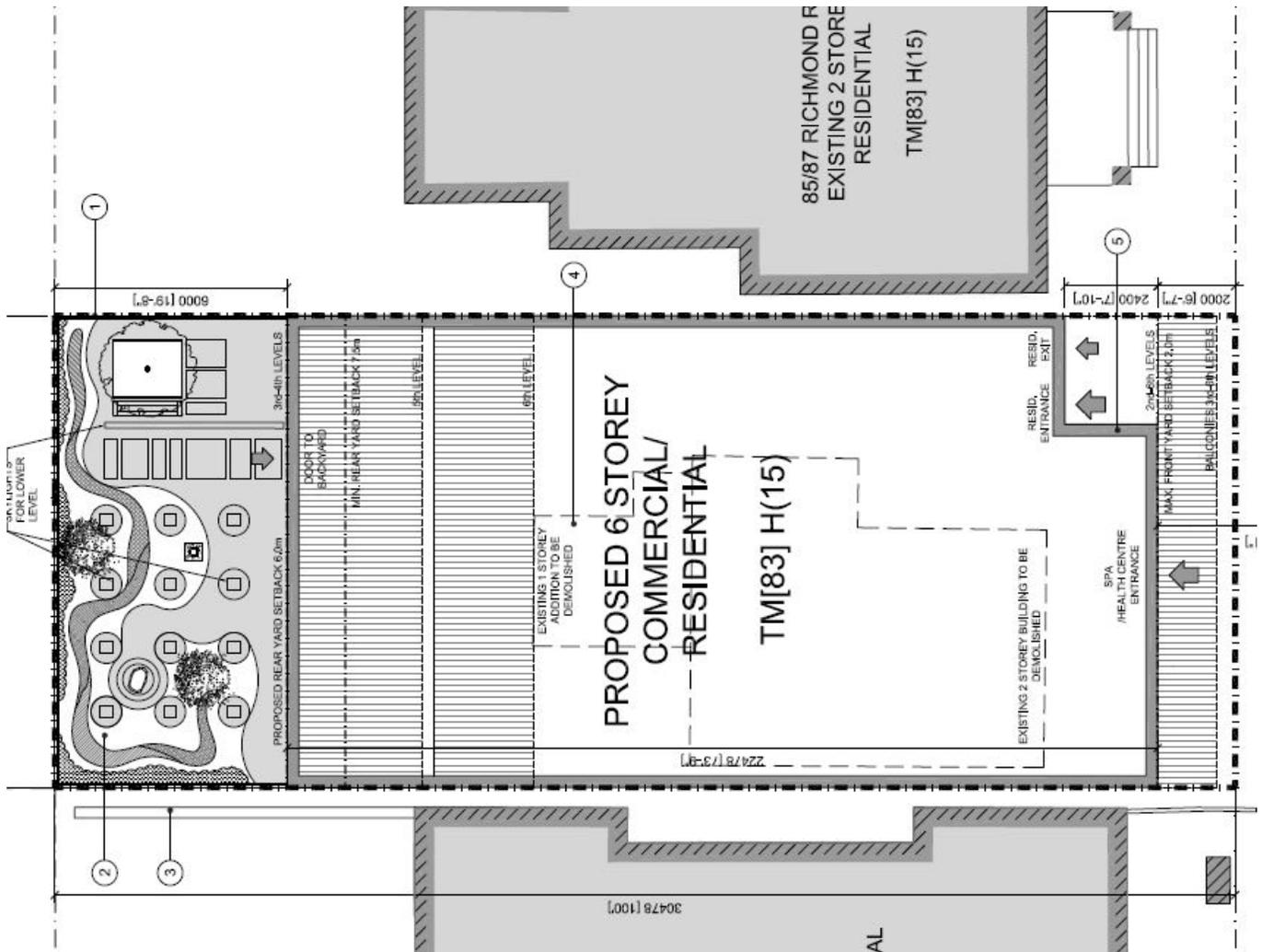


Figure 8 Proposed Site Plan.

The proposed development has been designed to fit within the envelope and objectives of the existing zoning designation, with only a few minor amendments. The building massing along Richmond Road has been minimized by recessing the lower floors of the building. The design approach is consistent with other mixed-use buildings along Richmond Road, contributing to a consistent building rhythm and streetscape.

Extensive glazing at-grade will allow the activities within to animate the street. With its high ceilings and prominent location, this commercial space has the potential to contribute in a way that can animate the public realm and engage the broader community.

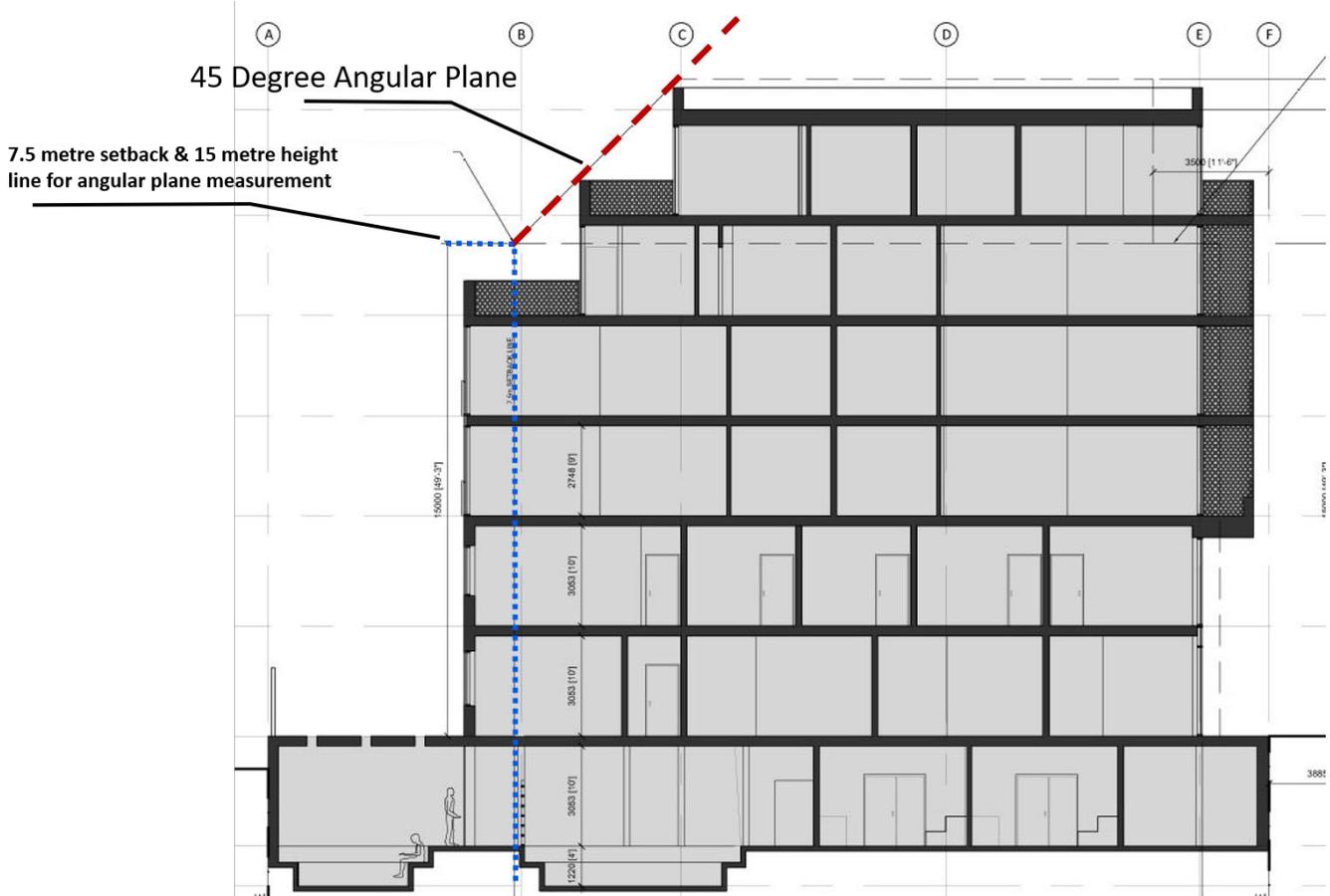


Figure 9 45 Angular Plane to ensure adequate transition to rear-yard properties.

