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Planning Rationale

URBANDALE CORPORATION COWEN'S GROVE COMMERCIAL PLAZA 4791 BANK STREET



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Planning Rationale Urbandale Corporation, 4791 Bank Street Cowen's Grove Commercial Plaza

1.0 Introduction

J.L. Richards & Associates Limited (JLR) has been retained by Urbandale Corporation to prepare a Planning Rationale in support of a Site Plan Control application for the property located at 4791 Bank Street (known as the Cowen's Grove Commercial Plaza) in the Former Township of Osgoode, City of Ottawa as shown on Figure 1. This application seeks to permit the development of a commercial plaza development consisting of 5 locally-oriented commercial use buildings on both sides of Shuttleworth Drive, as shown on Figure 2 and a larger plan can be found in Appendix 'A'.



Figure 1 – Aerial view of Cowen's Grove Commercial Plaza

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Urbandale Corporation, 4791 Bank Street

Cowen's Grove Commercial Plaza

1.1 Purpose

This Report will demonstrate how the development will be consistent with the Provincial Policy Statement, and is in conformity with the City of Ottawa Official Plan. This rationale also demonstrates that this proposal conforms to the current Zoning By-law, as well as the Drive-Through Design Guideline.

The layout of the proposed development will provide logical access to commercial services and ensure multimodal connectivity that meets the needs of both this growing community and the City as a whole.

1.2 Site Context

1.2.1 Location and Description

The legal description of the Subject Site is Part Lot 19, Concession 5 Rideau Front (Gloucester), Parts 2 to 5 on 4R-27185 and Part 1 on 4R-30466 in the Former Township of Osgoode, now the City of Ottawa. The entire Subdivision property is approximately 29 ha in size while the land subject to this application is approximately 1.4 ha in size. The Subject Site is divided into two smaller blocks (Blocks 223 and 222) by the proposed subdivision entrance off of Bank Street which will be known as Shuttleworth Drive. Those these blocks are situated on the south west side along Bank Street. Only the south portion of Block 223 is subject to this application. The remainder will be development with residential uses in the future.

As shown in Figure 1, the site currently consists of undeveloped urban lands. These lands are have recently been redesignated to General Urban Area and rezoned to General Mixed Use (GM) to allow for a range of uses including but not limited to: a bank, day care, drive through facility, medical facility, office, restaurant, personal service business and recreational and athletic facility, retail food store and retail store.

1.2.2 Community Context

The Subject Site is located within the Leitrim/Findlay Creek Community. An existing commercial plaza is located on the opposite side of Bank Street which is surrounded with low and medium density residential development, as shown on Figure 2. Low and medium density residential is also proposed for the lands immediately to the north of the Subject Site and on the remainder of the Cowen's Grove Subdivision lands.

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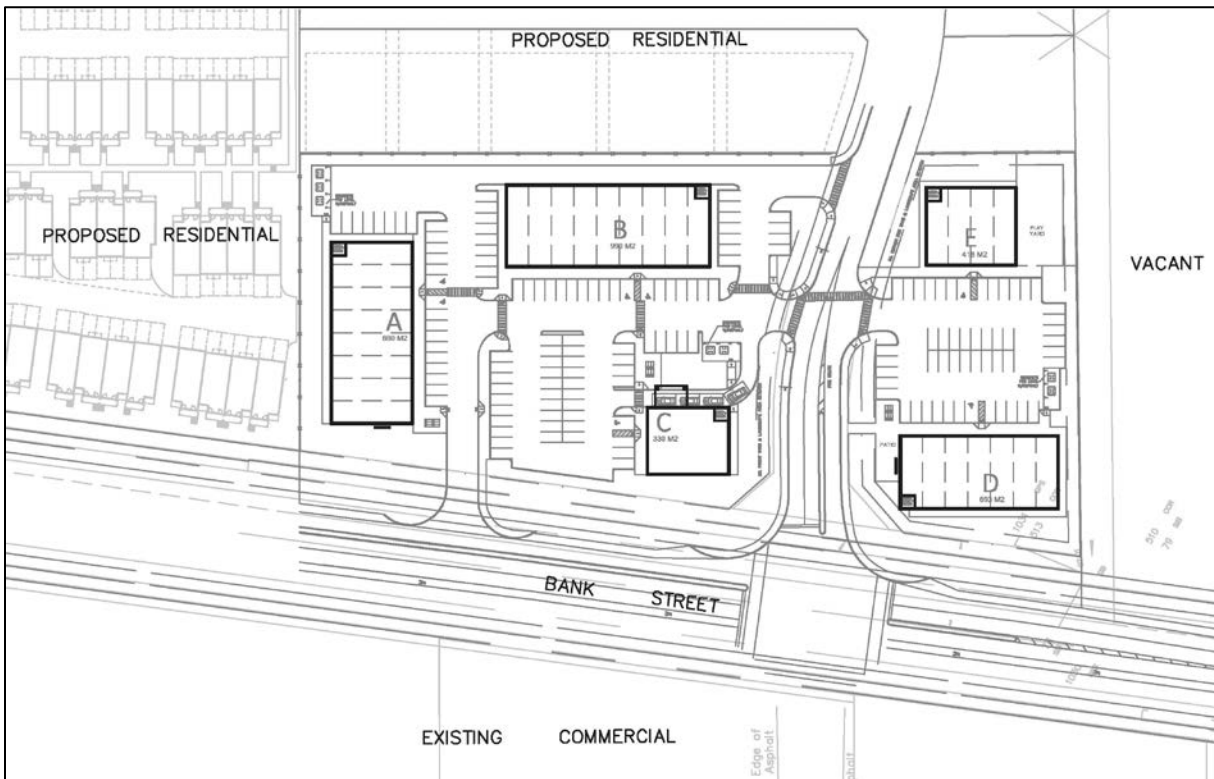


Figure 2 – Surrounding Context: Cowen's Grove Commercial Plaza

The Subject Site will be located within close proximity to two schools; Vimy Ridge Public School and a proposed school block which has been accommodated for in the Subdivision development to reflect the requirements of the French Catholic School Board.

