

FOTENN

770 BROOKFIELD ROAD





Prepared for:

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1.0 INTRODUCTION

On behalf of Atlantis Investments, Owner, Fotenn Planning + Design has prepared this Planning Rationale in support of a Site Plan Control application for the lands municipally known as 770 Brookfield Road in the City of Ottawa.

1.1 Application History

Atlantis Investments, with assistance from Fotenn Consultants and Hobin Architecture Incorporated, is submitting an application for Site Plan Control to permit a proposed mixed-use development on the western portion of the property municipally known as 770 Brookfield Road.

The broader property previously accommodated a two-storey office building and a commercial parking lot. While the office building has since been demolished, the parking lot is still in operation at the western portion of the property. The property was rezoned under the 1998 City of Ottawa Zoning By-law in 2004, with provisions tailoring to a site-specific development proposal for a nine-storey mixed-use residential / commercial development with 330 apartment dwellings, ground floor commercial space, and underground parking. The existing office building was proposed to be retained, with the proposed building envisioned as a redevelopment of the parking lot on the west side of the property.

Two site-specific Schedules were included in the site-specific zoning. The intent of the first schedule, now referenced as Schedule 147, was to apply maximum building heights up to 27 metres to the western portion of the property, where the mixed-use building was proposed to be located. The intent of the second schedule, now referenced as Schedule 148 was to consider the entire lot as a single lot for zoning purposes. The site-specific provisions of the resulting zone, including the Schedules, were carried forward in Comprehensive Zoning By-law 2008-250, with the General Commercial Zoning amended to General Mixed-Use Zone. The current zoning is General Mixed Use Subzone 1, Exception 155, Maximum Floor Space Index 1.5, Schedules 147 and 148 (GM1 [155] F(1.5) S147, 148).

This application for Site Plan Control applies to Phase 1 of the proposed development on the western portion of the property. Development applications associated with the balance of the property, including a Site Plan Revision application, will be submitted at a later time.

A Minor Variance application to the Committee of Adjustment has been submitted and is scheduled to be heard on November 1, 2017. The variances seek to:

- / Reduce building heights established in Zoning Schedule 147, based on the building design developed through consultations with community members;
- / Permit dwelling units on the ground floor to be located within 31.5 metres of Brookfield Road.



Figure 1: Subject Property in Community Context

Atlantis Investments proposes to develop the subject property with a mixed-use, multi-building complex consisting of ground-related retail uses and residential dwelling units. This Site Plan Control application applies only to the western portion of the property, which consists of five separate buildings. The proposed site plan is illustrated in Figure 2 below.