POLICY AND REGULATORY FRAMEWORK

4.1 Provincial Policy Statement

The Provincial Policy Statement (PPS), issued under the authority of Section 3 of the Planning Act and in effect since April 30, 2014, provides direction on matters of provincial interest related to land use planning and development. The Planning Act requires that decisions affecting planning matters “shall be consistent with” policy statements issued under the Act.

The PPS emphasizes intensification in built-up areas in order to promote the efficient use of land and existing infrastructure and public service facilities to avoid the need for unjustified and uneconomic expansion. To achieve this goal, planning authorities must identify and promote opportunities for intensification and redevelopment.

Section 1.1.1 (Managing and Directing Land Use to Achieve Efficient and Resilient Development and Land Use Patterns) of the PPS identifies the ways in which healthy, liveable and safe communities are sustained, including:

- a) Promoting efficient development and land use patterns which sustain the financial well-being of the Province and municipalities over the long term;
- b) Accommodating an appropriate range and mix of residential, employment, institutional (including places of worship, cemeteries, and long-term care homes), recreation, park and open space, and other uses to meet long-term needs;
- c) Avoiding development and land use patterns which may cause environmental or public health and safety concerns;
- e) Promoting cost-effective development patterns and standards to minimize land consumption and servicing costs;
- f) Improving accessibility for persons with disabilities and older persons by identifying, preventing and removing land use barriers which restrict their full participation in society;
- g) Promoting development and land use patterns that conserve biodiversity and consider the impacts of a changing climate.

Policy 1.1.3.1 requires that settlement areas shall be the focus of growth and development, and their vitality and regeneration shall be promoted. Policy 1.1.3.2 states that land use patterns within settlement areas shall be based on:

- a) Densities and a mix of land uses which:
 1. Efficiently use land and resources;
 2. Are appropriate for, and efficiently use, the infrastructure and public service facilities which are planned or available, and avoid the need for their unjustified and / or uneconomical expansion;
 3. Minimize negative impacts to air quality and climate change, and promote energy efficiency;
 4. Support active transportation;
 5. Are transit-supportive, where transit is planned, exists or may be developed.

Policy 1.1.3.4 states that appropriate development standards should be promoted which facilitate intensification, redevelopment, and compact form, while avoiding or mitigating risks to public health and safety.

Policy 1.1.3.6 requires that new development taking place in designated growth areas should occur adjacent to the existing built-up area and shall have a compact form, mix of uses and densities that allow for the efficient use of land, infrastructure and public service facilities.

Policy 1.4.3 requires that planning authorities provide for an appropriate range and mix of housing types and densities to meet projected requirements of current and future residents, in part by permitting and facilitating all forms of housing required to meet the social, health and well-being requirements of current and future residents,

as well as all forms of residential intensification and redevelopment. Additionally, the policy requires directing the development of new housing towards locations where appropriate levels of infrastructure and public service facilities are or will be available to support current and projected needs.

Policy 1.6.7.2 of the PPS requires that efficient use of existing and planned infrastructure shall be made. Policy 1.6.7.4 further specifies that a land use pattern, density and mix of uses should be promoted that minimize the length and number of vehicle trips and support current and future use of transit and active transportation.

Policy 1.8.1 of the PPS requires that planning authorities shall support energy conservation and efficiency, improved air quality, reduced greenhouse gas emissions, and climate change adaptation through land use and development patterns which, among other things:

- a) Promote compact form and a structure of nodes and corridors;
- b) Promote the use of active transportation and transit in and between residential, employment, and institutional uses and other areas;
- e) Improve the mix of employment and housing uses to shorten commute journeys and decrease transportation congestion.

The proposed development is consistent with the policies of the Provincial Policy Statement. As part of the overall concept, the re-development offers an efficient, cost-effective pattern of growth, capitalizing on an intensification opportunity within the city's urban area, in close proximity to significant transit opportunities and important amenities and employment hubs to promote ease of walking and cycling as an alternative to personal vehicle use. Intensification of the underutilized subject property will make efficient use of existing infrastructure, public service facilities, and will support the City's investment and commitment to public transit including the Richmond Road Transit Priority Corridor, soon to open Tunney's Pasture LRT station, and the existing Westboro BRT station.

4.2 Official Plan (2003, as amended)

The City of Ottawa Official Plan is composed of eight (8) sections, each addressing a different aspect of the planned function of the City as a whole, with Section 2 providing Strategic Directions for growth and development within the City. The City is anticipated to grow by approximately 143,000 homes by 2036. The City intends to meet this growth challenge by managing it in ways that support liveable communities and healthy environments. This means that growth will be directed towards key locations with a mix of housing, shopping, recreation and employment and that are easily accessible by transit while also promoting and encouraging walking and cycling.

The Official Plan recognizes that concentrating growth to specific areas is required to support a high-quality transit service and to make better use of existing roads and other municipal infrastructure. Intensification at higher densities in nodes around transit terminals and along corridors served by transit is recognized as the most affordable form of development.

The Official Plan addresses the pressures of growth by establishing a set of strategic objectives. Two overarching Official Plan objectives which are applicable to the proposed development are:

1. Managing Growth

- / The City will manage growth by directing it to the urban area where municipal services already exist or where they can be provided efficiently;
- / Growth in the existing designated urban areas will be directed to areas where it can be accommodated in compact and mixed-use development, and served with quality transit, walking and cycling facilities;

- / The Central Area, Traditional and Arterial Mainstreets, Mixed Use Centres, and Town Centres will be compact, liveable, and pedestrian-oriented with a vibrant mix of residential uses, and social, cultural and economic activity; and,
- / Infill and redevelopment will be compatible with the existing context or planned function of the area and contribute to the diversity of housing, employment, or services in the area.

2. Building Liveable Communities

- / Attention to urban design will help create attractive communities where buildings, open space and transportation work well together;
- / Growth will be managed in ways that create complete communities with a good balance of facilities and services to meet people's everyday needs, including schools, community facilities, parks, a variety of housing, and places to work and shop; and,
- / The City will pursue a more affordable pattern of growth that allows for more efficient use of municipal infrastructure and reduces the need to build and maintain new infrastructure throughout its life-cycle.

These strategic directions are developed further in the policies of Section 2.2 (Managing Growth) and 2.5 (Building Liveable Communities) as discussed below.

4.2.2 Managing Growth (OP Section 2.2)

Concentrating growth within the urban area makes efficient use of existing services and infrastructure and allows for a pattern and density of development that supports transit, cycling, and walking as viable and attractive alternatives to private automobiles. Growth will be distributed throughout the urban area to strengthen liveable communities through intensification and infill, and new development on vacant land in designated growth areas.

In all areas, the density, mix of uses, and land use pattern will work together to make the most efficient use of transit. Transit service is to be accessible by other active modes of transportation (i.e. walking and cycling) and densities will be highest adjacent to transit stations. A mix of uses in close proximity to transit routes and stations will also ensure that residents can meet many of their daily needs within the community or nearby.

Section 2.2.2 addresses the management of growth within the urban area and recognizes that intensification is generally the most cost-effective pattern of development for the provision of municipal services, transit, and other infrastructure. Consequently, the Official Plan employs a hierarchy of nodes and corridors for managing growth.

