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Heritage Hills Retail Plaza 471 Terry Fox Drive Planning Rationale

Engineering excellence. Planning precision. Inspired landscapes.

**HERITAGE HILLS RETAIL PLAZA AT
471 TERRY FOX DRIVE
OTTAWA, ON**

**PLANNING RATIONALE
IN SUPPORT OF APPLICATIONS FOR
SITE PLAN CONTROL AND ZONING BY-LAW AMENDMENT**

Prepared For:

triMterra Development Corporation

Prepared By:

NOVATECH

Suite 200, 240 Michael Cowpland Drive
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January 25, 2019
Novatech File: 118133
Ref: R-2018-157

January 25, 2019

Ms. Amanda Marsh
Planning, Infrastructure and Economic Development Department
City of Ottawa, Planning Services Branch
110 Laurier Ave. West, 4th Floor
Ottawa, Ontario, K1P 1J1

Attention: Ms. Marsh, Planner II

**Reference: Heritage Hills Retail Plaza – 471 Terry Fox Drive
Site Plan Control and Zoning By-Law Amendment
Our File No.: 118133
City File No.: D07-01-18-0047**

Novatech has prepared this Planning Rationale on behalf of triMterra Development Corporation (the owner) in support of applications for *Site Plan Control* and *Zoning By-Law Amendment* for the lands municipally known as 471 Terry Fox Drive in Ward 5 West Carleton – March, Ottawa, Ontario.

triMterra Development Corporation intends to develop a retail plaza consisting of one (1) single-storey multi-unit retail building as well as a Shell gas bar/convenience store with four (4) pump islands and a drive-through car wash on the property site. The proposed development will be accessed by three (3) entrances: one entrance off Terry Fox Drive; one entrance off Tillsonburg Street; and one entrance off Kanata Avenue.

A *Zoning By-Law Amendment* application will accompany the proposed development. The application will seek relief for the multi-unit retail building setbacks, which allows for the location of the building closer to Terry Fox Drive and Tillsonburg Street. The orientation of the multi-unit retail building closer to the street frontages provides for a more animated streetscape while encouraging walkability and pedestrian-scaled activities. The orientation of buildings closer to the public street was further supported by City staff at the pre-consultation meeting.

This Planning Rationale will demonstrate that the proposed development is consistent with the *Provincial Policy Statement (PPS)*, conforms to the *City of Ottawa's Official Plan*, generally complies with the provisions of the *Zoning By-Law 2008-250* and respects the applicable urban design guidelines found in the *Official Plan*.

Yours truly,

NOVATECH



Robert Tran, M.PL.
Planner, Planning & Development

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1.0 INTRODUCTION AND PROPOSED DETAILS

Novatech has been retained by triMterra Development Corp. (the owner) to prepare this Planning Rationale in support of applications for *Site Plan Control* and *Zoning By-Law Amendment* for the property municipally known as 471 Terry Fox Road in Ward 5 West Carleton – March, Ottawa, Ontario. The property herein will be referred to as the ‘Subject Site’.

triMterra Development Corporation intends to develop a retail plaza consisting of one (1) single-storey multi-unit retail building as well as a Shell gas bar/convenience store with four (4) pump islands and a drive-through car wash. A total of one hundred-twelve (112) surface parking spaces will be provided for the proposed development which satisfies the parking requirements. The proposed development will be accessed by three (3) entrances: one entrance off Terry Fox Drive; one entrance off Tillsonburg Street; and one entrance off Kanata Avenue.

A *Zoning By-Law Amendment* application will accompany the proposed development. The application will seek relief for the multi-unit retail building setbacks, which allows for the location of the building closer to Terry Fox Drive and Tillsonburg Street. The orientation of the multi-unit retail building closer to the street frontages provides for a more animated streetscape while encouraging walkability and pedestrian-scaled activities. The *Zoning By-Law Amendment* which is minor in nature will be discussed further below.

This Planning Rationale will demonstrate that the proposed development is consistent with the *Provincial Policy Statement (PPS)*, conforms to the *City of Ottawa’s Official Plan*, generally complies with the provisions of the *Zoning By-Law 2008-250* and respects the applicable urban design guidelines found in the *Official Plan*.

1.1 Site Description and Surrounding Uses

The Subject Site is situated in the growing community of Kanata Lakes, which has experienced significant growth and development over the last decade. A review of historical aerial mapping has demonstrated that the Subject Site has been undeveloped with no structures or known development having previously occurred on the property. Further information regarding the history of the Subject Site can be found in the *Phase I – Environment Site Assessment Report* prepared by Paterson Group, Report PE4407-1, dated September 5, 2018 which will be submitted with the *Site Plan Control* application.

The Subject Site is situated adjacent to an embankment with a low-density residential subdivision overlooking the site property to the north. The Subject Site has an area of approximately 1.17 ha and is located at the corner of Terry Fox Drive and Kanata Avenue as shown on **Figure 1**. The legal description of the Subject Site is: BLOCK 170, PLAN 4M1413 CITY OF OTTAWA, PIN 045100292. The Subject Site was originally part of the Broughton Subdivision that was registered in 2010, with Block 170 on 4M-1413 intended for commercial development as evident in a staff report to the *Planning and Environment Committee* implementing the CG-X zoning (General Commercial) dated September 16, 2005. The CG-Z zoning as per *Zoning By-Law 167-93* permits an automobile service station with a convenience store.



Figure 1: Subject Site location and existing land use context.

The following describes the land uses adjacent to the Subject Site as shown in **Figure 1**:

North: An open space embankment owned by the City of Ottawa forms the northern boundary of the Subject Site. A low-density residential subdivision on Ingersoll Crescent situated at the top of the embankment overlooks the Subject Site to the south.

