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Fernbank Crossing – Block 135 360 Haliburton Heights, Ottawa Planning Rationale

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FERNBANK CROSSING – BLOCK 135

PLANNING RATIONALE IN SUPPORT OF A SITE PLAN CONTROL APPLICATION

Prepared For:



Fernbank Crossing Regional Inc.

Prepared By:



NOVATECH

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March 22, 2019
Novatech File: 117089
Ref: R-2019-055

March 22, 2019

City of Ottawa

Planning, Infrastructure and Economic Development Department

110 Laurier Ave. West, 4th Floor

Ottawa, Ontario

K1P 1J1

Attention: Kathy Rygus, Planner III

**Reference: Fernbank Crossing – Block 135, 360 Haliburton Heights
Site Plan Control Application, Planning Rationale**

Novatech is pleased to submit this Planning Rationale on behalf of Fernbank Crossing Regional Inc. in support of a Site Plan Control application for Block 135 in the Fernbank Crossing subdivision, municipally known as 360 Haliburton Heights.

The attached Planning Rationale details the proposed Site Plan for Block 135 and demonstrates that the it is consistent with the relevant provincial and municipal policy documents and guidelines.

Should you have any questions or comments, please do not hesitate to contact the undersigned.

Sincerely,

Novatech



Ellen Potts, BES (PI)

Planner

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1.0 INTRODUCTION

Novatech was retained by Fernbank Crossing Regional Inc. (eQ Homes) to prepare a Planning Rationale in support of a Site Plan Control application to permit the development of 360 Haliburton Heights (originally Block 536 on the approved Draft Plan of Subdivision for Fernbank Crossing [City File No.: D07-16-09-0034]), known as Block 135 on registered Plan 4M-1551.

eQ Homes intends to develop Block 135 with 58 back-to-back townhouse units on private streets.

This rationale describes the proposed development and demonstrates that it is consistent with the Provincial Policy Statement (2014), conforms to the City of Ottawa's applicable policies and guidelines, and represents good planning.

2.0 CONTEXTUAL ANALYSIS

2.1 Site Location and Context

The Fernbank Crossing subdivision was draft-approved on February 2, 2012 (City File No. D07-16-09-0034) and has been incrementally registered as phases through Plan 4M-1503 in 2013, Plan 4M-1551 in 2014 and Plan 4M-1608 in 2018. The Subject Site (360 Haliburton Heights) was registered as Block 135 on Plan 4M-1551, as shown in Figure 1 (see Appendix A for full plan).

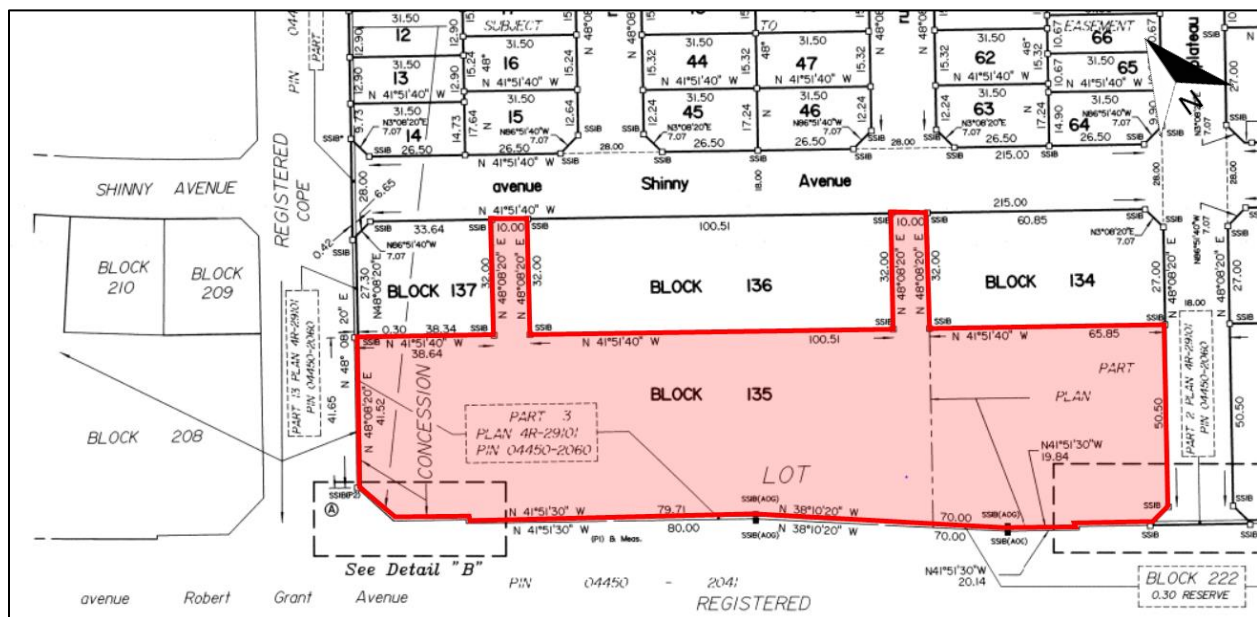


Figure 1: Excerpt of Plan 4M-1551 indicating Block 135 (the Subject Site) in red

The Fernbank Crossing subdivision is centrally located within the City of Ottawa's developing Fernbank Community. Figure 2 shows the location of the Subject Site in the Fernbank Crossing Subdivision and in relation to the larger Fernbank Community.