Policy 1 of Section 2.2.2 defines residential intensification as the “intensification of a property, building or area that results in a net increase in residential units or accommodation and includes:

- / Redevelopment (the creation of new units, uses or lots on previously developed land in existing communities), including the redevelopment of Brownfield sites;
- / The development of vacant or underutilized lots within previously developed areas, being defined as adjacent areas that were developed four or more years prior to new intensification;
- / The conversion or expansion of existing industrial, commercial and institutional buildings for residential use; and,
- / The conversion or expansion of existing residential buildings to create new residential units or accommodation, including secondary dwelling units and rooming houses.

Policy 4 and Policy 11 of Section 2.2.2 identify target areas for intensification including the Central Area, Mixed-Use Centres, Traditional Mainstreets, Town Centres, and Transit-Oriented Development Areas as defined by the Plan. Traditional Mainstreets, like Richmond Road, and especially on Transit Priority Corridors or in close proximity to Rapid Transit corridors are areas where density is encouraged.

The proposed development of the subject property constitutes residential and commercial intensification as defined by the Official Plan. The planned development efficiently intensifies a previously under-utilized site in an area currently well served by community facilities, commercial services, park spaces, and transportation options. The continued redevelopment and intensification of the subject property will complement the existing retail uses and contribute to the creation of a complete community along a Transit Priority Corridor and in close proximity to Rapid Transit Stations.

4.2.3 Design Priority Areas (Section 2.2 & 2.5.1)

Section 2.2 of the Official plan states that Richmond Road is designated a Design Priority Area on Schedule B of the Official Plan and is therefore subject to the Ottawa's Urban Design Review Panel process.



Figure 10 Design Priority Area.

The Official Plan aims to direct growth to Design Priority Areas and to protect and enhance the character and sustainability of these places as mixed-use communities, and to provide a focus for coordinating urban design efforts and enhancements. Policy 5 of Section 2.5.1 States that in Design Priority Areas, projects within and adjacent to the public realm will be reviewed for their contribution to an enhanced pedestrian environment and their response to the distinct character and unique opportunities of the area.

The proposal utilizes a range of design approaches and treatments such as, lighting, high-quality architectural and façade treatments to foster design that generates a building that promotes and facilitates an improved street interface, public realm, and pedestrian experience while respecting the existing and planned character and form of the area.

As the proposed development is within a Design Priority Area it will therefore be subject to review by the Urban Design Review Panel (UDRP) as part of the application review process.

4.2.4 Land Use Designation

The Subject Property is designated Traditional Mainstreet on Schedule B: *Urban Policy Plan* of the Ottawa Official Plan. Schedule B assigns land use designations to properties with the City.



Figure 11 Official Plan Land Use Designation - Schedule B.

The Traditional Mainstreet designation encourages a mixed-use, compact, and pedestrian oriented development pattern. The policies of the designation encourage more compact forms of development, a lively mix of uses, and a pedestrian-friendly environment.

In accordance with Section 3.6.3, Policy 5, a broad range of uses are supported on Traditional Mainstreets, including residential uses and commercial uses.

Policy 10 of Section 3.6.3 encourages redevelopment and infill along Mainstreets in order to optimize the use of the land. The Official Plan states that the building format should define the street edge and provide direct pedestrian access to the sidewalk. The Plan generally supports mid-rise building heights (five to nine-storeys,) on Traditional Mainstreet with greater building heights supported under a number of circumstances, including within areas already characterized by high-rise buildings with direct access to an arterial road, within areas in close proximity to rapid transit, and within areas identified for high-rise buildings where the building profiles are already permitted in the Zoning By-law.

Specifically, relevant policies under the Traditional Mainstreet Designation include:

Policy 1: Traditional Mainstreets are planned as compact, mixed-use, pedestrian-oriented streets that provide for access by foot, cycle, transit and automobile. Both Traditional and Arterial Mainstreets will fulfill and take advantage of their multi-modal transportation corridor function.

The development takes advantage of its location along Richmond Road; a Transit Priority Corridor in the Official Plan, and proximity to transit stations, local bus routes, and other local amenities and transportation routes (arterial and collector roadways, cycling and pedestrian facilities) to accommodate a more compact form of development through the introduction of a mix of commercial residential uses. The building has been oriented to enclose and define street edges for a pleasing and comfortable pedestrian environment.

Policy 5: A broad range of uses is permitted on Traditional Mainstreets, including retail and service commercial uses, offices, residential, and institutional uses.

The development provides a mixed-use, mid-rise building, contributing to a broad and varied mix of residential typologies and units, including single- and two-bedroom units to foster a vibrant and diverse community in close proximity to key amenities and the transit system. The proposal also provides additional commercial opportunities in the form of the spa and health centre to serve the community and region as a whole.

Policy 9: On Traditional Mainstreets surface parking will not be permitted between the building and the street. The location of surface parking will avoid interruption of building continuity along the Traditional Mainstreet street frontage and will minimize impacts on pedestrians.

No surface parking is proposed for this development.

Policy 10: Redevelopment and infill are encouraged on Traditional Mainstreets in order to optimize the use of land through intensification, in a building format that encloses and defines the street edge with active frontages that provide direct pedestrian access to the sidewalk.

The development transforms an existing underutilized lot to facilitate a re-development program that brings residential and commercial intensification to the area. The site layout and building design incorporates an approach that improves the public interface along Richmond Road, with building elements and treatments that define the street edge. Design, built form, and land use decisions were all thoughtfully consider in the development of this proposal to ensure appropriate neighbourhood fit, transition, and compatibility.

Policy 11: This Plan supports mid-rise building heights on Traditional Mainstreets, but secondary plans may identify circumstances where different building heights may be permitted. Building heights greater than those specified in this Section will only be permitted through a Secondary Plan.

The proposed building heights, design, and provision of an active street frontage are directly adherent to the policy of the Official Plan and Secondary Plan. The plan establishes criteria that if achieved allow for the heights proposed on the subject property.

Policy 13: To achieve the vision for Mainstreets, changes within the public environment as well as within the abutting private property environment may be necessary. The function and design of a road may influence the nature of land use along it and changes to the street may be necessary in order to facilitate a more intense, pedestrian-oriented form of development adjacent to it.

Along Richmond Road, the proposed building provides a form that will better frame the street. The changes to the public realm in terms of the streetscape are in keeping with the objective to enhance the public realm for Mainstreets. Access to the residential portions of the building will be provided through distinct and logical entry points along Richmond Road.

Overall, the proposed development contributes to the continued development of a compact mixed-use area by adding a higher-density mixed-use building in proximity to a range of existing employment and service commercial uses. Given the existing mix of uses in proximity to the subject property, residents will be able to access convenient transit and meet many of their daily needs by walking or cycling, reducing reliance on personal vehicles and promoting opportunities for active transportation.

The design and layout of the commercial portion of the building with the inset lower levels and ample glazing at-grade will create a positive interface between the new building and the public realm.

4.2.5 Building Liveable Communities

Section 2.5 proposes that Ottawa's communities be built on the basics of appropriate and affordable housing, ample greenspace, places for people to shop, socialize, and play nearby, access to community services, and workplaces within a reasonable commute. More liveable communities will be created by focusing more on community design and through engaging in collaborative community building, particularly around established neighbourhood hubs and nodes including mixed-use centres and main streets.

Section 2.5.1 (Urban Design and Compatibility) sets out design and compatibility objectives, principles, and policies applicable to intensification and infill development within the urban area. The policy states that compatible development is development that enhances an established community and coexists with existing development without causing undue adverse impact on surrounding properties.

The City guides the built environment using design objectives within Section 2.5.1 that are broadly stated and are intended to be applied to all land use designations. Design principles provide further detail on how each of the objectives may be achieved.

The following objectives are considered the most applicable to the proposed development:

To enhance the sense of community by creating and maintaining places with their own distinct identity.

Enhances the sense of community by responding to the surrounding built and cultural character of the area, through massing and materiality.

The proposed development seeks to redevelop and intensify an underutilized site, located in close proximity to current and future transit infrastructure, thereby advancing the objectives of the Traditional Mainstreet designation and implementing the City's vision for intensification. The proposed residential use will offer additional housing options in proximity to transit, retail, employment, and other amenities, contributing to the evolution of a more complete community.