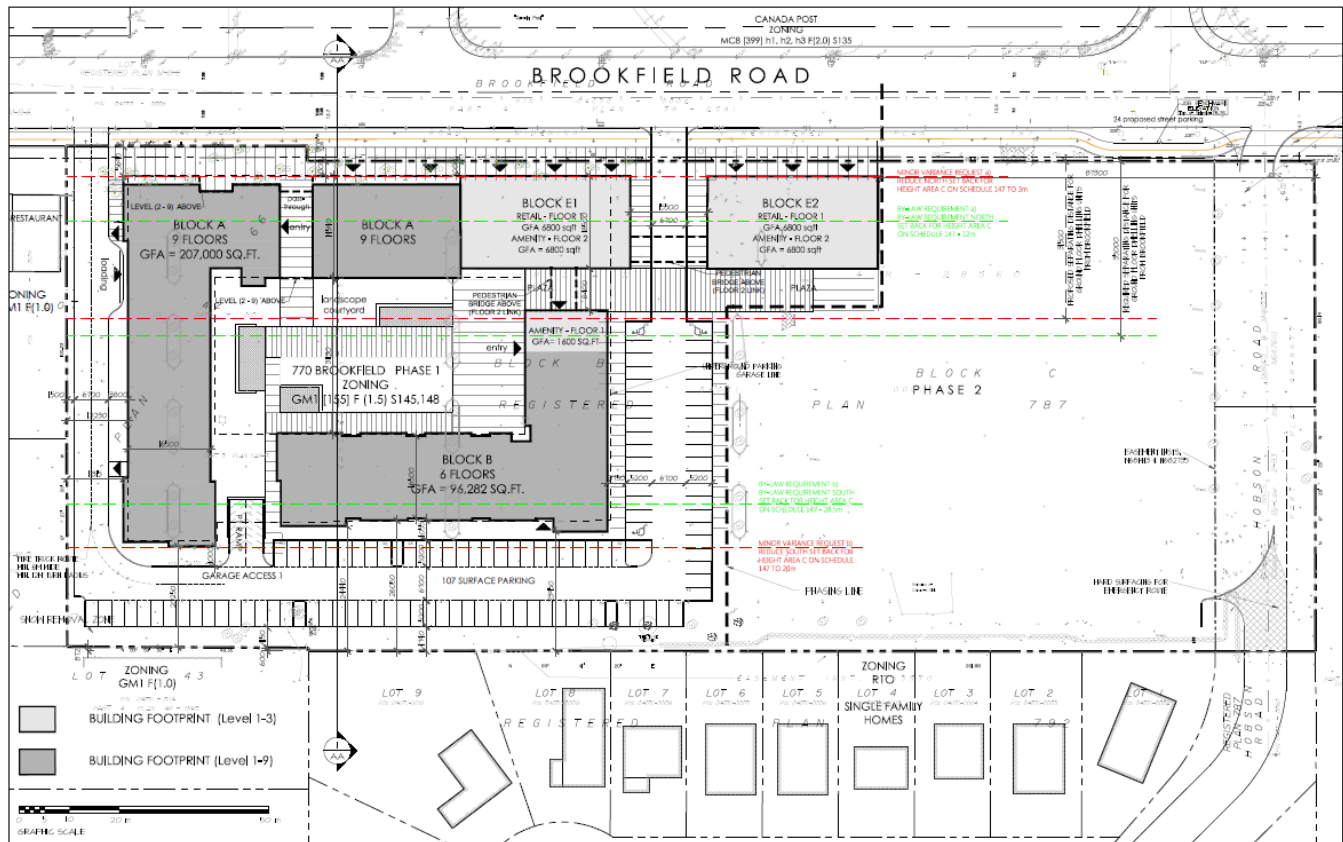


Figure 2: Proposed Site Plan for Phase 1

The proposed development features predominately residential uses in Blocks A and B, containing a total of 355 residential units. The west and south residential blocks are L-shaped, with one rectangular block positioned along Brookfield Road. The west residential building is proposed to be nine storeys in height, with the southern building proposed at six storeys.

The massing of the buildings is strategically designed to re-allocate permitted density in a manner that reduces impacts on the adjacent residential uses. The site-specific height limits established in Schedule 147 of the Zoning By-law restricts building heights on the west portion of the property to 18 metres at the front, with the intention of incorporating a podium with setbacks above the sixth storey. The proposal deliberately adds density to the front of the property, incorporating a transition to the south and shifting density further away from the adjacent residential area.

The development also includes two additional buildings with the capability to accommodate retail uses (labelled E1 and E2) with 632 m² (6,800 sq. ft.) of square footage at grade, both oriented to face Brookfield Road. If retail uses are not determined to be feasible at this location, the space can be used for additional amenity space for the residential uses on the site.

The residential blocks vary in shape and size, but are generally oriented around a rectangular courtyard in the centre of the development. The courtyard is proposed to be landscaped, with a mix of hard and soft landscaping elements. The courtyards provide amenity areas for building residents and visitors and are generally shielded from the traffic noise of Brookfield Road. Direct pedestrian access to the courtyards is provided from the adjacent buildings, as well as from a common plaza proposed at the north-centre area of the development between the buildings of each phase.

Vehicular access to the development is provided along the west portion of the subject property, as well as between the retail blocks. At full build-out of the development, the eastern access will be located in the centre of the development. Both accesses lead to a rear surface parking lot containing 107 surface parking spaces. An underground parking garage is proposed beneath the development accommodating 136 parking spaces.

In addition to the interior courtyards, soft landscaping is proposed along the perimeter of the property. The Landscape Plan provided in the application illustrates the proposed landscape elements.

POLICY & REGULATORY FRAMEWORK

3.1 Provincial Policy Statement

The 2014 Provincial Policy Statement (PPS), issued under the authority of Section 3 of the Planning Act, provides direction on matters of provincial interest related to land use planning and development. The Planning Act requires that decisions affecting planning matters be consistent with policy statements issued under the Act.

The PPS promotes the development of strong communities, which rely on the establishment of efficient land use and development patterns and the accommodation of an appropriate range and mix of uses.

The proposed development meets the applicable policies of the PPS, including:

- / Promotes the efficient development and land use patterns which sustain the financial well-being of the Province and municipalities over the long term;
- / Accommodates an appropriate range and mix of residential, employment, institutional, recreation, park and open space, and other uses to meet long-term needs;
- / Promotes cost-effective development patterns and standards to minimize land consumption and servicing costs;
- / Proposes development in a settlement area, which is the focus of growth and development;
- / Proposes a density and mix of land uses which efficiently use land and resources, are appropriate for available infrastructure, and supports active transportation;
- / Contributes to an appropriate range and mix of housing types and densities that efficiently use land, resources, infrastructure and public service facilities, and supports the use of active transportation and transit;
- / Connects to municipal water services and municipal sewage services, the preferred form of servicing in settlement areas;
- / Contributes to a land use pattern, density and mix of uses that minimizes the length and number of vehicle trips and supports current and future use of transit and active transportation.

3.2 City of Ottawa Official Plan (2003, as amended)

The City of Ottawa Official Plan provides a vision of Ottawa's future growth and a policy framework to guide its physical development to the year 2031. Additionally, the Plan addresses matters of provincial interest, as defined by the Provincial Policy Statement, and serves as a basis for a wide range of municipal activities.

The subject property is designated General Urban Area on Official Plan Schedule B (Urban Policy Plan). The designation permits all types and densities of housing, as well as employment, retail uses, service, industrial, cultural, leisure, greenspace, entertainment and institutional uses. Policy 3 of Section 3.6.1 specifies that residential intensification through infill or redevelopment in the General Urban Area will:

- / Recognize the importance of new development relating to existing community character so that it enhances and builds upon desirable established patterns and built form;
- / Applies the policies of Official Plan Sections 2.5.1 and 4.11;
- / Considers its contribution to the maintenance and achievement of a balance of housing types and tenures to provide a full range of housing for a variety of demographic profiles throughout the General Urban Area.

Policy 5 states that the General Urban Area permits uses that may generate traffic, noise, or other impacts that have the potential to create conflicts with the surrounding residential community. Such uses will be directed to:

- / Locations along the rapid transit system, or an arterial or major collector road with sufficient capacity to accommodate the anticipated traffic generated and where frequent, all-day transit service can be provided;
- / Suitable locations on the perimeter of, or isolated from, established residential neighbourhoods. In this regard, existing or proposed building orientation, massing and design, and the presence of mitigating circumstances such as distance, changes in topography, or the presence of features such as significant depths of mature forests may be taken into account.