Figure 2: Aerial view indicating the Subject Site in red, the Fernbank Crossing subdivision in yellow and the Fernbank Community boundary in white (image source: Google Maps)

More specifically, the Subject Site is bounded by Haliburton Heights to the south, Robert Grant Avenue to the west, Cope Drive to the north and Shinny Avenue to the east, as shown in Figure 3. It has 190.51 metres of frontage along Robert Grant Avenue, 41.52 metres of frontage along Cope Drive, 20.00 metres of frontage along Shinny Avenue and 50.50 metres of frontage along Haliburton Heights.

The Fernbank Crossing subdivision surrounds the Subject Site to the north, east and south; low-rise condos are located on the north side of Cope Drive (City File No.: D07-12-13-0194) and the south side of Haliburton Heights (City File No.: D07-12-16-0030), and the rear yards of the existing two-storey Shinny Avenue townhomes abut the east side of the Subject Site. Fernbank Road delineates the urban boundary; the lands south of Fernbank Road are part of the City's rural area. The lands west of Robert Grant Avenue are owned by CRT Developments Inc. and are intended for future residential development.

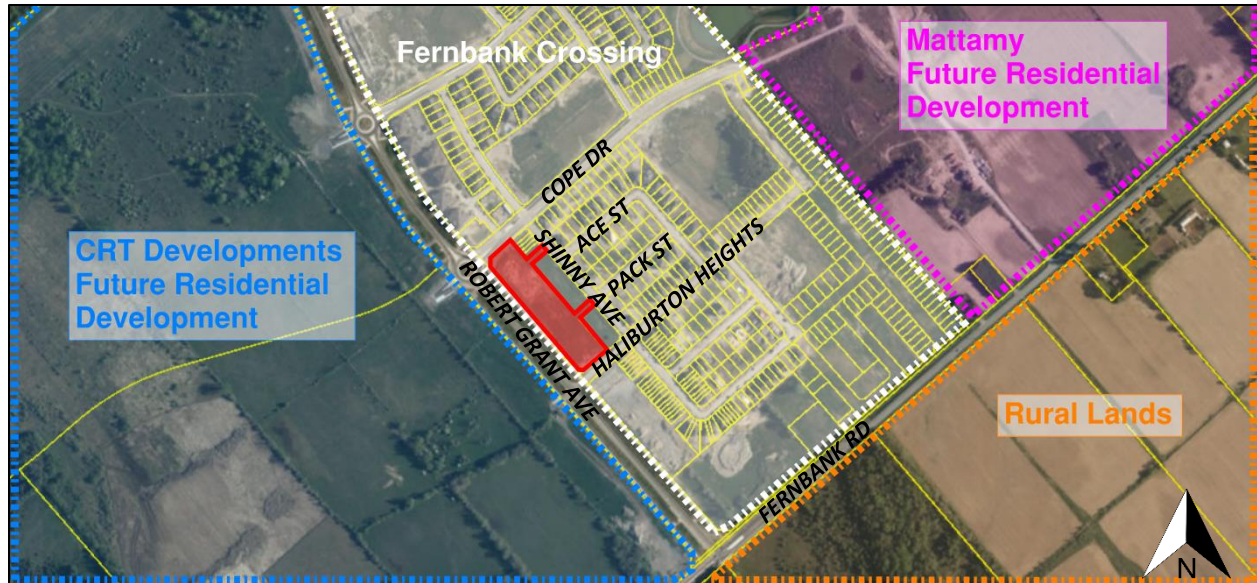


Figure 3: Subject Site location, indicated in red, and surrounding land uses (image source: GeoOttawa)

2.2 Planning Context

2.2.1 City of Ottawa Official Plan

Per Schedule B – Urban Policy Plan of the City of Ottawa *Official Plan*, the Subject Site is located within the General Urban Area, as shown in Figure 4. Please see Section 4.2 of this rationale for a discussion of how the proposed development meets the applicable General Urban Area policies.

Per Schedule C – Primary Urban Cycling Network, Robert Grant Avenue and Fernbank Road are designated as On-Road Cycling Route, as shown in Figure 5.

