



WESTGATE REDEVELOPMENT PHASE 1

PREPARED FOR THE URBAN DESIGN REVIEW PANEL: DECEMBER 6, 2018 (FORMAL REVIEW)

The Westgate Lands & Application History

The Westgate Shopping Centre, known municipally as 1309 Carling Avenue, is located in the Carlington Neighbourhood of the City of Ottawa. The property is irregular in shape and has an area of 368 992m² with 220 metres of frontage along Carling Avenue and 213 metres of frontage along Merivale Road. The property is currently occupied by the Shopping Centre and surface parking. Phase 1 of the Westgate redevelopment will replace the existing restaurant pad at the corner of Carling Avenue and Merivale Road with a mixed-use 24-storey building with below grade parking.

In 2015, an Official Plan Amendment and Zoning By-law Amendment application was submitted by Fotenn Consultants on behalf of RioCan Management for the entire Westgate site. The applications proposed the adoption of the Westgate Secondary Plan, with site-specific policies and zoning to enable the redevelopment of the lands based on a phased development that would enable greater building heights and densities. The Westgate Secondary Plan and Zoning By-law Amendment were approved by Council and have been in-effect since May 2017.

The recently submitted Site Plan Control application proposes minor changes to the site layout and building elevations from the original concept plan. While the Phase 1 Site Plan reflects a change in tower orientation, it maintains conformity with the Secondary Plan policies and zoning compliance.

The Surrounding Area

Located to the north of Phase 1 lands is the existing of the Westgate Shopping Centre. Further north of the Westgate Shopping Centre is Highway 417, Hampton Park and NCC-managed open space and the community of Hampton Park, and Westboro.

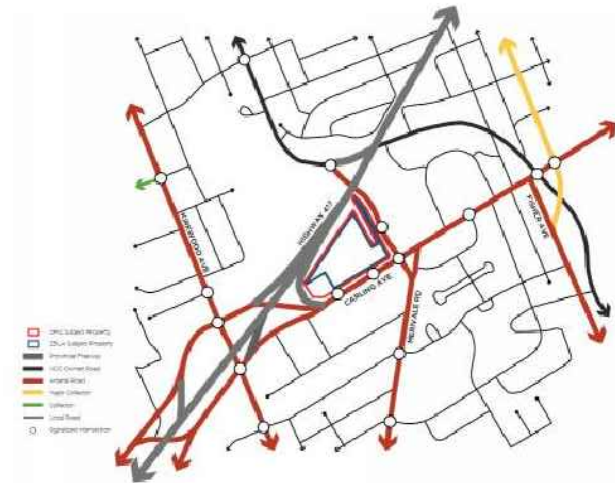
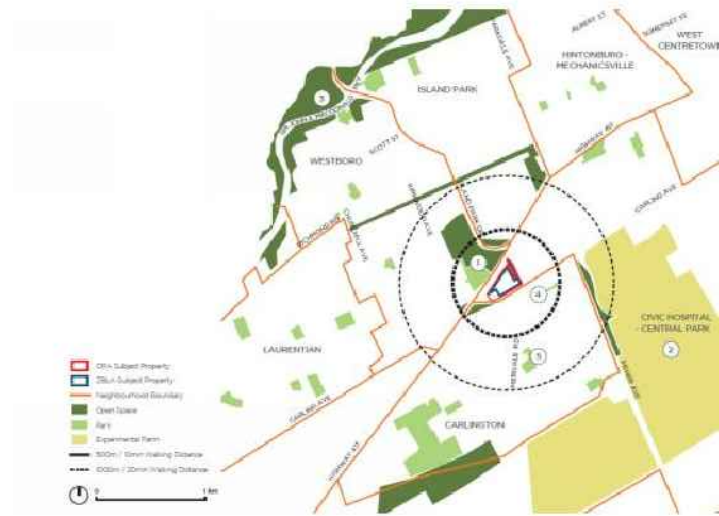
Directly east of the Phase 1 lands is a Hydro utility plant. Further to the east is the Ottawa Civic Hospital campus and the Experimental Farm, as well as low-rise residential areas, including the Civic Hospital neighbourhood.

On the south side of Carling Avenue are mid- to high- rise buildings with associated surface parking currently used for residential and hotel uses. The City of Ottawa has received several development applications for lands to the south of Carling Avenue.

Directly west of the Phase 1 lands is surface parking associated with the Westgate Shopping Centre (future POPS), and 1335 Carling Avenue, a 6-storey office building.

SITE PHOTOS





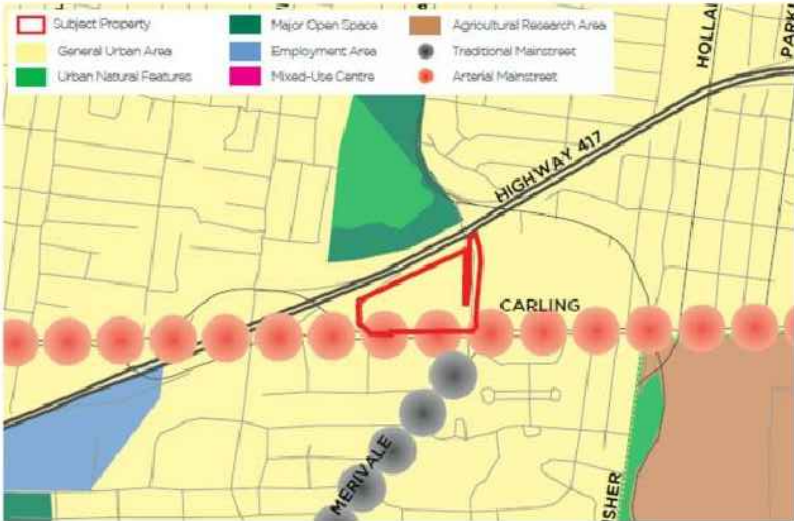
Proposed Development

RioCan Management has submitted a Site Plan Control application to permit the development of a 24-storey mixed-use building on the subject property with ground floor retail/commercial uses and 216 dwelling units above. The mixed-use building, including a 4-storey podium, will contribute to an active and animated frontage with direct access to both Merivale Road and Carling Avenue; helping to achieve the policy goals of the Official Plan and Westgate Secondary Plan. Vehicular access to the building will be provided via existing entrances from both Carling Avenue and Merivale Road, whereas access/egress to the parking garage will be located along the north facing elevation, away from the Carling Avenue frontage.

Since approval of the Westgate Secondary Plan, RioCan has proposed minor changes to the site design and building layout. The tower orientation has been modified from the original concept design in order to minimize views on the existing hydro utility plant to the east. Additionally, above-grade parking has been reconsidered and moved to two underground parking levels.

Policy & Regulatory Framework

The City of Ottawa Official Plan



The subject property is designated as Arterial Mainstreet on Schedule B – Urban Policy Plan of the City’s Official Plan. The Official Plan identifies the Mainstreet designation as streets that offer some of the most significant opportunities in the City for compact and pedestrian oriented forms of intensification. As a result, a broad range of uses are permitted in the Arterial Mainstreet designation.

In general, higher densities and a mix of uses can be introduced where appropriate. For example, surface parking lots offer opportunities for redevelopment, and will help to improve the pedestrian environment. The Mainstreet designation permits greater building heights through a Secondary Planning Process.

The Official Plan identifies several criteria for urban design and compatibility in Sections 2.5.1 and 4.11 of the Plan. Phase 1 of the Westgate redevelopment has been designed to the be consistent with and address the criteria identified by the Official Plan.

Overall, the proposed development conforms to the policies of the City of Ottawa Official Plan. It achieves the goals of the Arterial Mainstreet designation, by redeveloping an underutilized surface parking lot to create a well-defined street edge and pedestrian environment through the transit-oriented development.

The Westgate Secondary Plan



The Westgate Secondary Plan established guiding principles to inform the redevelopment of the Westgate Shopping Centre. These principles are meant to: enhance the public realm; create active and transit-supportive uses; support the various roles of Carling Avenue; ensure high-quality urban design; and, create an active frontage along Carling Avenue. Additional design principles noted in the Secondary Plan include:

- / The interior of the site (Phase 1 lands) may contain active frontage retail uses that extend the main street atmosphere into the site without detracting from the Carling Avenue frontage as the dominant frontage that connects the site to the rest of the corridor.
- / Establish a central location for the Westgate POPS and public uses that respond to the needs and services the community envisioned within the area.
- / Ensure direct, convenient, safe, attractive walking and cycling connections within the Westgate Lands, to the surrounding community and future transit station.
- / Establish a range of land uses, with the tallest buildings located along Highway 417, with high-rise buildings (15-24 storeys) along Carling Avenue and in proximity to the future rapid transit station

The Phase 1 lands have been identified to have a maximum of 24 storeys, ensuring that future development in this node will transition to the low-profile residential area located south of Carling Avenue. Overall, the proposal for Phase 1 meets the building heights identified in the Secondary Plan.

The building and tower location are in conformity with the direction identified in the Westgate Secondary Plan and will contribute positively to the Carling Avenue frontage. Parking will be located underground and will have minimal visual impacts to the surrounding area.

Phase 1 of the Westgate redevelopment will enhance the public realm by introducing direct, safe and attractive access for pedestrians to the retail / commercial uses located in the building podium. The four (4) storey mixed-use building podium will create a strong street edge all the while contributing the human-scale along Carling Avenue and Merivale Road.

Future phases of the Westgate redevelopment will facilitate the development of the Privately-Owned Public Space (POPS) and pedestrian realm plan identified during the Secondary Planning Process.

City of Ottawa Urban Design Guidelines

The proposal meets the intent and design directions of the City’s Urban Design Guidelines along Arterial Mainstreets. The following guidelines are of relevance to the Phase 1 proposal:

- / Locate new buildings along the public street edge and provide a minimum 2m wide pedestrian sidewalk;

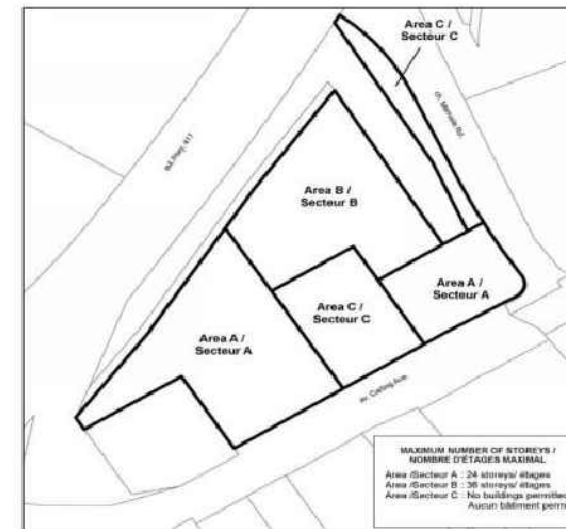
- / Utilize buildings and landscaping to create a continuous streetscape;
- / Landscape the area in front of a building wall and using projections, awning, colours and textures to reduce the visual impact of any unglazed walls;
- / Connect pedestrian walkways between properties to facilitate pedestrian circulation between sites;
- / Provide direct, safe, continuous and clearly defined pedestrian access from public sidewalks to building entrances;
- / Share vehicular access to parking area with the adjacent property to reduce the extent of interruption along the sidewalk and the streetscape;
- / Locate surface parking areas at the side or rear of buildings;
- / Orient parking spaces to minimize the number of traffic aisles that pedestrians must cross;
- / Provide a minimum three (3) metre wide landscape area along the edge of the property where parking areas are adjacent to the public street and using trees and shrubs to screen cars from view while allowing eye level visibility into the site;
- / Design buildings to accommodate signs that respect building scale and architectural features;
- / Screen utility equipment, loading areas, and garbage collection areas from view of the Arterial Mainstreet and the adjacent properties;
- / Design lighting so that there is no glare or light spilling onto surrounding uses; and,
- / Providing lighting that is appropriate to the street character and mainstreet ground floor use with a focus on pedestrian areas.

