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1795 Montreal Road

Planning Rationale



Engineering excellence. Planning precision. Inspired landscapes.

1795 Montreal Road

Planning Rationale

In support of

**Zoning By-law Amendment &
Site Plan Control Applications**

Prepared By:

NOVATECH

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Ottawa, Ontario
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March / 29 / 2018

Novatech File: 116151
Ref: R-2018-041

March 29, 2018

City of Ottawa
Planning Infrastructure and Economic Development Department
110 Laurier Avenue West, 4th Floor
Ottawa, ON
K1P 1J1

Attention: Michael Boughton, Planner III, Planning Services

Dear Mr. Boughton:

**Reference: Zoning By-law Amendment & Site Plan Control Applications
1795 Montreal Road
Our File No.: 116151**

The following Planning Rationale has been prepared in support of a Zoning By-law Amendment application and a Site Plan Control application to facilitate the development of the property at 1795 Montreal Road (the "Subject Property"). 1795 Montreal Road is legally described as Lot 27, Plan 462, Gloucester.

The Subject Property is designated Arterial Mainstreet on Schedule B of the City of Ottawa's Official Plan. The property is zoned Residential First Density, Subzone AA (R1AA) in the City of Ottawa's Zoning By-law 2008-250.

This Planning Rationale examines the location and context of the Subject Property, provides a design statement, details the proposed Zoning By-law Amendment, provides the planning policy and regularity framework of the site, and makes a recommendation on the Zoning By-law Amendment and proposed development.

Should you have any questions regarding any aspect of this Planning Rationale, please feel free to contact either Murray Chown or the undersigned.

Yours truly,

NOVATECH

Ryan Poulton
Ryan Poulton, M.PL.
Planner

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1.0 INTRODUCTION

Novatech has prepared this Planning Rationale in support of a Zoning By-law Amendment application and a Site Plan Control application for the property at 1795 Montreal Road (the “Subject Property”). The Subject Property is designated Arterial Mainstreet on Schedule B (Urban Policy Plan) of the City of Ottawa’s Official Plan (OP). The Subject Property is zoned Residential First Density, Subzone AA (R1AA) in the City of Ottawa’s Zoning By-law 2008-250. The Zoning By-law Amendment and Site Plan Control applications will facilitate the development of the currently vacant lot. The proposed Zoning By-law Amendment will rezone the Subject Property to Arterial Mainstreet (AM). Site-specific zoning provisions are requested to reduce the maximum permitted building height, and to reduce the number of required parking spaces for the proposed development.

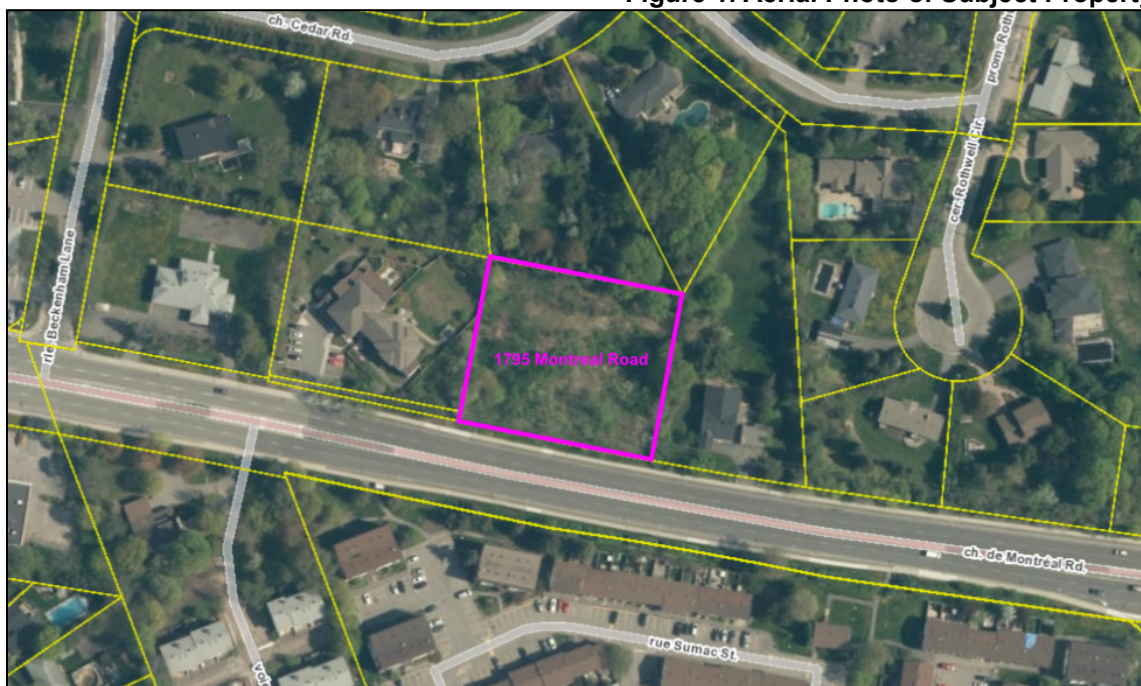
This Planning Rationale will demonstrate that the proposed Zoning By-law Amendment and development will:

- Be consistent with the policies of the Provincial Policy Statement (2014);
- Conform to the policies of the City of Ottawa Official Plan (up to and including Official Plan Amendment 180 (2016));
- Establish appropriate zoning standards for the Subject Property; and
- Is compatible with surrounding uses.

1.1 Site Location and Context

1795 Montreal Road is located in the Beacon Hill – Cyrville Ward in the City of Ottawa, on the north side of Montreal Road (see Figure 1). The Subject Property is legally described as Lot 27, Plan 462, Gloucester, in the City of Ottawa. The property has approximate area of 4,184 m², and approximately 70 metres of frontage along Montreal Road. The property is currently vacant.

Figure 1. Aerial Photo of Subject Property

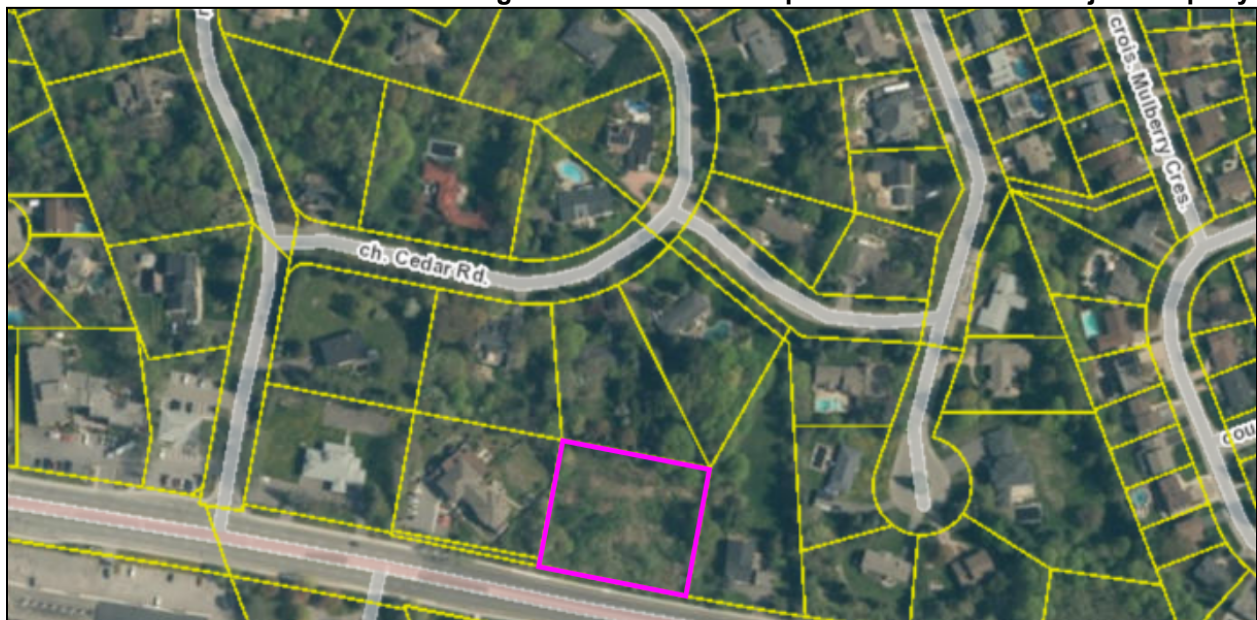


A right-of-way measured from the centreline of Montreal Road of 18.75 metres is to be protected in accordance with Annex 1 of the OP. A hydro power line runs along the north side of Montreal Road and across the frontage of the Subject Property. All new buildings and structures on the Subject Property will be required to be setback from the power line. The extent of the required road widening will be determined in coordination with the City of Ottawa through the Site Plan Control process.