To define quality public and private spaces through development.

The building as proposed defines quality public and private spaces by proposing a building that defines the street edge while providing active frontage along the Traditional Mainstreet.

The proposed development will add quality commercial and private spaces. The proposed commercial space along the Richmond Road frontage will provide active frontage, ensure eyes of the street, and contribute to advancing the planned character of the area.

To create places that are safe, accessible and are easy to get to.

The building creates places that are safe, accessible, and are easy to get to, and move through by proposing residential uses on upper floors with adequate glazing and balconies that improve passive surveillance along the street.

The proposed development and the overall concept for the lands recognizes the importance of the subject property in creating a vibrant and safe atmosphere along this portion of Richmond Road. The grade oriented commercial portion of the building has been designed to appropriately frame the main entrance and entry area, without overwhelming or over-shadowing the public realm.

The ground level commercial space directly fronts Richmond Road and will be actively used throughout the day and evening creating an animated and inclusive experience.

To ensure that new development respects the character of existing areas.

The proposal respects the character of the existing community by maintaining a continuous street frontage along Richmond Road and applying appropriate and contextual relevant form and height.

The design of the building has contemplated a built form that is compatible with existing context. The six (6) storey building along Richmond Road adheres to policies established for mid-rise building heights while the rear-yard step-back ensures that rear interface of the development transitions down to the existing low-rise community.

To consider adaptability and diversity by creating places that can adapt and evolve easily over time and that are characterized by variety and choice.

As Richmond Road is designated Traditional Mainstreet in the Official Plan and Zoning By-law it is reasonable to anticipate various future commercial/retail uses within the building.

The proposal considers adaptability and diversity by intensifying an existing development lot and providing a mix of unit types and accommodations for new residents.

The design and layout of the commercial component includes considerations and design features to accommodate a wide array of future tenants including floor to ceiling heights, space for fascia signage, and a direct street level entrance.

4.2.6 Compatibility

To achieve compatibility of scale and use requires a careful design response that appropriately addresses impact generated by infill or intensification. The policies of Section 4.11 in the Official Plan are intended to set the stage for requiring both high-quality urban design in all parts of the City and design excellence in design priority areas.

Policy 2 of Section 4.11 establishes compatibility criteria that the City will use to evaluate the compatibility of development applications. These criteria are discussed below:

Traffic	The proposals location in close proximity to key transit services and neighbourhood services and amenities will contribute to a modal share with a balanced proportion of resident using public transit, active transportation, and private vehicles. Transportation analysis completed for the proposed development concludes that sufficient roadway capacity is available on abutting arterial, collector and local roadways to service the proposed units.
Vehicular Access	There is no vehicular access included in this proposal. There is no change or drastic increases expected due to the proposed vehicle access to the site.
Parking Requirements	<p>The Zoning By-law requires one (1) parking space for this development; however, the proposal does not provide parking spaces. Given the restrictive dimensions of the site and its optimal location in close proximity to employment, retail, parks, schools, the bicycle network, and rapid transportation options vehicle use and parking demand are expected to be low for the tenant and clients of this building. In addition, including a driveway or parking space(s) would necessitate breaking up the continuous street frontage promoted for Traditional Mainstreet development patterns.</p> <p>A total of 17 bicycle parking spaces have been provided within the basement level. When considering the total amount required for both the residential use (7 spaces) as well as the commercial portion of the building (1 space), this represents nine (9) additional bicycle spaces above the requirement.</p>
Outdoor Amenity Areas	Given its relatively small scale, and close proximity to previously constructed buildings of greater massing, it is not expected that the proposed development will have any impact on nearby outdoor amenity areas. The development provides a rear-yard Zen Garden for clients of the spa which will assist in softening the transition to the abutting rear—yard residential properties
Loading Areas, Service Areas, and Outdoor Storage	The garbage and recycling storage area will be located internal to the building. Loading and pick-up drop-off is expected to occur along the Richmond Road frontage. Given the smaller scale of the residential component of the building and the nature of the proposed business operations, it is not expected that loading requirements will pose any significant adverse impact of the operations of Richmond Road.

Lighting	Lighting will be designed and installed to provide a safe and secure environment while meeting the City's requirements and ensuring no significant impacts on adjacent properties.
Noise and Air Quality	No significant impacts related to noise or air quality are expected as a result of the proposed development. Recommendations for noise mitigation measures will be implemented through this site plan control approval and the building permit process, as appropriate.
Sunlight	As indicated in the Sun-Shadow Study included in this submission; due to the small scale of the proposal and the existing massing and built form of the immediately abutting properties it is not expected that this proposed building will have any significant shadowing impact on nearby properties or the public realm.
Supporting Neighbourhood Services	The proposed development is located in close proximity to several neighbourhood amenities including schools, parks, and retail amenities. The site is supported by frequent transit connectivity, is along a Transit Priority Corridor, and in close proximity to future LRT Stations, and also well connected to existing and future walking and cycling routes.

The proposed development meets the policies of the Official Plan, including the Traditional Mainstreet policies and the Urban Design and Compatibility policies of Sections 2.5.1 and 4.11.

4.3 City of Ottawa Official Plan Amendment No. 150

In 2013, the City of Ottawa reviewed the Official Plan resulting in numerous policy changes. Ottawa City Council adopted Official Plan Amendment (OPA) 150 in December 2013, and the Ministry of Municipal Affairs and Housing approved the revisions in April 2014. Significant portions of OPA 150 remain under appeal, while other policy changes have been incorporated and reviewed in Section 4.2 above. As the appeals of OPA 150 continue to be settled, the in-force policy context analysed in Section 4.2 is fluid and it is anticipated that policies may be changed or added following submission of this application for Site Plan Control. Several policies remain under appeal; however, they represent the intended direction of Council and are discussed below.

Revisions to Section 4.11, to be renamed Urban Design and Compatible Development, contain more robust policies related to building and site design, organized into several categories. The applicable categories are addressed below.

Building Design	The proposed development promotes a complementary architectural style that respects and follows the established built form and policy direction for the area.
	The building utilizes different materials, form / massing and articulation to distinguish the building's base from its upper floors. The lower levels and inset entrance area break up the mass at-grade, highlights the building's entrance and provides an additional pedestrian connection from Richmond Road for added connectivity. The treatment and design approach is in character and consistent with the rhythm of the Richmond Road streetscape to the east and west.

	<p>The proposed building ground-level is designed to appear differentiated to clearly distinguish the commercial area from the upper floors of residential units. The podium-like design does not over-shadow the entrance of the building along Richmond Road. Glazing on the ground floor allows people to see in and out of the ground floor commercial space to help activate the public realm.</p>
Massing and Scale	<p>The massing and scale of the building is consistent with the existing built-context and the planned direction within the established policy by the City for the lands.</p>
Outdoor Amenity Areas	<p>The development proposes private amenities areas for the individual units in the form of private balconies and terraces projecting from the front and rear of the building</p>
Design Priority Areas	<p>It is recognized that the subject property is located within a Design Priority Area. As such, the portions of the building adjacent to the public realm have been designed as follows:</p> <ul style="list-style-type: none"> / The base of the building is designed for flexibility for ground floor uses. / The ground floor facades are transparent and frame the public streets, to achieve a continuous building frontage that defines and encloses streets and public spaces and contributes to the animation of public areas.

The proposed development continues to conform to the policies for Traditional Mainstreets by intensifying a site in proximity to transit routes and community amenities, and contributing to the creation of a vibrant, mixed-use building on a currently underutilized site. The additional housing and commercial space will provide opportunities for people to live and work in proximity to parks, schools, transit routes, employment opportunities, and retail and entertainment uses.