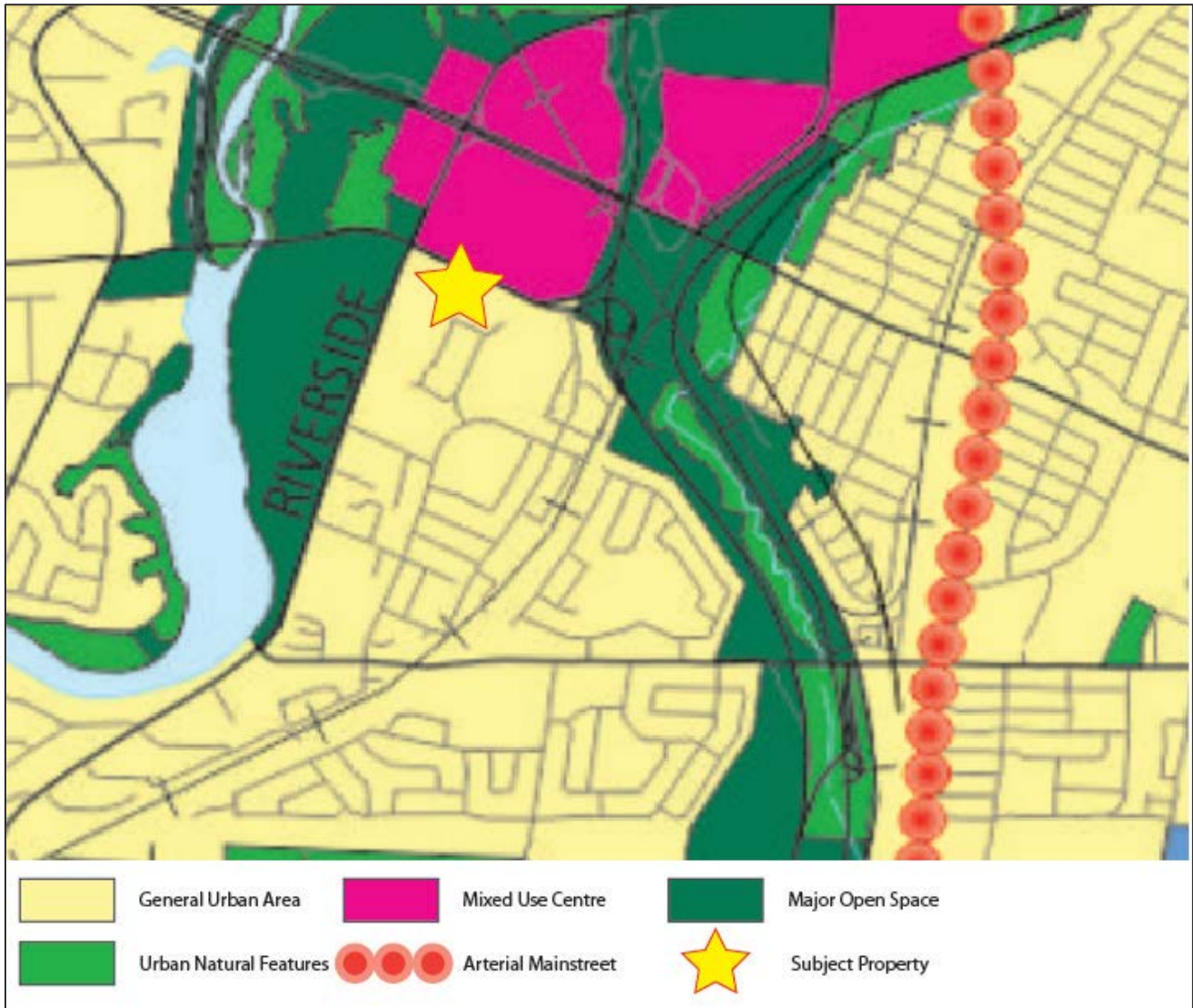


Figure 3: Extract from Official Plan Schedule B (Urban Policy Plan)

Policy 3 of Section 2.2.2 requires that all intensification will occur in accordance with the provisions of Sections 2.5.1 and 4.11. Policy 14 supports intensification in the General Urban Area, in the following cases, provided that all other Official Plan policies are met:

- / Lands within 600 metres of future or existing rapid-transit stations with potential to develop as compact, mixed-use and pedestrian-friendly cores;
- / Lands that are no longer viable for the purpose for which they were intended, such as older industrial areas [...];
- / Lands where the present use is maintained but the addition of residential uses or other uses can be accomplished in a complementary manner, such as on under-utilized shopping centre sites;
- / Lands currently or formerly used as parking lots or other extensive storage purposes;
- / Lands where records indicate existing contamination due to previous commercial or industrial use, but which can be made suitable for development if cleaned up.

Section 2.5.1 contains a set of objectives and principles to achieve compatible urban design for new development. The proposed development meets the applicable design objectives:

- / Enhances the sense of community by responding to the surrounding physical context and surrounding uses;
- / Defines quality public and private spaces through development by delineating the frontage of the property along the public street and incorporating an enclosed private courtyard for users of the building;
- / Creates places that are safe, accessible, and are easy to get to and move through by including multiple ingress / egress points for vehicles and an integrated sidewalks and pathway network;
- / Respects the character of existing areas by creating a mix of uses appropriate to the context, creating a transition between the commercial uses to the north and the residential area to the south;
- / Considers adaptability and diversity by contributing to a diversity of housing options in the community and introducing transit-supportive uses on the property;
- / Understands and respects natural processes and features by proposing stormwater management infrastructure on the property, as well as soft landscaping to encourage natural infiltration of storm water; and
- / Maximizes energy-efficiency and promotes sustainable design by proposing an appropriate density within proximity to rapid transit facilities.

