



Planning Rationale in Support of an Application for Site Plan Control

**Innes Crossing
1501 Innes Road
City of Ottawa**

DRAFT

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1.0 Introduction

Holzman Consultants Inc. has been retained to prepare a planning rationale in support of an application for Revision to Site Plan Control, Manager Approval, Public Consultation at 1501 Innes Road in the Beacon Hill – Cyrville ward of the City of Ottawa (the “Subject Property”). The purpose of the application is to facilitate the development of a single-storey retail building with a Gross Floor Area (GFA) of 625 m².

This report provides a description of the existing conditions and proposed development and contains a review of the applicable land use planning policies, including the Provincial Policy Statement (PPS), the Official Plan (OP), and Zoning By-law 2008-250. The proposed development is supported by the PPS, OP, and Zoning By-law and represent good and defensible land use planning.

2.0 Site Overview

The Subject Property is part of the Innes Crossing commercial development in the East Industrial neighbourhood. The site is located approximately 200 m to the west of the intersection of Innes Road and Cyrville Road, and is 400 m to the east of the Highway 417 / Innes Road interchange (**Exhibit A**).

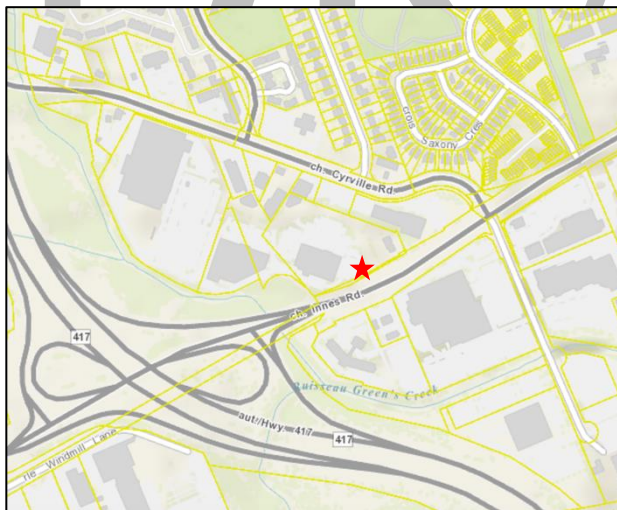


Exhibit A – Site context, Subject Property is shown in red (GeoOttawa)



Exhibit B – Site aerial view, development area is outlined in red (GeoOttawa)

1501 Innes Road is part of a larger complex that includes three parcels for a total area of 4.04 hectares. The development area is located immediately to the northwest of the site driveway and Innes Road, as shown in **Exhibit B**. The development area is currently used as a paved surface parking lot which serves individuals visiting the

nearby retail stores. A sewer easement and a hydro line running east-west are located immediately to the south of the development area.

The site context is defined by the following land uses:

- North: A surface parking lot with 139 spaces and additional parking areas.
- West: Two large-format retail stores with a cumulative GFA of 3,800 m².
- South: Innes Road, with a hotel, restaurants, and a gas station further to the south.
- East: A fast-food restaurant and furniture store are located across the site driveway.



Exhibit C – Site view from Innes Road, facing northeast (Google Earth)

3.0 Description of Proposed Development

The site will be redeveloped with a single-storey retail building with a GFA of 625 m², a width of 19 m, and a length of 35 m (**Exhibit D**). The proposed building is oriented internally to the site (northwest). Soft landscaping is provided along the southern and eastern site frontages, including a mix of deciduous trees, shrubs, and ornamental grasses. The external garbage area, located to the west of the building, is screened from view from Innes Road by a 2.4 m high fence. The existing sign pylon at the eastern edge of the site will be retained.

A total of 37 parking spaces are removed to permit the proposed development, with 16 spaces retained adjacent to the proposed building. Two accessible parking spaces are located to the west of the building, with depressed curbs provided to facilitate access to the retail store. Vehicular access will be provided from Innes Road via the existing site driveway and parking lot. Loading is to be accommodated on the southern edge of the building, with a dedicated truck area provided. Pedestrian access will be maintained through the existing sidewalk to the east and north of the building.



Exhibit D – Site plan, development area is outlined in red

4.0 Planning Context

The applicable policy framework includes an examination of the Province of Ontario's land use planning directives expressed in the Provincial Policy Statement, and the City of Ottawa's policies expressed in the Official Plan and Zoning By-law 2008-250.

4.1 Provincial Policy Statement

The Provincial Policy Statement (PPS) provides provincial-level policy direction on matters related to land use planning and development, with the current PPS released in 2014. Issued under the legislative authority of the *Planning Act*, development plans are to be consistent with the PPS.