4.4 Richmond Road/Westboro Secondary Plan

The subject property is located within the Richmond Road/Westboro Secondary Plan, and specifically within the East Village Character Area, as shown in Figure 12.

The Secondary Plan provides more detailed objectives and direction for the Richmond Road/Westboro area. The Secondary Plan identifies a vision for the area as an attractive and liveable urban community, based on “green” principles outlined in the Official Plan, with a wide mix of uses including employment, neighbourhood services and facilities, and a range of housing types that will enhance the area’s diversity and vibrancy.

4.4.1 Objectives and Principles

Section 1.2 of the Secondary Plan establishes the overarching objectives and principles that are meant to support infill and intensification within the Plan Area and envisions the provision of policy that permits building height that is generally four (4) to six (6) storeys.

The Secondary Plan establishes intensification as a key objective of the overarching plan. To successfully accommodate appropriate intensification the Plan aims to minimize any adverse impacts of intensification

through preserving the scale and character of established residential neighbourhoods and preserving and enhancing the desirable character of the Westboro Village Traditional Mainstreet.

To achieve compatible infill/intensification the Secondary Plan establishes the following criteria as summarized and reviewed below:

Appropriate setbacks and transition in heights:

- / **The proposed building design is compatible within the existing context of the community as it provides a height that is comparable to the area at-large and to the most immediate neighbours to the south and west. The design also includes a six (6) metre rear-yard setback at grade and a 45-degree angular plane after the 4th storey to ensure adequate transition when considering the proposed interface between the Subject Property and the low-rise residential buildings to the north.**

Contributing to the restoration of the urban fabric:

- / **The proposal contributes to the restoration of the urban fabric by replacing the existing abandoned, fire-damaged building with a mixed-use infill project that frames and enhances the public realm while providing for increased community vibrancy with the provision of both commercial and residential opportunities. The building design with frames the street frontage and reflects and enhances the existing built character of the immediate area.**

Helping promote transit usage:

- / **The transit supportive intensification included within this proposal will promote transit use for both the local and rapid transit system. The proposal situated along Richmond Road; a Transit Priority Corridor in the Official Plan and is within walking distance to the Westboro Transit Station which is considered to have capacity and potential to accommodate higher levels of intensification.**

Conforming to the recommended general maximum building height ranges for each sector:

- / **The proposal is within mid-rise height range as established within this plan and envisioned within the Official Plan designation for the site.**

Conforming to the Richmond Road/Westboro CDP design guidelines:

- / **The proposal respects the general Community Design Guidelines informing built form, setbacks, transitions, relationship of the building to adjacent neighbourhood, to achieve compatible intensification through infill development while minimizing impacts on adjacent residential neighbourhoods.**
- / **The buildings design along Richmond utilizes design tools such the inset ground floor, glazing and materiality changes, and front facing balconies to provide a positive approach to Richmond Road.**

4.4.2 Richmond Road and Scott Street Traditional Mainstreets

Under Section 1.3.3 of the Secondary Plan, redevelopment and infill are encouraged on Richmond Road and Scott Street Traditional Mainstreets in order to optimize the use of land through increased building height and density. In these areas, the Secondary Plan supports building heights generally in the range of four (4) to six (6) storeys, with greater building heights considered in any of the following circumstances:

Section 1.3.3 Provisions	Proposal
<p>The proposed building height conforms with prevailing building heights or provides a transition between existing buildings.</p>	<p>The immediate area in proximity to the Subject Property contains buildings with heights of six (6) to eight (8) storeys. The street corridor is a mix of new and previously existing buildings with height varying along this stretch. However, the six (6) storey height as proposed is within the existing established range of the buildings directly south and west of the subject property. Further afield from the subject property the established height of the Richmond Wellington Corridor is within the six (6) storey range with new buildings constructed in close proximity achieving this established height precedent as well. The proposal also achieves the rear yard 45-degree angular plane criteria of the Zoning By-law to ensure adequate transition to the low-rise rear-yard neighbours.</p>
<p>The development fosters the creation of a community focus where the proposal is on a corner lot, or at a gateway location or at a location where there are opportunities to support transit at a transit stop or station.</p>	<p>The Subject Property is on Richmond Road, a Transit Priority Corridor within the Official Plan. The proposal is also in close proximity to both the Westboro BRT Station and the Tunney's Pasture LRT Station helping to provide important transit supportive intensification to the area.</p> <p>Although not directly on the corner/gateway of the Island Park/Richmond Road Intersection, the proposal contributes to the creation of a community focal point by building upon the redevelopment and reinvention of this portion of Richmond Road.</p>
<p>The development incorporates facilities, services or matters as set out in Section 5.2.1 of the Official Plan with respect to the authorization of increases in height and density that, in the opinion of the City, significantly advance the vision for Mainstreets.</p>	<p>As outlined in Section 5.2.1 of the Official Plan, and within the Provincial Policy Statements relating to infill and urban intensification, the proposal provides an effectual, cost-effective pattern of growth, that responds appropriately to an intensification opportunity within the city's urban area, in close proximity to adjacent transit routes/facilities, active transportation routes and nearby amenities and public facilities.</p>
<p>Where the application of the provisions of Section 2.5.1 and Section 4.11 of the Official Plan determine that additional height is appropriate.</p>	<p>As stated in Sections 4.2.5 and 4.2.6 of this document, the proposal achieves numerous policy objectives as established in Official Plan Sections 2.5.1 and 4.11. Through careful design consideration, the proposed building achieves compatible development and complements the established community without causing undue adverse impact on surrounding properties.</p>

	<p>Through providing a 6-storey design with commercial uses at grade and residential on the upper levels, the proposed building achieves compatibility of scale and use.</p>
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As outlined in the above table, the proposed height increase is appropriate as it adequately addresses the relevant criteria as established in Section 1.3.3 of the Secondary Plan.

4.4.3 Sector 4 – East Village

The Secondary Plan establishes distinct sectors throughout the Plan Area and sets out policy directives for each sector’s land use character and appropriate general building height range. The subject property is within the boundaries of the East Village Character area of the Secondary Plan with the policies relevant to that sector outlined and discussed below to understand their potential impacts on the subject property.

Relevant policies within the East Village Designation are as follows:

- / Encourage the evolution of the East Village to a more traditional mainstreet character as an extension to Westboro Village and a link to the West Wellington traditional mainstreet east of Island Park Drive, recognizing the varying character of the existing buildings and lot and block sizes; and,
- / Maintain maximum building heights at a maximum four storeys where lots are less than 45 metres in depth and back onto low-rise residential areas, and generally up to six storeys on lots of greater depth.