East: Kanata Avenue forms the eastern boundary with most lands already have been developed containing low-density residential development, except for an undeveloped triangular parcel of land immediately opposite of the Subject Site.

South: Terry Fox Drive forms the southern boundary of the Subject Site. The lands immediately south of Terry Fox Drive are comprised of natural open space owned by the City of Ottawa with ongoing rehabilitation plans for the Carp River.

West: Tillsonburg Street forms the western boundary of the Subject Site. The lands immediately west of Tillsonburg Street are comprised of various residential built forms including single-detached and stacked dwelling development ranging from low to medium-densities.

1.2 Additional Reports

This report should be read in conjunction with the following reports and plans completed by Novatech, Paterson Group, Dreessen Cardinal Architects Inc., and AECOM in support of the proposed development. Two different Site Plans have been prepared for the proposed retail plaza development. The Site Plan prepared by Dreessen Cardinal Architects Inc. provides an overall layout of the Subject Site whereas the Site Plan prepared by AECOM provides specific details for the Shell gas bar/convenience store as Shell Canada intends to lease a portion of the site. Although the pre-consultation list of required plans and studies identified both a Tree Conservation Report (TCR) and Archaeological Resource Assessment to be completed, it was

later determined that a TCR was not warranted and a previously completed Archaeological Resource Assessment as part of the Broughton Subdivision identified no concerns regarding archaeological issues.

- *Phase I – Environmental Site Assessment, 471 Terry Fox Drive, Ottawa, Ontario, completed by Paterson Group, Report: PE4407-1, dated September 5, 2018;*
- *Geotechnical Investigation, Proposed Commercial Development, Terry Fox Drive at Kanata Avenue, Ottawa, Ontario, completed by Paterson Group, Report PG4564-1 Revision 1, dated November 7, 2018;*
- *Heritage Hills Retail Plaza, 471 Terry Fox Drive, Development Servicing and Stormwater Management Report, Ref: R-2018-158, completed by Novatech, dated January 25, 2019;*
- *Heritage Hills Retail Plaza, 471 Terry Fox Drive, Transportation Impact Assessment, Ref: R-2018-119, completed by Novatech, dated January 2019;*
- *Heritage Hills Retail Development, Landscape Plan, Drawing: 118133-L1 REV 1, produced by Novatech, dated January 25, 2019;*
- *Heritage Hills Retail Development, Landscape Details Plan, Drawing: 118133-L2 REV 1, produced by Novatech, dated January 25, 2019;*
- *Heritage Hills Gas Station and Retail Plaza – Noise Impact Study Report, produced by AECOM;*
- *Heritage Hills Retail – 471 Terry Fox Drive, Ottawa, Ontario; Site Plan, Ground Floor Plan & Roof Plan, and Elevations REV 2, produced by Dreessen Cardinal Architects Inc., dated January 17, 2019; and*
- *Heritage Hills – 471 Terry Fox Drive, Kanata, Ontario; Site Plan, Fuel Pump Plan & Roof Plan, Exterior Elevations, Floor Plan, Site Servicing Plan, Landscape Plan & Landscape Details REV B, produced by AECOM Canada Architects Ltd., dated November 16, 2018.*

1.3 Proposed Development

The owner of the Subject Site is seeking *Site Plan* approval to construct a retail plaza consisting of one (1) single-storey multi-unit retail building as well as a Shell gas bar/convenience store with a drive-through car wash. The multi-unit retail building will consist of nineteen (19) retail units with ninety-six (96) surface parking spaces and ten (10) bicycling parking spaces, which satisfies the parking and bicycle zoning requirements.

The gas bar/convenience store development will consist of: four (4) pump islands with sixteen (16) parking spaces, six (6) bicycling parking spaces, two (2) vacuum cleaning stalls, and a drive-through car wash as shown on **Figure 2**. The proposed development will be accessed by three (3) entrances: one entrance off Terry Fox Drive; one entrance off Tillsonburg Street; and one entrance off Kanata Avenue.

A larger version of the following Site Plan is provided in **Appendix A1**. A detailed Shell gas bar/convenience Site Plan is also provided in **Appendix A2** of this report. As previously mentioned, two different Site Plans will be provided for the proposed development with one

prepared by Dreessen Cardinal Architects Inc. and another provided by AECOM, as Shell Canada intends to lease a portion of the site for the gas bar/convenience store.