1.2 Surrounding Uses

North: North of the Subject Property are large residential properties that accommodate single-detached dwellings. These residential lots are irregularly shaped. Houses are typically setback from the road with large front yards.

Figure 2. Residential Properties North of the Subject Property



South: Across Montreal Road to the south is a planned unit development including low-rise apartment buildings and townhouses. Further south is Rothwell United Church and Elmridge Park.

Figure 3. View South of the Subject Property on Montreal Road



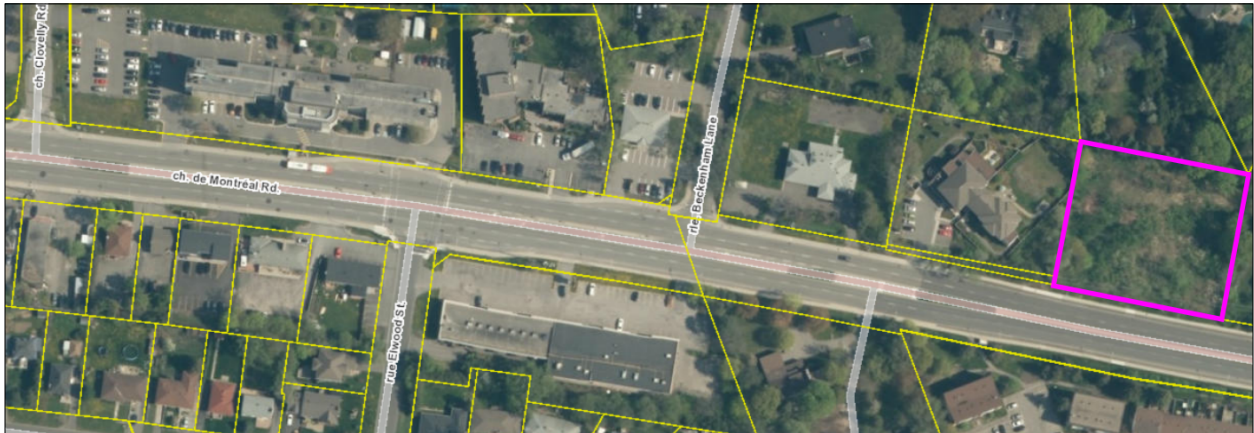
East: Immediately east of the Subject Property is a single-detached dwelling. Further east is a mix of low-rise and high-rise residential buildings that do not address Montreal Road. A three-storey retirement home is located at the intersection of Montreal Road and Elmsmere Road.

Figure 4. View looking east along Montreal Road



West: Immediately west of the Subject Property is an institutional building accommodating the Montfort Renaissance and the Ottawa Withdrawal Management Centre. There are commercial properties and a mid-rise residential care facility further west along Montreal Road.

Figure 5. Development along Montreal Road



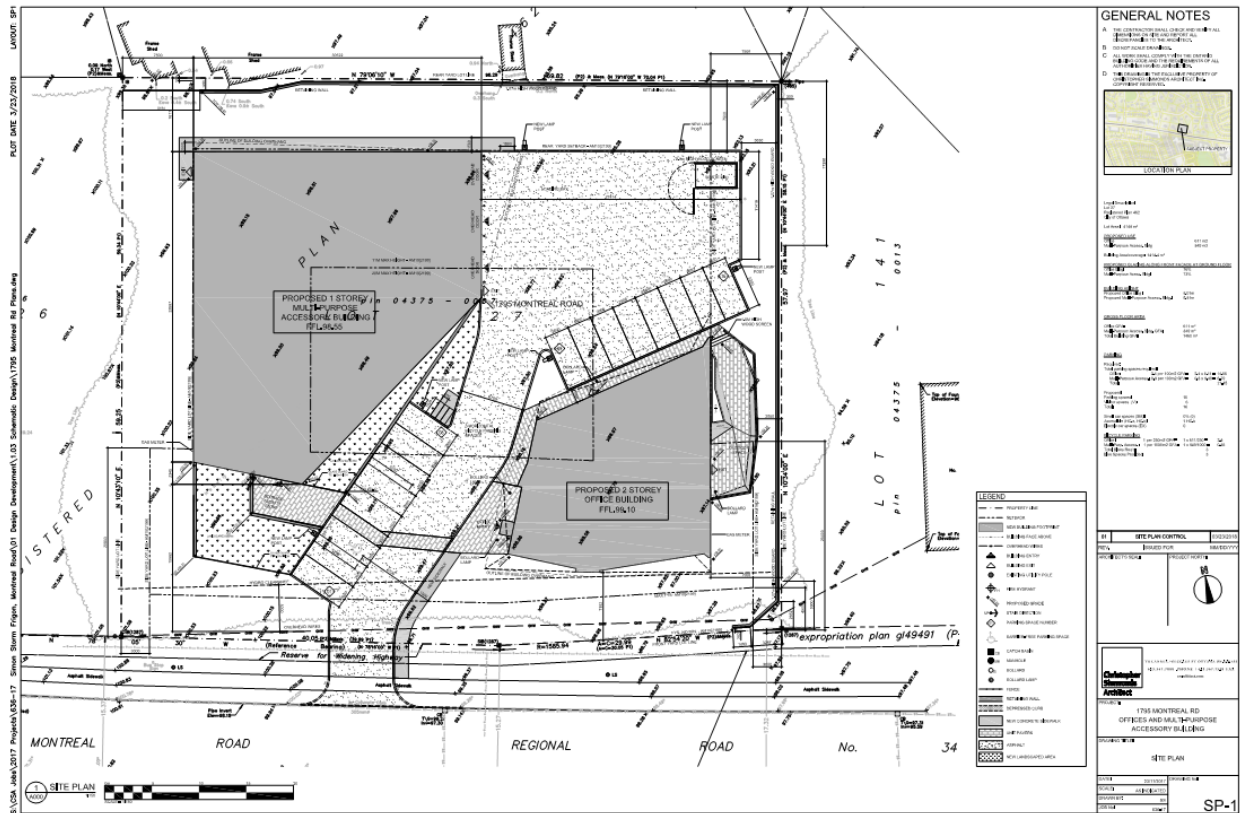
2.0 PROPOSED DEVELOPMENT

1795 Montreal Road is currently vacant. The Subject Property is proposed to be developed to accommodate a two-storey office building and a one-storey multi-purpose accessory building (see Figure 6). The office building will have a gross floor area of 611 m², and the accessory building will have a gross floor area of 849m².

Given the building setback requirements from the power lines along the front of the property, the office building and accessory building must be set back from the front property line. A 6.7-metre-

wide driveway off of Montreal Road is proposed to provide access to surface parking and loading spaces on the Subject Property. Sixteen surface parking spaces are proposed on the site. Eight parking spaces are located between the office building and multi-purpose accessory building. The remaining eight parking spaces are located north of the office building. Three loading spaces are located within the accessory use building.