There are also two City parks proposed within the Draft Subdivision to the east of the site.

Both Bank Street and Leitrim Road are arterial roads in the City that are heavily used by residents traveling to and from the Leitrim/Findlay Creek Community (see Figure 3).



Figure 3 – Existing City of Ottawa Official Plan Schedule E: Urban Road Network

The site is well serviced by OC Transpo which operates around the site. There are currently two service routes available: Route 294 and Route 94.

The existing connectivity of the site is also well developed. Routes provide a beneficial feature for pedestrians and cyclists traveling nearby. There are existing sidewalks on the west side of Bank Street and are proposed along the east side, both of which are currently well lit. A southbound bike lane exists along Bank Street, and a paved shoulder exists northbound along Bank Street. In addition, the intersection at Bank Street and Shuttleworth Drive has existing and proposed traffic and crosswalk signals which promote road safety.

Improvements to and road widening of Bank Street in order to accommodate the Cowen's Grove Subdivision along with other adjacent developments are also proposed. Details can be found in the accompanying Transportation Impact Assessment (TIA) Report.

2.0 The Proposal

2.1 Site Plan Control Application

Figure 2 and Appendix 'A' show the site plan for the proposed development. This commercial shopping centre consists of 4, one storey commercial, retail and service buildings and 1, one storey bank with a drive-through located nearest to the intersection of Bank Street and

Planning Rationale

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Shuttleworth Drive. The proposed bank and two of the commercial buildings are to be located north of Shuttleworth Drive while the remaining two commercial buildings are located on the south side.

There are 157 surface parking spaces provided. This is 43 spaces more than the City's zoning by-law requirements of 102 spaces for the shopping centre and 12 spaces for the bank use. At this time, the uses and tenants for all 4 proposed shopping centre buildings are not finalized.

Proposed Use	Required	Provided
Building C - 330 m2 (Bank with Drive-Through)	Bank 3.4 spaces per 100m2 (12 spaces required)	157
Building A – 880 m2 Building B – 990 m2 Building D – 693 m2 Building E – 418 m2 (Retail/Service/Commercial Shopping Plaza)	Shopping Centre 3.4 spaces per 100m2 (102 spaces required)	

The minimum number of queuing spaces for the proposed drive-through have also been identified, with 3 spaces before/at each machine and 1 after each machine for a total of 4 spaces.

Municipal water, sanitary sewer and stormwater sewer services is available in both Bank Street and will be available in Shuttleworth Drive. A site servicing plan and report accompanies this submission.

As shown in Appendix 'A', vehicular access to the southern commercial block will be from Shuttleworth Drive while access to the northern commercial block will be from Shuttleworth Drive and Bank Street.

The proposed site will also include:

- 18 bicycle parking spaces
- 157 outdoor parking spaces
- 7 accessible parking spaces
- 1 driveway from the Arterial (Bank Street)
- 2 driveways from the proposed Collector (Shuttleworth Drive)
- 1 outdoor play yard area (393 m2)
- 1 patio area (60 m2)
- 7 Earth Bin garbage bins to serve all five buildings (designed in accordance with Section 110 of Zoning By-law).

3.0 Policy Context

3.1 Planning Act (R.S.O. 1990, Chapter P.13)

Section 41 (4) of the *Planning Act* provides details regarding the development criteria to be considered with a Site Plan Control application. Please see Appendix 'B' for the detailed review of the Section 41 development criteria.

The proposed Site Plan for the Cowen's Grove Commercial Plaza has addressed all of the criteria identified in the Act. This rationale confirms our opinion that the proposed site plan has met the criteria identified in the Act and is appropriate for development.

Based on the assessment of this rationale, the application for site plan control for the Subject Site meets the policies of the Planning Act, is appropriate and represents good land use planning.

3.2 Provincial Policy Statement (2014)

This Site Plan is consistent with the vision and ideas of the Provincial Policy Statement (PPS) as issued under Section 3 of the *Planning Act*. For example, Section 1.1 of the PPS focuses on promoting efficient land use and development patterns that occur adjacent to built-up areas and which provide opportunities for intensification. It is our opinion that this proposal is consistent with the PPS, 2014. Please see Appendix 'C' for the detailed review of the relevant PPS policies.

Based on the assessment of this rationale, the application for site plan control for the Subject Site is consistent with the Provincial Policy Statement, is appropriate and represents good land use planning.

3.3 The City of Ottawa Official Plan

The City of Ottawa Official Plan, adopted by City Council in May 2003, has been updated and amended numerous times by both Council and the Ontario Municipal Board. For the purposes of this Planning Rationale, the on-line consolidated version of the Official Plan, including Amendment #150 and #180, were used as reference along with Official Plan Amendment (OPA) #196.

The Subject Site is located within the urban boundary of the City of Ottawa and is part of the settlement area for the City. Schedule 'B' of the Official Plan shows the site designated as a General Urban Area (refer to Figure 4) within the Urban Area. This form of development is identified as a part of the General Urban Area.