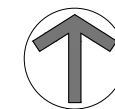
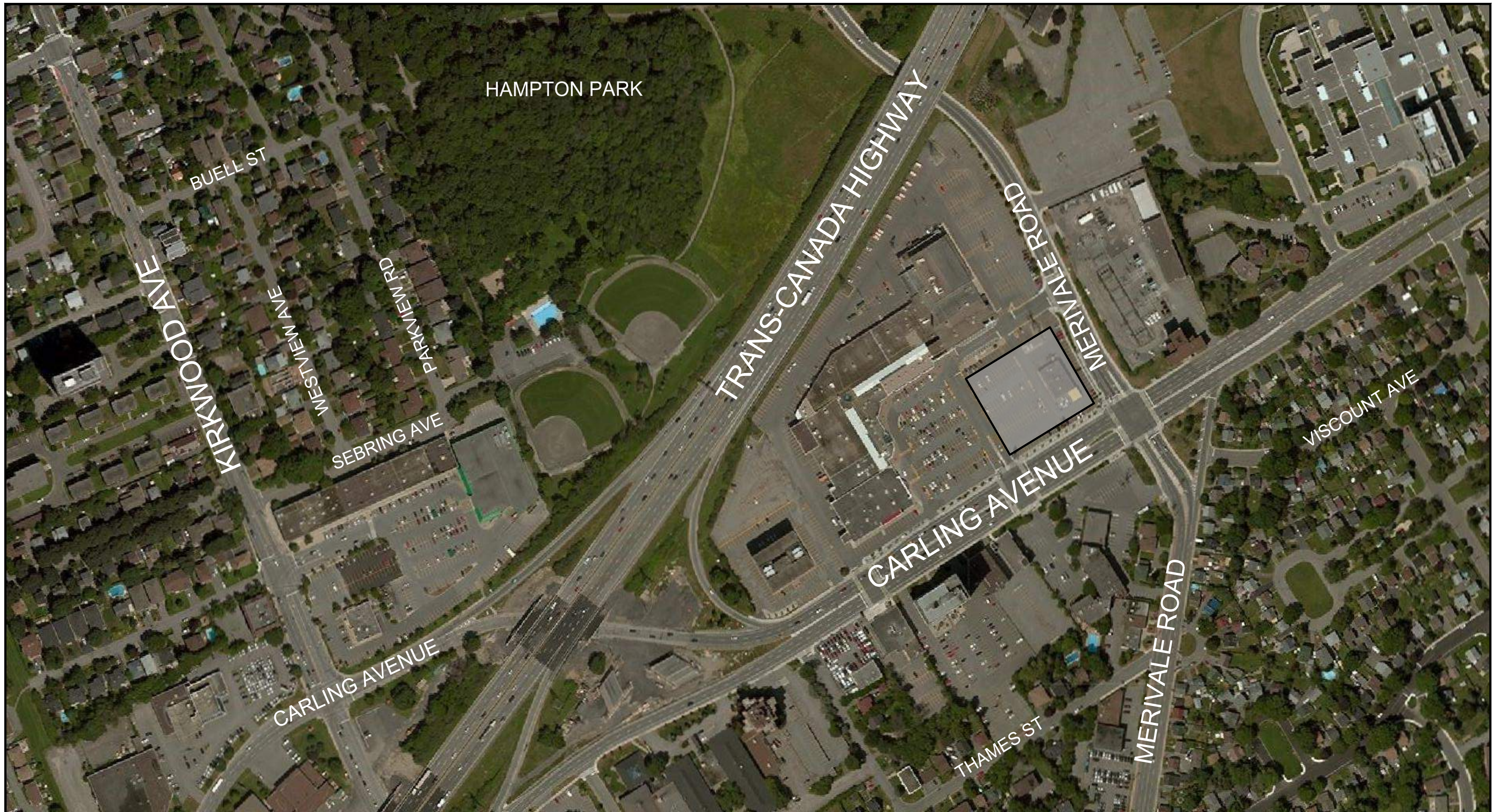
The proposal meets the intent and design directions of the City’s Urban Design Guidelines for High-rise Buildings. The following guidelines are of relevance to the Phase 1 proposal:

- / When a high-rise building or group of high-rise buildings are proposed within an identified growth area, design the buildings nearer the edge of the growth area to be progressively lower in height than those in the “centre”;
- / Include base buildings that relate directly to the height and typology of the existing or planned streetwall context;
- / The lot should abut the public realm, including streets, parks, plazas, and privately-owned public spaces (POPS) on at least two sides;
- / Enhance and create the overall pedestrian experience in the immediate surrounding public spaces (including POPS) through the design of the lower portion, typically the base;
- / Enhance and create the image of a community and a city through the design of the upper portion of the building, which is often comprised of a middle and a top that: protects and/or creates views and landmarks; and respects and/or enriches urban fabric and skylines;
- / Place the base of a high-rise building to form continuous building edges along streets, parks, and public spaces or Privately Owned Public Space (POPS): where there is an existing context of street wall buildings, align the facades of the base with

- / The minimum height of the base should be 2 storeys;
- / Respect the character and vertical rhythm of the adjacent properties and create a comfortable pedestrian scale by: breaking up a long façade vertically through massing and architectural articulation to fit into the existing finer grain built form context and introducing multiple entrances, where possible; through creative store layout and organization where a large format retail use is located on the ground floor;
- / Use high-quality, durable, and environmentally sustainable materials, an appropriate variety in texture, and carefully crafted details to achieve visual interest and longevity for the façade;
- / The ground floor of the base should be animated and highly transparent;
- / Encourage small tower floor plates to minimize shadow and wind impacts, loss of skyviews, and allow for the passage of natural light into interior spaces;
- / Step back the tower, including the balconies, from the base to allow the base to be the primary defining element for the site and the adjacent public realm;
- / Orient and shape the tower to minimize shadow and wind impacts on the public and private spaces;
- / Articulate the tower with high-quality, sustainable materials and finishes to promote design excellence, innovation, and building longevity;
- / For a background building, create a fenestration pattern, and apply colour and texture on the facades that are consistent with and complement the surrounding context;
- / Integrate roof-top mechanical or telecommunications equipment, signage, and amenity spaces into the design and massing of the upper floors;
- / The public spaces should: complement and be integrated into the existing network of public streets, pathways, parks, and open space; provide direct visual and physical connections to the surrounding public streets, pathways, parks, and open spaces;
- / Locate the main pedestrian entrance at the street with a seamless connection to the sidewalk
- / Locate parking underground or at the rear of the building;
- / Internalize and integrate servicing, loading, and other required utilities into the design of the base of the building, where possible; and,
- / Coordinate, and where possible integrate, public transit stop elements such as benches and shelters within the site and building design.

The proposed development is a permitted use within AM10[2392] S283 zone and complies with the applicable provisions of the Zoning By-law. The proposed 24 storey building respects the maximum building height and complies with the required yard setbacks, parking rates, glazing requirements, and landscape and amenity areas of the Arterial Mainstreet Zone.





SUBJECT PROPERTY

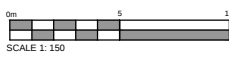
the king's highway No. 417 (ottawa queensway)
(designated as a controlled-access highway by order in council oc2271/88 inst. no. n464845)

the king's highway No. 417 (ottawa)
(designated as a controlled-access highway by order in council oc2271/88)

LOT 33 CONCESSION 1 (OTTAWA FRONT)

PART PLAN SR-1714
PIN 04025-0173
1 story commercial building
No. 1220 Carling Avenue
(pin and level at grade noted)

1 SITE PLAN (OVERALL)
SP-1 SCALE = 1: 400





GROSS BUILDING - AREAS			
(CITY OF OTTAWA'S DEFINITION)			
PARKING LEVELS (4 LEVELS U/G)		0.0 sq. m.	
		(000) sq. ft.	
GROUND FLOOR		1,969.5 sq. m.	
		(21,200) sq. ft.	
MEZZANINE LEVEL		0.0 sq. m.	
		(000) sq. ft.	
2nd to 4th FLOOR		3 x 1,225.0 sq. m.	3,969.6 sq. m.
		3 x (13,186) sq. ft.	(42,729) sq. ft.
5th FLOOR			375.0 sq. m.
			(4,037) sq. ft.
6th to 20th FLOOR		15 x 522.6 sq. m.	7,838.7 sq. m.
		15 x (5,625) sq. ft.	(84,375) sq. ft.
21st & 22nd FLOOR		2 x 417.3 sq. m.	834.6 sq. m.
		2 x (4,492) sq. ft.	(8,984) sq. ft.
23rd & 24th FLOOR		2 x 369.3 sq. m.	738.6 sq. m.
		2 x (3,975) sq. ft.	(7,950) sq. ft.
MECHANICAL FLOOR		0.0 sq. m.	
		(000) sq. ft.	
TOTAL AREA		11,664.4 sq. m.	
		(125,555) sq. ft.	
UNIT STATISTICS			
STUDIO UNIT		6	
1 BEDROOM UNIT		111	
1 BEDROOM + UNIT		12	
2 BEDROOM UNIT		86	
2 BEDROOM + UNIT		-	
TOTAL		215	
COMMERCIAL RETAIL		1,969.5 sq. m.	
		(21,200) sq. ft.	
CAR PARKING TOTAL SITE - PHASE 1			
REQUIRED by ZONING BY-LAW			
MALL - RETAIL	11,073 sq. m.	- 1.7 PER 100m ² OF G.F.A.	188
	119,184 sq. ft.		
MALL - OFFICE	3,810 sq. m.	- 1.0 PER 100m ² OF G.F.A.	38
	41,015 sq. ft.		
TOWER - RESIDENTIAL	216 UNITS	- 0.5 PER UNIT AFTER 12	102
TOWER - RESIDENTIAL VISITOR	216 UNITS	- 0.1 PER UNIT AFTER 12	20
TOWER - RETAIL	1,765 sq. m.	- 1.25 PER 100m ² OF G.F.A.	22
	19,000 sq. ft.		
TOWER - CAFÉ	295 sq. m.	- NOT REQUIRED UNDER 350m ²	0
	3,175 sq. ft.		
TOTAL			370
PROVIDED			
TOTAL ON SITE - EXISTING			295
TOTAL UNDERGROUND PARKING			192
TOTAL			487
TOTAL OFF SITE (HYDRO LAND)			301

LEGEND

PROPERTY LINE
ROAD WIDENING ALLOWANCE
EXTENT OF POP LANDSCAPED AREA



HYDRO TOWER



TWO WAY VEHICLE CIRCULATION



MAIN ENTRANCE



SECONDARY DOOR



TREE



INDICATES EXISTING NON-CONFORMING PARKING SPACE

OVERALL SITE STATISTICS

COMMERCIAL:	RESIDENTIAL:	OVERALL SITE TOTALS:
LEASABLE AREA: 8,800 M ²	UNITS: 1210	REQUIRED PARKING: 993 SPACES
REQUIRED PARKING: 304 SPACES	REQUIRED R. PARKING: 605 SPACES	PROVIDED PARKING: 979 SPACES
PROVIDED PARKING: 163 SPACES	PROVIDED R. PARKING: 620 SPACES	NOTE: EXCLUDES 280 NON-CONFORMING SPACES
	REQUIRED V. PARKING: 121 SPACES	
	PROVIDED V. PARKING: 121 SPACES	
TOTAL SITE GFA:		
EXISTING WESTGATE MALL:		
PHASE I, II & III:		
TOTAL:		