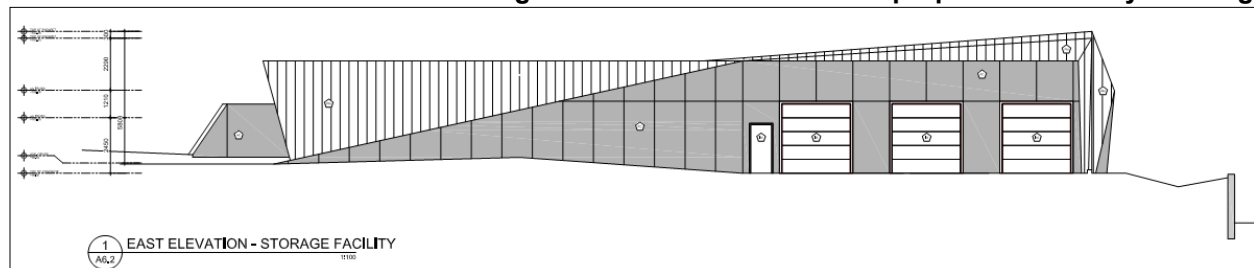
Figure 6. Site Plan



2.1 Multi-purpose Accessory Building

The proposed buildings are designed for a company that specializes in the repair and refurbishment of household furniture and furnishings after fire and flood hazards. A multi-purpose accessory building is required in support of the office building (see Figure 7). This building will accommodate three loading spaces, a show room for display purposes, closed-in storage bays, staff locker rooms, and a cleaning room, with the remaining area uses as flexible work space.

Figure 7. East Elevation – Multi-purpose Accessory Building



3.0 DESIGN BRIEF

3.1 CONTEXT

1795 Montreal Road is designated Arterial Mainstreet on Schedule B of the OP and is located in the community of Rothwell Heights/Beacon Hill North. The property is located along the north side of Montreal Road and east of Blair Road. The Subject Property is surrounded by a broad mix of low rise uses including Residential (R1AA) to the north and east, Multiunit Residential (R4Z) to the south, and Mixed use (AM10) and Minor Institutional (I1) to the west. Montreal Road is a 4-lane arterial mainstreet. The properties along Montreal Road in this area have relatively large lot sizes and include trees and vegetation.

3.2 DESIGN PROPOSAL

Architecture

The overall vision of the design is responsive to these main factors:

- A need for compatibility with the existing low rise neighbourhood fabric
- A built form that is integrated with the landscape to maintain a low profile that is suited to its natural surroundings
- The buildings' functional use
- The need to visually address Montreal Road

This project encompasses two mid-size buildings on one site – a 2-storey office building as well as a single storey multi-purpose accessory building. The overall site design strategy includes locating the two buildings in a way that they frame the site with the traffic held in between. The more public, dynamic, and glazed areas face Montreal Road. Parking is located in between the two proposed buildings, which contains the facility functions deep within the site. It avoids parking becoming a buffer between the street and the building, or being placed in the rear yard adjacent to the residential property.

Although the structures stretch across the majority of the site, the design strategy minimizes the overall impact of the buildings on neighbouring properties. Taking advantage of the inherent slope on the property, the overall massing concept centers around the design of a landscape oriented form. The proportion of these forms, as well as their material design highlights the office building while allowing the multi-purpose accessory building to blend into the landscape.

The two buildings, with their sloped surfaces, create a formal landscape that frame the property. Although the forms are prominent, the use of natural materials works to retain a softness and connection to the surrounding natural context.

While the office building is 2 storeys and located as close to the street as possible (it is limited by hydro lines), the single level multi-purpose accessory building is located further toward the rear of the site and utilizes some of the natural slope on the site to keep it low (site drainage ultimately determines the floor level of this building). The grade is built up along the front edge and buries some of the mass into the earth. The form and the composition of the materials along the north and west facades streamline it and further emphasize its low profile.

Figure 8. Rendering looking North



Figure 9. Rendering looking South



3.3 MATERIAL PALETTE

This proposal uses a simple and natural palette of standing seam metal panel, wood, and concrete. The standing seam metal panel is used on the roof as well as an exterior cladding material and creates a smooth transition between sloped exterior building elements. The wood is located at strategic common areas at the ground level where it can give some warmth, such as the main entrance and staff terrace. The concrete and concrete panels are used in areas to both emphasize and break up the building form and keep the overall appearance light. This natural and simple material palette, in conjunction with concept for the building form, integrates the building into the landscape and keeps it very much grounded.

4.0 PROPOSED ZONING BY-LAW AMENDMENT

1795 Montreal Road is currently zoned Residential First Density, Subzone AA (R1AA) in the City of Ottawa Zoning By-law 2008-250. The Zoning By-law Amendment application proposes to rezone the property to Arterial Mainstreet (AM). The proposed development will not be in conformity with all provisions of the Zoning By-law 2008-250. Site-specific zoning provisions are requested. The proposed site-specific zoning provisions are detailed below.

Proposed Site-specific Provisions

- Maximum building height - 11 metres.
- No parking spaces are required for uses located within an accessory building.

The requested site-specific provisions are considered appropriate for the Subject Property. The maximum building height of 11 metres results in development that is compatible with surrounding low-rise residential properties. The accessory building is designed to support the principle office use of the property. Ten employees are anticipated to work in the office building, and two to five employees are anticipated to work in the accessory building. The proposed 16 parking spaces meets the expected demand for parking generated by the proposed development. The proposed site-specific zoning provisions are discussed in Section 5.4 of this Planning Rationale. The proposed wording for the Zoning By-law Amendment is attached as Appendix A.

5.0 PLANNING POLICY AND REGULATORY FRAMEWORK

5.1 Provincial Policy Statement

The 2014 Provincial Policy Statement (PPS) provides policy direction on land use planning and development matters of provincial interest. The PPS was issued under the authority of Section 3 of the Planning Act and came into effect on April 30, 2014. All decisions affecting planning matters “*shall be consistent with*” policies issued under Section 3 of the Planning Act.

Section 1 of the PPS speaks to managing and directing land use to achieve efficient and resilient development and land use patterns.

“1.1.1 Healthy, livable and safe communities are sustained by:

- a) promoting efficient development and land use patterns which sustain the financial well-being of the Province and municipalities over the long term;*

- b) accommodating an appropriate range and mix of residential (including second units, affordable housing and housing for older persons), employment (including industrial and commercial), institutional (including places of worship, cemeteries and long-term care homes), recreation, park and open space, and other uses to meet long-term needs;*
- c) avoiding development and land use patterns which may cause environmental or public health and safety concerns;*
- d) avoiding development and land use patterns that would prevent the efficient expansion of settlement areas in those areas which are adjacent or close to settlement areas;*
- e) promoting cost-effective development patterns and standards to minimize land consumption and servicing costs;*
- f) improving accessibility for persons with disabilities and older persons by identifying, preventing and removing land use barriers which restrict their full participation in society;*
- g) ensuring that necessary infrastructure, electricity generation facilities and transmission and distribution systems, and public service facilities are or will be available to meet current and projected needs; and*
- h) promoting development and land use patterns that conserve biodiversity and consider the impacts of a changing climate.”*

The proposed Zoning By-law Amendment and Site Plan Control application will facilitate the efficient economic development of a vacant parcel fronting onto an Arterial Mainstreet in the City of Ottawa. The proposed development will generate jobs that will contribute to the sustained financial well-being of the City of Ottawa over the long term. The proposed office building will add to the mix of uses along Montreal Road. The proposed development will not create public health and safety concerns, nor will it prevent the efficient expansion of settlement areas, as the Subject Property is located within the urban area of Ottawa and is surrounded by existing development. The development of the vacant property will promote cost-effective development patterns and minimize land consumption and servicing costs in the community.