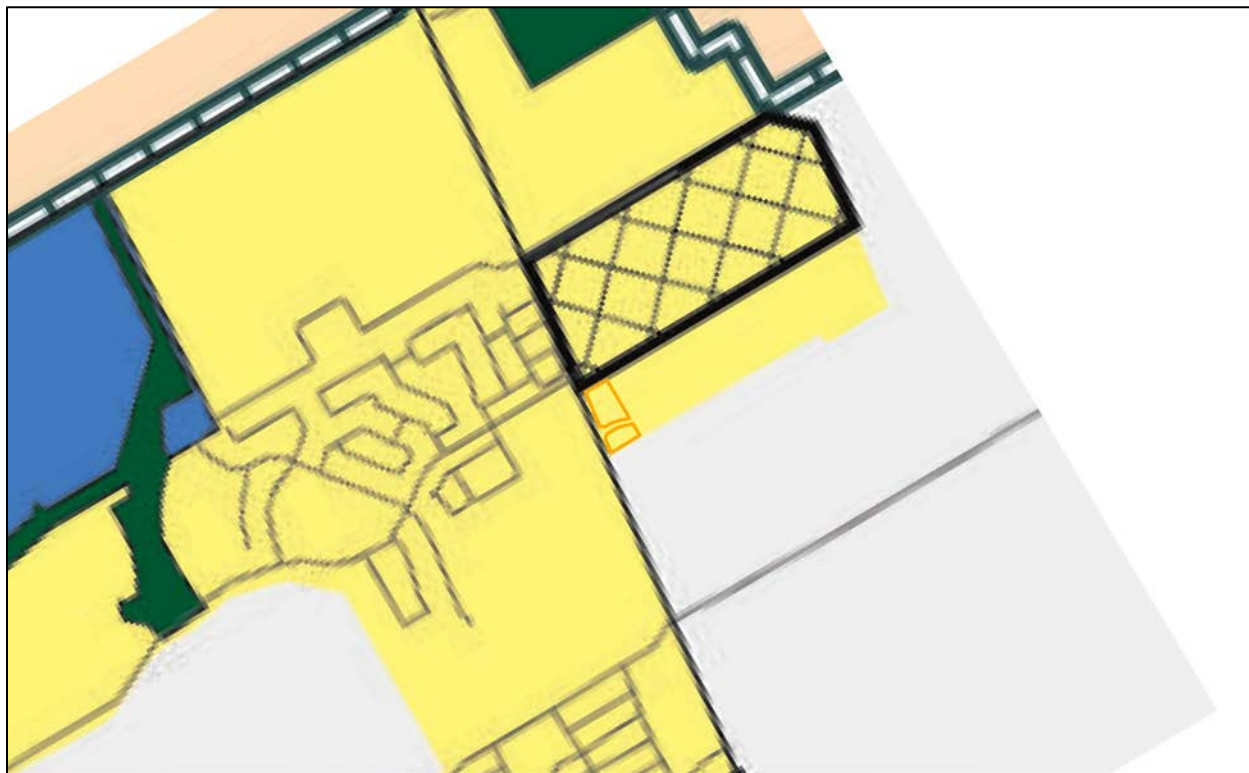


Figure 4 – Existing City of Ottawa Official Plan Schedule B: ‘General Urban Area’

3.3.1 Urban Design Guidelines

The proposed Site Plan was created with special consideration for the policies of the General Urban Area with emphasis on the policies for design, compatibility, and the related guideline for Drive-Through Facilities. Please see Appendix ‘D’ for the detailed review of the relevant Official Plan policies and Appendix ‘E’ for the detailed review of the Urban Design Guidelines for Drive-Through Facilities.

Based on the assessment of this rationale, the application for site plan control for the Subject Site conforms to the policies of the City of Ottawa Official Plan and relevant design guidelines, is appropriate and represents good land use planning

3.4 City of Ottawa Zoning By-law 2008-250

On June 27th, 2018 the Subject Site was rezoned to GM – General Mixed Use in the City of Ottawa Comprehensive Zoning By-law 2008-250 by By-law #221-2018.