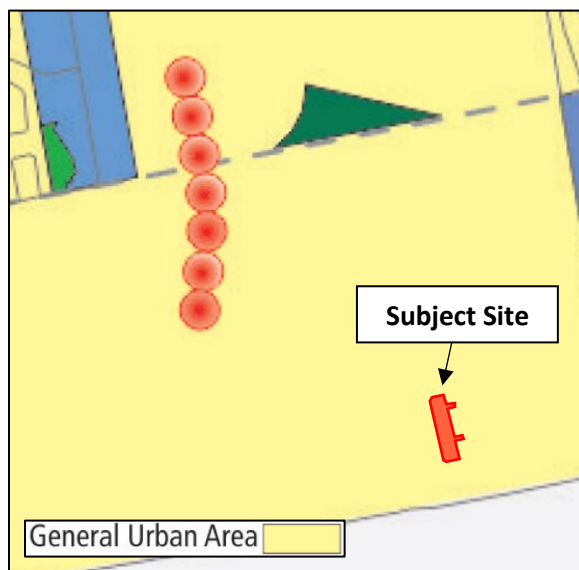


Figure 4: Excerpt of OP Schedule B outlining Subject Site in red

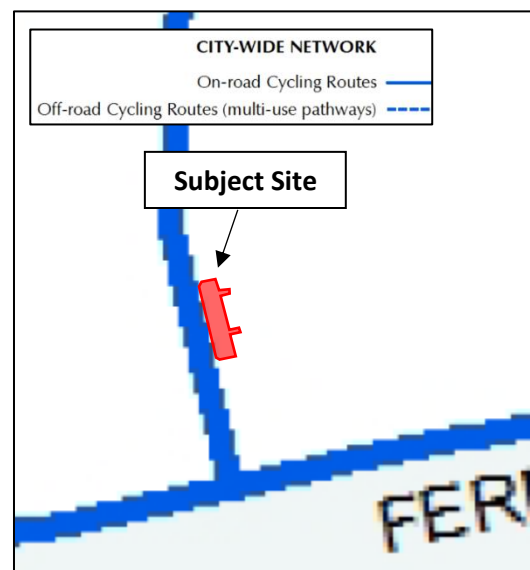


Figure 5: Excerpt of OP Schedule C showing Subject Site in red

Per Schedule D – Rapid Transit Network, Robert Grant Avenue is designated as Bus Rapid Transit (BRT) – At-Grade Crossings. A future Park and Ride Station is identified for the corner of Robert Grant Avenue and Fernbank Road, as shown in Figure 6.

Per Schedule E – Urban Road Network, Robert Grant Avenue is designated as a Proposed Arterial and Cope Drive is designated as a Proposed Major Collector, as shown in Figure 7.

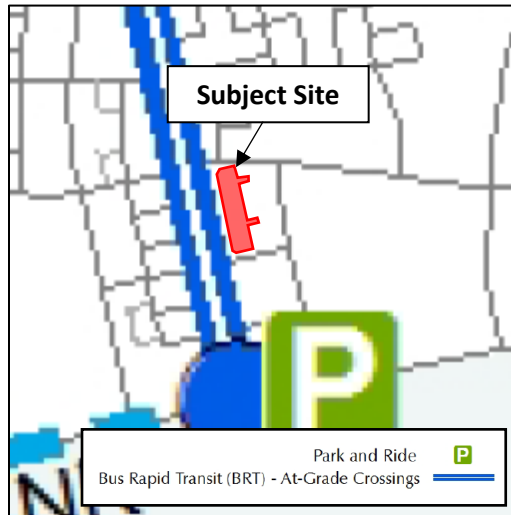


Figure 6: Excerpt of OP Schedule D showing Subject Site in red

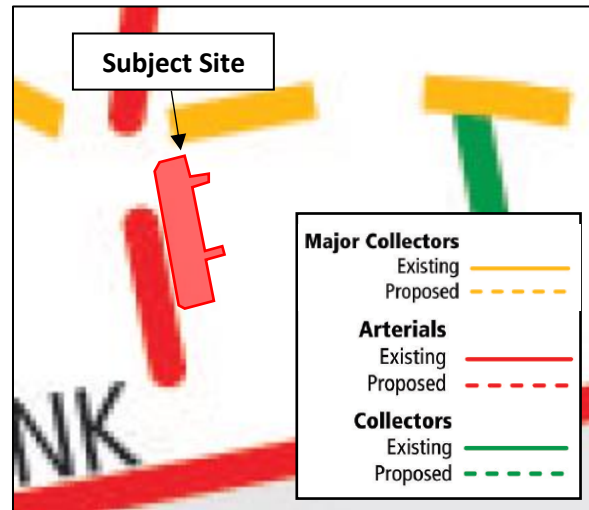


Figure 7: Excerpt of OP Schedule E showing Subject Site in red

Per Schedule K – Environmental Constraints, the Subject Site is not within proximity to environmental constraints, as shown in Figure 8.