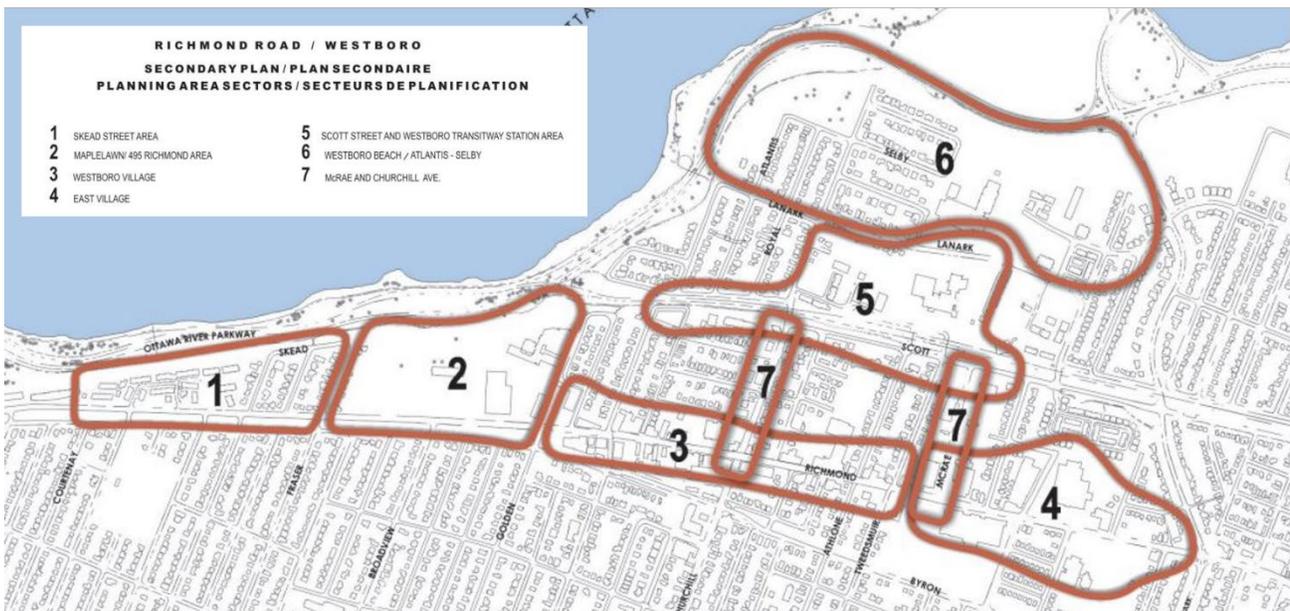


Figure 12 Secondary Plan Character Areas.

The proposed development promotes the ongoing transformation of the East Village Sector Character Area to a form and function that respects and promotes the objectives of the traditional mainstreet and

further the extension of this important commercial corridor east while complementing the diverse elements of the existing buildings and community fabric.

As the subject property lot is not 45 metres in depth, a site-specific Official Plan Amendment (OPA) is required to address the maximum height provisions of Policy 1.3.4: Sector 4 – East Village of the Secondary Plan. Although on a lot less than 45 metres in depth, the proposed development at six (6) storeys achieves appropriate transitions to low-rise areas to the north through setbacks, building step backs, and a 45-degree angular plane above the 4th storey. It should be noted that similar setback conditions would be present on a 45 metre deep lot, creating the same condition for low-rise neighbours abutting a rear yard in those cases. In addition, balconies for 2nd, 3rd, and 4th floors have been located on the front of the building, mitigating further intrusion into the rear yard, overlook, and noise impacts. The proposed development also meets the high-level policies for greater height as set out in Section 1.3.3 of the Secondary Plan.

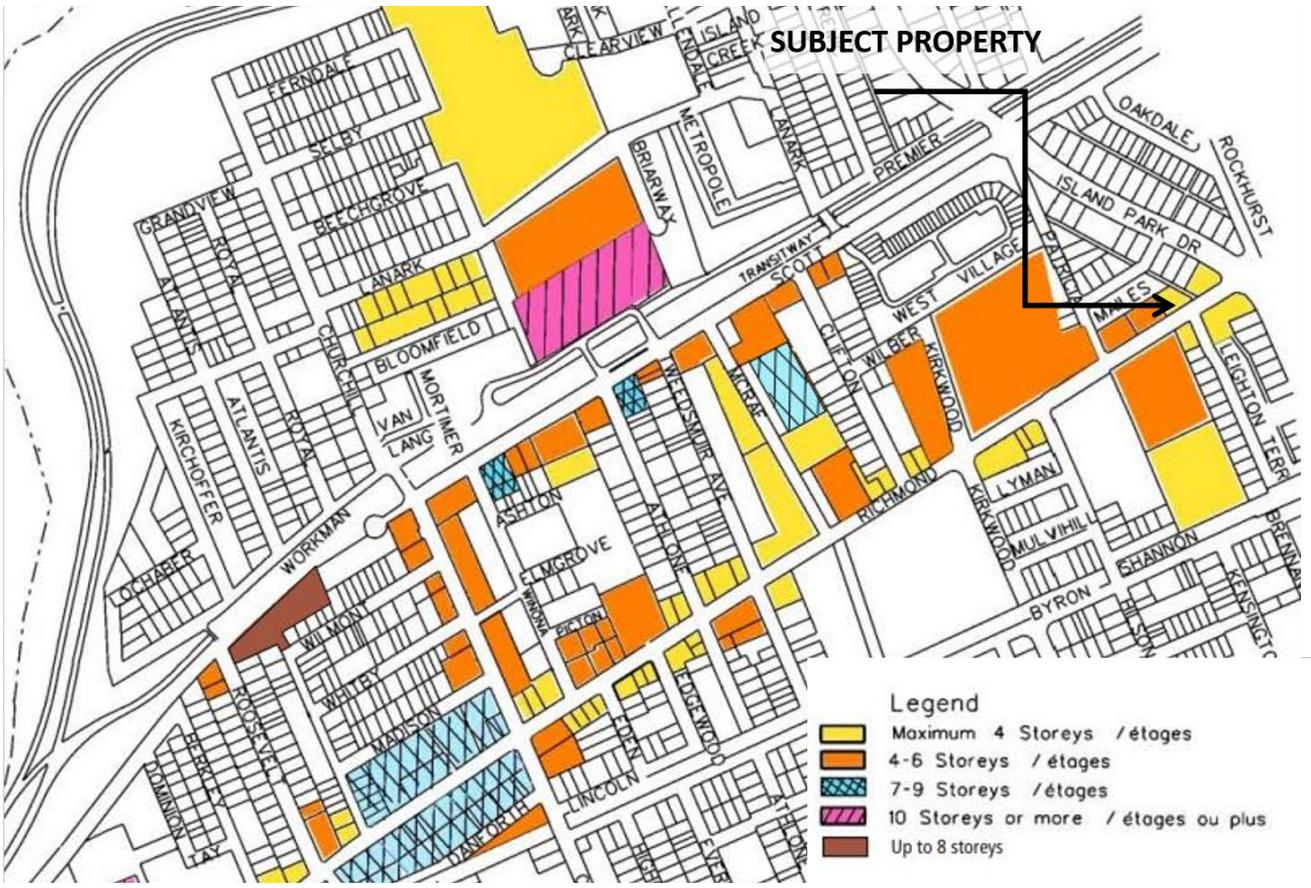


Figure 13 Secondary Plan Height Limits – Schedule C2 of the Secondary Plan.

Schedule C2 (above) of the Secondary Plan establishes permitted heights of four (4) storeys on the Subject Property.

4.5 Richmond Road Westboro Community Design Plan

The Subject Property falls within the Richmond Road/Westboro Community Design Plan (CDP). The CDP envisions the community as continuing to be an attractive and liveable urban community, based on “green” principles outlined in the City of Ottawa Official Plan, with a wide mix of uses including employment neighbourhood services and facilities, range of housing types and choices, excellent transit service and well designed, compact and inclusive development that will enhance the area’s diversity and vibrancy.

The CDP includes several overarching objectives and principles which echo the objectives and principles in the Secondary Plan. The applicable objective and principles include the encouragement of continued commercial and residential intensification that is at a human scale and is compatible with the existing community and on appropriate redevelopment sites.

Section 8.2 of the Community Design Plan situates the subject property in the East Village Area. Specifically, relevant design guidelines for this portion of the Plan Area promote the following objectives:

Create a strong edge on the street.	The building creates a strong edge on Richmond Road through providing well-design commercial uses at grade that are in close proximity to the public right of way and complement the existing edge and fenestration pattern.
Maintain a ground-level orientation of buildings.	The commercial uses at-grade will have ample transparent glazing. The building has both the commercial and residential entrances at grade and will assist in activating and improving this area of Richmond Road’s outward facing character.
Create buildings to blend with the rhythm and alignment of the area.	The proposed building is of a similar and complementary size and height to blend with the existing and planned scale of the area.
Maintain the distinction between ground floor and upper levels.	Through a change in style, materiality, and a slight inset, the commercial at-grade and the residential above are sufficiently distinguished.