Section 1.3 of the PPS includes policies on employment. Policy 1.3.1 states:

“Planning authorities shall promote economic development and competitiveness by:

- a) providing for an appropriate mix and range of employment and institutional uses to meet long-term needs;*
- b) providing opportunities for a diversified economic base, including maintaining a range and choice of suitable sites for employment uses which support a wide range of economic activities and ancillary uses, and take into account the needs of existing and future businesses;*
- c) encouraging compact, mixed-use development that incorporates compatible employment uses to support liveable and resilient communities; and*

- d) ensuring the necessary infrastructure is provided to support current and projected needs.”*

The proposed Zoning By-law Amendment and Site Plan Control application will permit the development of an office building on the Subject Property. The proposed development will add to the mix of employment uses, and contribute to maintaining a wide range of economic activities, along Montreal Road. The proposed office building and multi-purpose accessory building have been designed to be compatible with surrounding residential and institutional uses. In support of the proposed development, a Development Servicing and Storm Water Management Report has been prepared by Novatech, dated March 9, 2018.

Section 1.7 of the PPS includes policies on long-term economic prosperity. The relevant policies in Section 1.7.1 are discussed below:

“Long-term economic prosperity should be supported by:

- a) promoting opportunities for economic development and community investment-readiness;*
- c) maintaining and, where possible, enhancing the vitality and viability of downtowns and mainstreets;*
- d) encouraging a sense of place, by promoting well-designed built form and cultural planning, and by conserving features that help define character, including built heritage resources and cultural heritage landscapes;*
- i) promoting energy conservation and providing opportunities for development of renewable energy systems and alternative energy systems, including district energy;”*

The proposed Zoning By-law Amendment and Site Plan Control application is an opportunity to encourage economic development along Montreal Road. The office building and multi-purpose accessory building will enhance the vitality and viability of Montreal Road as an arterial mainstreet. The high-quality design and orientation of the office building to address Montreal Road encourages a sense of place. The office building will incorporate a cool roof. The cool roof promotes energy conservation by reducing the energy requirements of the building during the summer months, and by reducing the heat island effect of the proposed development.

Section 2.0 of the PPS provides policies related to the use and management of resources. The proposed rezoning adheres to the policies in Section 2.0 as follows:

- Relating to Section 2.1 (Natural Heritage), there are no natural features on the Subject Property.
- Relating to Section 2.2 (Water), there are no water features on the Subject Property;
- Relating to Section 2.3 (Agriculture), the Subject Property is within the urban area of the City of Ottawa. There are no lands designated Agricultural Resource Area near the Subject Property;
- Relating to Section 2.4 (Minerals and Petroleum), the Subject Property has no known areas of mineral or petroleum potential;

- Relating to Section 2.5 (Mineral Aggregate Resources), there are no lands on or near the Subject Property designated Mineral Aggregate Resource Areas;
- Relating to Section 2.6 (Cultural Heritage and Archaeology), the Subject Property is identified as having possible archaeological potential, as shown on the City of Ottawa Archaeological Potential mapping. The PPS requires that if any archaeological resources are discovered, the resources are conserved by removal and documentation;

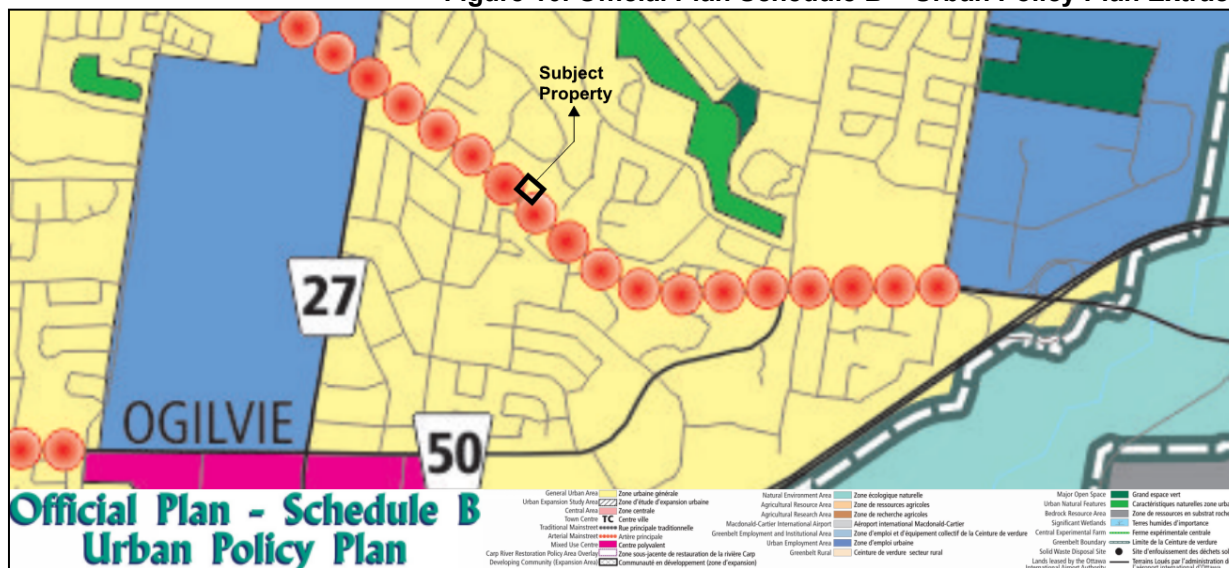
The proposed Zoning By-law Amendment and Site Plan Control applications are consistent with the Provincial Policy Statement. Approval of the requested zoning provisions and Site Plan Control application will have no negative impacts to natural heritage and features or cultural heritage resources. The proposed development is consistent with the policies of the PPS.

5.2 City of Ottawa Official Plan

The City of Ottawa Official Plan was adopted by City Council on May 14, 2003 and modified by the Minister of Municipal Affairs on November 10, 2003. There have been numerous modifications and amendments approved by City Council and the Ontario Municipal Board. For the purpose of this Planning Rationale, the annotated online version of the City of Ottawa Official Plan, showing the changes as per OPA 180 (the “Official Plan”) was used for reference.

1795 Montreal Road is located within the urban area of the City of Ottawa. The Subject Property is designated Arterial Mainstreet on Schedule B of the Official Plan (see Figure 10). Section 3.6.3 of the Official Plan includes policies for Mainstreets.

Figure 10. Official Plan Schedule B – Urban Policy Plan Extract



Policy 1 in Section 3.6.3 of the OP states:

“The latter (arterial mainstreets) also are planned to provide a mix of uses and have the potential to evolve, over time, into more compact, pedestrian-oriented and transit friendly places. To facilitate this evolution, the Zoning By-law may

define the portion of the street frontage of an Arterial Mainstreet to be occupied by buildings located at or set back minimally from the sidewalk. Both Traditional and Arterial Mainstreets will fulfill and take advantage of their multi-modal transportation corridor function.”

The proposed Zoning By-law Amendment will permit the development of an office building and accessory building on the currently vacant Subject Property. The proposed development will add to the mix of uses, and will be pedestrian-oriented and transit friendly. Employees and visitors to the site will have easy access to sidewalks and the bus stop on Montreal Road. The buildings will be located as close to Montreal Road as possible given the required setback from the existing overhead hydro line that runs east-west across the frontage of the Subject Property.

Policy 5 in Section 3.6.3 states:

“A broad range of uses is permitted on Traditional and Arterial Mainstreets, including retail and service commercial uses, offices, residential and institutional uses. Uses may be mixed in individual buildings or occur side by side in separate buildings.”

The proposed Zoning By-law Amendment and Site Plan Control application will facilitate the development of an office building and multi-purpose accessory building on the Subject Property. These buildings will help provide a broad range and mix of uses along Montreal Road, consistent with the arterial mainstreet designation.

Policy 9 in Section 3.6.3 states: *“On Arterial Mainstreets, the location of surface parking will be evaluated in the context of Section 2.5.1 and Section 4.11.”* Section 2.5.1 and 4.11 of the Official Plan are discussed in the flowing sections of this Planning Rationale.

Policy 10 in Section 3.6.3 states:

“Redevelopment and infill are encouraged on Traditional and Arterial Mainstreets in order to optimize the use of land through intensification, in a building format that encloses and defines the street edge with active frontages that and provides direct pedestrian access to the sidewalk.”