1 SITE PLAN (OVERALL)

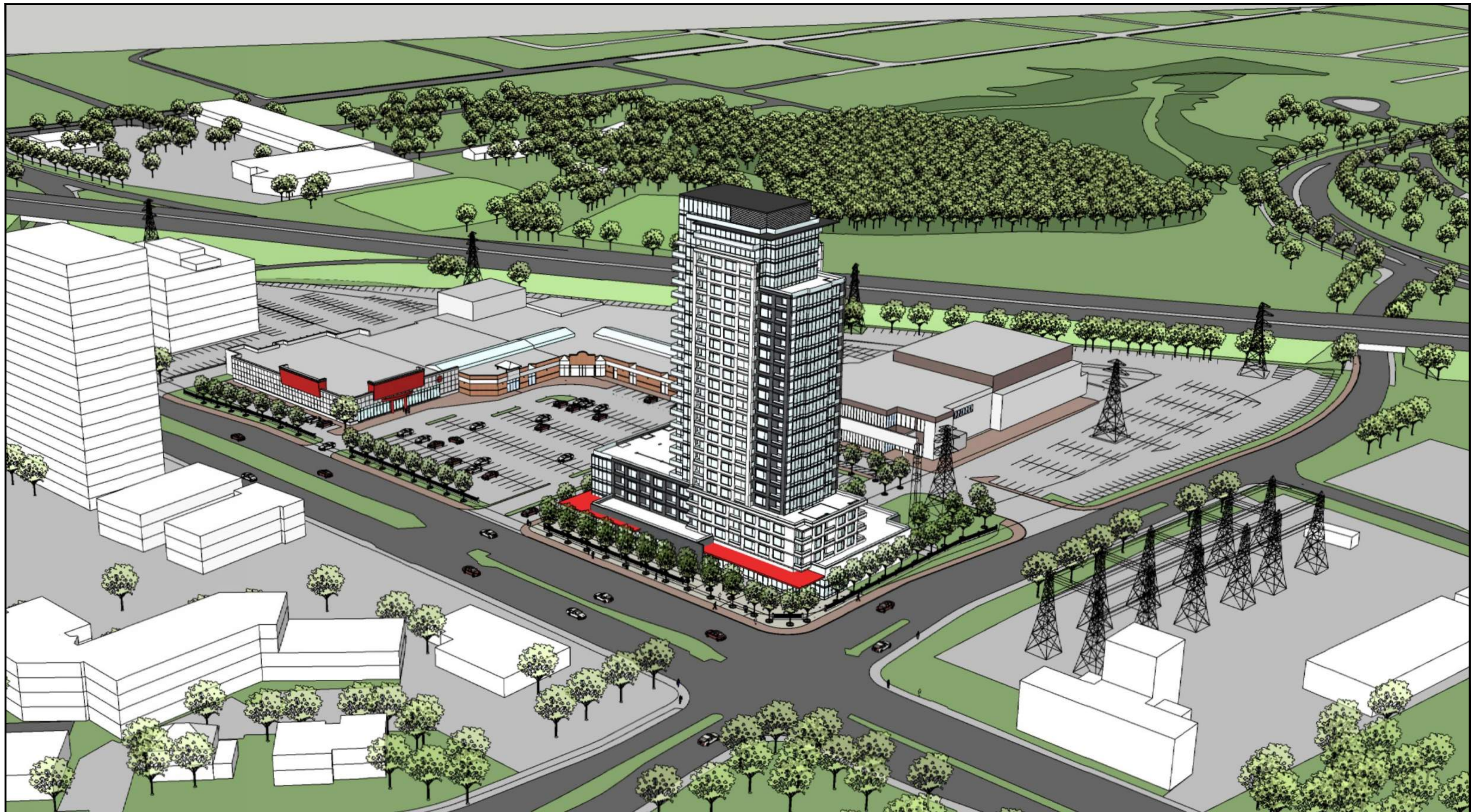
SCALE = 1:400



SCALE 1:150



CURRENT SITE CONDITION



PROPOSED PHASE 1 DEVELOPMENT

rla/architecture
roderick lahey architect inc.

SITE OVERALL

Thursday, November 22, 2018

SCALE:

ISSUED FOR UDRP

WESTGATE CENTRE
OTTAWA ONTARIO

RIO CAN
REAL ESTATE INVESTMENT TRUST

1 1



FUTURE DEVELOPMENTS

rla/architecture
roderick lahey architect inc.

SITE OVERALL

Thursday, November 22, 2018

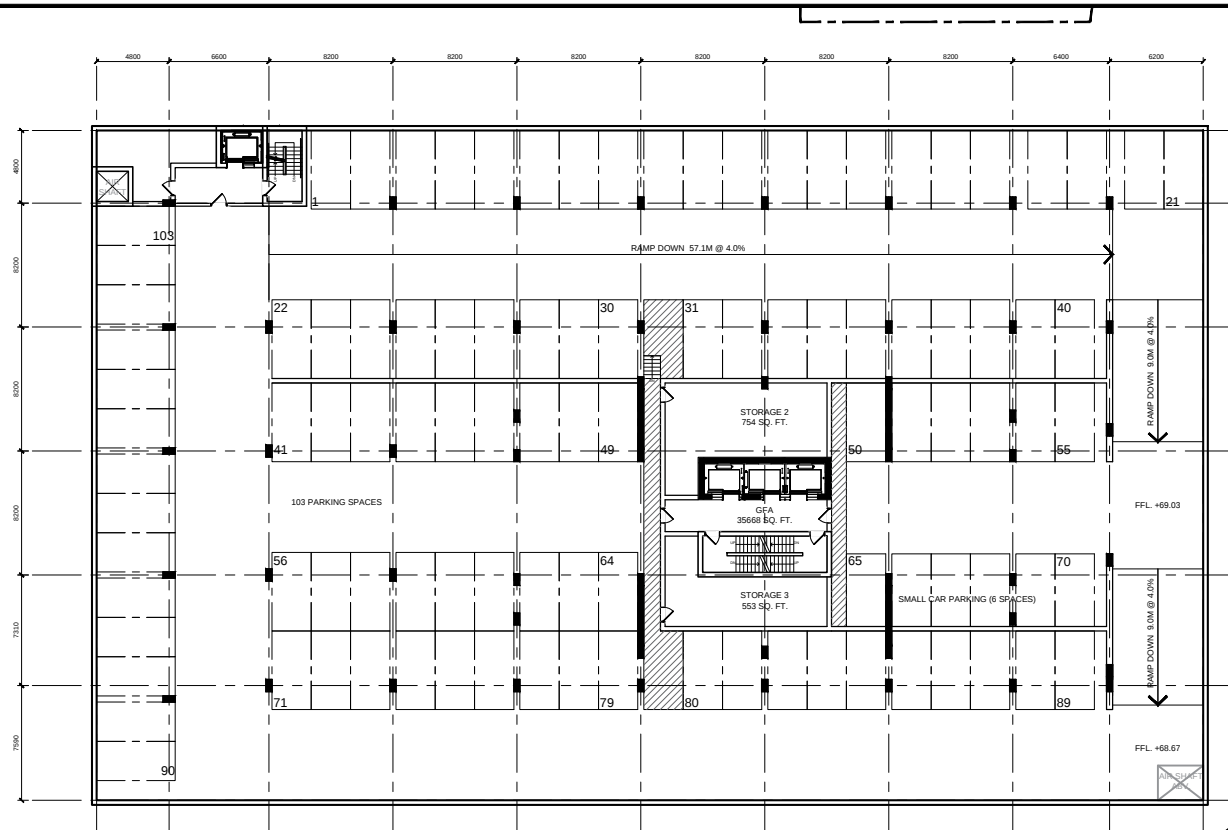
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ISSUED FOR UDRP

WESTGATE CENTRE
OTTAWA ONTARIO

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1 2

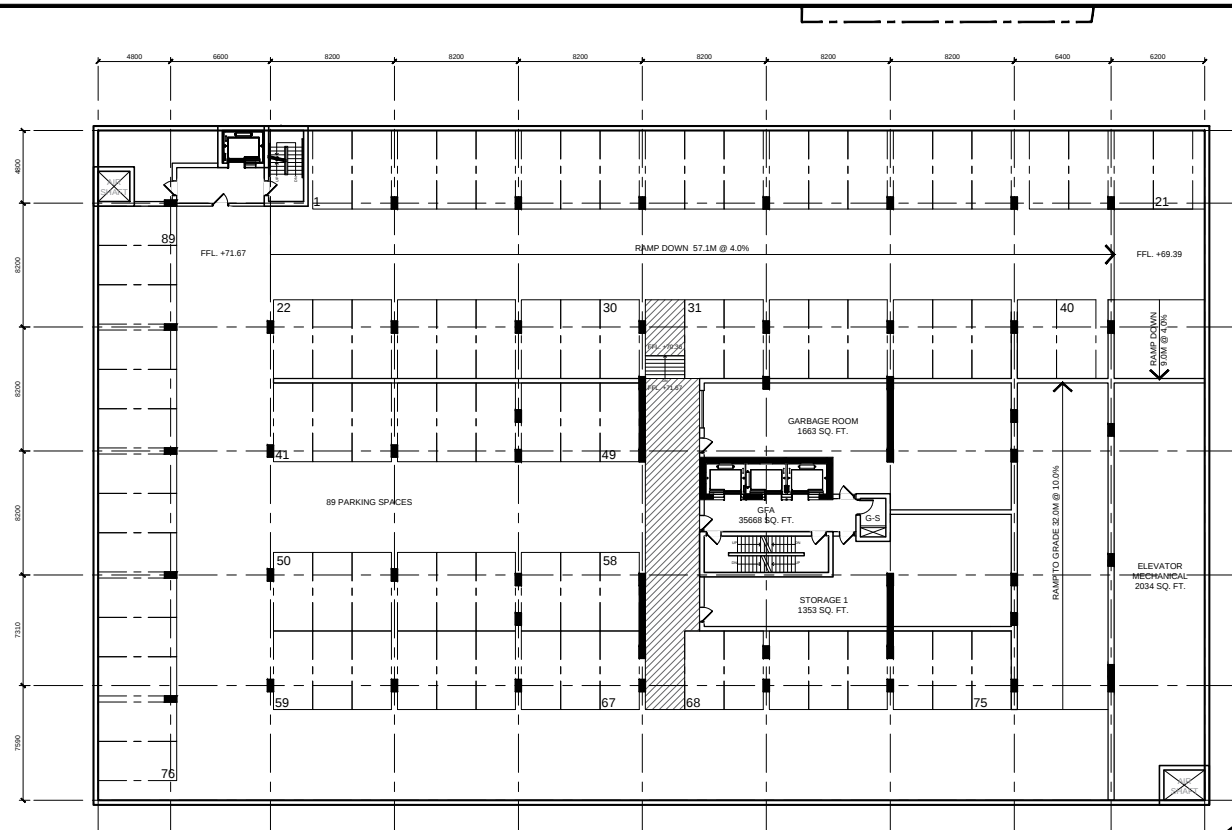
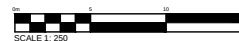