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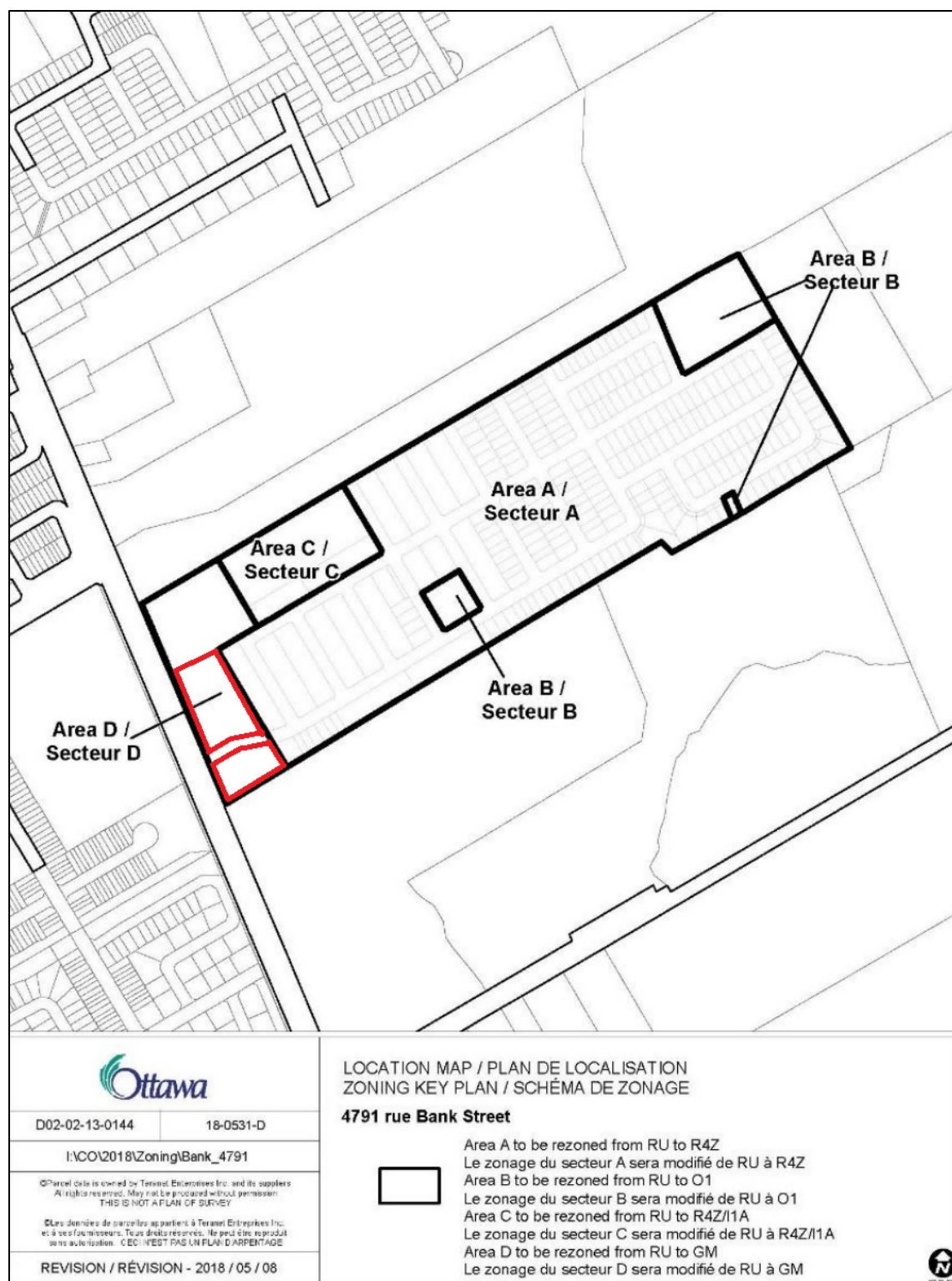


Figure 5 – City of Ottawa Zoning By-law 2008-250: Cowen's Grove Commercial Plaza

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Urbandale Corporation, 4791 Bank Street

Cowen's Grove Commercial Plaza

The proposed Site Plan has been designed to comply with the zoning standards of the GM zone and all other relevant provisions including the General and Parking requirements.

The site plan includes a detailed table demonstrating conformity. Please see Appendix 'F' for the detailed review of the relevant Zoning By-law policies and standards.

Based on the assessment of this rationale, the application for site plan control for the Subject Site meets all the required provisions and standards of the City of Ottawa Zoning By-law, is appropriate and represents good land use planning

3.5 Background Studies

The following detailed background studies have been completed in support of the draft approval and this application:

- Servicing Brief (2018);
- Transportation Impact Assessment (2018);
- Phase 1 ESA (2013); and
- Noise Impact Assessment (2018).

4.0 Conclusion

This Report has been prepared in support of an application for a Site Plan Control application for the Cowen's Grove Commercial Plaza, which will see a new a commercial shopping centre plaza development consisting of 4 commercial, retail and service use buildings and 1 bank with a drive-through.

1. The current site plan control application is consistent with the Provincial Policy Statement, 2014 as the proposed development is located within a designated Settlement Area, is a complementary use for the neighbourhood, is compatible with its surroundings and is consistent with the provincial interest identified in the Provincial Policy Statement.
2. The application respects the provisions of the Official Plan which has identified these lands as General Urban Area. This proposal conforms to the intent of this designation in that the proposed uses are permitted and are located in an area suitable for their development.
3. The proposed site plan has been design to incorporate the City's guidelines with respect to the establishment of Drive-Through Facilities.
4. The proposed development conforms to the provisions of the GM zone which allows for the development of a commercial plaza which will consist of a variety of small, locally-oriented convenience, service and retail uses.

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Urbandale Corporation, 4791 Bank Street
Cowen's Grove Commercial Plaza

Based on the above-noted rationale, the application for site plan control for the Subject Site is appropriate and represents good land use planning.

This report has been prepared for the exclusive use of Urbandale Corporation, for the stated purpose, for the named facility. Its discussions and conclusions are summary in nature and cannot be properly used, interpreted or extended to other purposes without a detailed understanding and discussions with the client as to its mandated purpose, scope and limitations. This report was prepared for the sole benefit and use of Urbandale Corporation and may not be used or relied on by any other party without the express written consent of J.L. Richards & Associates Limited.