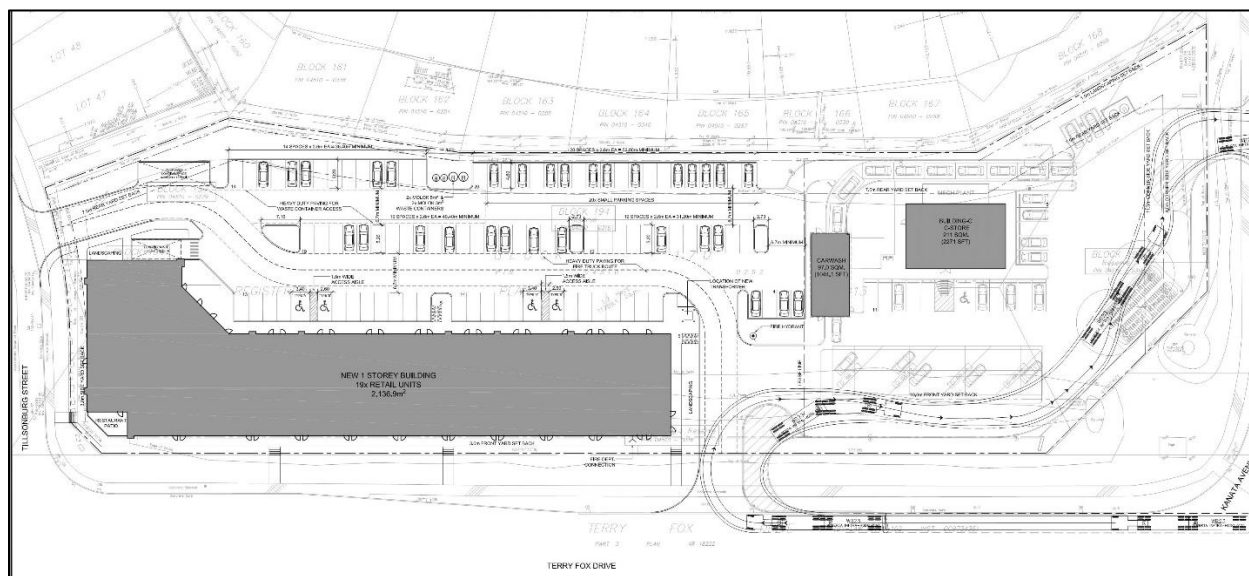


Figure 2: Site Plan for the proposed multi-unit building and gas station development.

The proposed development will incorporate both soft and hard landscape elements throughout the Subject Site, with special consideration around parking areas to ensure adequate buffering between pedestrian movements and vehicular traffic flows. The parking areas for the proposed development have been located towards the rear of buildings to reduce their visual impact to Terry Fox Drive. The loading areas and outdoor garbage areas of the proposed development will incorporate both soft and hard landscaping features to soften their visual impact from adjacent sensitive land uses.

The details of the proposed development describing the architectural features, landscape details, building materials, urban design guidelines and principles will be discussed further below in the report.

2.0 PLANNING POLICY AND REGULATORY FRAMEWORK

This section of the report will provide background on the current policy context of the Subject Site and then demonstrate how the proposed development is consistent with the *Provincial Policy Statement*, conforms to the *City of Ottawa's Official Plan*, *City of Ottawa's Urban Design Guidelines* and generally complies with the *Zoning By-Law 2008-250*.

2.1 Provincial Policy Statement

The *Provincial Policy Statement (PPS)* provides policy direction on land use planning and development matters of provincial interest by setting the policy foundation for regulating the development and use of land. The decisions that affect all planning matters "shall be consistent with" policies issues under the authority of Section 3 of the *Planning Act*. The following is an overall review of the 2014 PPS:

Managing and Directing Land Use to Achieve Efficient and Resilient Development and Land Use Patterns

Section 1.1 of the PPS speaks to managing and directing land use to achieve efficient and resilient development and land use patterns.

Policy 1.1.1 of the PPS notes that healthy, liveable and safe communities are sustained by promoting efficient land use patterns with a range and mix of uses arranged so to minimize land consumption and servicing costs. Further, development patterns should avoid environmental or public health and safety concerns, conserve bio-diversity, and consider the impacts of a changing climate.

- The proposed retail plaza development maximizes and achieves an efficient land use pattern by contributing to the creation of a complete community within the Kanata Lakes community. The proposed development will not cause any environmental, public health or safety concerns and is discussed further in Sections 2.0 and 3.0 of the PPS. The proposed development will utilize municipal servicing and is well served by existing arterial and collector roadways. The site will be accessible for local area residents by various means of transportation such as walking, cycling, public transit, and driving with accessibility parking provided throughout the Subject Site.

Policy 1.1.2 of the PPS speaks to planning and accommodating “an appropriate range and mix of land uses to meet projected needs for a time horizon of up to 20 years...”

- The proposed development will allow for additional uses to serve the long-term needs of community residents through local services, employment, and retail.

Wise Use and Management of Resources

Section 2.0 of the PPS discusses the conservation and protection of provincial features such as natural heritage, water, agriculture, minerals and petroleum, mineral aggregate resources, and cultural heritage and archaeology.

- The proposed development will not have any negative impacts to any natural or environmental areas immediately south of the Subject Site known in the *City of Ottawa's Official Plan* as the Carp River Restoration Policy Area.
- A completed archaeological assessment of the Subject Site as part of the previous draft plan of subdivision application identified no concerns regarding archaeological significance. Further information can be found in the “Stage 2 and 3 Archaeological Assessment of the Phase 1 Areas of the Broughton Lands, Part Lot 5, Concession 1, March Township, City of Ottawa” dated January 31, 2007 and completed by Northern Archaeological Associates as well as correspondence dated June 3, 2008 from the Ministry of Culture.
- There are no natural heritage, significant water resources, prime agricultural areas, minerals or petroleum resources, mineral aggregate resources, or cultural heritage and archaeological issues identified on the Subject Site.

Protecting Public Health and Safety

Section 3.0 of the PPS speaks to the protection of the province's long-term prosperity, environmental health, and social well-being of residents from any from development that contributes to natural or human-made hazards.

- A Phase I – Environmental Site Assessment (ESA) was prepared by Paterson Group dated September 5, 2018 to assess the potential site contamination by current or past uses. The Phase I ESA concludes that a Phase II ESA is not warranted.
- A Geotechnical Investigation was prepared by Paterson Group dated November 7, 2018 to determine subsurface soil and groundwater conditions and provide geotechnical recommendations for the design of the proposed development. The findings from the report are: the subject site is satisfactory for the proposed development from a geotechnical perspective; due to the presence of the silty clay layer, the proposed development will be subjected to grade raise restrictions; and a significant slope presently exists along the north property line.

2.2 City of Ottawa Official Plan [OPA #150]

The Subject Site is designated under the *City of Ottawa's Official Plan Schedule B* as General Urban Area as shown on **Figure 3**. The Subject Site is situated north of the *Carp River Restoration Policy Area Overlay* with the proposed development not anticipated to have any negative implications to these sensitive lands.