-97 STANDARD
-6 SMALL CAR
=103 PARKING SPACES

BUILDING TOTALS:
- 194 TOTAL STANDARDS (97%)
- 6 TOTAL SMALL CAR (3%)
= 200 TOTAL PARKING SPACES

P2 PARKING LEVEL

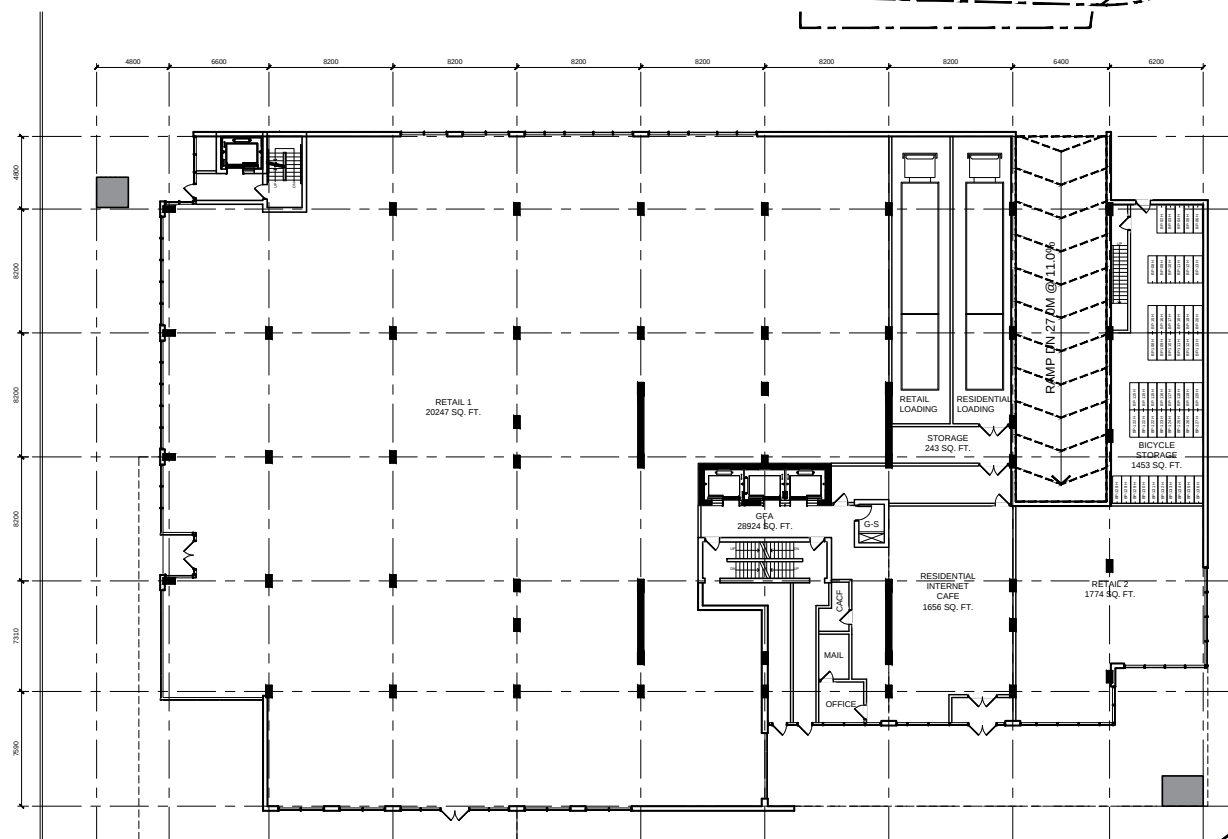
TOTAL AREA: 32,405



-97 STANDARD
-0 SMALL CAR
=97 PARKING SPACES

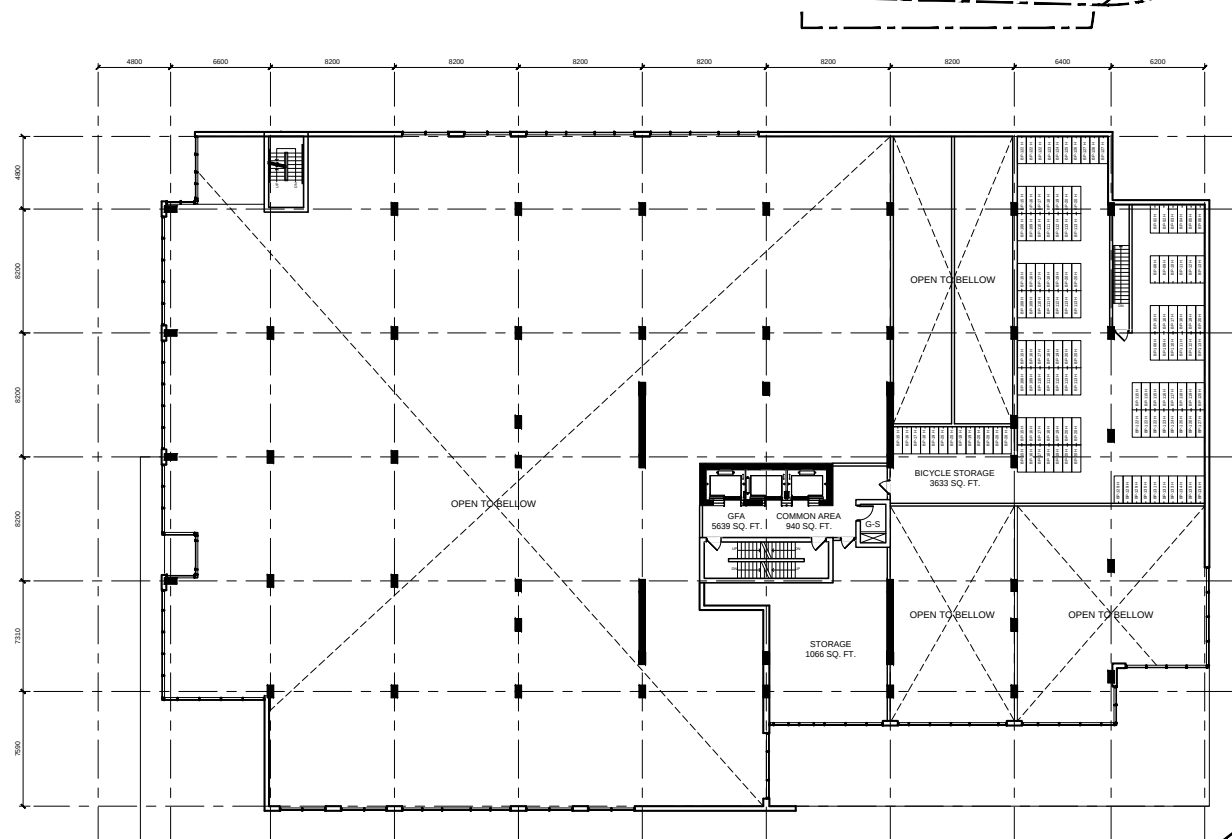
P1 PARKING LEVEL

TOTAL AREA: 32,405



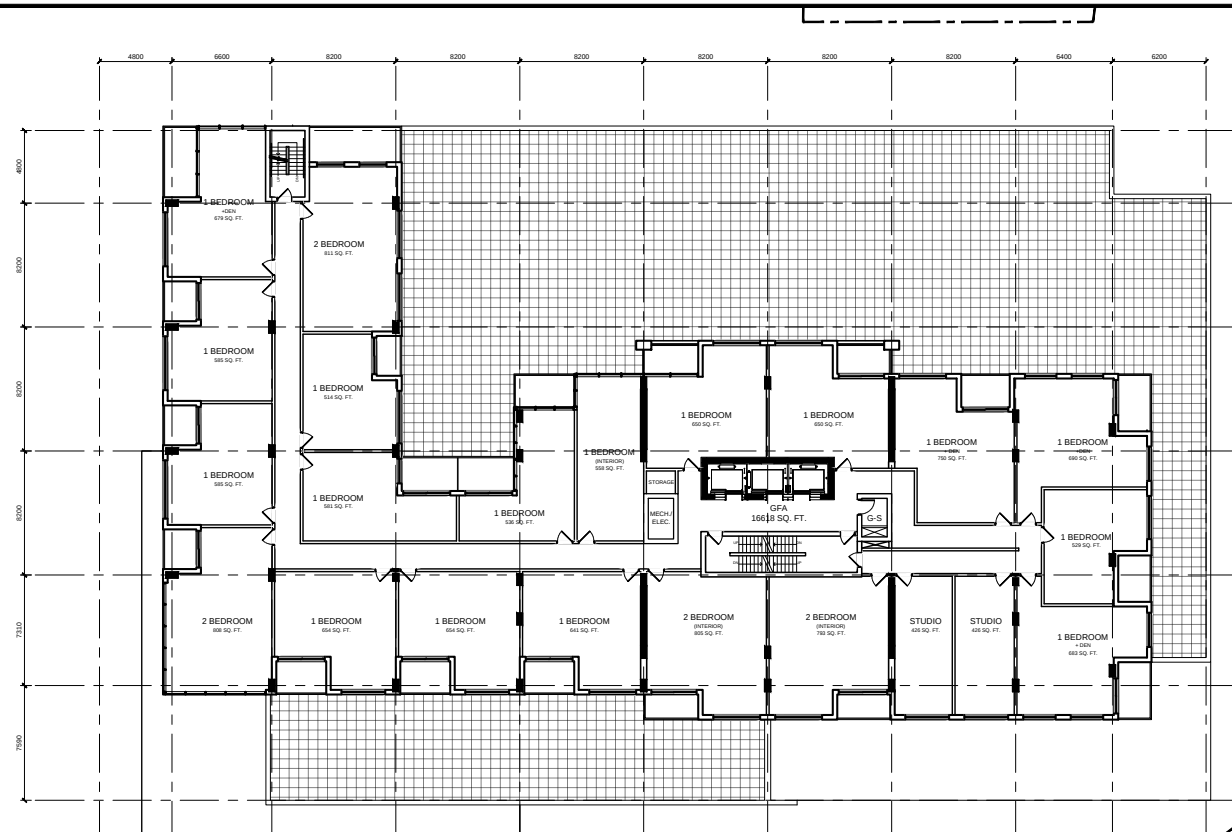
GROUND

TOTAL AREA: 30517.4 SQ.FT



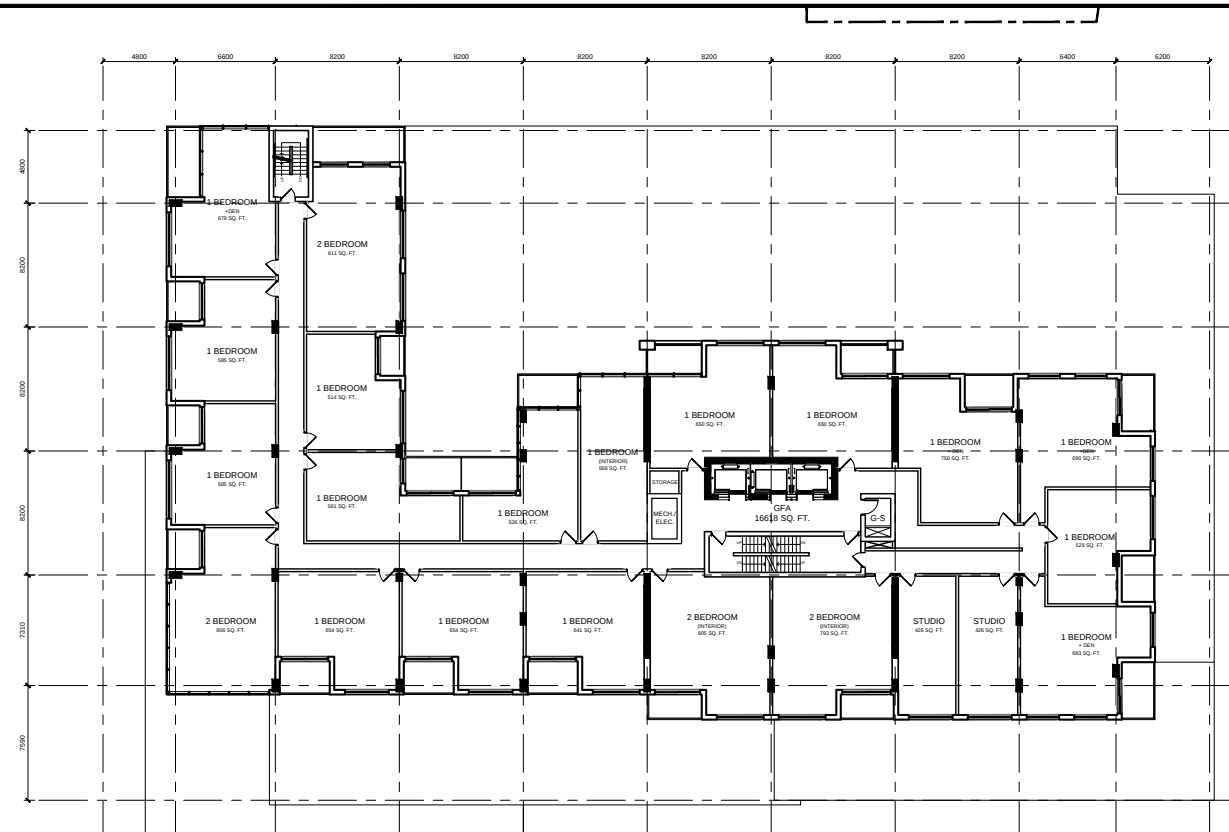
MEZZANINE

TOTAL AREA: 5639.4 SQ.FT