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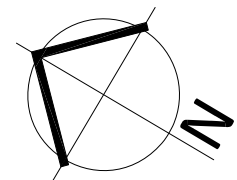


Timothy F. Chadder, MCIP, RPP
Chief Planner

Appendix A

Reduced Site Plan

0 5 10 m 20 m



SITE PLAN LEGEND	
SYMBOL	DESCRIPTION
	PROPERTY LINE
	SETBACK LINE
	NEW CURB
	NEW DEPRESSED CURB
	SITE / BUILDING ENTRANCE
	LAMP STANDARD
	BOLLARD
	ACCESSIBLE PARKING SIGN
	FIRE ROUTE SIGN
	ELECTRIC CAR CHARGING STATION
	ACCESSIBLE PARKING SPACE
	STANDARD PARKING SPACE (2.6 X 5.2 M.)
	BICYCLE PARKING SPACE (2.6 X 1.8 M.)
	LANDSCAPED AREA
	PAINTED LINES
	NEW 2M. HIGH PRIVACY WOOD FENCE
	EARTH BIN SYSTEM WASTE CONTAINER

Client:

Key Plan:

Plan Clé:

T.N. = TRUE NORTH
P.N. = PROJECT
NORTH

P.N.
T.N.

APPROVED ☐ REFUSED ☐
THIS ____ DAY OF _____, 20__

ISSUED FOR COORDINATION 07 / 04 / 2018

ISSUED FOR COORDINATION 07 / 03 / 2018

Issue: Date:

Prime Consultant:

Expert-Consult:

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Project: COWAN'S GROVE PLAZA

4791 BANK ST. OTTAWA, ON

Drawing:

Dessin:

SITE PLAN

Drawn by:

Dessiné par:

S.G.

Designed by:

Conçu par:

S.G.

Approved by:

Approuvé par:

M.D.

Seal:

Scauc:

Scale:

Echelle:

1 : 300

Date:

06/15/18

Client Project No.

No. du Projet du Client:

Project No.:

No. du Projet:

1420

Sheet No.:

No. de la feuille:

A-1

1 SITE PLAN
A-1 SCALE: 1 : 300

Appendix B

Review of Planning Act

Planning Act, R.S.O, 1990

Section 41 of the Planning Act provides the legal framework from establishment of Site Plan Control Areas, approvals of plans or drawings and conditions for approval of plans or drawings.

Approval of plans or drawings

(4) No person shall undertake any development in an area designated under subsection (2) unless the council of the municipality or, where a referral has been made under subsection (12), the Municipal Board has approved one or both, as the council may determine, of the following:

- 1. Plans showing the location of all buildings and structures to be erected and showing the location of all facilities and works to be provided in conjunction therewith and of all facilities and works required under clause (7) (a), including facilities designed to have regard for accessibility for persons with disabilities.*

The site plan for the Subject Site was designed showing the location of all buildings and structures, the location of all facilities and works to be provided in conjunction therewith and of all facilities and works including facilities designed to have regard for accessibility for persons with disabilities.

- 2. Drawings showing plan, elevation and cross-section views for each building to be erected, except a building to be used for residential purposes containing less than twenty-five dwelling units, which drawings are sufficient to display,*
 - (a) the massing and conceptual design of the proposed building;*
 - (b) the relationship of the proposed building to adjacent buildings, streets, and exterior areas to which members of the public have access;*
 - (c) the provision of interior walkways, stairs, elevators and escalators to which members of the public have access from streets, open spaces and interior walkways in adjacent buildings;*
 - (d) matters relating to exterior design, including without limitation the character, scale, appearance and design features of buildings, and their sustainable design, but only to the extent that it is a matter of exterior design, if an official plan and a by-law passed under subsection (2) that both contain provisions relating to such matters are in effect in the municipality;*
 - (e) the sustainable design elements on any adjoining highway under a municipality's jurisdiction, including without limitation trees, shrubs, hedges, plantings or other ground cover, permeable paving materials, street furniture, curb ramps, waste and recycling containers and bicycle parking facilities, if an official plan and a by-*

law passed under subsection (2) that both contain provisions relating to such matters are in effect in the municipality; and

- (f) facilities designed to have regard for accessibility for persons with disabilities. R.S.O. 1990, c. P.13, s. 41 (4); 2002, c. 9, s. 56 (1); 2006, c. 23, s. 16 (3, 4); 2009, c. 33, Sched. 21, s. 10 (9).*

The elevation drawings found in Appendix "B" incorporate all of the requirements of Section 41 (4)(2) of the Planning Act.

Conditions to approval of plans

(7) As a condition to the approval of the plans and drawings referred to in subsection (4), a municipality may require the owner of the land to,

- (a) provide to the satisfaction of and at no expense to the municipality any or all of the following:*

- 1. Subject to the provisions of subsections (8) and (9), widenings of highways that abut on the land.*
- 2. Subject to the Public Transportation and Highway Improvement Act, facilities to provide access to and from the land such as access ramps and curbings and traffic direction signs.*
- 3. Off-street vehicular loading and parking facilities, either covered or uncovered, access driveways, including driveways for emergency vehicles, and the surfacing of such areas and driveways.*
- 4. Walkways and walkway ramps, including the surfacing thereof, and all other means of pedestrian access.*
- 4.1 Facilities designed to have regard for accessibility for persons with disabilities.*
- 5. Facilities for the lighting, including floodlighting, of the land or of any buildings or structures thereon.*
- 6. Walls, fences, hedges, trees, shrubs or other groundcover or facilities for the landscaping of the lands or the protection of adjoining lands.*
- 7. Vaults, central storage and collection areas and other facilities and enclosures for the storage of garbage and other waste material.*
- 8. Easements conveyed to the municipality for the construction, maintenance or improvement of watercourses, ditches, land drainage works, sanitary sewage facilities and other public utilities of the municipality or local board thereof on the land.*

9. Grading or alteration in elevation or contour of the land and provision for the disposal of storm, surface and waste water from the land and from any buildings or structures thereon;

The proposed development for the Subject Site has already provided a road widening to the municipality and will provide the additional features as found under Section 41 (7) of the Planning Act:

- facilities to provide access to and from the land such as curbing and traffic direction signs;
- off-street vehicular parking facilities;
- asphalt access driveways, including driveways for emergency vehicles;
- concrete walkways and walkway ramps;
- facilities designed to have regard for accessibility for persons with disabilities (including accessible parking spaces);
- site lighting;
- sunken storage containers for the storage of garbage and other waste material;
- easements will be conveyed to the municipality for the construction, maintenance or improvement of land drainage works, sanitary sewage facilities and other public utilities of the municipality on the land; and
- grading of the site elevation and provision for the disposal of storm, surface and waste water from the land and from any buildings or structures thereon.