The proposed Zoning By-law Amendment and Site Plan Control application will allow intensification along Montreal Road through infill development of a currently vacant parcel. Walkways on the Subject Property will provide direct access to the proposed buildings from the public sidewalk.

Policy 11 in Section 3.6.3 states:

“On Arterial Mainstreets, unless a secondary plan states otherwise, building heights up to 9 storeys will predominate. The tallest buildings will be located at the nodes described below. Subject to a zoning amendment, taller buildings up to a maximum of 12 storeys may be considered, where the development provides a community amenity, where it is demonstrated that the development meets the urban design and compatibility policies in Section 4.11 and where the site is located at one or more of the following nodes:

- a. *within 400 metres walking distance of a Rapid Transit Station on Schedule D of this Plan; or*
- b. *directly abutting an intersection of the Mainstreet with another Mainstreet or a Transit Priority Corridor on Schedule D of this Plan; or*
- c. *directly abutting a Major Urban Facility.*

The Zoning By-law may establish building heights lower than nine storeys based on site conditions, existing character and compatibility.”

The Zoning By-law Amendment proposes to limit the maximum permitted building height on the Subject Property to 11 metres. The requested 11-metre building height is appropriate for the Subject Property based on the character of development along Montreal Road, and proximity to adjacent low-rise/low density residential uses. The apartment buildings along Montreal Road across from the Subject Property are 3.5 stories. There are one and two-storey residential dwellings north and east of the Subject Property. Based on the scale of adjacent residential uses, a maximum permitted building height of 11 metres is appropriate for the development of the Subject Property.

Policy 7 in Section 3.6.3 of the OP states:

“development proposals on Mainstreets will be evaluated in the context of the policies in this section and the Design Objectives and Principles in Section 2.5.1, and the Compatibility policies set out in Section 4.11.”

The policies of Section 2.5.1 and 4.11 of the OP are discussed below.

5.2.1 Designing Ottawa

Section 2.5.1 of the Official Plan sets out design objectives for how the City wants to influence the built environment as the City matures and evolves. The design objectives are broadly applied to all plans and development in the City of Ottawa. These design objectives are addressed below.

- To enhance the sense of community by creating and maintaining places with their own distinct identity.

The proposed development is designed to a high standard with quality materials incorporated into the façade of the office building and oriented towards Montreal Road. The proposed development will create a location along Montreal Road with a distinct identity.

- To define quality public and private spaces through development.

The front yard is proposed to be landscaped with deciduous trees, sod, shrubs, grasses, and hard landscaping (see Figure 11). The landscaping will define a quality private front yard between the office building and the public sidewalk along Montreal Road.

- To create places that are safe, accessible and are easy to get to, and move through.

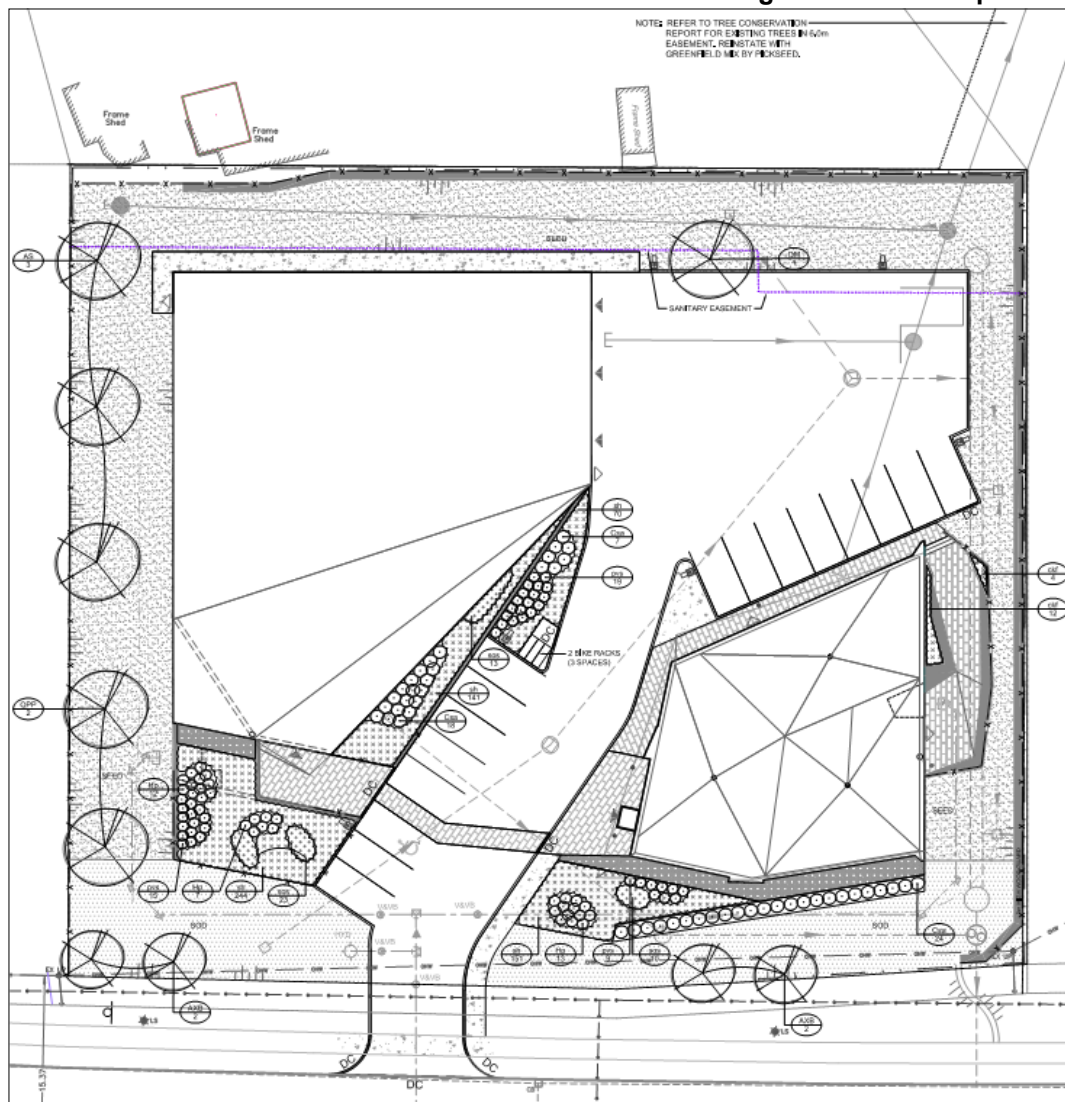
A driveway is proposed off Montreal Road to provide access to 16 parking spaces. The public sidewalk will continue across this driveway. A walkway will extend from the public sidewalk to the main entrance of the office building. This walkway will wrap around the office building to provide access to a second entrance on the east side of the building. A walkway will also extend across the driveway to provide access to the front entrance of the multi-purpose accessory building.

- To ensure that new development respects the character of existing areas.

The existing development along Montreal Road near the Subject Property does not address the road, or is hidden from view by trees and landscaping. The proposed development will address Montreal Road and provide a high quality landscaped front yard. The proposed development will respect and enhance the existing character of Montreal Road.

- To consider adaptability and diversity by creating places that can adapt and evolve easily over time and that are characterized by variety and choice.

Figure 11. Landscape Plan



The proposed Zoning By-law Amendment will permit a wide range of uses consistent with the Arterial Mainstreet designation of the Subject Property. The proposed zoning will allow development on the Subject Property to adapt and evolve over time. The proposed development will locate a new office use along Montreal Road.

- To understand and respect natural processes and features in development design.

No natural processes or significant natural features were identified as part of the due diligence process prior to the design of the proposed office building and multi-purpose accessory building.