PODIUM FLOOR
L2

TOTAL AREA: 16618 SQ.FT
NFA: 14008 SQ.FT



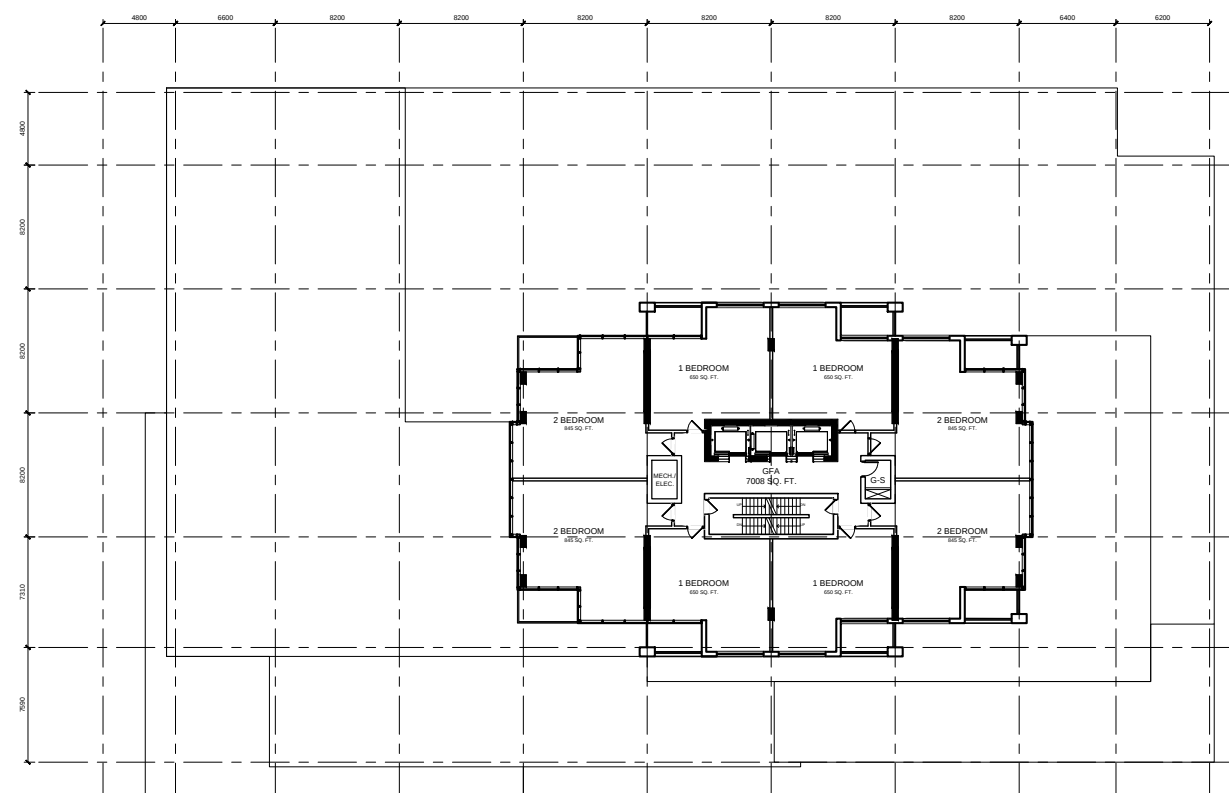
PODIUM FLOOR
L3-4

TOTAL AREA: 16618 SQ.FT
NFA: 14008 SQ.FT



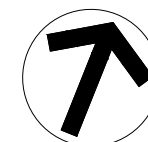
TOWER FLOOR -
5TH

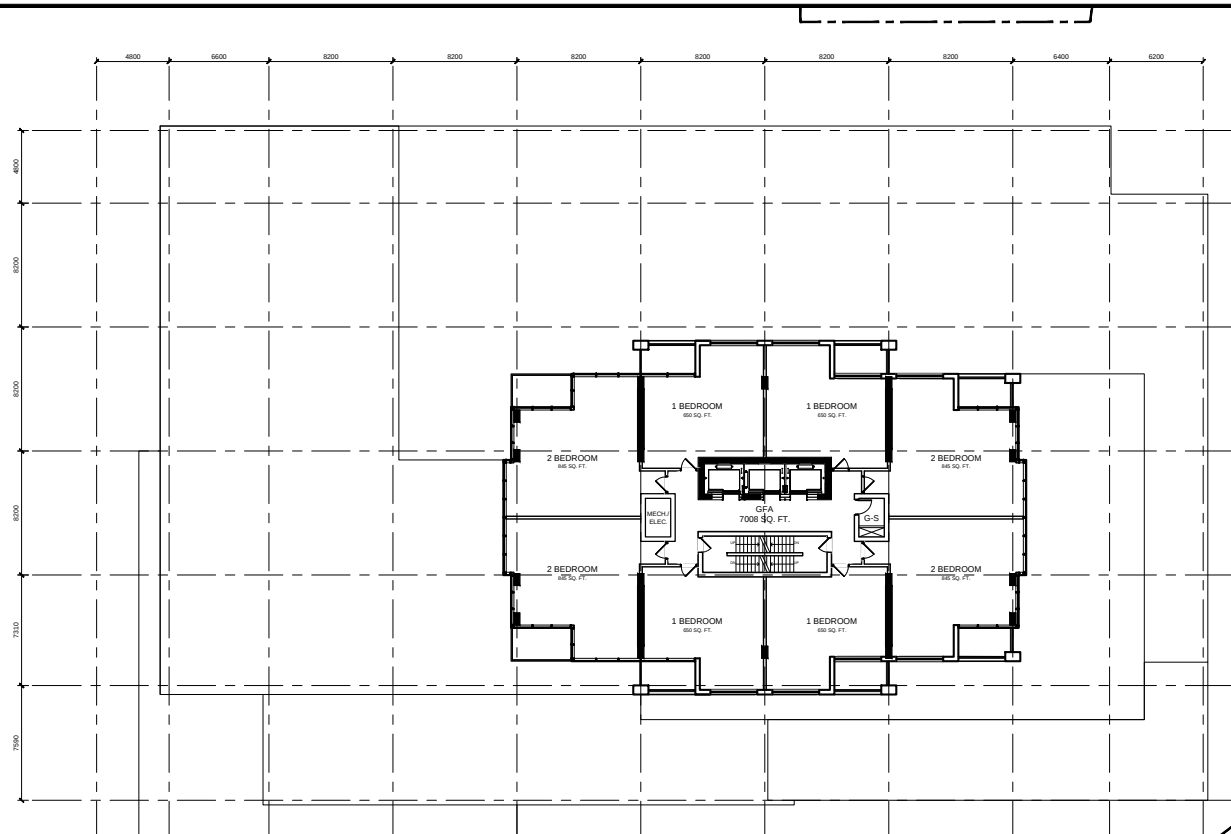
TOTAL AREA: 7008 SQ.FT
NFA: 3655 SQ.FT



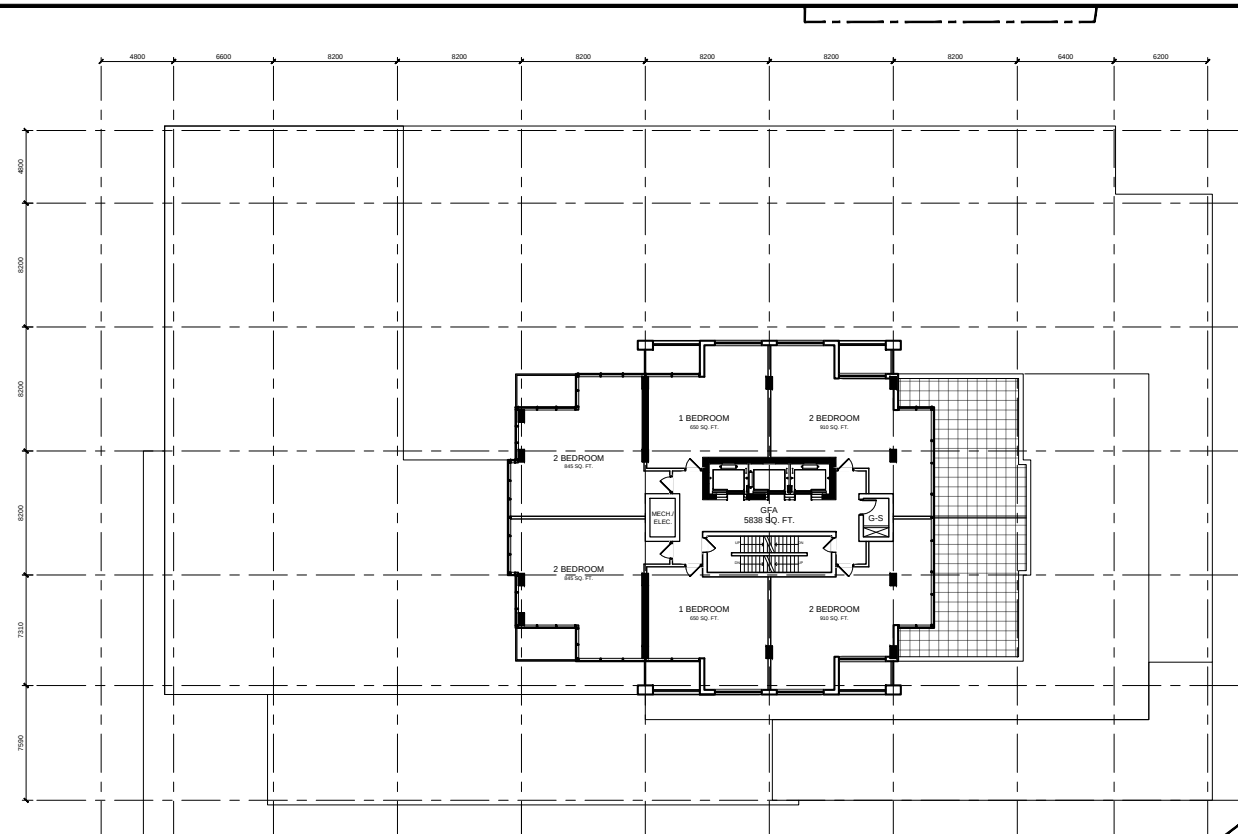
TOWER FLOORS -
6-20

TOTAL AREA: 7008 SQ.FT
NFA: 5980 SQ.FT
EFFICIENCY: 85.33%

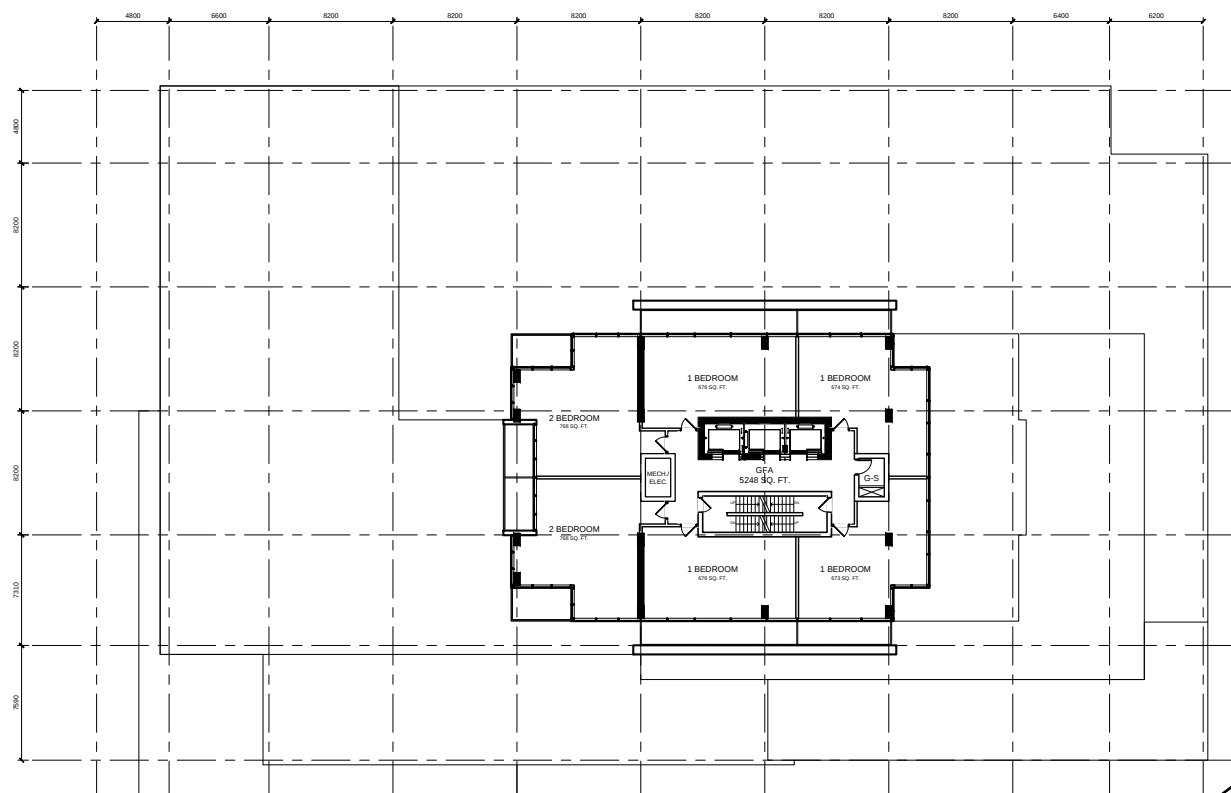
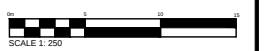