Appendix C

Review of Provincial Policy
Statement (2014)

Provincial Policy Statement (2014)

This Site Plan is consistent with the vision and ideas of the Provincial Policy Statement (PPS) as issued under Section 3 of the *Planning Act*. Section 1.1 of the PPS focuses on promoting efficient land use and development patterns that occur adjacent to built-up areas and which provide opportunities for intensification. Section 1.1.3.1 states:

‘Settlement Areas shall be the focus of growth and their vitality and regeneration shall be promoted.’

Section 1.0 of the PPS supports building strong healthy communities through a number of strategies, are achieved by:

- a) Promoting efficient development and land use patterns which sustain the financial wellbeing of the Province and municipalities over the long term;
- b) Accommodating an appropriate range and mix of residential (including second units, affordable housing and housing for older persons), employment (including industrial and commercial), institutional (including places of worship, cemeteries and long-term care homes), recreation, park and open space, and other uses to meet long-term needs;
- c) Avoiding development and land use patterns which may cause environmental or public health and safety concerns;
- d) Avoiding development and land use patterns that would prevent the efficient expansion of settlement areas in those areas which are adjacent or close to settlement areas;
- e) Promoting cost-effective development patterns and standards to minimize land consumption and servicing costs;
- f) Improving accessibility for persons with disabilities and older persons by identifying, preventing and removing land use barriers which restrict their full participation in society;
- g) Ensuring that necessary infrastructure, electricity generation facilities and transmission and distribution systems, and public service facilities are or will be available to meet current and projected needs; and
- h) Promoting development and land use patterns that conserve biodiversity and consider the impacts of a changing climate.”

The proposed commercial plaza development meets the vision of the PPS, as it:

- Provides employment opportunities within the settlement area;
- Supports the existing infrastructure capacity of the neighbourhood;
- Promotes growth on vacant lands; and
- Establishes retail and service uses that will complement the existing infrastructure of the surrounding community.

Appendix D

Review of City of Ottawa
Official Plan
(Including OPA #196 and #150,
#180 as they have been Council
Approved)

City of Ottawa Official Plan, 2003

The City of Ottawa Official Plan, adopted by City Council in May 2003, has been updated and amended numerous times by both Council and the Ontario Municipal Board. For the purposes of this planning rationale, the on-line consolidated version of the Official Plan, including Amendment #150 and #180, was used as reference along with OPA #196.

The Subject Site is located within the urban boundary of the City of Ottawa and is part of the settlement area for the City. Schedule 'B' of the Official Plan shows the site location in the south part of the City of Ottawa designated as a General Urban Area (refer to Figure 3) within the Urban Area.

Section 3.6.1 of the Official Plan provides a framework for the General Urban Area. The Plan states that:

“The General Urban Area designation permits the development of a full range and choice of housing types to meet the needs of all ages, incomes and life circumstances, in combination with conveniently located employment, retail, service, cultural, leisure, entertainment and institutional uses. This will facilitate the development of complete and sustainable communities. A broad scale of uses is found within this designation, from ground-oriented single-purpose to multi-storey mixed-use; from corner store to shopping centre.

While the City is supportive of the establishment of a broad mix of uses in Ottawa's neighbourhoods, this is not meant to imply that all uses will be permitted everywhere within areas that are designated General Urban Area. The zoning by-law will continue to regulate the location, scale and type of land use in accordance with the provisions of this Plan. Within neighbourhoods, the zoning by-law will allow those uses that provide for the local, everyday needs of the residents, including shopping, schools, recreation and services. Uses that also serve wider parts of the city will be located at the edges of neighbourhoods on roads where the needs of these land uses (such as transit, car and truck access, and parking) can be more easily met and impacts controlled. Subject to the policies below, the City supports infill development and other intensification within the General Urban Area in a manner that enhances and complements the desirable characteristics and ensures the long-term vitality of the many existing communities that make up the city.”

The proposed Site Plan was created with special consideration for the policies of the General Urban Area, Strategic Directions and Urban Design and Compatibility in the Official Plan and

based upon the required technical studies. The proposed Site Plan and current zoning are the proper reflection of these policies as refined in the supporting studies.

The proposed development is designated General Urban Area in Schedule B, which supports the establishment of a variety of uses. Within this area, the City encourages a variety of small, locally-oriented convenience and service uses that complement adjacent residential land uses, and are of a size and scale consistent with the needs of nearby residential areas. The City manages this growth by ensuring that:

1. The uses are compatible and complement surrounding land uses, and can be developed in accordance with Section 2.5.1 and Section 4.11;
2. Are conveniently located with respect to concentrations of residential development and provide direct access for pedestrians and cyclists from adjacent residential areas;
3. Are permitted to cluster with other community-oriented uses, such as parks, pedestrian linkages, community centres or leisure facilities, in order to facilitate interaction among residents and contribute to a sense of community;
4. Are situated to take advantage of pedestrian and cycling patterns;
5. Are of a size and scale that will not result in the attraction of large volumes of vehicular traffic from outside the immediate area.