The General Urban Area designation permits a full range of land uses including various housing types, employment, retail, service, cultural, leisure, entertainment, and institutional. The proposed development is a compatible land use for the community as it will provide access to services, employment, and retail that is located within walking distance of existing residential development. Presently, access to several community amenities such as employment, retail, and services are not accessible without the reliance upon automobiles or use of public transit. The proposed retail plaza development contributes to the creation of a complete community within Kanata Lakes.

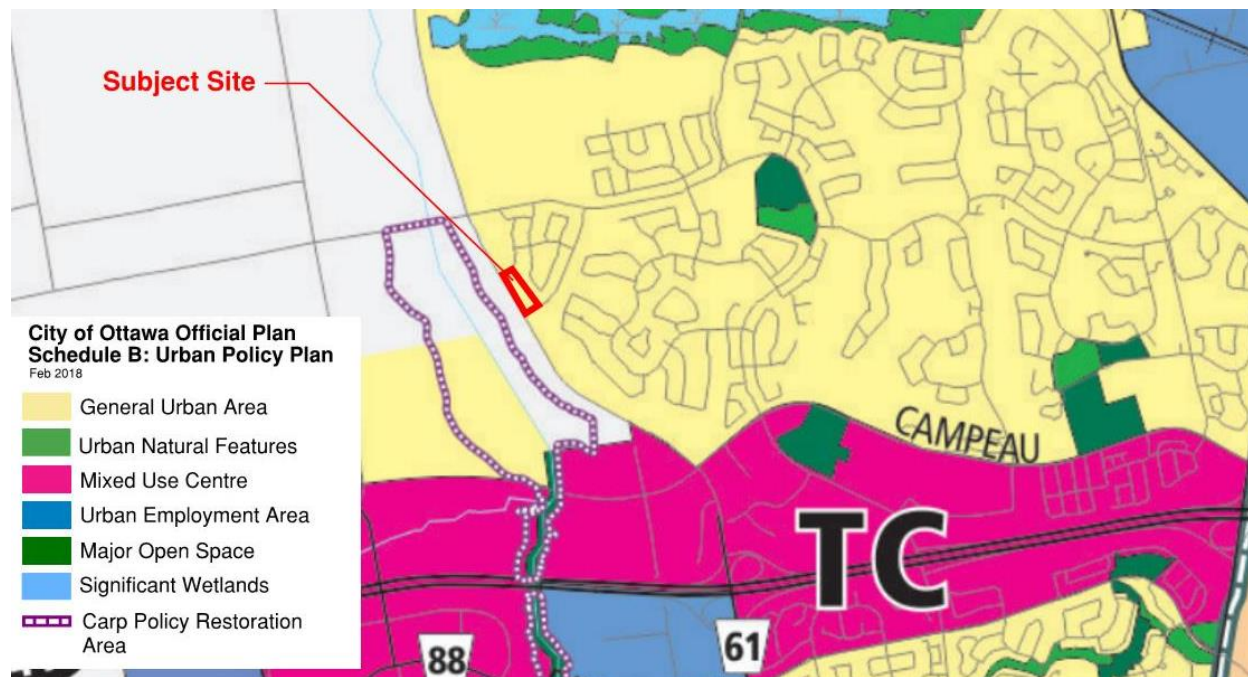


Figure 3: City of Ottawa Official Plan – Schedule B: Urban Policy Plan.

Section 2.5.1 of the *Official Plan* speaks to the Urban Design and Compatibility which is concerned with how buildings, landscapes, and public spaces function together. Design elements should work together to complement and enhance the unique aspects of a community's history, landscape, and its culture. Encouraging good urban design and quality as well as innovative architecture will help stimulate the creation of lively community places with unique and distinct character while attracting people and investment to the City. Compatible development speaks to enhancing the features of an established community and coexisting with existing development without causing undue adverse impacts on surrounding properties.

The following is a review of the design objectives and summary of how the proposed development is consistent with these objectives:

- 1) *To enhance the sense of community by creating and maintaining places with their own distinct identity.*
 - The proposed development features a modern architectural design that provides for its unique and distinct retail identity while also compatible with existing commercial developments in the surrounding area.
 - The selected colours and building materials for the proposed gas bar/convenience store ensures that the Shell corporate brand is maintained universally as shown on **Figure 4** and **Figure 5**.
 - The exterior finishes for the multi-unit retail building will feature aluminum panels, prefinished corrugated metal siding, prefinished steel siding, and masonry veneer as shown on **Figure 6**, **Figure 7**, and **Figure 8**. The exterior window features along with the selected finishes and colours provide for a modern and streamline architectural design that requires low-maintenance.

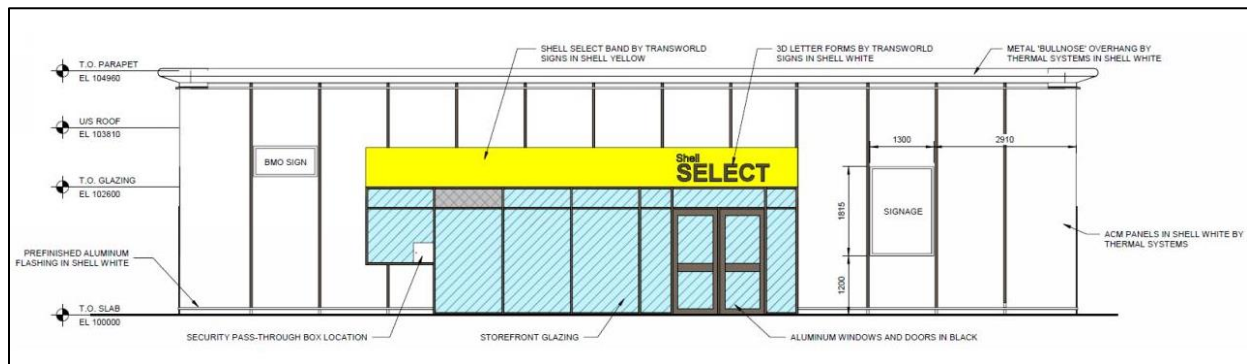


Figure 4: Conceptual gas bar/convenience store elevation – View looking north (shown as West Elevation on A201.1 dated November 16, 2018).