Policy 2 of Section 4.11 establishes a set of compatibility criteria for evaluating development applications. The proposed development meets the applicable criteria:

Criteria	Evaluation
Traffic	As confirmed in a Traffic Study prepared by Parsons, the surrounding road network contains sufficient capacity to accommodate the traffic generated by the proposed development.
Vehicular Access	Vehicular access is proposed from Brookfield Road, a designated Major Collector Road in the Official Plan.
Parking Requirements	The proposed development contains sufficient parking to meet the minimum requirement established in the Zoning By-law.
Outdoor Amenity Areas	A large landscaped courtyard is proposed in each phase of the proposed development.
Loading Areas, Service Areas, and Outdoor Storage	One loading space is provided in Phase 1 of the development.

Criteria	Evaluation
Lighting	Lighting is designed to meet City standards and is not anticipated to result in undue adverse impacts on adjacent properties.
Noise and Air Quality	The proposed development is not anticipated to result in undue adverse impacts on adjacent properties.
Sunlight	Shadows cast by the development will fall primarily on Brookfield Road right-of-way.
Microclimate	The proposed development is not anticipated to result in microclimate impacts.
Supporting Neighbourhood Services	The property is located in proximity to community services, including parks and public transit.

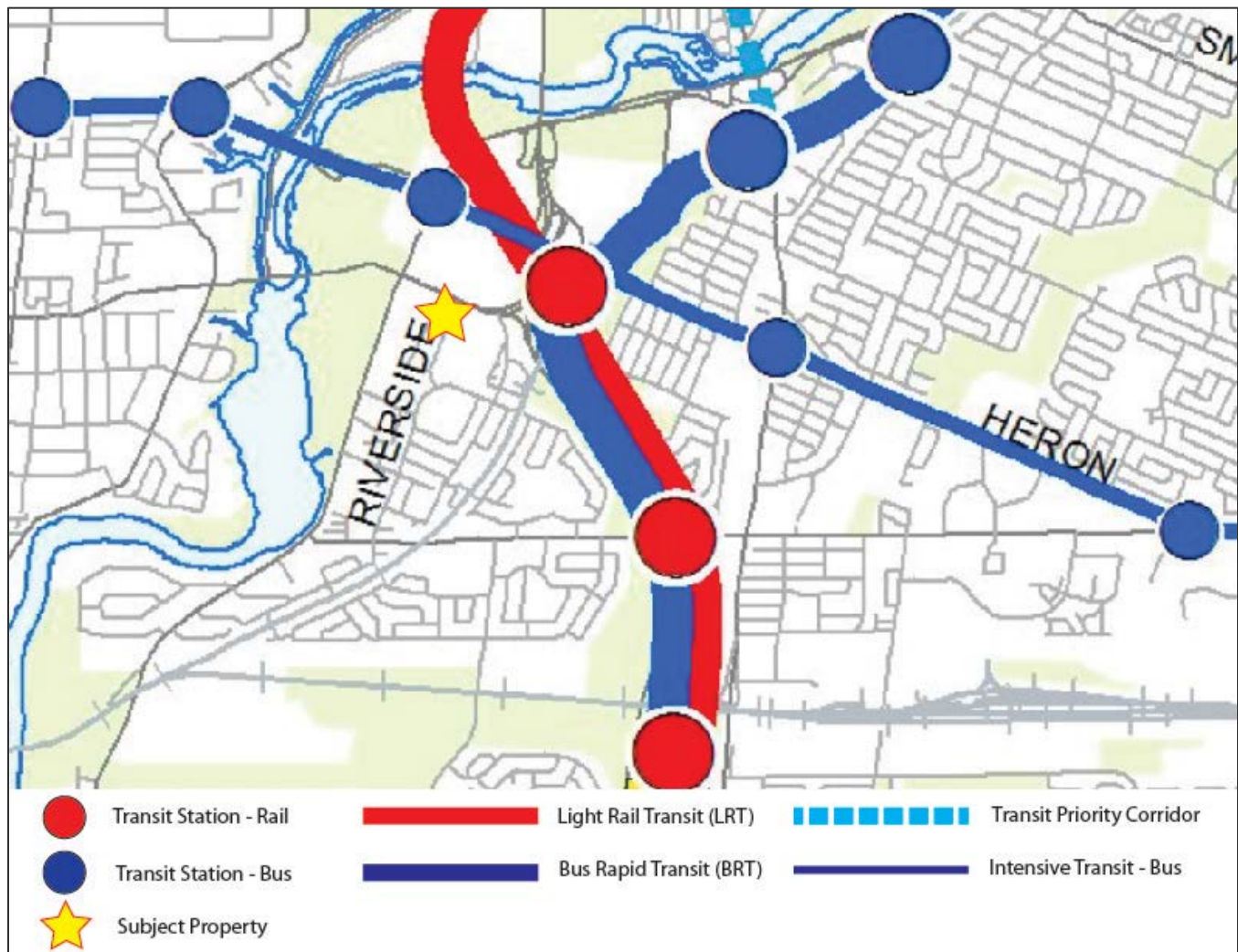


Figure 4: Extract from Official Plan Schedule D (Rapid Transit Network)