Section 1.0 of the PPS describes how strong, healthy communities are built. Policy 1.1.1 provides eight criteria to be considered in the creation of such communities, six of which are addressed as follows:

- a) The proposed development will contribute to the municipal tax base and the activity of the local economy, improving the financial well-being of the City of Ottawa.
- b) The development will introduce new retail space, boosting local employment and contributing to the commercial needs of Ottawa's east end.
- c) No public health or safety concerns can reasonably be expected.
- e) Land consumption is minimized as the retail building is to occupy a vacant portion of an already developed site, promoting efficiency in land usage.
- f) The retail building will be accessible in line with the latest Ontarian standards.

- g) The site is already serviced, and existing infrastructure has residual capacity with which to accommodate the proposed use.

The development of the vacant portion of the Subject Property is an example of intensification, in keeping with Policy 1.1.3.3. The Subject Property is located within the existing Ottawa settlement area; by introducing new development here, the vitality of the settlement area is promoted per Policy 1.1.3.1. Beyond the considerations noted above with respect to Policy 1.1.1, the proposal aligns with the PPS land use standards for settlement areas, as:

- The site is accessible by active transportation from the nearby neighbourhood of Pineview;
- The development can be accessed by OC Transpo's Route 42: Blair-Hurdman; and,
- Transport vehicles required to support the retail use can access the site without issue.

Section 1.3 of the PPS considers how employment should be accommodated in communities. While the retail store proposed is small-scale, it contributes to the overall economic development of Ottawa. The retail use will support the residents of Ottawa and provide new job opportunities in a place where existing infrastructure can accommodate growth without expansion.

Employees and store guests alike must make use of the municipal transportation system to travel to the Subject Property. Policy 1.6.7.2 states that efficient use shall be made of existing and planned transportation infrastructure. The small-scale nature of the proposed use will not require any changes to the municipal transportation system. The Subject Property is located in an area that inherently prioritizes an auto-centric mode share. However, through connections to the municipal sidewalk network, the site can be reached by active transportation from nearby neighbourhoods and the Route 42 bus stops.

Therefore, the proposed commercial development is consistent with the Provincial Policy Statement, 2014.

4.2 City of Ottawa Official Plan

The City of Ottawa Official Plan (OP) encompasses three documents – the Official Plan, Official Plan Amendment #150, and Official Plan Amendment #180. Amendment #150 was undertaken as a 5-year review to the original document which was then appealed, where further changes were made that became the basis for Amendment #180. These policies have been assessed below as they pertain to the Subject Property. The

documents have been interpreted to give precedence to the latest opinion of City Council where relevant.

General Urban Area Designation (Section 3.6.1)

The Subject Property is designated General Urban Area per Schedule B of the Official Plan. Per Policy 1 of Section 3.6.1, the General Urban Area permits retail uses given their contribution to complete communities. The proposed retail store will generate traffic from employees and visitors travelling to the site. Per Policy 5, the proposed use is accessible from Innes Road, an Urban Arterial roadway, and is supported by OC Transpo service (Route 42). Disruption of nearby residential areas will be minimized, as the site is over 200 m from the nearest residential dwellings. Therefore, the proposed development aligns closely with the General Urban Area designation.

Urban Design and Compatibility (Section 2.5.1)

Section 2.5.1 expresses high-level objectives and principles with respect to the urban design of new developments. The proposed development implements three of these objectives, which are discussed below.

The proposed development is an opportunity to better define public and private spaces. The development area borders the public space of Innes Road and the private space of Innes Crossing, yet the existing surface parking lot does not define either of these spaces. Further, the proximity of the development area to the site driveway means that it is a key interface between these two spaces. Introducing a new building at this interface will improve the enclosure of the public street and site driveway.

The proposed retail use should be a safe and accessible destination. Accordingly, the store is linked to the internal site pathway and municipal sidewalk networks, with depressed curbs provided to improve accessibility. While the proposed use is not expected to significantly increase transit ridership in the area, the site can be accessed by OC Transpo's Route 42, with multiple stops to the east along Cyrville Road.

Finally, the proposed retail use respects the existing character of Innes Crossing. The site context is defined by a mix of large-format retail and smaller commercial uses; therefore, the scale of the building proposed is not out of place. Further, efforts have been made to ensure that the architectural character of the building contributes to the overall development.