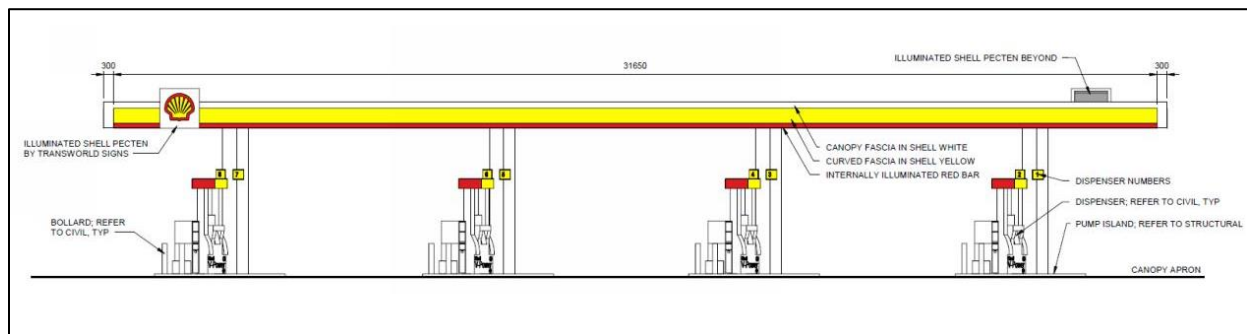


Figure 5: Conceptual fuel pump elevation – View looking south (shown as South Elevation on A201.2 dated November 16, 2018).

2) To define quality public and private spaces through development.

- The orientation of the multi-unit building closer to the public street frontages will provide for a more animated streetscape while encouraging walkability and pedestrian-scaled activities through private spaces. A potential restaurant patio located at the southwest corner of the Subject Site will allow for an active and lively outdoor space while also providing views onto Terry Fox Drive and Tillsonburg Street overlooking the Carp River.

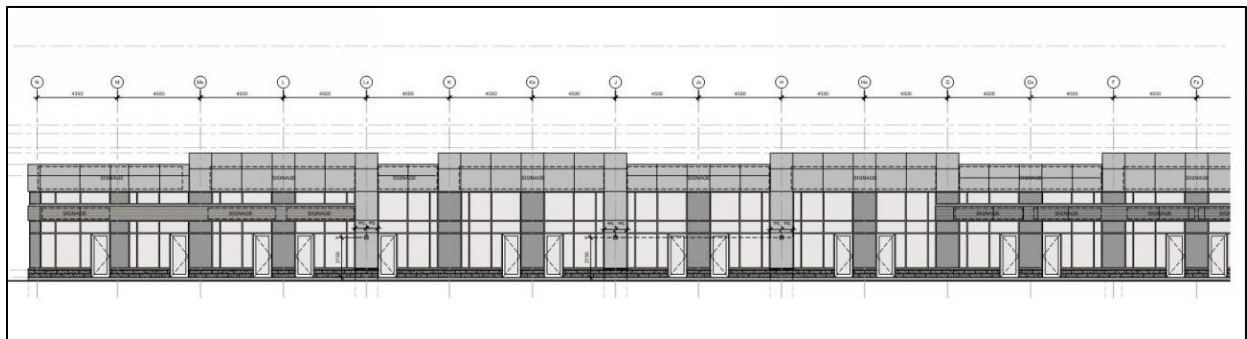


Figure 6: Conceptual multi-unit retail building partial elevation – View looking south (shown as North Elevation on 1 – A300 dated January 16, 2019).

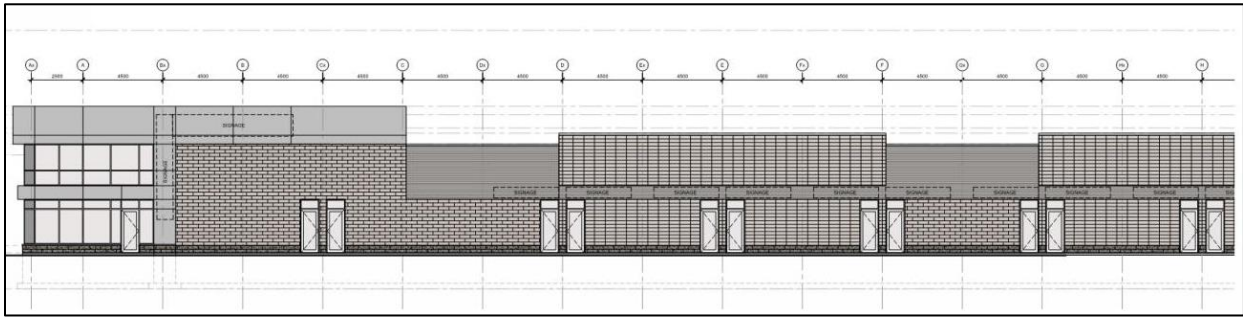


Figure 7: Conceptual multi-unit retail building partial elevation – View looking north (shown as South Elevation on 1 – A301 dated January 16, 2019).