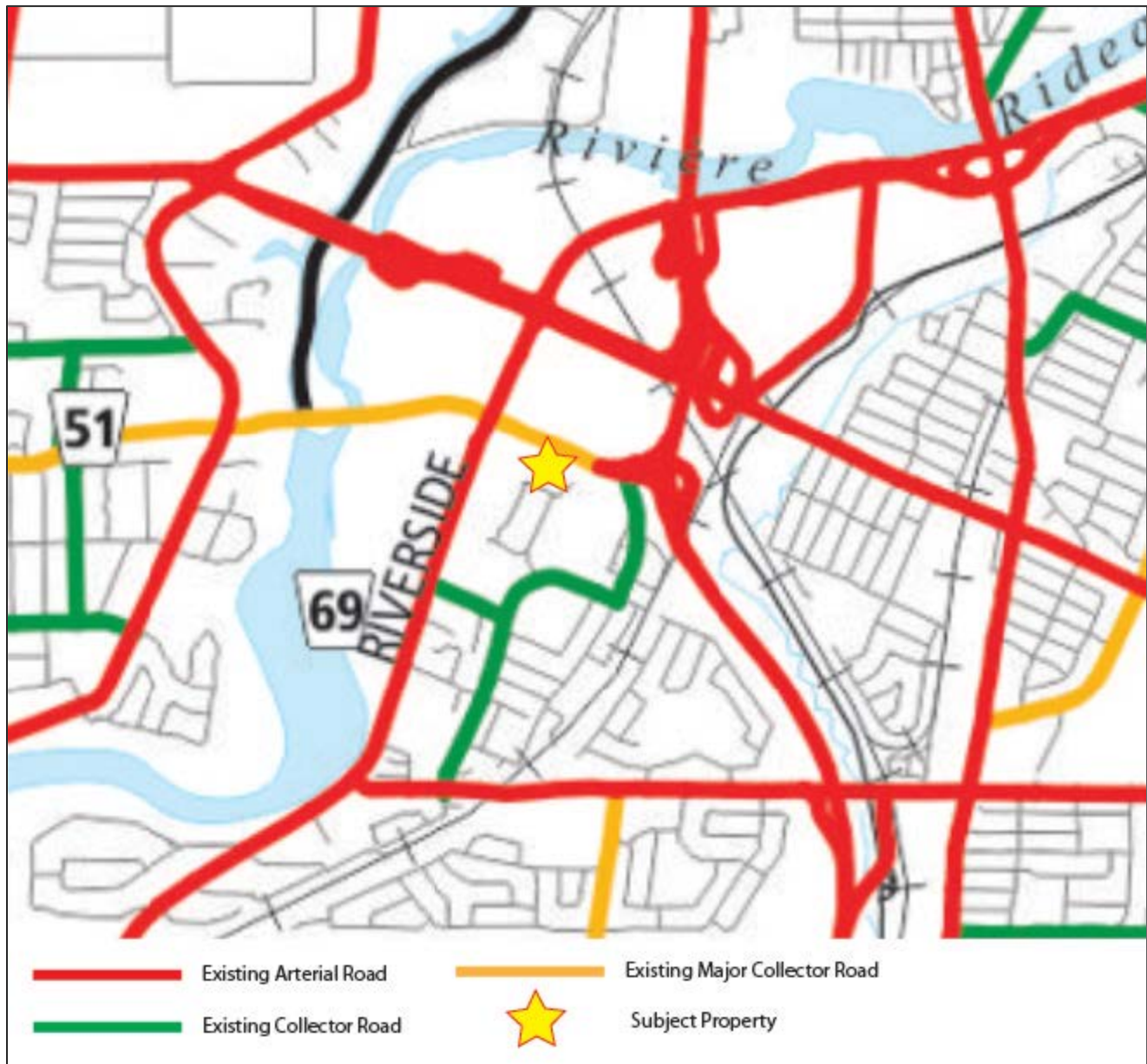


Figure 5: Extract from Official Plan Schedule E (Urban Road Network)