Per Schedule L3 – Natural Heritage System (West) the Subject Site is not located within the Natural Heritage System, as shown in Figure 9.

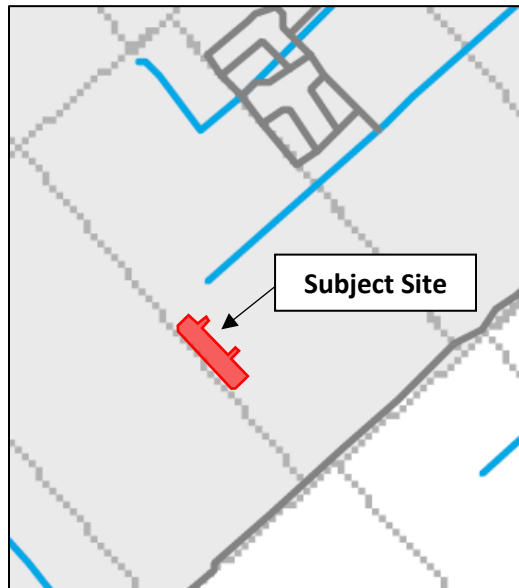


Figure 8: Excerpt of OP Schedule K showing Subject Site in red

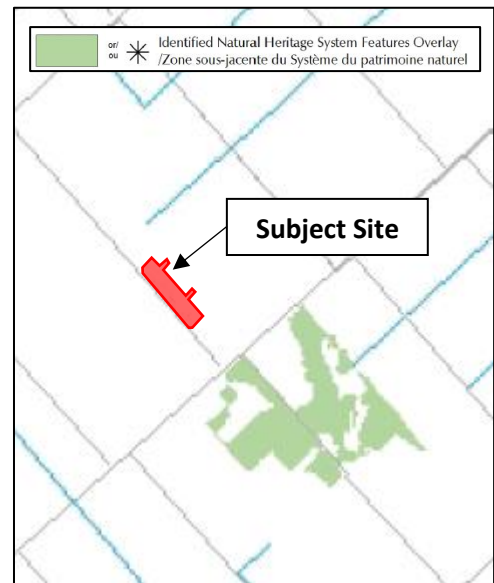


Figure 9: Excerpt of OP Schedule L3 showing Subject Site in red

2.2.2 Fernbank Community Design Plan

The Subject Site is part of the Council-approved Fernbank Community Design Plan (CDP), which was implemented in 2009 through Official Plan Amendment #77. Per the Demonstration Plan in the Fernbank CDP, the Subject Site is identified for medium-density residential development, as shown in Figure 10.