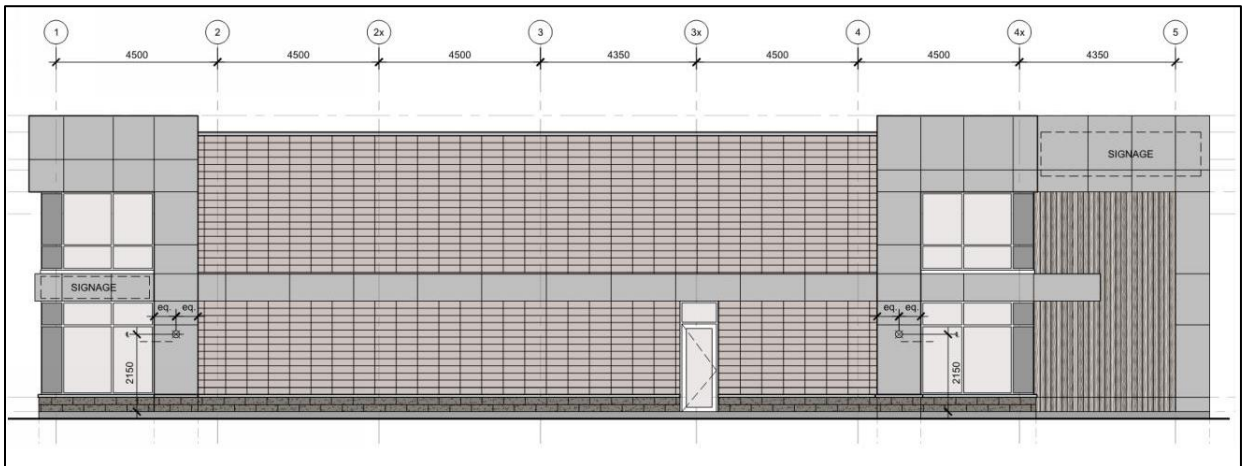


Figure 8: Conceptual multi-unit retail building elevation – View looking east (shown as West Elevation on 4 – A300 dated January 16, 2019).

3) *To create places that are safe, accessible and are easy to get to, and move through.*

- The proposed development features a pedestrian walkway layout that limits interaction with vehicular movement throughout the Subject Site. The proposed development will be accessible by various means of transportation such as walking, cycling, public transit, and driving.

4) *To ensure that new development respects the character of existing areas.*

- The design of the proposed development will ensure that the character of the existing residential development is respected while also compatible with the design of other commercial developments in the surrounding area. The design of the multi-unit retail building and gas bar/convenience store features large windows which provides for active storefront businesses and allows for the passing through of natural light.
- Signage located on the façade of the buildings will be visible from the pedestrian-scale level as well as vehicular traffic travelling along the existing roadways.

- 5) *To consider adaptability and diversity by creating places that can adapt and evolve easily over time and that are characterized by variety and choice.*
 - Although the proposed development provides additional variety and choice for the existing community, it is not anticipated to significantly evolve over time.
- 6) *To understand and respect natural processes and features in development design.*
 - The proposed development will not negatively impact or affect the environmental features surrounding the area of development. The surrounding area's environmental feature includes the Carp River which is currently undergoing restoration efforts to rehabilitate the natural area.
- 7) *To maximize energy-efficiency and promote sustainable design to reduce the resource consumption, energy-use, and carbon footprint of the built environment.*
 - The surface parking areas have been designed to incorporate landscaped islands which provides for shade and reduces the impact of urban heat island effect. Bicycle parking will be provided throughout the Subject Site to encourage an active and sustainable mode of transportation to and from the property.

Section 4.11 of the *Official Plan* also speaks to Urban Design and Compatibility and works in conjunction with the policies found in Section 2.5.1 as previously reviewed. The policies within this section focus on urban design and compatibility at a more localized scale for neighbourhoods and individual properties. Although it is important to consider all policies in the design aspects, the policies that are relevant to the proposed development are reviewed below:

- 1) *When evaluating compatibility of development applications, the City will have regard for the policies of the site's land use designation, and all applicable Community Design Plans, Secondary Plans...*
 - The Subject Site is not subject to any *Community Design Plans* or *Secondary Plans* in the City of Ottawa. The proposed development will incorporate high quality architecture, urban design, and landscaping features that respects the design of existing development in the area.
- 2) *In addition to those matters set out in Policy 1, above, the City will evaluate the compatibility of development applications on the basis of the following compatibility criteria. The measures of compatibility will vary depending on the use proposed and the planning context. Hence in any given situation individual criteria may not apply and/or may be evaluated and weighed on the basis of site circumstances.*
 - a. *Traffic:*
 - The proposed development is not anticipated to generate high traffic volumes as the retail plaza will primarily serve residents in the immediate community. The existing road networks will adequately serve the vehicle traffic generated from the proposed development as discussed in the Transportation Impact Assessment (TIA) completed by Novatech, dated January 2019.

b. *Vehicular Access:*

- The proposed development will be accessed by three (3) entrances: one entrance off Terry Fox Drive; one entrance off Tillsonburg Street; and one entrance off Kanata Avenue. The proposed development will not egress directly onto any other developments within the surrounding areas.

c. *Parking Requirements:*

- The proposed development will provide one hundred-twelve (112) surface parking spaces as well as sixteen (16) bicycle parking spaces which satisfies the required amount as outlined in the zoning by-law provisions for retail and convenience store parking.

d. *Outdoor Amenity Areas:*

- The proposed development will respect the privacy of outdoor amenity areas for adjacent residential developments. The location of the Subject Site at the bottom of an embankment further mitigates any concerns regarding privacy for adjacent residential developments.