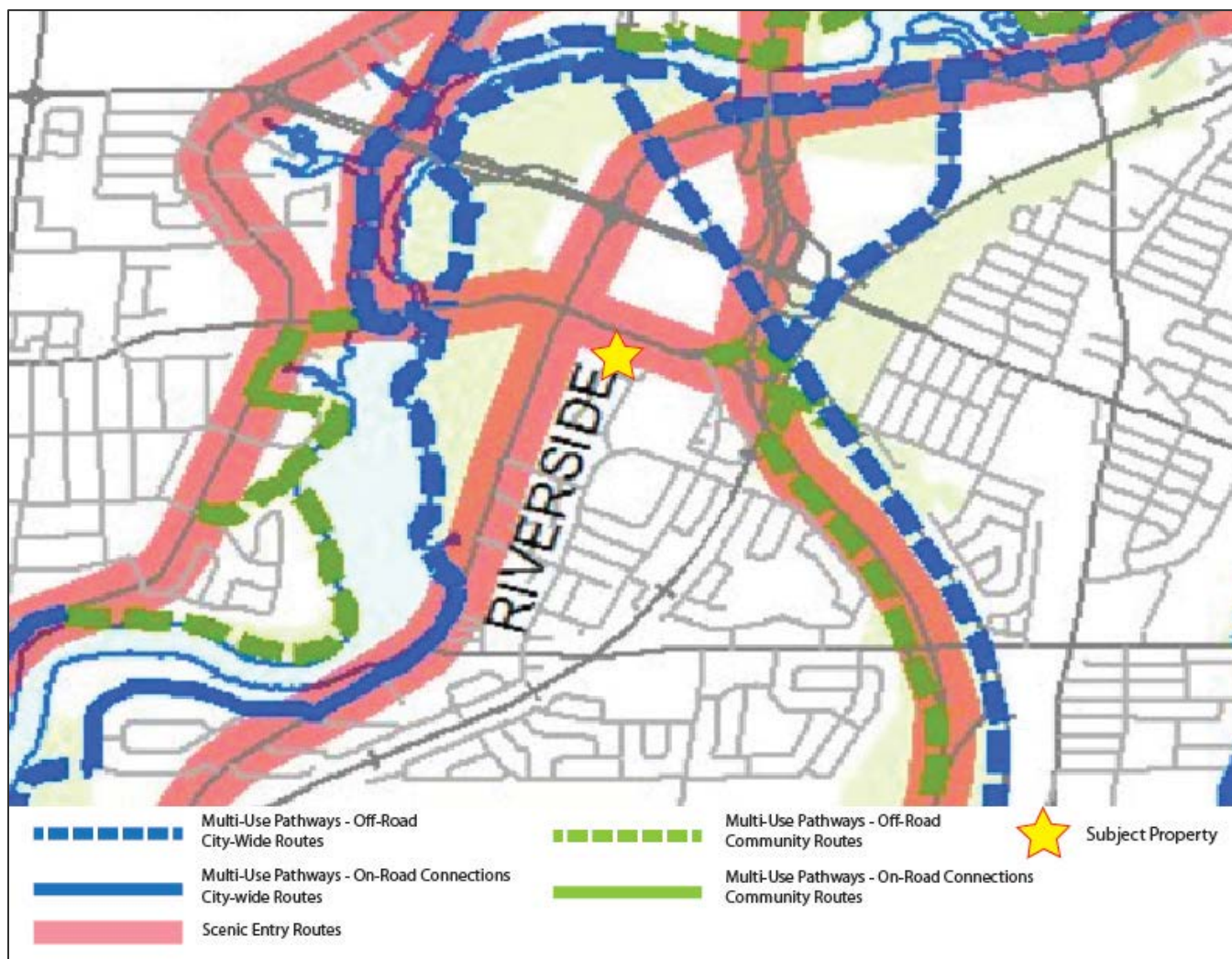


Figure 6: Extract from Official Plan Schedule I (Multi-Use Pathways and Scenic Entry Routes)

The proposed development is consistent with the policies of the Official Plan, including Sections 2.2.2, 2.5.1, and 4.11. The proposed development is permitted within the General Urban Area designation.

3.3 City of Ottawa Official Plan Amendment (OPA) 150

In 2013, the City of Ottawa updated its Official Plan, resulting in numerous changes to its land use policies. The Ministry of Municipal Affairs and Housing issued approval of Official Plan Amendment (OPA) 150 in April 2014, but the Amendment is currently under appeal before the Ontario Municipal Board (OMB). Until the OMB renders its decision, the current policies of the City of Ottawa Official Plan (2003, as amended) remain in full force and effect.

Revisions to Section 3.6.1 establishing policies for the General Urban Area include a maximum building height of four storeys. The revised Section 4.11 contains more robust policies related to building and site design. Policy 5 requires proponents to also demonstrate how the design of the development fits with the existing desirable character and planned function.

Revised Policy 6 contains new design requirements for principal entrances, windows, and other architectural elements. Revised Policy 12 states that building height and massing transitions will be accomplished through a variety of means, including incremental changes in building height, massing, and building setbacks and stepbacks. Revised Policy 18 contains special design directions for Design Priority Areas, to ensure that proposed buildings interface with streetscape elements to enhance the public realm.

While the proposed development exceeds the four-storey maximum height policy proposed in OPA 150, it generally meets the design policies introduced in Section 4.11.

3.4 Secondary Plans

The subject property is located within both the Confederation Heights Secondary Plan area and the Riverside Park Secondary Plan area. Under the Confederation Heights Secondary Plan, the site is within a Mixed Use land category, which is intended for low-profile retail and residential intensification in accordance with the zoning in place at the time of the Secondary Plan, having since been amended, and references policies under the Riverside Park Secondary Plan.

Under the Riverside Park Secondary Plan, the site is in the Mixed Use Residential / Commercial land category and development within this area is to provide a transition from the Confederation Heights Mixed Use Centre to the north, the residential area to the south, and Mooney's Bay Park to the west. There are guidelines for development with respect to parking, amenity space, entrances, building and site design and landscaping, which will contribute to the transition. There are no specific heights to describe how the transition is to take place from a height perspective.

The property on the north side of Brookfield Road is identified under the Confederation Heights Secondary Plan as Primary Employment Centre, and the associated policy direction envisions development ranging from low (4 storeys) to high (12 storeys) around focal points.

The proposed development is consistent with the policies of the Confederation Heights Secondary Plan and the Riverside Park Secondary Plan.

3.5 Transit-Oriented Development Guidelines

The Transit-Oriented Development Guidelines were approved by Ottawa City Council in September 2007. The guidelines are intended to provide design guidance to development applications within proximity to rapid transit stations. The proposed development meets the following guidelines:

- / Provides a transit-supportive land use within a 600-metre walking distance of a rapid transit station;
- / Provides a mix of different land uses that support a vibrant area community and enable people to meet many of their daily needs locally.
- / Locates buildings close to each other and along the front of the street to encourage ease of walking between buildings and to public transit.
- / Create transition in scale between higher intensity development around the transit station and adjacent lower intensity communities by stepping down building heights and densities from the transit station.
- / Use clear windows and doors to make the pedestrian level façade of walls facing the street highly transparent in order to provide ease of entrance, visual interest and increased security through informal viewing.
- / Design pedestrian connections that are convenient, comfortable, safe, easily navigable, continuous and barrier-free.