- To maximize energy-efficiency and promote sustainable design to reduce the resource consumption, energy use, and carbon footprint of the built environment.

A cool roof is included in the design of the office building. The cool roof will consist of reflective materials which will keep the office building cooler in summer months. The cool roof maximizes energy-efficiency and promotes sustainable design by reducing the energy requirements of the office building, and by reducing the heat island effect of the proposed development.

The proposed Zoning By-law Amendment and development are consistent with the design objectives in Section 2.5.1 of the Official Plan.

5.2.2 Urban Design and Compatible Development

Section 4.11 of the Official Plan discusses the relationship between new and existing development and provides policies to evaluate the compatibility of a development with regard to Views, Building Design, Massing and Scale, High-Rise Buildings, Outdoor Amenity Areas, Public Art, and Design Priority Areas.

Views - The proposed development is not a high-rise building and is not within any of the viewshed areas directly addressed in Section 4.11.

Building Design - Policy 5 of Section 4.11 requires proponents of new development to demonstrate how the design of their development fits with the existing desirable character and planned function of the surrounding area in the context of:

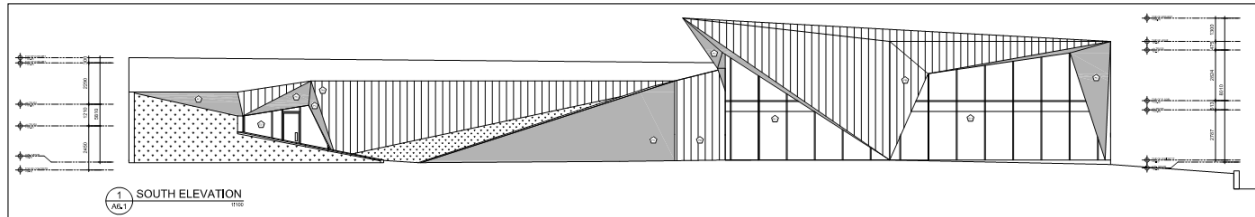
- a) Setbacks, heights and transition;*
- b) Façade and roofline articulation;*
- c) Colours and materials;*
- d) Architectural elements, including windows, doors and projections;*
- e) Pre- and post-construction grades on site; and*
- f) Incorporating elements and details of common characteristics of the area.*

The requested 11 metres maximum building height conforms with the maximum building height permitted in the AM zone in an area up to 20 metres from a property line abutting an R1 zone. The requested 11-metre maximum building height will permit development that is compatible with adjacent low-rise residential uses.

The front façade of the office building is designed using a variety of materials including glass. The office building will incorporate a flat roof design (see Figure 12). The design of the proposed office

building is compatible with existing adjacent residential uses, and fits the planned function of Montreal Road as an arterial mainstreet.

Figure 12. South Elevation – Office Building



Policy 6 of Section 4.11 requires that all applications for new development:

- a) *Orient the principal façade and entrance(s) of main building(s) to the street. Where a building abuts more than one street, the building façade and entrances will be oriented in order of priority to the arterial, collector and local street;*
- b) *Include windows on the building elevations that are visible from public spaces;*
- c) *Use architectural elements, massing, and landscaping to accentuate main building entrances.*

The principle façade of the office building is oriented toward Montreal Road. The primary entrance to the office building is on the western elevation, and is easily accessible from the public sidewalk. The front façade of the office building incorporates glass that will be visible from Montreal Road. A projecting roofline provides shelter for people using the front entrance. The roof line and front yard landscaping accentuates the main entranceway to the office building.

Policy 8 of Section 4.11 discusses loading facilities, service areas, mechanical equipment (including roof-top), vents and metering devices. Three loading spaces are proposed on the Subject Property. These loading spaces are proposed within the multi-purpose accessory building and are not visible from Montreal Road. A garbage and recycling area is proposed in the northeast corner of the property and will be screened by fencing.

Massing and Scale – *“Massing and scale describes the form of the building, how tall it is, how much of the lot it occupies and how it is positioned in relation to the street and surrounding buildings.”* The proposed two-storey office building and one-storey accessory building are similar in height and massing to the surrounding residential dwellings along Montreal Road.

Policy 9 of Section 4.11 of the OP states: *“Where there are no established criteria provided in an approved Plan, the City will assess the appropriateness of the proposal relying upon its approved Design Guidelines, as applicable, and the following criteria:*

- a) *Building height, massing and scale permitted by the planned function of adjacent properties as well as the character established by the prevailing pattern of abutting development and development that is across the street;*
- b) *Prevailing patterns of rear and side yard setbacks, building separation and landscaped open spaces and outdoor amenity areas as established by existing zoning where that pattern is different from the existing pattern of development;*

- c) *The need to provide a transition between areas of different development intensity and scale as set out in policy 12 of this section.*

The Subject Property is not within a planning area of a Secondary Plan. The City's Urban Design Guidelines for Arterial Mainstreets are discussed in Section 5.4 of this Planning Rationale. Section 2.5.1 of the OP states: "*Planned function refers to a vision for an area which is established through a community design plan or similar Council-approved planning exercise, or the Zoning By-law.*" The Subject Property is designated Arterial Mainstreet on Schedule B of the OP. The Zoning By-law Amendment proposed to rezone the property to the Arterial Mainstreet zone, consistent with the Arterial Mainstreet designation. The proposed two-storey office building conforms to the building height, massing, and scale generally permitted within the Arterial Mainstreet zone.

7.5-metre-wide rear yard and interior side yards are proposed on the Subject Property. These yards are landscaped with soft landscaping. A screened garbage collection area and parking space are located within the easterly interior side yard. The proposed interior rear and side yards comply with the zoning provisions of the Arterial Mainstreet zone.

The requested 11 metre maximum building height will provide a transition between the proposed development and the low-rise residential development north and east of the Subject Property.

The City of Ottawa's "Urban Design Guidelines for Low-rise Infill Development" are discussed in Section 4.3 of this Planning Rationale.

High-Rise Buildings – The proposed development is not a high-rise building.

Outdoor Amenity Areas – Outdoor amenity areas are "*the private communal areas of a property that are designed to accommodate a variety of leisure activities.*" There are residential properties north and east of the Subject Property. The proposed buildings are setback from the existing property lines, providing a 7.5-metre-wide rear yard and interior side yards. A wooden fence is proposed along the rear lot line and easterly interior side lot line to screen parking spaces, loading spaces, and the garbage and recycling area from the adjacent residential properties.

Public Art – The proposed development is not a Major Urban Facility or High-Rise Building, and will not include site-specific public art.

Design Priority Areas – The Subject Property is within a design priority area. The design of the building and site must be consistent with the design priority area policies in Section 4.11 of the OP.

Policy 18 of Section 4.11 of the OP states:

"18. The portion of the building(s) which are adjacent to the public realm will be held to the highest building design standards by incorporating specific building design features:

- a. Design the building(s) first storey to be taller in height to retain flexibility or opportunity for ground floor uses in the future;*
- b. Locate front building façades parallel to the street; however, consideration may be given to allow for interruptions of continuous building facades at*

- strategic locations to provide pocket parks, plazas or other open spaces that provide a supportive function to the street activity or enable views and vistas;*
- c. Transparent windows at grade to give views into the building to observe the function of the building and out of the building to enhance natural surveillance;*
 - d. Using architectural treatments (e.g. projections from continuous building lines, awnings, canopies, alcoves and bays) to soften the interface between buildings and the public realm;*
 - e. Sufficient lighting sources for public uses after dark and to accentuate and animate buildings, natural features, public monuments and public spaces;*
 - f. Utilize façade treatments to accentuate the transition between floors and interior spaces to provide visual interest and relief; and*
 - g. Signage that contributes to the character of the surrounding area and architectural design of the building through appropriate architectural design elements, materials, and colour.”*

The Office building has been designed to the highest standards and incorporates a variety of different building materials including transparent glass and architectural details. The front façade of the office building is parallel to Montreal Road. The front façade of the office building is constructed with transparent glass. The roof line has been designed to give architectural detail to the building. There is a clear transition in the façade between the first floor and second floor of the office building.