e. *Loading Areas, Service Areas, and Outdoor Storage:*

- The loading areas of the proposed development are located to respect adjacent residential development while ensuring efficient and convenient access for the multi-unit retail building. The waste collection/outdoor storage area and use of Molok waste receptacles will ensure that the appearance of waste is hidden, and any emitted odours are contained. Loading and outdoor storage areas of the proposed development will feature screening measures such as soft and hard landscaping details to ensure the privacy of adjacent residential development is respected.

f. *Lighting:*

- The proposed development will not generate a spillover or glare from any lighting sources onto adjacent light-sensitive areas and will meet the City standards of 0.5 foot-candles at the property line.

g. *Noise and Air Quality:*

- The layout of the proposed development as well as building setbacks will ensure that any adverse effects pertaining to potential noise, odours, and other emissions are minimized for adjacent sensitive land uses.

h. *Sunlight:*

- The height of the proposed building and pump island canopy structure will not generate excessive shadowing on streetscapes or adjacent land uses. The potential restaurant patio is oriented to the west for views over the Carp River.

j. *Supporting Neighbouring Services:*

- The proposed development will contribute to providing additional services for the community such as retail, employment, and office uses which currently are not readily available within walking distance. Presently, access to existing community services and amenities are not available without the use of public transit or reliance upon automobiles.

2.3 City of Ottawa Zoning By-Law 2008-250

The Subject Site is currently zoned as Local Commercial – LC7 [411] as shown on **Figure 9**. The purpose of the Local Commercial Zone is to:

- allow a variety of small, locally-oriented convenience and service uses as well as residential uses in the General Urban Areas and in the Residential Character Areas of the Central Area designations of the Official Plan;*
- restrict the non-residential uses to individual occupancies or in groupings as part of a small plaza that would meet the needs of the surrounding residential uses;*
- provide an opportunity to accommodate residential or mixed uses development; and*
- impose development standards that will ensure that the size and scale of development are consistent with that of the surrounding residential area.*



Figure 9: Existing zoning for the Subject Site and surrounding area.

The proposal to construct a retail plaza consisting of a one-storey multi-unit retail building and gas bar/convenience store on the Subject Site complies with the permitted land uses as described in the zoning by-law provisions for a Local Commercial Zone and the urban exception LC7[411] as shown on **Table 1**. The proposed development will complement and serve the long term needs of the surrounding residential developments.

The Subject Site is located within the *Area C: Suburban on Schedule 1A to Zoning By-Law 2008-250 of Areas for Minimum Parking Space Requirements*. The parking requirements for the proposed retail development as well as convenience store requires a minimum of 3.4 parking spaces per 100 m² of gross floor area which amounts to a total of eighty (80) required parking spaces for Subject Site. The proposed gas bar and drive through car wash are exempt from providing any required parking spaces. The proposed development will provide a total of one hundred-twelve (112) surface parking spaces which will adequately serve the development and avoid any potential parking spillover onto other surrounding areas.

The LC7[411] zoning by-law provisions that apply to the Subject Site are summarized in **Table 1** below:

I Exception Number	II Applicable Zone	Exception Provisions		
		III Additional Land Uses Permitted	IV Land Uses Prohibited	V Provisions
411 (By-law 2012-33)	LC7[411]	<ul style="list-style-type: none"> - automobile service station - car wash - gas bar - restaurant, fast food 		<ul style="list-style-type: none"> - the total cumulative gross leasable floor area for the additional uses and a convenience store must not exceed 300 m² - the provisions of Table 189 of subsection 189(3) do not apply and the following provisions apply: - minimum lot area: - additional uses and a convenience store: 4,000 m² - other non-residential uses: 1,800 m² - minimum lot width: 30 m - minimum front yard setback: 10 m including pump islands - minimum corner side yard setback: - pump islands: 11.5 m - other buildings and structures: 8 m - minimum interior side yard setback: 2 m - minimum rear yard setback: 5 m - the property line abutting Terry Fox Drive is deemed to be the front property line

Table 1: LC7[411] zoning by-law provisions for the Subject Site.

2.4 Proposed Zoning By-Law Amendment

The proposed development generally complies with the zoning by-law provisions for permitted land uses in a Local Commercial Zone and general building setbacks, except for the front yard and westerly corner side yard setbacks. As previously mentioned, a *Zoning By-Law Amendment* application which is minor in nature, will accompany the proposed multi-unit retail development. The application will seek relief to the minimum front side yard setback fronting Terry Fox Drive and the westerly minimum corner side yard setback fronting Tillsonburg Street.

In February 2012, a minor variance application, File No.: D08-02-12/A-00063 was submitted to accompany a previously proposed development for the Subject Site. The previous proposal for the Subject Site featured a commercial plaza consisting of three (3) one-storey retail buildings with surface parking spaces. In the minor variance application, relief was sought for the front yard

setback, from 10.0 m to 3.0 m for all three retail buildings as well as the corner side yard setback from, 8.0 m to 3.0 m for the one westerly retail building. The minor variance application was granted in May 2012 as it satisfied the “four tests” under the *Planning Act* for authorizing variances in relation to the previously purposed development.

Presently, the reduction in setbacks that are being requested for the multi-unit building development are identical to those that were granted in May 2012 as part of the minor variance application for the previously proposed development.

The details of the proposed minor rezoning are summarized in **Table 2** below.

Setbacks	Existing Zoning Setbacks (m)	Proposed Zoning Setbacks (m)
Minimum front yard (fronting Terry Fox Drive)	10.0	3.0
Minimum corner side yard (fronting Tillsonburg Street)	8.0	3.0

Table 2: Existing and proposed zoning setbacks.