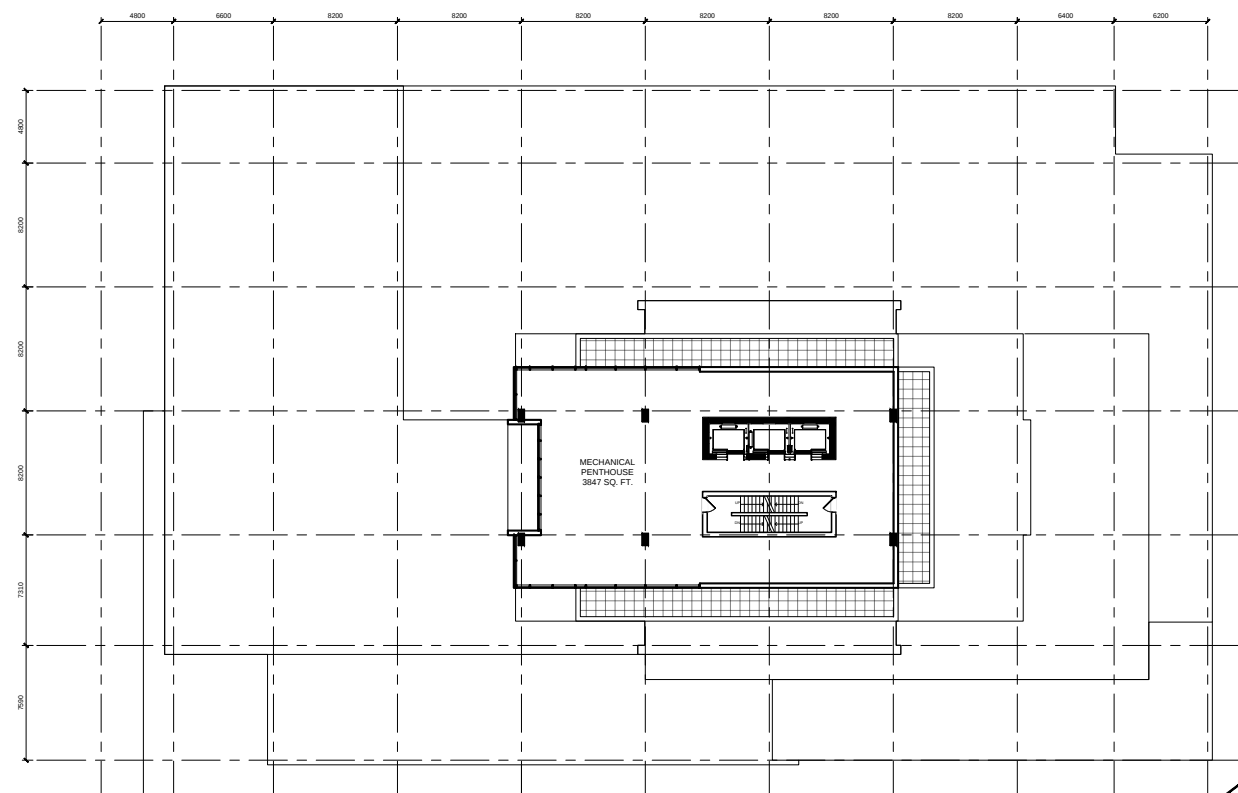
TOWER FLOORS -
6-20
TOTAL AREA: 7008 SQ.FT
NFA: 5980 SQ.FT
EFFICIENCY: 85.33%



TOWER FLOORS -
21-22
TOTAL AREA: 5838 SQ.FT
NFA: 4810 SQ.FT

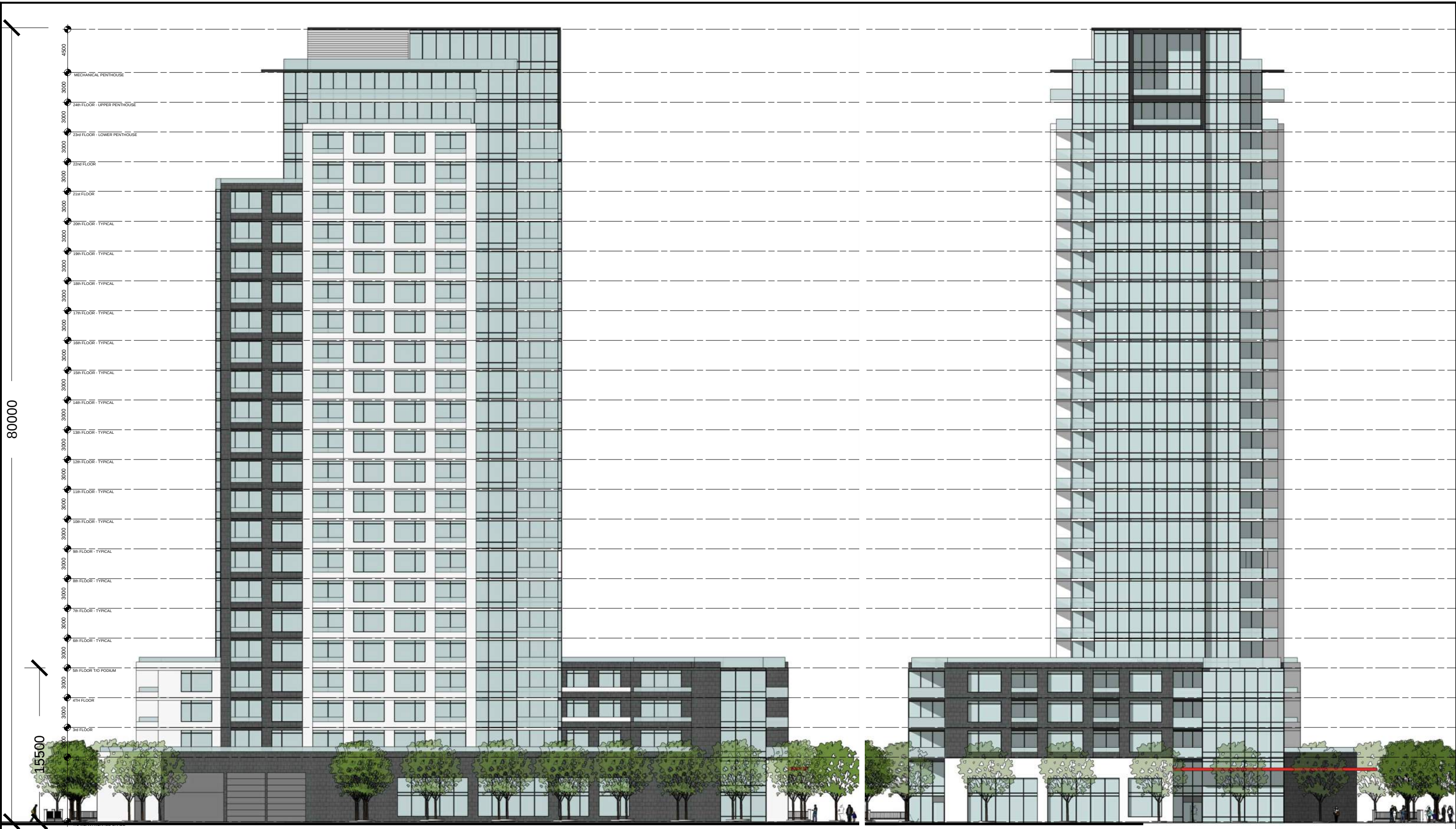


TOWER FLOORS -
23-24
TOTAL AREA: 5248 SQ.FT
NFA: 4234 SQ.FT



MPHOUSE FLOOR
TOTAL AREA: 3847 SQ.FT





NORTH ELEVATION

WEST ELEVATION

rla/architecture
roderick lahey architect inc.

BUILDING ELEVATIONS
Thursday, November 22, 2018

SCALE: 1:500
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SOUTH ELEVATION

EAST ELEVATION

rla/architecture
roderick lahey architect inc.

BUILDING ELEVATIONS

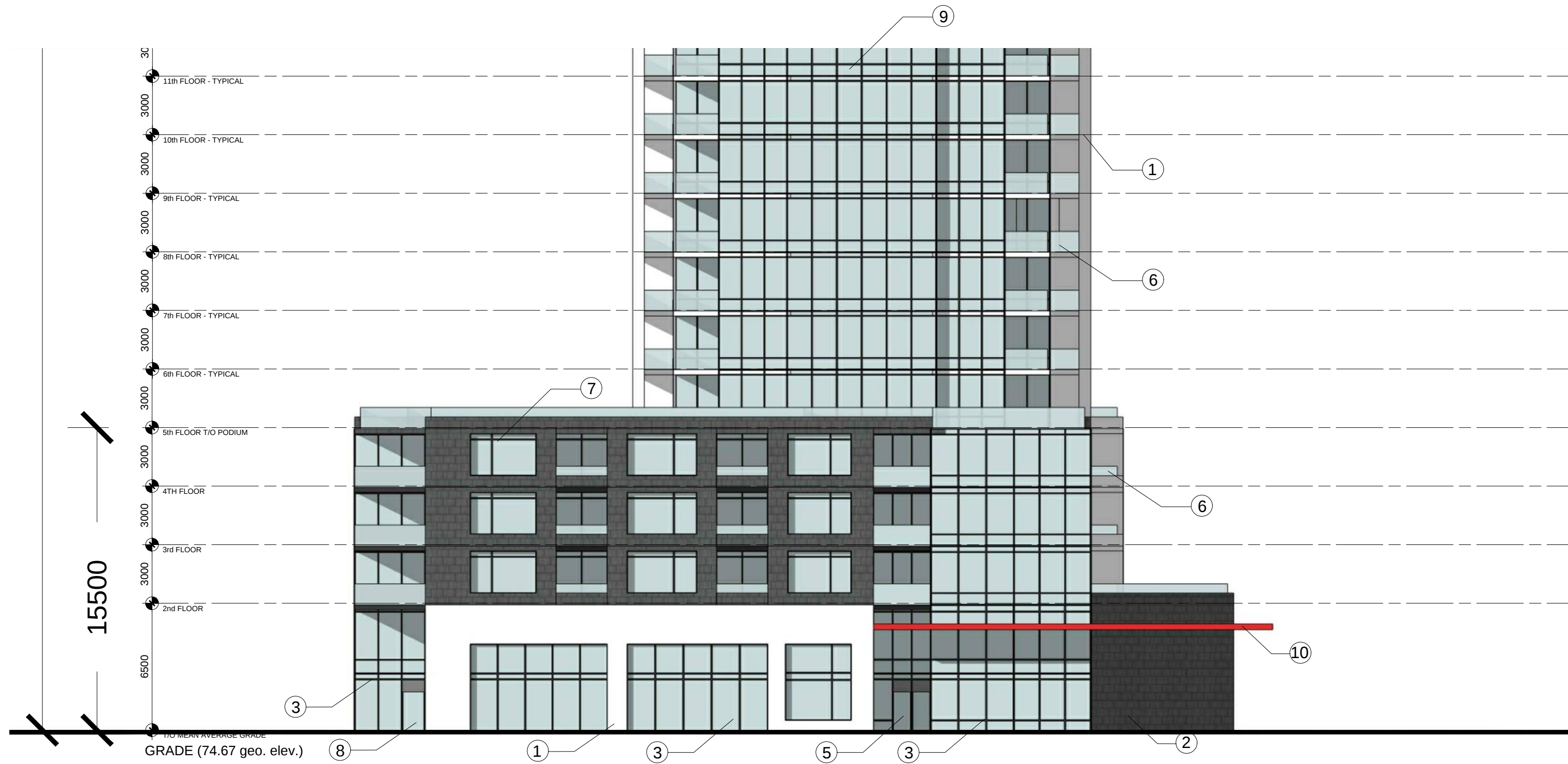
Thursday, November 22, 2018

SCALE:
ISSUED FOR UDRP

WESTGATE CENTRE
OTTAWA ONTARIO

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REAL ESTATE INVESTMENT TRUST