The proposed commercial plaza has been designed in accordance with Sections 2.5.1 and 4.11 and to a scale that will not result in the attraction of large volumes of vehicular traffic from outside the immediate area. The Subject Site is also located within vacant area of the greater residential community providing multimodal connectivity to the surrounding neighbourhood and is clustered with other community-oriented uses such as the two schools, parks and retirement facilities.

4.2.1 Urban Design and Compatibility – Sections 2.5.1 and 4.11

Sections 2.5.1 and 4.11 in the Official Plan outlines guiding policies to utilize when evaluating where and how land use should be permitted to develop. The proposed development has reviewed these sections and has been designed to meet and address the design objectives of these policies.

The proposed commercial plaza development has addressed the design objectives of Section 2.5.1 as follows:

Design Objective	Proposed Development
1. To enhance the sense of community by creating and maintaining places with their own distinct identity	The proposed development provides an opportunity to bring a commercial hub to the existing residential and institutional neighbourhood.
2. To define quality public and private spaces through development	The proposed development has been designed creating quality exterior space for each commercial tenant and their anticipated needs.
3. To create places that are safe, accessible and are easy to get to, and move through	The proposed development has been designed to City of Ottawa standards including adequate lighting to provide easy access onto the Subject Site from roads, sidewalks, and parking areas.
4. To ensure that new development respects the character of existing areas	The surrounding area is primarily residential with an existing commercial plaza within the immediate area. Therefore, the architectural design will respect and complement the existing uses and architecture of the area.
5. To consider adaptability and diversity by creating places that can adapt and evolve easily over time and that are characterized by variety and choice. [OMB decision #2649, September 21, 2006]	The proposed development provides a commercial retail hub for all ranges of potential commercial uses.
6. To understand and respect natural processes and features in development design	The proposed development has been designed in a manner which utilizes the natural topography of the Subject Site to manage drainage.

7. To maximize energy-efficiency and promote sustainable design to reduce the resource consumption, energy use, and carbon footprint of the built environment	The proposed development has been designed to provide as much landscaping as possible, while utilizing energy efficient building construction.
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The proposed development has addressed the Policies of Section 4.11 as follows:

Policies	Proposed Development
Traffic: Roads should adequately serve the development, with sufficient capacity to accommodate the anticipated traffic generated. Generally, development that has the potential to generate significant amounts of vehicular traffic should be located on arterial or major collector roadways so as to minimize the potential for traffic infiltration on minor collector roadways and local streets.	A Traffic Impact Study was completed by IBI Group which determined that the existing network can support the proposed development.
Vehicular Access: The location and orientation of vehicle access and egress should address matters such as the impact of noise, headlight glare and loss of privacy on development adjacent or immediately opposite. Vehicular access and egress for development that has the potential to generate a significant amount of vehicular traffic should be oriented on streets other than local streets, wherever the opportunity exists, considering traffic safety and other transportation objectives of this Plan.	The Subject Site will be accessible from Bank Street and Shuttleworth Drive to ensure that traffic flows efficiently.
Parking Requirements: The development should have adequate on-site parking to minimize the potential for spillover parking on adjacent areas. A range of parking forms, including surface, decked, and underground, should be considered taking in account the area context and character. Opportunities to	A total of 157 surface parking spaces are proposed for this development which meets the zoning by-law parking requirements.

reduce parking requirements and promote increased usage of walking, cycling and transit will be pursued, where appropriate, particularly in the vicinity of transit stations or major transit stops in accordance with the provisions of Section 4.3.	
Outdoor Amenity Areas: The development should respect the privacy of outdoor amenity areas of adjacent residential units and minimize any undesirable impacts through the siting and design of the buildings and the use of screening, lighting, landscaping or other mitigative design measures.	A 3 metre landscape buffer is provided around each block of the Plaza.
Loading Areas, Service Areas, and Outdoor Storage: The operational characteristics and visual appearance of loading facilities, service areas (including garbage), parking and areas for the outdoor storage of goods or materials should be mitigated using a variety of methods (e.g., location, containment, screening, berms, and/or landscaping). These uses and activities should be located away from residences where possible.	The sizes of the proposed buildings do not require loading spaces per the Zoning By-law.
Lighting: The potential for light spill over or glare from any lighting source onto adjacent light-sensitive areas should be avoided or mitigated.	The lighting standards will adhere to the City's standards with no anticipated impacts onto adjacent properties.
Noise and Air Quality: The development should be located and designed to minimize the potential for significant adverse effects on adjacent sensitive uses related to noise, odours, and other emissions.	It is anticipated that any potential noise or air quality impacts are mitigated through the design of the Subject Site.
Sunlight: The development should minimize shadowing on adjacent properties, to the extent practicable, particularly on outdoor	The proposed maximum height of approximately 6 m metres as well as the

amenity areas, through the siting of buildings or other design measures.	ample distance provided between buildings should positively address this policy.
Microclimate: The development should be designed to minimize adverse effects related to wind, snow drifting, and temperature on adjacent properties.	The proposed density and spacing on site should positively address this policy and mitigate most wind or snow drifting concerns.
Supporting Neighbourhood Services: The development should contribute to or be adequately served by existing or proposed services and amenities such as health facilities, schools, parks and leisure areas. Where the proposed development itself is to contribute such services and amenities, they should be of a scale appropriate to the needs and character of the area.	The proposed development will contribute to the neighbourhood services through the provision of commercial and service uses that will complement the existing General Urban Area.