The proposed development meets the intent and purpose of the policies of the Richmond Road/Westboro Community Design Plan. The proposal respects the general Community Design Guidelines informing built form, setbacks, transitions, relationship of the building to adjacent neighbourhood, to achieve compatible intensification through infill development while minimizing impacts on adjacent residential neighbourhoods.

The buildings design along Richmond utilizes design tools such the inset ground floor, glazing and materiality changes, and front facing balconies to provide a positive approach to Richmond Road.

4.6 Urban Design Guidelines for Traditional Mainstreets

The Urban Design Guidelines established for Traditional Mainstreets are a Council-approved set of guidelines intended to provide urban design guidance at the planning application stage in order to assess, promote and achieve appropriate development along Traditional Mainstreets.

The proposed development meets applicable design guidelines in that the building:

- / Promotes development that will enhance and reinforce the recognized or planned scale and character of the street.
- / Achieves high-quality built form and strengthens building continuity along a Traditional Mainstreets.
- / Aligns the building with the average setback of the adjacent buildings in order to create a visually continuous streetscape.
- / Uses periodic variations to add interest to the streetscape.
- / Designed with architectural detailing and respects the rhythm of existing mainstreet.
- / Uses large clear windows at-grade to maintain a pedestrian-focused public realm.
- / Provides pedestrian weather protection through the inset lower portion of the building.

The proposed development achieves the objectives of the design guidelines by fostering compatible development that locates building frontage along the Traditional Mainstreet and will contribute to the planned character of the street and achieve high-quality built form with a sense of identity and human scale.

4.7 City of Ottawa Comprehensive Zoning By-law

The property is currently zoned Traditional Mainstreet - TM [83] (15), as shown on Figure 14.

The Intent of the Traditional Mainstreet Zone is to:

- / Accommodate a broad range of uses including retail, service commercial, office, residential and institutional uses, including mixed-use buildings but excluding auto-related uses, in areas designated Traditional Mainstreet in the Official Plan;
- / Foster and promote compact, mixed-use, pedestrian-oriented development that provide for access by foot, cycle, transit and automobile;
- / Recognize the function of Business Improvement Areas as primary business or shopping areas; and,
- / Impose development standards that will ensure that street continuity, scale and character is maintained, that the uses are compatible and complement surrounding land uses.

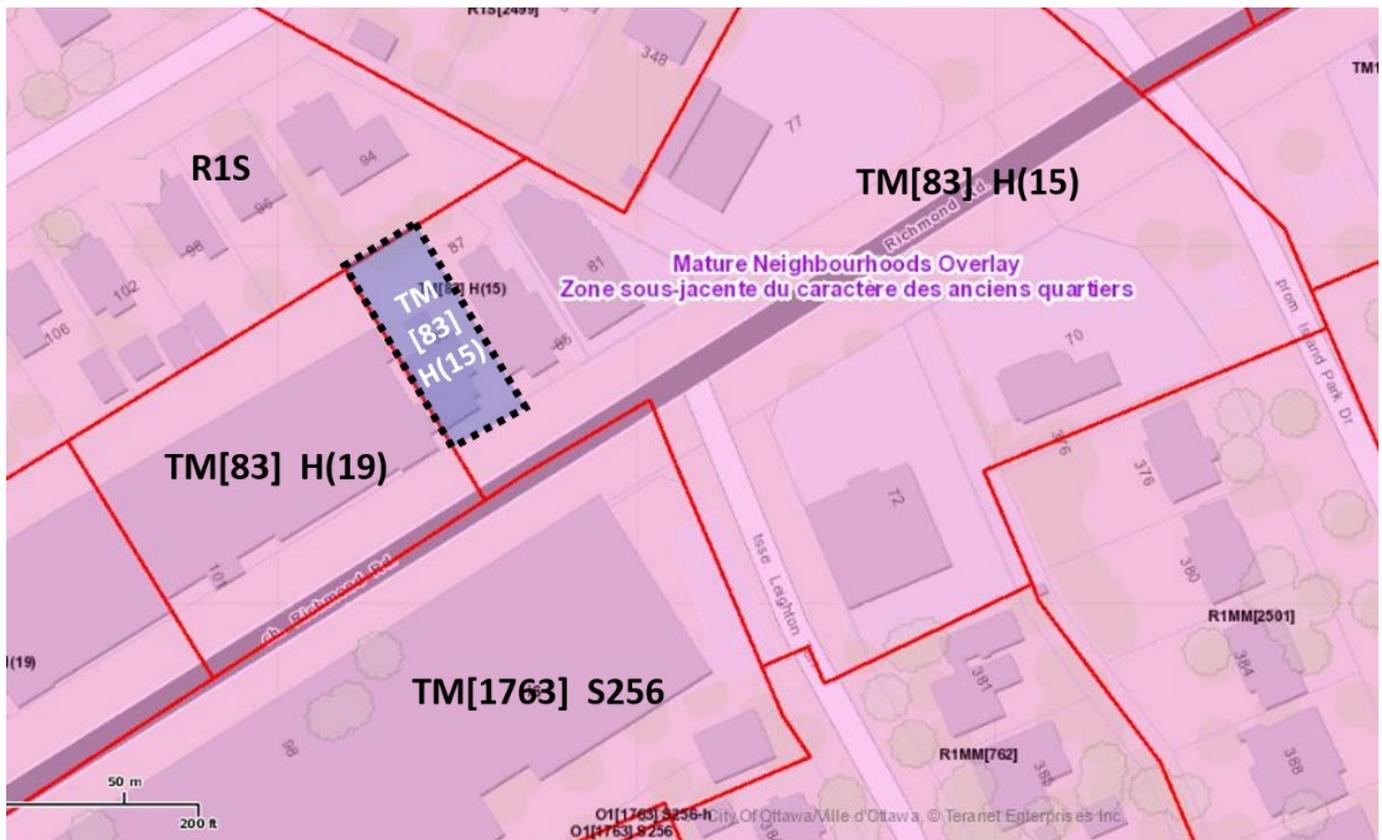


Figure 14 Zoning Map

As indicated in the zoning map above, the subject property is predominantly surrounded by Traditional Mainstreet Zoning with the exception of the rear-yard neighbours to the north which are zoned Residential First Density, Subzone S (R1S). The lower profile nature of the R1S zone is considered in the proposed building design in the application of the 45-degree angular plane in place on the rear of the building.

4.7.1 Site Specific Zoning Exception

The Subject Property also contains a Site-Specific Exception (83) and height limit (15 metres maximum). Zoning Exceptions are often utilized to include additional guidance for a specific policy that is unique from the provisions of the parent zone. On the Subject Property, Zoning By-law Exception 83 permits four additional uses as listed below:

- / Automobile dealership;
- / Automobile rental establishment;
- / Gas bar;
- / Automobile service station.

Two provisions are also included within Exception 83:

- / An automobile service station and gas bar is permitted only on a corner property; and
- / The provisions of Section 197(4)(d) do not apply to the lands at 93 Richmond Road.

The additionally permitted uses and associated provisions are not relevant to the current proposal on the Subject Property.

4.7.2 Zone Provisions

The following table evaluates the proposed development against the provisions applicable to development in the TM.