Urban Design and Compatibility (Section 4.11)

Section 4.11 of the Official Plan translates the objectives and principles of Section 2.5.1 into clear criteria against which to evaluate development applications. Specifically, Policy 2 provides 10 compatibility criteria, five of which are addressed as follows:

- a) **Traffic:** The Subject Property is accessed from Innes Road, an Urban Arterial roadway. The small GFA of the retail use will not generate traffic levels that will exceed the residual capacity of Innes Road.
- b) **Vehicular Access:** Vehicular access to the site will not change, with the driveway to the east maintained.
- c) **Parking Requirements:** A total of 425 parking spaces are provided throughout the Innes Crossing complex which exceeds the Zoning By-law minimum of 383 spaces. The redevelopment of a portion of the surface parking lot will reduce the overall parking level of the complex without compromising the functionality of the site.
- d) **Outdoor Amenity Areas:** The development area is 200 m from the nearest residential property, with several commercial land uses in this area. No increased negative impacts on the amenity areas of these properties can be expected.
- e) **Loading Areas, Service Areas, and Outdoor Storage:** The outdoor garbage area is screened from view from Innes Road by a 2.4 m fence and two Colorado Spruce trees.

Policy 4 states that buildings will be used to clearly define public spaces, such as Innes Road. The streetscape of Innes Road is discontinuous, populated with isolated buildings, and includes deep setbacks. While the small scale of the proposed building will not significantly alter the Innes Road streetscape, it does represent an improvement by occupying an otherwise vacant area caused by surface parking.

Road Classification and Rights-of-Way (Annex 1)

Innes Road is to be protected for a 44.5 m Right-of-Way between St. Laurent Boulevard and Blair Road. This road widening has been included in the proposed site plan.

Therefore, the proposal conforms to the City of Ottawa Official Plan.

4.3 Zoning By-law No. 2008-250

Part 4 – Parking, Queueing, and Loading Provisions

Table 1 shows the parking requirements of the site per Section 101 of the Zoning By-law, with the Shopping Centre (N83) use applied to calculate the parking requirements. The 23 parking spaces required by the proposed retail building can be accommodated

given the existing oversupply of parking on the Subject Property, with 425 spaces proposed in the completed development.

Table 1 - Parking Requirements

Row – Land Use	Gross Leasable Floor Area (m²)	Parking Rate	Spaces
Retail Buildings A-E	9,988	3.6 per 100 m² gross leasable floor area	360
Retail Building F (New)	625		23
REQUIRED			383
PROVIDED			425

Note: Parking rates use N83 – Shopping Centre, Area C per Schedule 1A

Landscaping, as described above in Section 3.0, is provided to screen the parking lot of the proposed development. The landscaped buffer exceeds the minimum by-law requirements and includes a mix of shrubbery to improve the aesthetics of the parking lot. The outdoor garbage area, located to the west of the proposed building, is screened from view by a 2.4 m high fence and two trees.

Table 111A establishes the minimum required bicycle parking space rates. The proposed retail store, with a GFA of 625 m², requires 3 bicycle parking spaces.

Per Table 113A, no loading spaces are required for the proposed retail store. One loading space is shown on the southern façade of the building, sufficiently dimensioned to permit access for the types of delivery vehicles anticipated to serve the building.

Part 10 – Mixed Use / Commercial Zones

The Subject Property is zoned General Mixed Use, Subzone 12 (GM12). A retail store is a permitted use in this zone. The GM zoning provisions are shown below in **Table 2**. The additional permitted uses of the GM12 subzone are not relevant to the subject application.

Table 2 - GM Zoning Provisions

Zoning Mechanisms	Provisions	Proposed
(a) Minimum lot area	No minimum	4.04 ha
(b) Minimum lot width	No minimum	163 m
(c) Minimum front yard setback	3 m	10.1
(d iii) Minimum interior side yard setback	No minimum	Not applicable
(e iv) Minimum rear yard setback	No minimum	Not applicable
(f) Maximum building height	18 m	??
(g) Maximum floor space index	2	0.3
(h i) Minimum width of landscaped area	3 m	??

Therefore, the proposed development conforms to all relevant provisions of Zoning By-law 2008-250.

5.0 Technical Studies and Plans

Along with this Planning Rationale, the following technical studies and plans have been prepared in support of this Site Plan Control application.

1. Site Servicing Plan
2. Grade Control and Drainage Plan
3. Erosion and Sediment Control Plan
4. Stormwater Management Brief
5. Site Servicing Brief
6. Geotechnical Study (updated)
7. Site Plan
8. Landscape Plan
9. Survey Plan
10. Architectural Elevation Drawings
11. Phase I Environmental Site Assessment (updated)

6.0 Summary and Conclusions

1. The application for Site Plan Control to allow for the proposed retail use is consistent with the Provincial Policy Statement;
2. The application is consistent with the General Urban Area and urban design policies of the Official Plan;
3. The application conforms to the Zoning By-law; and,
4. The retail use will strengthen the vitality and viability of the Innes Crossing commercial complex, and will be beneficial for the surrounding community.

Based on the above noted rationale, this application for Site Plan Control is appropriate and represents sound and defensible land use planning.

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