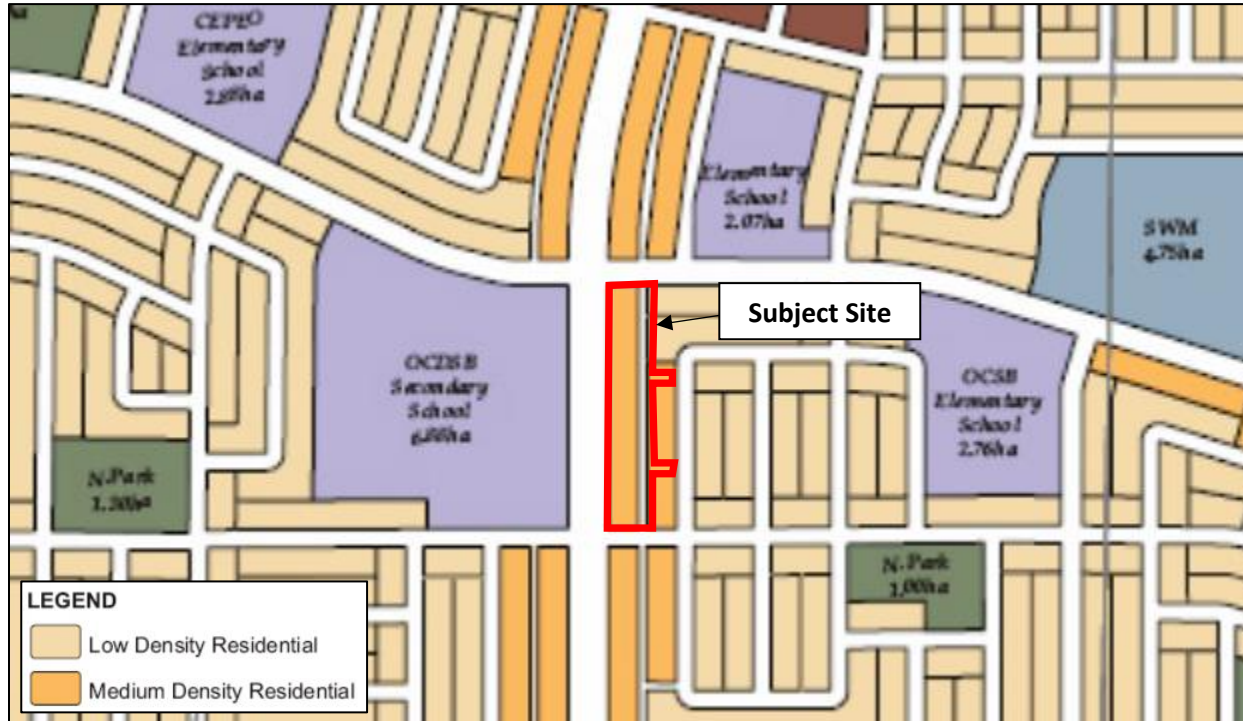


Figure 10: Excerpt from Fernbank CDP Demonstration Plan outlining approximate location of Subject Site in red

Section 4.3 of this rationale demonstrates that the proposed Site Plan is in conformity with the applicable Fernbank CDP policies.

2.2.3 City of Ottawa Zoning By-law 2008-250: Existing Zoning

The Subject Site is currently zoned Residential Third Density (R4M[2124]), as shown in Figure 11. This zone was established as part of the zoning by-law amendment that was Council-approved on July 8, 2015 for Phase 3 of the Fernbank Crossing subdivision (City File No.: D02-02-15-0005).

The purpose of the R4 - Residential Fourth Density Zone is to *allow a wide mix of residential building forms ranging from detached to low rise apartment dwellings, in some cases limited to four units, and in no case more than four storeys, in areas designated as General Urban Area in the Official Plan; allow a number of other residential uses to provide additional housing choices within the fourth density residential areas; permit ancillary uses to the principal residential use to allow residents to work at home; and regulate development in a manner that is compatible with existing land use patterns so that the mixed building form, residential character of a neighbourhood is maintained or enhanced.*



Figure 11: Existing zoning with Subject Site outlined in green (image source: GeoOttawa)

3.0 DESIGN PROPOSAL

eQ Homes is proposing to construct six back-to-back townhouse buildings containing a range of six to twelve dwelling units for a total of 58 dwelling units on the Subject Site, as shown in Figure 7 (see Appendix B for full Site Plan). The residential buildings are oriented along private streets perpendicular to Robert Grant Avenue to create quiet enclaves within the Subject Site and to respect the privacy of the abutting existing townhouses along Shinny Avenue.

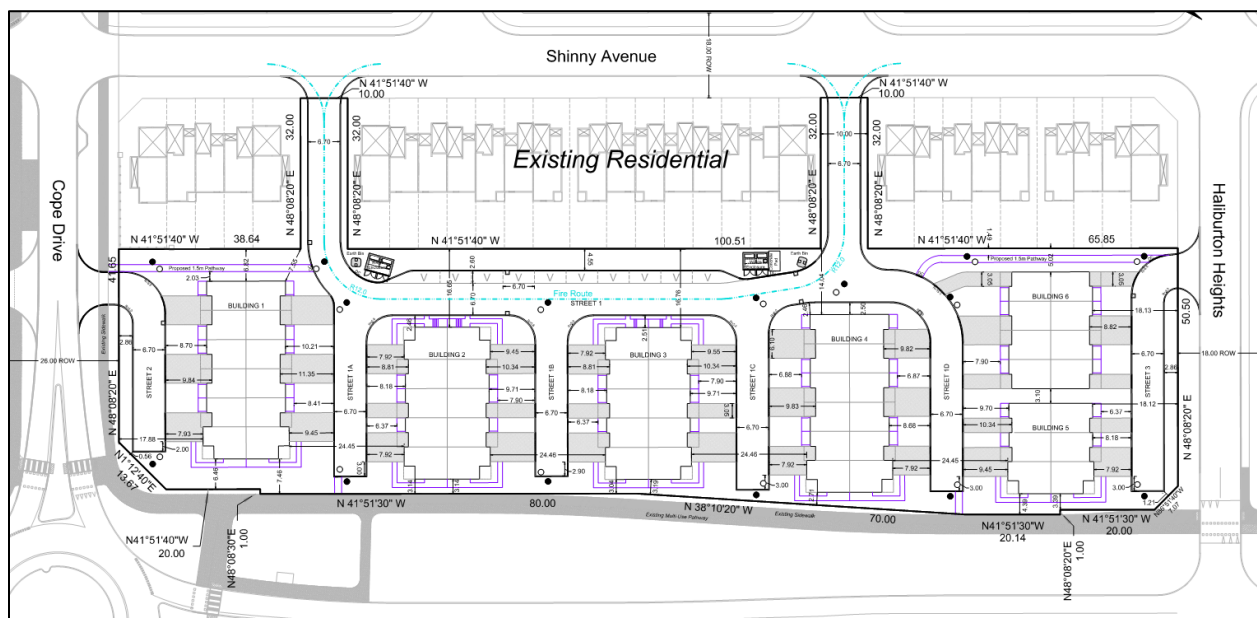


Figure 12: Excerpt of proposed Site Plan (see Appendix B for full plan)

A rendering of the proposed three-storey buildings is provided in Figure 13. A combination of materials, projections and windows are used to break up the building façades and to add visual interest to the streetscape. A consistent level of architectural detail is provided on the side façades of the end units. Primary entrances to end units are provided on the end unit façades, where possible, to further animate the streetscape of both the proposed private street and Robert Grant Avenue.