Appendix E

Review of Urban Design Guidelines
for Drive-Through Facilities (2006)

Urban Design Guidelines for Drive-Through Facilities (2006)

The proposed development introduces one drive-through facility, Building A. The development has addressed the Urban Design Guidelines for Drive-Through Facilities as follows:

The proposed development has been designed to the Streetscape and Built Form guidelines as it:

- Responds to the positive elements of the context such as the arterial and collector roads through such means as building height, setbacks, building orientation and architectural styles;
- Includes landscaping features and buffers to help define street edges and enhance the pedestrian street façade; and
- Provides a commercial retail hub for all residents including pedestrians, cyclists, transit used and those that which utilize automobiles.

The proposed development has been designed to the Pedestrians and Cyclists guidelines as it:

- Proposed a sidewalks along Bank Street which ties into the connections around the property allowing pedestrians to move safely through the Subject Site;
- Provides direct and accessible customer entrances on each proposed building, with distinguished walkways in front; and
- Features bicycle parking near each building.

The proposed development has been designed to the Vehicles and Parking guidelines as it:

- Minimizes the number and width of driveways to reduce interruptions to public connectivity;
- Provides adequate queuing spaces and efficient stacking lanes where the drive-through is proposed; and

The proposed development has been designed to the Landscape and Environment guidelines as it:

- Incorporates a 3-metre-wide landscaped area around the drive-through property; and
- Includes trees and shrubs adjacent to the drive-through to provide screening, while enhancing the environmental benefits.

The proposed development has been designed to the Signs guidelines as it:

- Will provide pavement markings and directional signage to provide clear direction of movement patterns on site; and
- Includes appropriate parking signage and wall-mounted signs on each building.

The propose development has been designed to the Servicing and Utilities guidelines as it:

- Provides clear sightlines between the Subject Site, proposed buildings and surrounding uses to promote road and pedestrian safety.

Appendix F

Review of the City of Ottawa Zoning
By-law (2008-250)

City of Ottawa Zoning By-law 2008-250

On June 27th, 2018 the Subject Site was rezoned to GM – General Mixed Use in the City of Ottawa Comprehensive Zoning By-law 2008-250 by By-law #221-2018, as shown on Figure 5.

The purpose of the GM zone is to:

- “(1) allow residential, commercial and institutional uses, or mixed use development in the General Urban Area and in the Upper Town, Lowertown and Sandy Hill West Character Areas of the Central Area designations of the Official Plan;*
- (2) limit commercial uses to individual occupancies or in groupings in well defined areas such that they do not affect the development of the designated Traditional and Arterial Mainstreets as viable mixed-use areas;*
- (3) permit uses that are often large and serve or draw from broader areas than the surrounding community and which may generate traffic, noise or other impacts provided the anticipated impacts are adequately mitigated or otherwise addressed; and*
- (4) impose development standards that will ensure that the uses are compatible and complement surrounding land uses.”*

The uses permitted in the GM zone and zoning standards are as follows:

GM – General Mixed Use Zone (Sec. 187-188)

187. In the GM Zone, the following non-residential uses are permitted subject to the provisions of subsections 187(3), (4) and (5):

- animal care establishment
- animal hospital
- artist studio
- bank
- bank machine
- catering establishment
- click and collect facility
- community centre

- community health and resource centre
- convenience store
- day care
- diplomatic mission
- drive-through facility
- emergency service
- funeral home
- home-based business
- home-based day care
- instructional facility
- library
- medical facility
- municipal service centre
- office
- payday loan establishment
- personal service business
- place of assembly
- place of worship
- post office
- recreational and athletic facility
- research and development centre
- residential care facility
- restaurant
- retail food store
- retail store
- service and repair shop
- shelter
- small batch brewery
- technology industry
- training centre
- urban agriculture

TABLE 187 – GM ZONE PROVISIONS

I ZONING MECHANISMS			II PROVISIONS
(a) Minimum lot area			No minimum
(b) Minimum lot width			No minimum
(c) Minimum front yard and corner side yard setbacks			3 m
(d) Minimum interior side yard setbacks	(i) for a non-residential use building or a mixed residential / non-residential use building, from that portion of a lot line abutting a residential zone		5 m
	(ii) for a residential use building	1. for a building equal or lower than 11 metres in height	1.2 m
		2. for a building higher than 11 metres in height	3 m
	(iii) other cases		No minimum
(e) Minimum rear yard setbacks	(i) abutting a street		3 m
	(ii) for a non-residential use building or a mixed residential / non-residential use building, from that portion of a rear lot line abutting a residential zone		7.5 m
	(iii) for a residential use building		7.5 m

	(iv) other cases	No minimum
(f) Maximum building height		18 m
(g) Maximum floor space index		2
(h) Minimum width of landscaped area	(i) abutting a street	3 m
	(ii) abutting a residential or institutional zone	3 m
	(iii) other cases	No minimum
(i) minimum width of landscaped area around a parking lot		<i>see Section 110 – Landscaping Provisions for Parking Lots</i>
(4)	Storage must be completely enclosed within a building.	
(5)	For other applicable provisions, see Part 2 – General Provisions, Part 3 – Special Use provisions, and Part 4 – Parking and Loading Provisions. (By-law 2016-249)	

The proposed Site Plan is in compliance with the zoning standards of the GM zone Section 187-188 and all other relevant General and Parking provisions.



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