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- / Ensure pedestrian walkways are an adequate width to accommodate anticipated pedestrian volumes, with a minimum width of 2.0 metres with accessible grade changes.
 - / Design ground floors to be appealing to pedestrians.
 - / Provide no more than the required number of vehicle parking spaces, as per the Zoning By-law.
 - / Locate parking lots to the rear of buildings and not between the public right-of-way and the functional front of the building.
 - / Design access driveways to be shared between facilities.
 - / Provides underground parking.
 - / Designs parking lots to include direct and safe pedestrian linkages while maintaining pedestrian comfort and access.
 - / Locates loading areas off the street, behind the buildings. Avoid routing deliveries through parking areas and across primary pedestrian, transit and cyclist routes.

3.6 Comprehensive Zoning By-law 2008-250

The subject property is zoned General Mixed Use Subzone 1, Exception 155, Maximum Floor Space Index of 1.5, Schedules 147 and 148 (GM1 [155] F(1.5) S147, 148). The intent of the GM zone is to:

- / Allow residential, commercial and institutional uses, or mixed-use development in the General Urban Area Official Plan designations, among others;
- / Limit commercial uses to individual occupancies or in groupings in well-defined areas such that they do not affect the development of the designated Traditional and Arterial Mainstreets as viable mixed-use areas;
- / Permit uses that are often large and serve or draw from broader areas than the surrounding community and which may generate traffic, noise or other impacts provided the anticipated impacts are adequately mitigated or otherwise addressed; and
- / Impose development standards that will ensure that the uses are compatible and complement surrounding land uses.



Figure 7: Zoning Map

The GM parent zone permits a range of uses:

Animal care establishment	Home-based day care
Animal hospital	Instructional facility
Artist studio	Library
Bank	Medical facility
Bank machine	Municipal service centre
Catering establishment	Office
Click and collect facility	Personal service business
Community centre	Place of assembly
Community health and resource centre	Place of worship
Convenience store	Post office
Day care	Recreational and athletic facility
Diplomatic mission	Research and development centre
Drive-through facility	Residential care facility
Emergency service	Restaurant
Funeral home	Retail food store
Home-based business	Retail store

Service and repair shop
Shelter
Small batch brewery

Technology industry
Training centre
Urban agriculture

Apartment dwelling, low-rise
Apartment dwelling, mid rise
Bed and breakfast
Dwelling unit
Group home
Planned unit development
Retirement home

Retirement home, converted
Rooming house
Rooming house, converted
Rooming unit
Stacked dwelling
Townhouse dwelling

In the GM1 zone, the following provisions apply:

- / No more than 50% of the permitted floor space index may be used.
- / The provisions of subsection 187(3)(h)(ii) applies but may be reduced to 1 metre where a minimum 1.4 metre high opaque screen is provided; and
- / The 50% floor space index maximum cited above does not apply to the following uses, where the full floor space index may be used:
 - o Apartment dwelling, low rise
 - o Apartment dwelling, mid-high rise
 - o Community centre
 - o Community health and resource centre
 - o Day care
 - o Dwelling unit
 - o Group home
 - o Library
 - o Planned unit development
 - o Residential care facility
 - o Retirement home
 - o Retirement home, converted
 - o Rooming house
 - o Rooming house, converted
 - o Rooming unit
 - o Shelter
 - o Stacked dwelling
 - o Townhouse dwelling

Exception 155 permits parking lot and parking garage uses, and establishes additional provisions:

- / A parking lot attendant's shelter may locate anywhere in this zone.
- / The residential uses listed in subsection 187(2) are permitted provided that 22.5% of the ground floor area of each building is occupied by one or more of the non-residential uses listed in subsection 187(1), and provided that each building contains not less than 500 m² of non-residential space.
- / Dwelling units on the ground floor must not be located within 35 m of Brookfield Road.
- / Maximum building heights for a building having non-residential uses on the first or second floor only are as shown on Schedule 147.
- / For zoning purposes, the entire area to be considered as one lot.
- / The maximum Floor Space Index for a building having non-residential uses above the second floor is 1.0.