Figure 13: rendering of proposed back-to-back townhomes (Image Source: RJH Architecture)

Vehicular access to the dwelling units is provided via a series of private streets. Proposed Street 1 loops through the Subject Site from Shinny Avenue. Streets 1a through 1d branch off Street 1 to provide access to proposed buildings 2, 3 and 4 and half of Buildings 1, 5 and 6. Proposed Street 2 provides access to the northern facing half of Building 1 off Cope Drive and proposed Street 3 provides access to southward facing halves of Buildings 5 and 6 off Haliburton Heights. No vehicular access is provided on Robert Grant Avenue. The proposed private streets are 6.7 metres wide and will be created through subsequent Part Lot Control and Private Street Permit applications. Twelve-metre centre line turning radii are provided along Street 1 for emergency vehicles and for City waste collection trucks.

Two communal waste collection areas are provided on either end of the visitor parking area in accordance with the City's *Solid Waste Collection Guidelines for Multi-Unit Residential Development (October 2012)*.

Parking is provided in accordance with the City's Zoning By-law at a rate of 1 space per dwelling unit and 0.2 visitor spaces per dwelling unit. Each dwelling unit has a parking space in its garage and its driveway. Additional visitor spaces are provided as parallel parking along the eastern property line and are screened from the abutting Shinny Avenue rear yards by fencing and proposed landscaping. Driveways have been paired, where possible, to provide space for landscaping.

Pedestrian walkways connect the internal private streets directly to the existing sidewalk along the south side of Cope Drive and the existing sidewalk and multi-use pathway along the east side of Robert Grant Avenue.

To implement the proposal as described above, minor variances or a minor zoning by-law amendment application will be required to permit encroachments within the minimum required rear yard and interior side yard setbacks, as illustrated on the Zoning Compliance Plan in Appendix C. Specifically, a 7.5 metre rear yard setback is required along Robert Grant Avenue and a 7.5 metre interior side yard setback is required along the abutting rear yards of the Shinnery Avenue townhomes. Exception 2124 permits a 3.0 metre setback along Robert Grant Avenue for apartment dwellings within a Planned Unit Development. Although the proposed development is for back-to-back townhouse units, a 3.0 metre setback is reasonable and will create a consistent streetscape with the condominium development immediately north and south of the Subject Site which are subject to 3.0-metre setbacks along Robert Grant Avenue.

4.0 POLICY JUSTIFICATION

4.1 PROVINCIAL POLICY STATEMENT

The 2014 Provincial Policy Statement (PPS) was issued under Section 3 of the Planning Act and came into effect on April 30, 2014. Under the authority of Section 3 of the Planning Act, all decisions affecting planning matters shall be consistent with the PPS.

The Subject Site is part of the Fernbank Crossing subdivision. Regional Group provided a review of the PPS in their 2009 planning rationale in support of the Draft Plan of Subdivision application (City File No.: D07-16-09-0034). Through the subdivision review process, several studies and reports were prepared to support of the proposed development on matters of provincial interest including Archaeological Assessments, Environmental Site Assessments, and Fish Habitat Assessments. Regional Group's rationale demonstrated that the proposed Draft Plan of Subdivision conforms to the PPS, City of Ottawa Official Plan, and Council-Approved Fernbank Community Design Plan (CDP).

Novatech prepared a planning rationale in support of the zoning by-law amendment for Phase 3 of the subdivision lands (City File No.: D02-02-15-0005), at which time Sections 1 through 3 of the PPS were reviewed. Novatech concluded that all matters of provincial interest were adequately discussed and addressed through Regional's planning rationale, and the same conclusion applies to the subject Site Plan.

4.2 CITY OF OTTAWA OFFICIAL PLAN

As stated in Section 2.2.1 of this Rationale, the Subject Site is designated General Urban Area per Schedule B of the *Official Plan*. Section 3.6.1 of the *Official Plan* contains policies for this land use designation.

Policy 1 states that the *General Urban Area designation permits all types and densities of housing, as well as employment, retail uses, service, industrial, cultural, leisure, greenspace, entertainment and institutional uses.*

The proposed residential use is permitted within the General Urban Area.

Policy 2 states that the *evaluation of development applications, studies, other plans and public works undertaken by the City in the General Urban Area will be in accordance with Section 2.5.1 and Section 4.11.*

The following demonstrates conformity with these policies of Sections 2.5.1 and 4.11.