Due to the significant grade change on site, the proposed one-storey multi-purpose accessory building has been designed to not be visible from Montreal Road. The front façade of the multi-purpose accessory building blends in with surround landscaping to minimize the visual impact of the building on the street.

Policy 19 of Section 4.11 of the OP states:

“The portion of the development which impacts the public realm will be held to the highest site design standards and should incorporate enhanced public realm improvements, such as:

- d. weather protection elements, (e.g. colonnades, and awnings);*
- e. shade trees, median planting and treatments and other landscaping;*
- f. wider sidewalks and enhanced pedestrian surfaces;*
- g. coordinated furnishings and utilities, transit stops, and decorative lighting;*
and
- h. memorials and public art commissioned for the location.*

To achieve these public realm improvements, coordination with the City will be required in accordance with Section 2.5.1, policy 5(d).”

The proposed development includes a landscaped front yard between the office building and the public sidewalk. The roof line overhangs the main entranceway to the office building and provides weather protection. Deciduous trees, sod, grasses and hard landscaping are proposed in the front yard. The public sidewalk will be maintained across the Subject Property along Montreal Road.

Policy 20 of Section 4.11 of the OP states:

“The massing and scale of development will define and enclose public and private spaces (e.g. streets, parks, courtyards, squares) using buildings, structures and landscaping; and relate to the scale and importance of the space they define (e.g. street width to height ratios).”

The two-storey front façade of the office building will define the extent of the front yard of the property. The front yard will be landscaped with a mix of soft and hard landscaping, including deciduous trees.

The proposed development is consistent with criteria and policies of Section 4.11 of the Official Plan.

5.2.3 Review of Development Applications

Section 4 of the City of Ottawa’s Official Plan outlines the policies used to review development applications. These policies ensure that development applications meet the objectives contained on the Official Plan. The appropriate policies and related studies and plans were identified through a pre-application consultation meeting with the City at the beginning of the design and review process.

Required studies and plans were identified as relevant and have been prepared in support of the proposed development. Detailed and technical information can be obtained by reviewing the respective documents.

Relating to Section 4.3 – Walking, Cycling, Transit, Road and Parking Lots:

Section 4.3 states that the City will utilize transportation studies to assess the adequacy of the transportation network to meet the needs of the proposed development. A Transportation Impact Assessment Report has been prepared by Novatech, dated March 2018, for the Subject Property. The Transportation Impact Assessment Report concludes all applicable and required TDM-supportive design and infrastructure measures in the TDM checklist are met, the available sight distances are sufficient to meet the TAC guidelines, and the outbound auto movements for the proposed access is anticipated to meet the AM and PM peak hours target for an Arterial Mainstreet. The Transportation Impact Assessment Report conforms to the relevant policies in Section 4.3 of the Official Plan.

Relating to Section 4.4 – Water and Wastewater Servicing:

Section 4.4.1 states that the City will require development applications to be supported by an assessment of the adequacy of public services. A Development Servicing and Storm Water Management Report as well as a Site Servicing Plan and a Grading, Drainage, and Sediment Control Plan have been prepared by Novatech, dated March 9, 2018, for the Subject Property. The Development Servicing and Storm Water Management Report concludes *“there are no major obstacles to servicing the proposed development. It is recommended that the proposed site servicing and stormwater management design be approved for implementation.”* The report and plans conform to the relevant policies in Section 4.4.1 of the OP.

Relating to Section 4.7 – Environmental Protection:

Policy 1 of Section 4.7.2 requires applications for Site Plan Approval will be supported by a Tree Conservation Report and Landscape Plan. A Tree Conservation Report and Landscape Plan have been prepared by Novatech, dated March 29, 2018. The Tree Conservation Report and Landscape Plan conform to the relevant policies in Section 4.7.2 of the OP.

Section 4.7.6 states the City will require that stormwater site managements plans be submitted in accordance with the guidance set out in the environmental management, subwatershed and watershed plans. A Development Servicing and Storm Water Management Report, a Site Servicing Plan, and a Grading, Drainage, and Sediment Control Plan have been prepared by Novatech, dated March 9, 2018, for the Subject Property. The Development Servicing and Storm Water Management Report concludes *“there are no major obstacles to servicing the proposed development. It is recommended that the proposed site servicing and stormwater management design be approved for implementation.”* The report and plans conform to the relevant policies in Section 4.7.6 of the OP.

Relating to Section 4.8 – Protection of Health and Safety:

Policy 1 in Section 4.8.3 states that site plan applications shall be supported by a geotechnical study to demonstrate that the soils on site are suitable for development. In support of the proposed development, a Geotechnical Investigation has been prepared by Houle Chevrier Engineering, dated September 6, 2017.

Section 4.8.4 states that the identification of potentially contaminated sites is important in the planning application review process and that the City will utilize available information to help ensure that development takes place only on sites where the environmental conditions are suitable for the proposed use of the site. A Phase I Environmental Site Assessment has been prepared by exp Services Inc., dated September 29, 2016. The Phase I Environmental Site Assessment concludes *“exp has not identified any areas of potential environmental concern. No additional work is recommended at this time.”*

Section 4.8.7 states that the City will require a noise study where new noise sensitive development is proposed within 100 metres of an existing arterial roadway. An Environmental Noise Assessment has been prepared by Gradient Wind Engineering Inc., dated March 29, 2018, for the Subject Property. The Environmental Noise Assessment concludes *“Since the noise levels fall below ENCG criteria, the proposed development is expected to be compatible with the existing and future noise sensitive land uses.”* The study was prepared consistent with the MOECC NPC 300, and the City’s Environmental Noise Control Guidelines.

5.3 Urban Design Guidelines for Arterial Mainstreets

The proposed development at 1795 Montreal Road is an infill project on a vacant lot fronting onto an Arterial Mainstreet. The City of Ottawa’s Urban Design Guidelines for Arterial Mainstreets help guide infill projects proposed for properties designated Arterial Mainstreet. The proposed development has been designed to incorporate relevant design guidelines where possible.

Streetscape

Guideline 2: *Provide or restore a 2.0 metre wide unobstructed concrete sidewalk.*

A 1.5-metre-wide sidewalk is currently located along the north side of Montreal Road. This sidewalk is on municipally owned land between Montreal Road and the Subject Property.

Guideline 4: *Use buildings, landscaping and other streetscape elements to create continuous streetscapes.*

The landscaping proposed between the office and accessory buildings and Montreal Road includes deciduous trees and shrubs, sod and grasses, perennials, and river rock. These landscaping elements and materials, and the design of the front façade of the office building will contribute to the streetscape along Montreal Road.

Built Form

Guideline 7: *Design new development to be compatible with the general physical character of adjacent neighbourhoods. Protect the positive elements of the existing fabric including significant buildings, existing trees, pedestrian routes, public facilities and pedestrian amenities.*

Existing residential and commercial properties near the Subject Property along Montreal Road are set back from and do not face the arterial mainstreet. The proposed development improves upon existing development patterns along Montreal Road.

Guideline 11: *Create intensified, mixed-use development, incorporating public amenities such as bus stops and transit shelters, at nodes and gateways by concentrating height and mass at these locations.*

A bus stop is located at the southwestern corner of the Subject Property. The proposed office building will add to the existing commercial and residential uses along Montreal Road serviced by the bus stop.

Guideline 12: *Design the built form in relation to the adjacent properties to create coherent streetscapes.*

The institutional building and single-detached dwelling west and east of the Subject Property respectively are set back from Montreal Road and hidden from view by medium-sized trees and shrubs. The proposed office building respects the setback and building height of the adjacent properties and adds to the streetscape along Montreal Road.