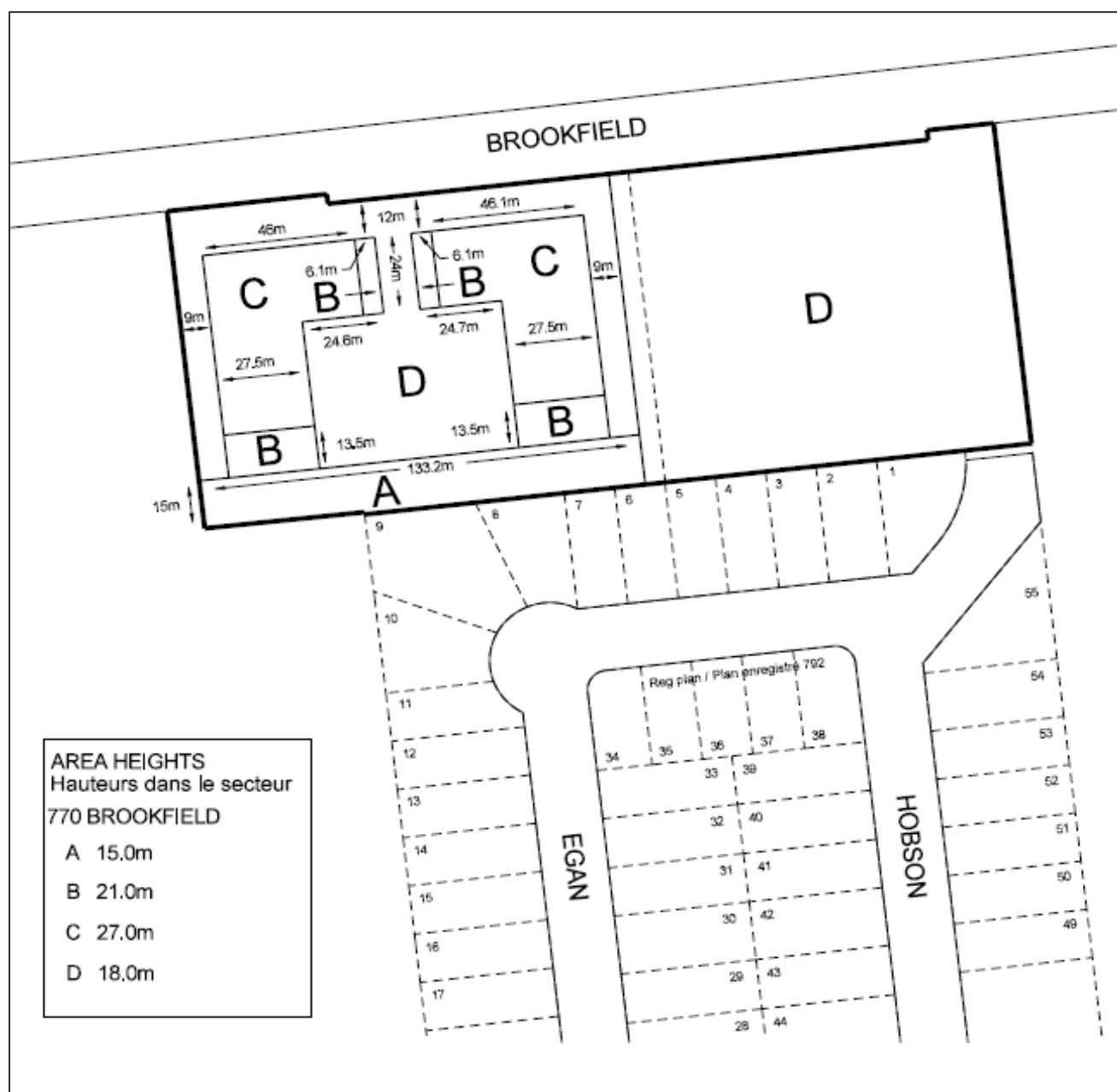


Figure 8: Schedule 147 of Comprehensive Zoning By-law 2008-250

The table below evaluates the proposed development against the applicable zoning provisions.

Table 1: Zoning Evaluation

Zoning Mechanism	Required	Proposed	Compliance
Minimum Lot Area	No minimum	2.76 ha	✓
Minimum Lot Width	No minimum	233 m	✓

Zoning Mechanism	Required	Proposed	Compliance
Maximum Front Yard Setback	3 m	3 m	✓
Minimum Interior Side Yard Setback	(i) For a non-residential or mixed-use building, from any portion of a lot line abutting a residential zone: 5 m	N/A	✓
	(ii) For a residential use building: Equal or lower than 11 metres in height: 1.2 m Higher than 11 metres in height: 3 m	11.3 m	✓
Minimum Rear Yard Setback	From any portion of a rear lot line abutting a residential zone: 7.5 m	20.25 m	✓
	For a residential use building: 7.5 m	20.25 m	✓
Maximum Building Height	As per Schedule 147	Variances required	✗
Maximum Floor Space Index	1.5	0.92	✓
Minimum Width of Landscaped Area	Abutting a street: 3 m	3 m	✓
	Abutting a residential or institutional zone: 1 m where a 1.4-metre fence is included	4.1 m	✓
Required Resident Parking	None for 12 units or less; 0.5 per unit after 12 units (343 x 0.5 = 172)	243 spaces	✓
Required Visitor Parking	0.2 per unit		✓
Required Amenity Space	6 m ² per dwelling unit (Minimum 50% must be communal)	3,339 m ²	✓

Zoning Mechanism	Required	Proposed	Compliance
	Block A: 193 units x 6 = 1158 Block B: 150 x 6 = 900		
Maximum Amount of Floor Space Used for Retail Use	No more than 50% of the permitted floor space index may be used	Retail accounts for less than 50% of permitted floor space	✓
Location of Parking Lot Attendant's Shelter	May be located anywhere in the zone	N/A	✓
Residential Use Condition	Residential uses permitted if minimum 22.5% of the ground floor area of each building is occupied by a permitted non-residential use, with minimum 500 m ² in each building.	Precise mix of uses to be determined.	✗ ✓
Minimum Setback for Dwelling Units	Dwelling units on the ground floor must be set back a minimum 35 metres from Brookfield Road	Dwelling units located within 31.5 metres of Brookfield Road	✗
Maximum Floor Space Index for Non-Residential Uses	1.0 for a building with non-residential uses above second floor	N/A	✓

4.0 CONCLUSION

It is our professional opinion that the proposed Site Plan Control application is appropriate, represents good planning, and is in the public interest.

The proposal is consistent with the Provincial Policy Statement (PPS) by providing efficient and appropriate development on lands within the urban boundary that contributes to the range of housing options available in the community.

The proposed development conforms to the Official Plan's vision for managing growth in the urban area, and meets the policies for the General Urban Area. The development meets the urban design and compatibility objectives, principles, and policies in Sections 2.5.1 and 4.11 of the Official Plan.

The proposed development meets many of the applicable requirements in Comprehensive Zoning By-law 2008-250, and will seek a Zoning By-law Amendment to bring the proposal into compliance with the provisions.

Sincerely,



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Planner



Carl Furney, MCIP RPP AICP
Associate Director, Planning + Development