The relief required for the minimum front yard and westerly minimum corner side yard setbacks that are presently being requested will allow the orientation of the multi-unit retail building closer to the public street frontages to provide for a more animated streetscape. The orientation of the multi-unit retail building closer to Terry Fox Drive and Tillsonburg Street was recommended and supported by City staff during several meetings and lengthy discussions during the conceptual site plan design stages. The relief being requested speaks exclusively to the minimum front yard setback fronting Terry Fox Drive and the westerly minimum corner side yard setback fronting Tillsonburg Street. The rezoning is considered minor in nature as it will not create any undue impacts to neighbouring properties and sensitive land uses.

3.0 CONCLUSION

It is our assessment that the proposed development is consistent with the *Provincial Policy Statement (PPS)*, conforms to the *City of Ottawa's Official Plan*, generally complies with the provisions of the *Zoning By-Law 2008-250* and respects the applicable urban design guidelines found in the *Official Plan*.

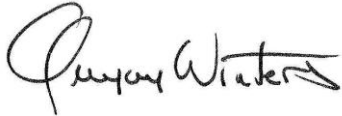
This planning rationale and the associated technical studies supports the development of the proposed retail plaza. The proposed development is compatible with the existing and planned surrounding uses and functions well within the surrounding context. The proposed development is an appropriate and desirable addition to the community and represents good planning.

Yours truly,

NOVATECH

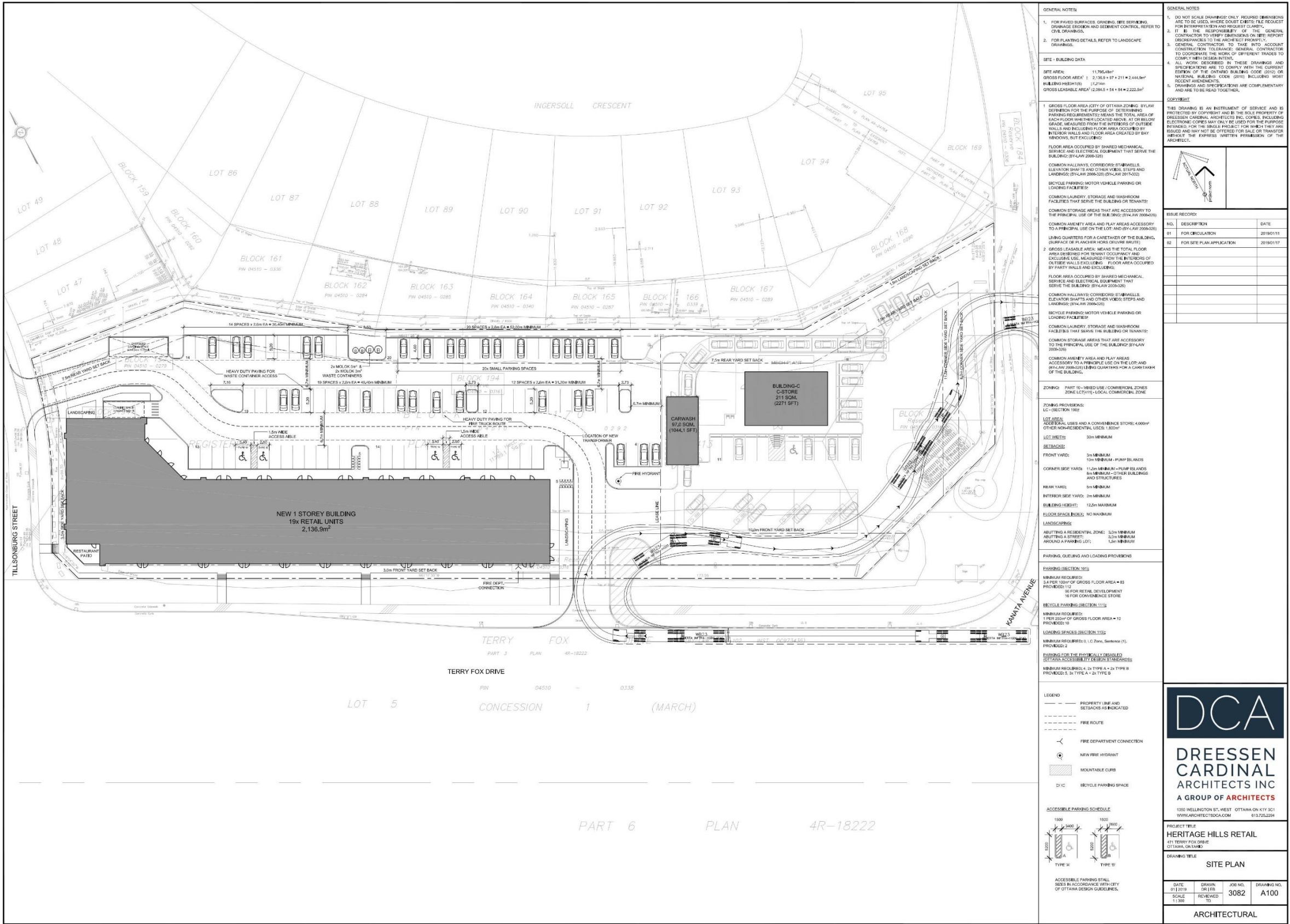
A handwritten signature in black ink, appearing to read 'Robert Tran'.

Robert Tran, M.PL.
Planner, Planning & Development

A handwritten signature in black ink, appearing to read 'Greg Winters'.

Greg Winters, MCIP RPP
Senior Project Manager, Planning & Development

**Appendix A1
471 Terry Fox Drive
Site Plan REV 2 A100, Dated January 17, 2019
By Dreessen Cardinal Architects Ltd.**



**Appendix A2
471 Terry Fox Drive
Shell – Site Plan REV D C101-0
Dated November 16, 2018
By AECOM**