Official Plan Section 2.5.1

Section 2.5.1 of the *Official Plan* presents Design Objectives which express how the City wants to influence the built environment as the city evolves. The following is a list of the Design Objectives with a description below each regarding the proposed development:

To enhance the sense of community by creating and maintaining places with their own distinct identity.

The proposed development maintains the residential nature of the surrounding area while creating a higher-density node at a prominent corner and entrance to the community, and along the Robert Grant Avenue future Bus Rapid Transit corridor.

To define quality public and private spaces through development.

The proposed development clearly defines the public street from the private space within the Subject Site by orienting the buildings to create a network of connected private enclaves while maintaining connections to and framing the abutting public streets with architectural details on the building façades.

To create places that are safe, accessible and are easy to get to, and move through.

The proposed development is easy to get to and move through for pedestrians and vehicles. The Subject Site is situated at the corner of an arterial street (Robert Grant Avenue), a major collector street (Cope Drive) and a collector street (Haliburton Heights) with vehicular access points provided from Shinnery Avenue, Cope Drive and Haliburton Heights. The proposed internal private street network allows for simple wayfinding to the proposed units. Proposed pedestrian walkways provide direct connections from the Subject Site to the existing sidewalks along Cope Drive and Robert Grant Avenue and to the multi-use pathway along Robert Grant Avenue. The Subject Site is also located less than 300 metres away from a future park-and-ride facility at the corner of Robert Grant Avenue and Fernbank Road and within 400 metres of three transit stops serving OC Transpo routes 167 and 252.

To ensure that new development respects the character of existing areas.

The proposed development is a piece within the larger developing Fernbank Community; it takes into consideration the surrounding phases of the Fernbank Crossing subdivision that have already been developed and are being developed to establish a cohesive community character. The proposed design is compatible with the low-density development east of the Subject Site, complimentary to the multi-family condos being developed to the north and south, and consistent with the Fernbank Community Design Plan.

To consider adaptability and diversity by creating places that can adapt and evolve easily over time and that are characterized by variety and choice. [OMB decision #2649, September 21, 2006]

The proposed development introduces an additional housing option to the community to increase variety and choice for residents of all ages and life stages.

To understand and respect natural processes and features in development design.

The proposed development is not located within or adjacent to the City's Natural Heritage System or located in proximity to environmental constraints, and it is clear of vegetation. A landscape plan (Prepared by Novatech, Drawing No.: 117089-L, Rev#1, dated March 22, 2019) has been prepared as part of this Site Plan application indicating the proposed vegetation for the Subject Site.

To maximize energy-efficiency and promote sustainable design to reduce the resource consumption, energy use, and carbon footprint of the built environment.

The proposed buildings represent a compact form of residential development which reduces land consumption and are in proximity to public transit services.

Official Plan Section 4.11

In accordance with Section 4.11 of the *Official Plan*, development applications will be evaluated against compatibility criteria to measure their appropriateness within the surrounding local context. The following is a compatibility analysis of the proposed development based on these criteria:

Traffic

A Transportation Impact Assessment (TIA) (Ref No.: R-2019-024) was prepared for the proposed development and demonstrates that sufficient traffic capacity exists.

Vehicular Access

Two private road connections are proposed onto Shinny Avenue, one onto Cope Drive and one onto Haliburton Heights. The proposed private streets have widths of 6.7 metres. Twelve-metre centre line turning radii are provided to accommodate emergency vehicles and city waste collection services. No vehicular access is proposed on Robert Grant Avenue.

Parking Requirements

On-site parking spaces are provided in accordance with the parking space rates in Sections 101 and 102 of the Zoning By-law. Each proposed unit has one parking space within its garage and one in its driveway. Additional visitor parking spaces are provided as parallel parking along the eastern property line.

Outdoor Amenity Areas

The proposed development respects the privacy of the abutting townhomes along Shinny Avenue by orienting the proposed buildings so that the end-unit side façade is facing the rear yards of the townhome lots and are setback between 5 and 17 metres from the rear lot lines. Within these setbacks, a proposed private street, a pedestrian pathway, visitor parking and vegetation serve as buffers between the proposed buildings and the adjacent outdoor amenity areas.

Loading Areas, Service Areas, and Outdoor Storage

The proposed development does not include a loading area. Two communal City waste collection areas are proposed on the immediate north and south ends of the parallel visitor parking area. Waste bins will be screened with an enclosure at each location. Each proposed unit has its own garage.

Lighting

The proposed buildings are oriented with their end façades facing the abutting residential dwellings to limit the potential for light spill-over or glare from outdoor exterior lights on the buildings which are more prominent on the primary façades of the buildings. Landscaping is proposed along the eastern property line to minimize the impact of headlight glare on the abutting townhomes.