- 1: PERMACON CARRERA WHITE BUFFED STONE

2: PERMACON ROCKLAND BLACK MELVILLE NORMAN BRICK

3: COMMERCIAL CURTAIN WALL, CLEAR ANODIZED

4: RESIDENTIAL ENTRANCE

5: COMMERCIAL ENTRANCE

6: GLASS BALCONY GUARD

7: CLEAR ANODIZED WINDOW FRAME
- 8: COMMERCIAL BACK ENTRANCE

9: WINDOW WALL SYSTEM, CLEAR ANODIZED

10: CANOPY
- WEST ELEVATION



LOOKING NORTH ON CARLING AVENUE (PHASE 1)



LOOKING NORTH ON CARLING AVENUE (FUTURE)



VIEW LOOKING NORTHWEST ON MERIVALE RD (PHASE 1)



VIEW LOOKING NORTHWEST ON MERIVALE RD (FUTURE)



VIEW LOOKING SOUTH ON MERIVALE RD (PHASE 1)



VIEW LOOKING SOUTH ON MERIVALE RD (FUTURE)



VIEW LOOKING WEST ON CARLING AVE (PHASE 1)



VIEW LOOKING WEST ON CARLING AVE (FUTURE)



VIEW LOOKING EAST ON CARLING AVE (PHASE 1)



VIEW LOOKING EAST ON CARLING AVE (FUTURE)



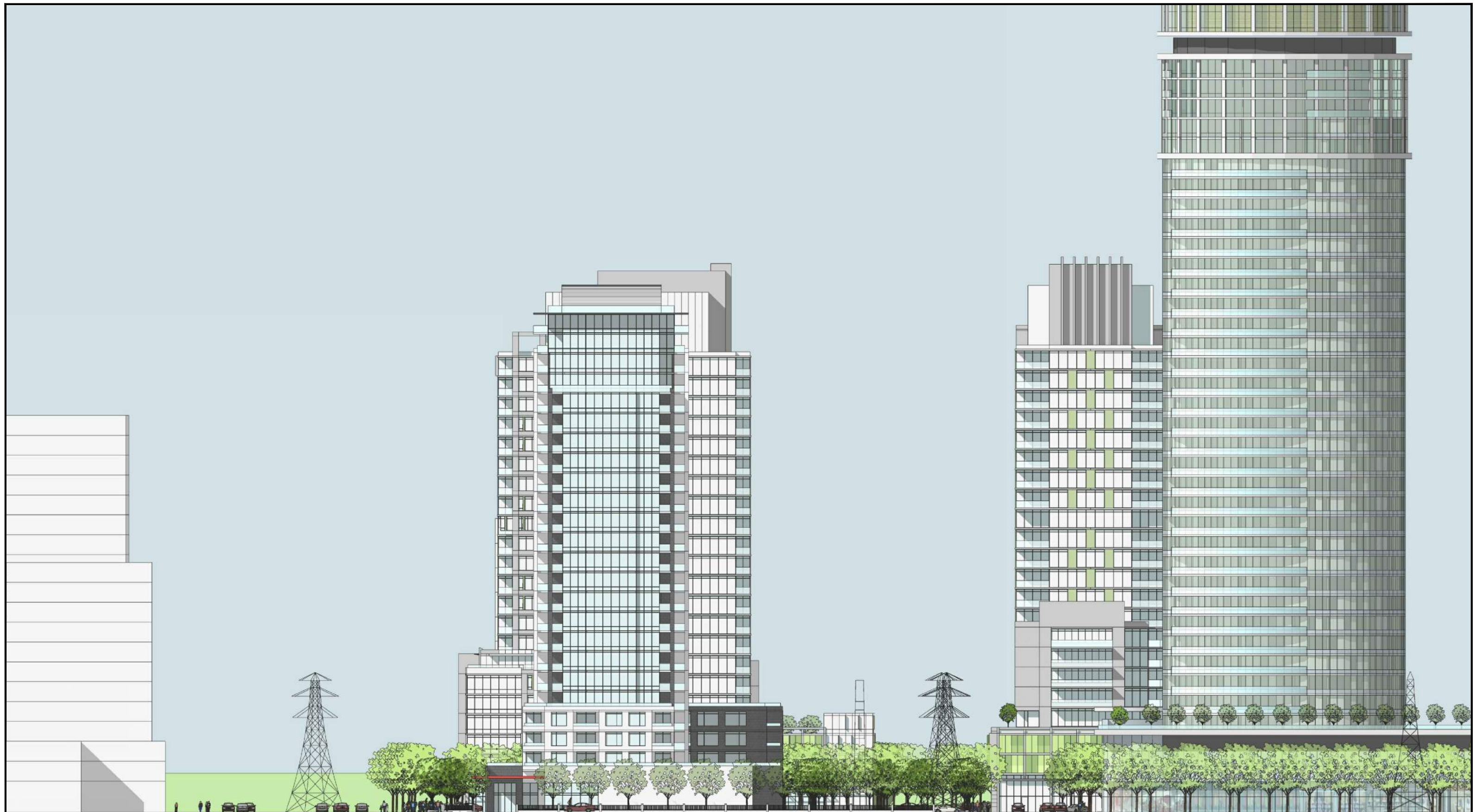
CARLING AVE - STREET ELEVATION SOUTH (PHASE 1)



CARLING AVE - STREET ELEVATION SOUTH (FUTURE)

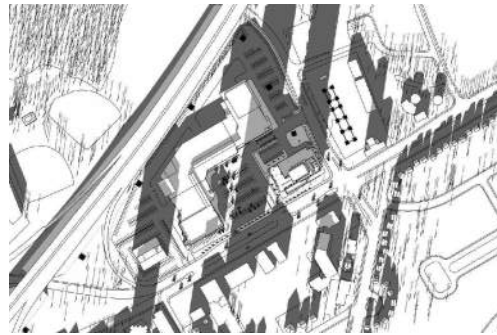


MERIVALE ROAD - STREET ELEVATION EAST (PHASE 1)

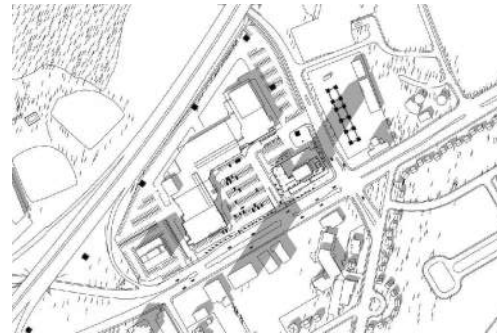


MERIVALE ROAD - STREET ELEVATION EAST (FUTURE)

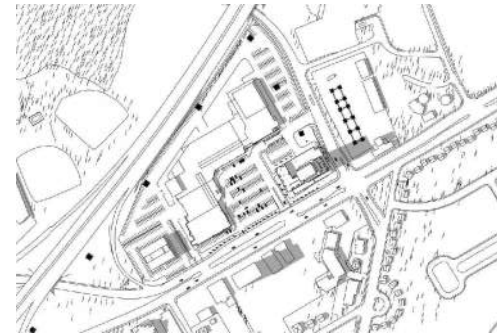
MARCH 21, DST



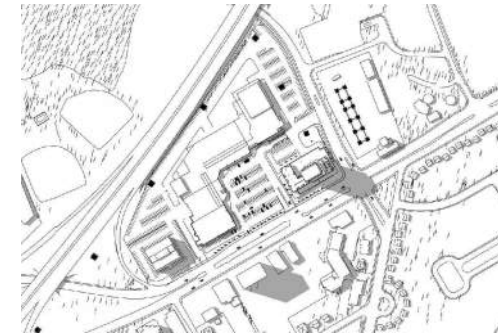
8:00 am



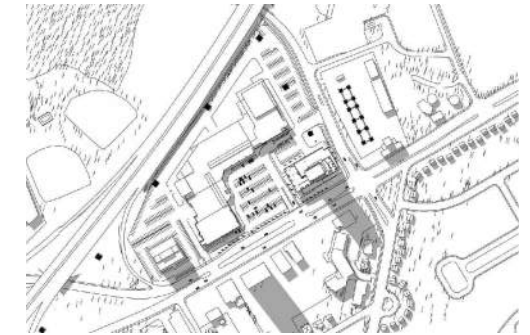
10:00 am



12:00 pm

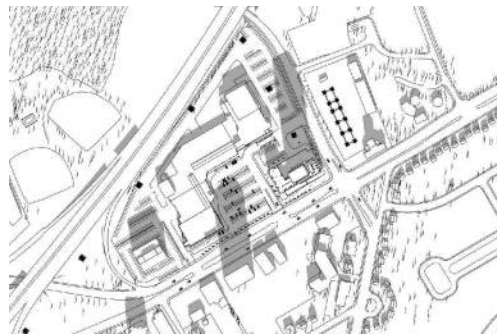


2:00 pm

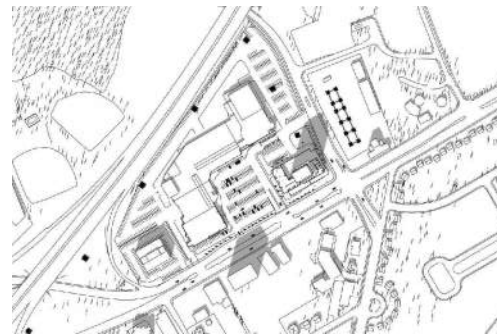


4:00 pm

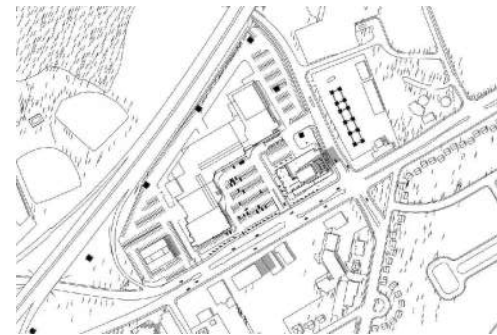
JUNE 21, DST



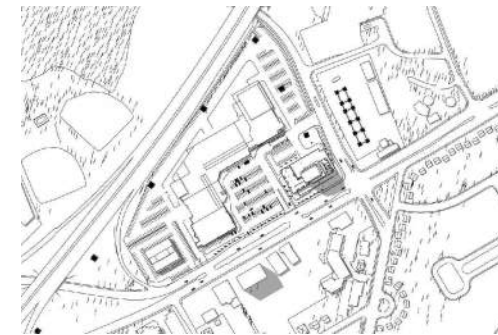
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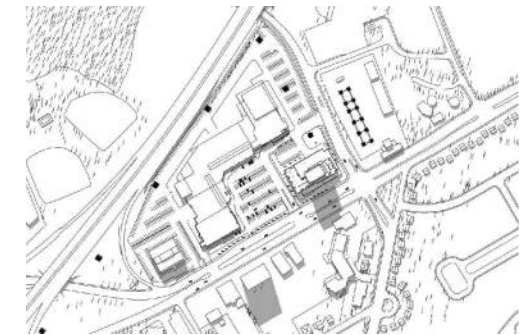
10:00 am



12:00 pm

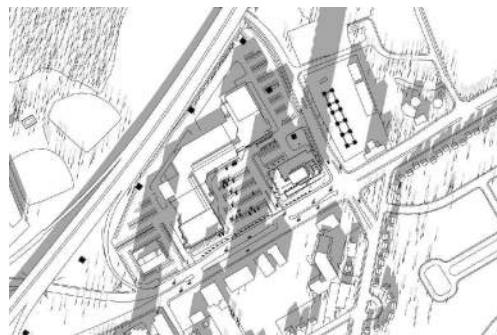


2:00 pm

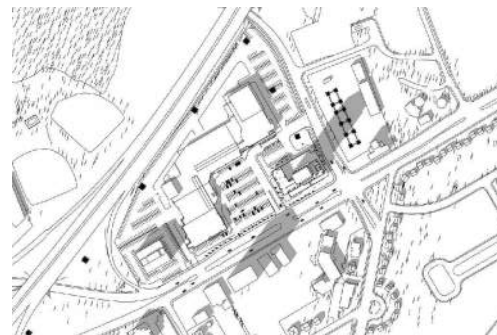


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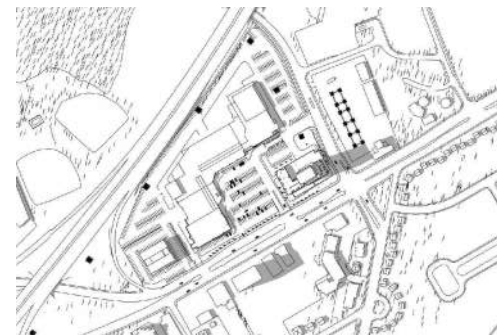
SEPTEMBER 21, DST