Traditional Mainstreet Zone

Performance Standard	ZBL Requirement	Building Conformance
Minimum lot area	No minimum	N/A
Minimum lot width	No minimum	N/A
Maximum front yard setback	2 metres, subject to the provisions of subsection 197(4) below. *1	2 metres ✓
Minimum Rear Yard Setback	Rear lot line abutting a residential zone: 7.5 metres	6 metres for the first four (4) storeys of the building, greater than 7.5m for 5 th and 6 th storeys ✗
Minimum Interior Side Yard Setbacks	Maximum: 3 metres between a non-residential use building or a mixed-use building and another non-residential use building or mixed-use building, except where a driveway is provided, in which case the setback must be a maximum of 6 metres where the driveway leads to a parking area of 20 or more spaces.	0 metres ✓
	Minimum: The maximum setback provisions of row (d)(i) above do not apply to the following cases and the following minimum setbacks apply: (1) 3 metres for a non-residential use building or a mixed-use building abutting a residential zone, and (2) 1.2 metres for a residential use building All other cases - no minimum (0 metres ✓
Height	Minimum 6.7 metres for a distance of 20 metres from the front lot line as set out under subsection 197(5) below	✓

	<p>Maximum</p> <p>(1) 15 metres as per the site-specific height maximum.</p> <p>(2) (where the building height is greater than four storeys or 15 metres, at and above the fourth storey or 15 metres whichever is the lesser a building must be setback a minimum of 2 metres more than the provided setback from the front lot line as set out under subsection 197(5) below, and from the a corner side lot line</p>	<p>6 storeys (19.07 metres) X</p> <p>Setback after 4th storey is 0 metres X</p>
	<p>3) no part of a building on a lot with a rear lot line abutting an R1, R2, R3 or R4 Zone may project above a 45 degree angular plane measured at a height of 15 metres from a point 7.5 metres from the rear lot line, projecting upwards towards the front lot line (see illustration below). (By-law 2012-349)</p>	<p>45-degree angular plane achieved ✓</p>
Minimum Width of Landscaped Area	<p>Abutting a residential zone: 3 m; may be reduced to one metre where a minimum 1.4 metre high opaque fence is provided.</p>	<p>6 metres ✓</p>
	<p>In all other cases: No minimum, except that where a yard is provided and not used for required driveways, aisles, parking or loading spaces, the whole yard must be landscaped</p>	
Frontage	<p>The façade facing the main street must include at least one active entrance serving each residential or non-residential use occupying any part of the ground floor (Bylaw 2015-190)</p>	<p>The ground floor along Richmond contains the main entrance for both the Residential and Commercial components of the building. ✓</p>
Glazing	<p>a minimum of 50% of the ground floor façade facing the main street, measured from the average grade to a height of 4.5 metres, must comprise transparent windows and active entrances; and where an active entrance is angled on the corner of the building, such that it faces the intersection of the main street and an intersecting side street,</p>	<p>58% ✓</p>

	it is deemed to face the main street;(By-law 2015-190)	
Amenity Space	6 m2 per unit=77 m2	243m2 ✓

4.7.3 Area Parking Requirements

The subject property is located in Area Y of Schedule 1A of the Zoning By-law. The parking rates below have been calculated based on the following building statistics:

- / 14 residential units,
- / 497 square metres GFA commercial uses.

Area Y Parking Requirements:	
Required Parking	Provided Parking
1 Vehicle Parking Spaces Required. 8 Bicycle Spaces Required.	✗ 0 Vehicle Parking Spaces Provided. ✓ 17 Bicycle Parking/Storage Spaces Provided.

The parking requirements are as follows:

- **Residential:** Residential (0.5 per dwelling unit - first 12 units): $14-12=2$ units. $2 \times 0.5=$ **1 resident parking space required.**
- **Commercial:** Within Area Y of Schedule 1A, in the case of any other non-residential use with a gross floor area of 500 square metres or less, no off-street motor vehicle parking is required to be provided = **0 parking spaces required.**
- **Visitors:** Visitor (Apartment Dwelling– first 12 units): 0.1 per dwelling unit: $14-12=2$ units. $2 \times 0.1=$ **0.2 (0 spaces required)**
- **Residential Bicycle:** (dwelling unit in same building as non-residential use: 0.50 per dwelling unit): $14 \times 0.5=$ **7 residential bicycle parking spaces.**
- **Commercial Bicycle: For a personal Service Use:** 1 per 500 m2 of gross floor area. With 497 square metres of commercial space = **1 bicycle parking space.**

4.7.4 Proposed Zoning By-law Amendment

Although the proposal generally conforms to the provisions of the TM [83] (15) zone; to facilitate the proposed re-development, a minor Zoning By-law Amendment application is required to amend provisions of the zone relating to height, parking, and building setbacks.

A Minor Zoning By-law Amendment is required to address non-compliance in the following areas:

- / **Height:** The proposed building is 19.07 metres in height whereas the site-specific zoning on the subject property regulates height to a maximum 15 metres.
- / **Front Yard Step-backs:** The building does not step-back after the fourth floor (or 15 metres) whereas the zoning of the subject property states that where building height is greater than four storeys or 15 metres, at and above the fourth storey or 15 metres a building must be setback a minimum of 2 metres more than the provided setback from the front lot.

-
- / **Rear Yard Setback:** The proposal includes a 6-metre rear-yard setback whereas the zoning for the subject property requires a 7.5 metre rear-yard setback.
 - / **Parking:** The proposed building does not provide on-site parking whereas based on the residential unit count and commercial GFA the building requires one (1) parking space.

4.7.5 Zoning By-law Amendment Rationale

- / **Height:** The six (6) storey height is in keeping with the surrounding built context and overarching planned context in the immediate area. the Traditional Mainstreet policies of the Official Plan, and Section 1.3.3 of the Richmond Road/Westboro Secondary Plan.
- / **Front yard Step-backs:** The building form is in context with the existing built form abutting and directly across the street on Richmond Road. Given the dimensions of the site and the prevailing built-form, relief form this provision is appropriate and necessary for this project. The building is set two (2) metres back from the front lot line, whereas it could be built directly to the lot line. This creates more space on the street edge and also allows balconies to be placed on the front of the building instead of on the back, mitigating overlook and noise impacts to rear-yard neighbours.
- / **Rear Yard Setback:** The rear yard setback is 1.5 metres deficient from the requirement of the Zoning By-law for the first four (4) storeys of the building. The impact from this requested minor amendment is not expected to cause a significant impacts on rear-yard neighbours given the contextual elements specific to the subject property, including:
 - o Existing, larger, building immediately abutting the subject property.
 - o The building achieves a setback greater then 7.5 metres for the 5th and 6th storeys of the building and achieves the 45-degree angular plane as required in the zoning by-law, limiting overlook and shadowing.
 - o The absence of balconies on the first four (4) floors of the rear of the building, which could legally project in to the required 7.5 metre rear yard setback and create a greater impact from the standpoint of overlook and noise generation.
- / **Parking:** The request to reduce the parking to zero spaces will not have a significant impact on the traffic and parking patterns in this area as the subject property is in close proximity to various modes of transportation infrastructure including local and rapid transit options, and the Ottawa cycling network. Moreover, the area is well positioned with multiple options for groceries, schools, parks, shops, and other community amenities in close proximity; reducing the need for personal vehicle trips. In addition, including a driveway or parking space(s) would necessitate breaking up the continuous street frontage promoted for Traditional Mainstreet development patterns.

5.0 CONCLUSIONS

It is our professional planning opinion that the proposed Official Plan Amendment, Zoning By-law Amendment and Site Plan Control application represents good planning as follows:

- / The development proposal is consistent with the intent of the Provincial Policy Statement with respect to infill development, particularly allowing development in established urban areas where services and infrastructure are readily available;
- / The proposed development conforms to the Official Plan policies for Traditional Mainstreets, particularly with respect to infill development that is compact, provides residential intensification, and defines the street edge;
- / The proposal complies with the City's urban design and compatibility criteria established in Section 2.5.1 and 4.11 of the Official Plan;
- / The proposal conforms to the policy direction and general intent contained in the Community Design Plan and Secondary Plan with regards to land use, community character, building design, and compatibility;
- / The proposed Official Plan Amendment is site-specific and provides for two (2) storeys of additional height through the use of design measures including step backs and angular planes to mitigate impacts and meet urban design and compatibility criteria established in the Official Plan;
- / The proposed development meets several of the Urban Design Guidelines for Development Along Traditional Mainstreets; and
- / The proposed development generally adheres to the intent of the Zoning By-law with only minor amendments required to permit the required built-form.

Yours Sincerely,



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Fotenn Consultants Inc.



Timothy Beed
Planner
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