Noise and Air Quality

The proposed residential use is not anticipated to generate excess noise levels or contribute to poor air quality.

Sunlight

The proposed three storey residential buildings will not create adverse shadowing effects on adjacent properties.

Microclimate

The proposed buildings will not have any adverse effects related to wind, snow drifting, and temperature on adjacent properties.

Supporting Neighbourhood Services

The proposed development is adequately served by existing and future services and amenities.

4.3 FERNBANK COMMUNITY DESIGN PLAN

The Subject Site is identified for medium-density residential in the Fernbank CDP. Section 4.2.2 of the CDP outlines the intent and policies for the 'Medium-Density Residential' land use designation and states:

Medium density residential land uses will be dispersed throughout the Fernbank Community to provide a variety of housing types and create diverse and attractive neighbourhoods. Such land uses shall generally be located:

- *Along arterial roads*
- *Along major or minor collector roads;*
- *In proximity to Community or District Parks;*
- *In proximity to community amenities and facilities;*
- *Along hydro transmission corridors; or,*
- *As a transition between non-residential and low-density residential uses (such as, a progression between industrial or mixed-use areas)*

The proposed development maintains the intent of the medium density residential designation by providing back-to-back townhomes at the corner of an arterial road and major collector road that are compatible with and create an appropriate transition to the adjacent low-density residential uses to the east.

Section 6.0 of the Fernbank CDP provides community design guidelines as a framework for the design, identity and structure of the overall Fernbank Community. Sections 6.4.1 and 6.4.2 lists guidelines for development adjacent to arterial roads. The proposed development meets the following guidelines:

- *Development located along the arterial road/transit corridor should be developed to reduce potential conflicts with vehicles and pedestrians by minimizing the crossing of pedestrian walkway and vehicular routes.*
 - No vehicular access points are proposed along Robert Grant Avenue.
- *The main entry of the residential dwelling units or non-residential buildings adjacent to the arterial road/transit corridor should be located towards the corridor.*
 - The proposed end-units along Robert Grant Avenue are designed to provide the principle entry to the dwelling units facing Robert Grant Avenue.
- *Where appropriate, landscaped pedestrian access points should be provided to the neighbourhood from the arterial roads.*
 - Pedestrian walkways are proposed along Robert Grant Avenue to the Subject Site.
- *Reverse lot frontages should be avoided on arterial roads and collector roads, so as to minimize the need for noise attenuation, where possible.*
 - The proposed development does not include outdoor amenity areas thereby avoiding the need for noise barriers. No reverse lot frontages are proposed.
- *Buildings backing onto the community edges should be designed to provide a strong edge condition and reinforce the image of the community. A landscape edge with acoustic/privacy fencing should also be provided.*
 - The proposed development provides a strong edge along Robert Grant Avenue with architectural features on the buildings and landscaping to reinforce the Fernbank Community identity.

Section 6.6.2 lists general guidelines for residential dwellings. The proposed development meets the following guidelines:

- *A variety of housing types and designs within each neighbourhood should be provided to enhance the streetscape.*
 - The proposed development adds variety to the housing types and designs within the Fernbank Community, which is predominantly low-density residential.

- *Rear and flankage elevations of corner lots should be consistent in the quality and detail of the front elevation.*
 - The proposed flankage elevations of the end-units is consistent in architectural quality and detail of the front elevation, as shown in Figure 13.
- *Avoid the dominance of the garage throughout the Community by controlling the projection of a garage in front of the main residential building wall or providing vehicular access from a laneway. Projecting garages on narrow lots may be considered, subject to the approval of the City.*
 - The proposed garages do not project beyond the main residential building wall and are oriented towards each other away from Robert Grant Avenue.
- *Driveways should be paired, where possible, to minimize the presence of garages on the streetscape, and provide for ample space for trees within the boulevard.*
 - Driveways have been paired wherever possible to increase the amount of greenspace for landscaping within the Subject Site.
- *Front entrances should face and be visible from the street.*
 - The front entrances to each of the dwelling units are visible from either the proposed private streets or the surrounding public streets.

6.0 CONCLUSION

It is our assessment that the proposed Site Plan is consistent with the *Provincial Policy Statement, 2014* and generally conforms to the *City of Ottawa Official Plan* and the Fernbank Community Design Plan.

This planning rationale, along with the associated technical studies, supports the development of Block 135 of the Fernbank Crossing subdivision. The proposed back-to-back townhouses are compatible with existing and planned uses and function well within the surrounding context, thereby representing good planning.

Sincerely,

NOVATECH



Prepared By:

Ellen Potts, BES (PI)
Planner



Reviewed By:

Greg Winters, MCIP RPP
Senior Project Manager

APPENDIX A: Plan 4M-1551

APPENDIX B: Site Plan

APPENDIX C: Zoning Compliance Plan