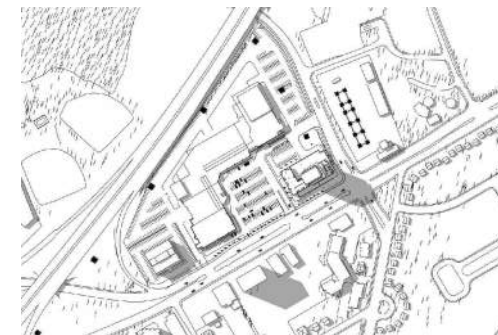
8:00 am



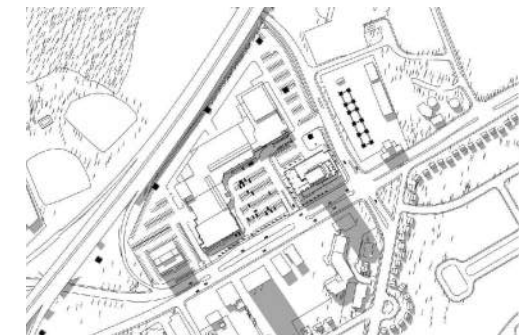
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12:00 pm



2:00 pm

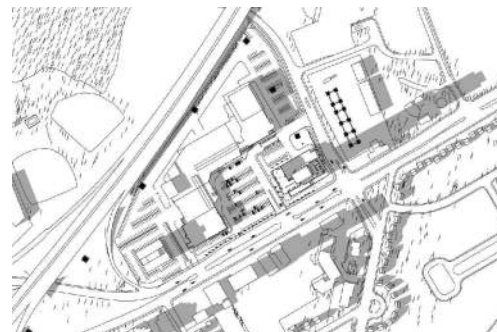


4:00 pm

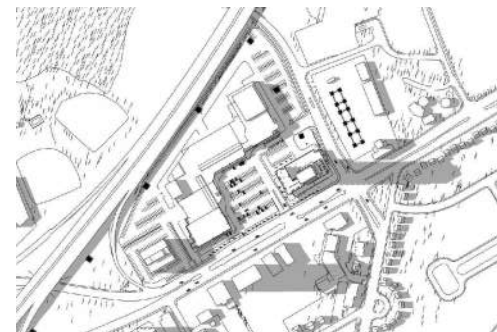
DECEMBER 21



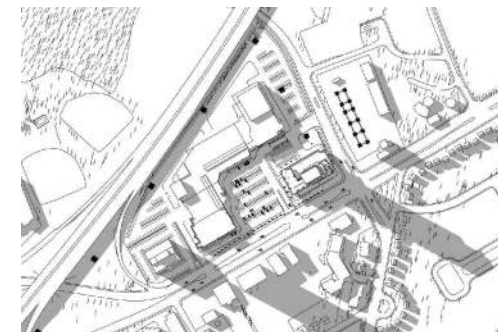
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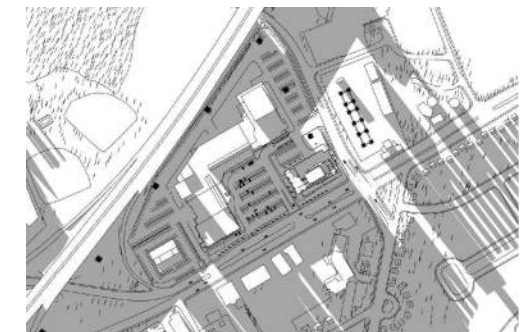
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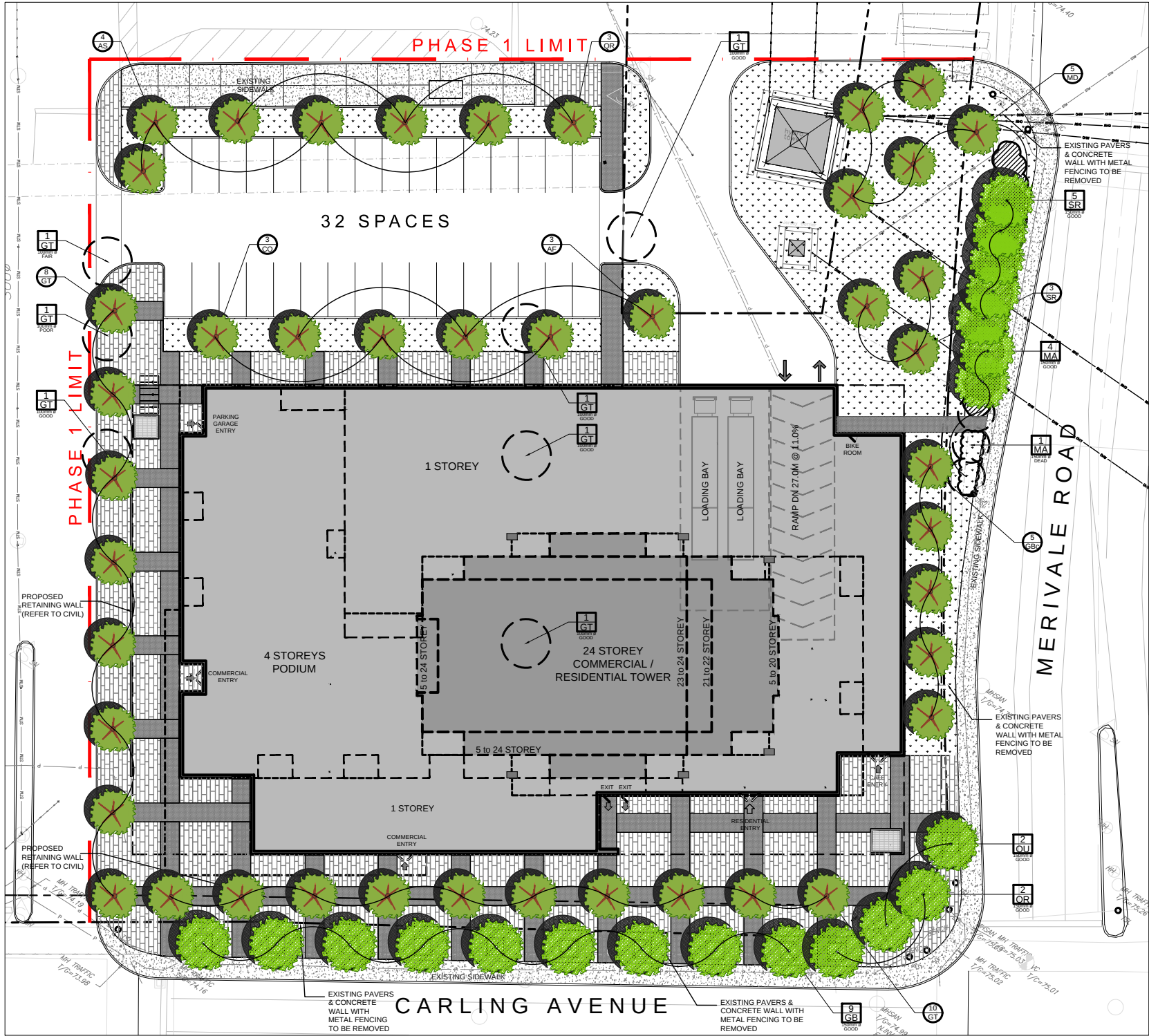
12:00 pm



2:00 pm



4:00 pm



GENERAL NOTES:

1. It is the responsibility of the appropriate contractor or official to report any errors, omissions or discrepancies on this plan with actual site conditions to the Landscape Architect before proceeding with construction.
2. The contractor is to notify all utility companies and authorities prior to any excavation and ascertain locations of underground services.
3. The contractor is to reinstate all areas and items damaged as a result of construction activity.
4. The contractor is to comply with all pertinent codes and by-laws.
5. The contractor is to maintain a positive surface run-off throughout the entire construction period.
6. The Landscape Architect is not responsible for subsurface conditions.
7. The contractor is to identify all existing trees to remain on site with the Landscape Architect prior to construction.
8. The contractor is to stake the proposed location of all plant material in conjunction with the Landscape Architect prior to construction.
9. Minimum distances for selected deciduous trees are as follows:
 - Building Foundations 7.5m
 - Sidewalks 1.5m
 - Public Streets 2.5m
 - Underground Infrastructure 2.0m
10. All trees within 1m of underground utility trenches are to be excavated by hand.
11. Remove all protective wrapping from tree trunks after installation.
12. Staking of trees shall only be performed if necessary.
13. Ensure that mulch is pulled back a min. distance of 75mm from base of tree trunk.

LEGEND

- EXISTING DECIDUOUS TREE TO BE REMOVED
- EXISTING DECIDUOUS TREE TO REMAIN
- EXISTING SHRUBS TO BE REMOVED
- EXISTING SHRUBS TO REMAIN
- PROPOSED DECIDUOUS TREE
- PROPOSED PRECAST CONCRETE PAVERS I
- PROPOSED PRECAST CONCRETE PAVERS II
- PROPOSED SOD

EXISTING TREE LIST

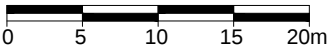
KEY	QTY.	BOTANICAL NAME	COMMON NAME	SIZE	CONDITION
TREES					
GB	9	Ginkgo biloba	Maidenhair Tree	150mm DBH	Good
GT	7	Gleditsia triacanthos	Honey Locust	100mm DBH	Poor-Good
MA	5	Malus sp.	Crabapple	150mm DBH	Dead - Good
QR	2	Quercus rubra	Red Oak	150mm DBH	Good
QU	2	Quercus robur	English Oak	150mm DBH	Good
SR	5	Syringa reticulata	Japanese Tree Lilac	150mm DBH	Good

PROPOSED PLANT LIST

KEY	QTY.	BOTANICAL NAME	COMMON NAME	SIZE	CONDITION
TREES					
AF	3	Acer x Freemanii	Freeman Maple	60mm ø	B&B
AS	4	Acer saccharinum	Silver Maple	60mm ø	B&B
CO	3	Celtis occidentalis	Common Hackberry	60mm ø	B&B
GBc	5	Ginkgo biloba 'Fastigiata'	Columnar Maidenhair Tree	60mm ø	B&B
GT	18	Gleditsia triacanthos	Honey Locust	60mm ø	B&B
MD	5	Malus 'Dolgo'	Flowering Crabapple	60mm ø	B&B
QR	3	Quercus rubra	Red Oak	60mm ø	B&B
SR	3	Syringa reticulata	Japanese Tree Lilac	60mm ø	B&B

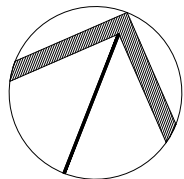
1
L.1

TREE CONSERVATION REPORT & LANDSCAPE PLAN
SCALE 1:500



JAMES B. LENNOX & ASSOCIATES INC.
LANDSCAPE ARCHITECTS
3332 CARLING AVE. OTTAWA, ONTARIO K2H 5A8
Tel. (613) 722-5168 Fax. 1(866) 343-3942

PROJECT NORTH



STAMP



4				
3	ISSUED FOR UDRP	11/22/2018	SS	JL
2	ISSUED FOR SPC	10/30/2018	SS	JL
1	ISSUED FOR COMMENTS AND REVIEW	10/26/2018	SS	JL
No.	Issue	Date MM/DD/YY	DR	CK

PROJECT
WESTGATE - PHASE I
1309 CARLING AVENUE
OTTAWA ON

DRAWING
**TREE CONSERVATION REPORT
AND LANDSCAPE PLAN**

CLIENT:
RIO CAN
REAL VISION, SOLID GROUND.

SCALE
AS SHOWN
DATE
22/11/2018

DRAWING NO.
L.1