Guideline 14: *Create a transition in the scale and density of the built form on the site when located next to lower density neighbourhoods to mitigate any potential impact.*

The Zoning By-law Amendment application proposes a building maximum height of 11 metres. The proposed maximum building height allows for an appropriate transition between the Subject Property and the adjacent institutional and low-rise residential properties.

Guideline 16: *Design richly detailed buildings that create visual interest, a sense of identity and a human scale along the public street.*

The proposed development has been designed to a high standard that is visually interesting, adds to a sense of identity, and is designed to a human scale along Montreal Road.

Guideline 18: *Use clear windows and doors to make the pedestrian level façade of walls, facing the street, highly transparent.*

The proposed office building has been designed using a variety of materials including glass to create a transparent façade facing Montreal Road.

Pedestrians and Cyclists

Guideline 20: *Provide direct, safe, continuous and clearly defined pedestrian access from public sidewalks to building entrances.*

New pedestrian walkways are proposed from the public sidewalk to both building entrances.

Vehicles and Parking

Guideline 27: *Locate surface parking spaces at the side or rear of buildings. Provide only the minimum number of parking spaces required by the Zoning By-law.*

Eight surface parking spaces are proposed at the rear of the office building, out of view of Montreal Road. The uses within proposed accessory building will not generate a demand for parking on the Subject Property. The Zoning By-law Amendment proposes no parking spaces be required for the uses within the accessory building.

Landscape and Environment

Guideline 33: *Plant trees away from the curb next to private property when the boulevard is narrower than 4.0 metres.*

Deciduous trees are proposed in the front yard of the Subject Property. These trees are setback from the northern curb along Montreal Road.

Guideline 35: *Provide a minimum 3.0 metre wide landscape area, which may include a solid wall or fence in addition to planting, at the edges of sites adjacent to residential or institutional properties.*

A minimum 3.0 metre wide landscaped area is proposed for the rear yard and both side yards. This minimum is exceeded for both the rear yard and western interior side yard.

Guideline 37: *Plant trees, shrubs and ground cover on any unbuilt portions of the site that are not required to meet minimum parking requirements. This includes any areas reserved for future phases of development.*

All unbuilt portions of the Subject Property are landscaped with a mix of hard and soft landscaping, including trees and shrubs.

Guideline 40: *Landscape areas between the building and the sidewalk with foundation planting, trees, street furniture, and walkways to the public sidewalk.*

The areas between the two buildings and the sidewalk are proposed to be landscaped with trees and a mix of hard and soft landscaping.

Guideline 41: *Provide a minimum 2.5 metre wide landscape area along the site's side and rear yards in order to provide screening and enhance environmental benefits.*

A minimum 3.0 metre wide landscaped area is proposed for the rear yard and both side yards.

Servicing and Utilities

Guideline 50: *Enclose all utility equipment within buildings or screen them from both the arterial mainstreet and private properties to the rear. These include utility boxes, garbage and recycling container storage, loading docks and ramps and air conditioner compressors.*

Three loading spaces are proposed on the Subject Property. These loading spaces are enclosed within the multi-purpose accessory building. A garbage collection area is proposed in the northeast corner of the Subject Property, away from Montreal Road. The garbage collection area will be screened from view by opaque wooden fencing.

The proposed zoning by-law amendment and site plan control application will facilitate the develop of a currently vacant lot to accommodate a new office building and multi-purpose accessory building. The proposed development is in keeping with the Urban Design Guidelines for new development along arterial mainstreets.

5.4 City of Ottawa Zoning By-law 2008-250

The Zoning By-law Amendment proposes to rezone 1795 Montreal Road from Residential First Density Subzone AA (R1AA) to Arterial Mainstreet (AM) Zone. Rezoning the Subject Property will facilitate the construction of a two-storey office building and one-storey multi-purpose accessory building, and 16 parking spaces. The purpose of the Arterial Mainstreet Zone is to:

- “(1) accommodate a broad range of uses including retail, service commercial, offices, residential and institutional uses in mixed-use buildings or side by side in separate buildings in areas designated **Arterial Mainstreet** in the Official Plan; and*
- (2) impose development standards that will propose intensification while ensuring that they are compatible with the surrounding uses.”*

The proposed Zoning By-law Amendment will permit the development of an office building and accessory building, consistent with the Arterial Mainstreet designation of the Subject Property. The Zoning By-law Amendment application and Site Plan Control application will promote intensification of a vacant parcel while ensuring development is compatible with surrounding low-rise residential uses.

The proposed development conforms to the minimum lot width, minimum lot area, and minimum yard setback provisions of the AM Zone. A site-specific zoning provision is requested to limit the maximum permitted building height to 11 metres. The maximum permitted building height in the AM Zone for land within 20 metres of an Residential First Density Zone is 11 metres. The lands

adjacent to the Subject Property are zoned Residential First Density, Subzone AA. Due to the lot depth and width of the Subject Property, the AM Zone permits a maximum building height of up to 30 metres on portions of the Subject Property. A 30-metre high building on the Subject Property is not compatible with adjacent low-rise residential properties. The requested site-specific 11-metre maximum building height is appropriate for the development of the Subject Property.

A site-specific zoning provision is requested to require no parking spaces for uses located within an accessory building. The multi-purpose accessory building is designed to support the principle office use on the Subject Property.

Table 101 in Section 101 of the Zoning By-law sets the minimum parking spaces rates for specific uses in certain areas of the City. The Subject Property is located in Area C on schedule 1A of the Zoning By-law. For an office use on the Subject Property, Row N59 in Table 101 of the Zoning By-law requires parking spaces be provided at a rate of 2.4 spaces / 100 m² of gross floor area. The proposed office building has a gross floor area of 611 m². 15 parking spaces are required for the proposed office building. The proposed 16 parking spaces on the Subject Property meet the parking space requirements of the Office building on the Subject Property.

Ten employees are anticipated to work in the office building, and two to five employees are anticipated to work in the accessory building. The proposed 16 parking spaces meets the expected demand for parking generated by the proposed development. The requested site-specific zoning provision to require no parking spaces for uses located within an accessory building is appropriate for the development of the Subject Property.

6.0 CONCLUSION

This Planning Rationale has been prepared in support of a Zoning By-law Amendment application and a Site Plan Control application to facilitate the development of 1795 Montreal Road. The Zoning By-law Amendment proposes to rezone the Subject Property from Residential First Density, Subzone AA (R1AA) to Arterial Mainstreet (AM) Zone. The Subject Property is proposed to be developed to accommodate a two-storey office building and a one-storey multi-purpose accessory building.

The proposed Zoning By-law Amendment and Site Plan Control applications are consistent with the policies of the PPS, and the policies of the City of Ottawa's Official Plan. The proposed Zoning By-law Amendment establishes appropriate zoning provisions for the development of the Subject Property, and permits development that is compatible with surrounding uses.

The proposed Zoning By-law Amendment and Site Plan Control applications are appropriate of the development of the Subject Property and represent good land use planning.

NOVATECH

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APPENDIX 'A'

BY-LAW 2017-XXX

A by-law of the City of Ottawa to amend By-law Numbered 2008-250 of the City of Ottawa to change the zoning for the property known municipally as 1795 Montreal Road.

The Council of the City of Ottawa, pursuant to Section 34 of the *Planning Act*, R.S.O. 1990, enacts as follows:

(a) The Zoning By-law Map of By-law 2008-250, entitled the "City of Ottawa Zoning By-law" is amended by rezoning the lands shown on Attachment No. 1 to this by-law as follows:

(a) Area A – rezoned from R1AA to AM [xxxx]

(b) Section 239 – Urban Exceptions of By-law No. 2008-250 entitled "City of Ottawa Zoning By-law" is amended by adding urban exception [xxxx] as follows:

Applicable Zones	Additional Land Uses Permitted	Land Uses Prohibited	Provisions
AM [xxxx]			<ul style="list-style-type: none">– Maximum building height - 11 metres.– No parking spaces are required for uses located within an accessory building.

ATTACHMENT NO. 1

