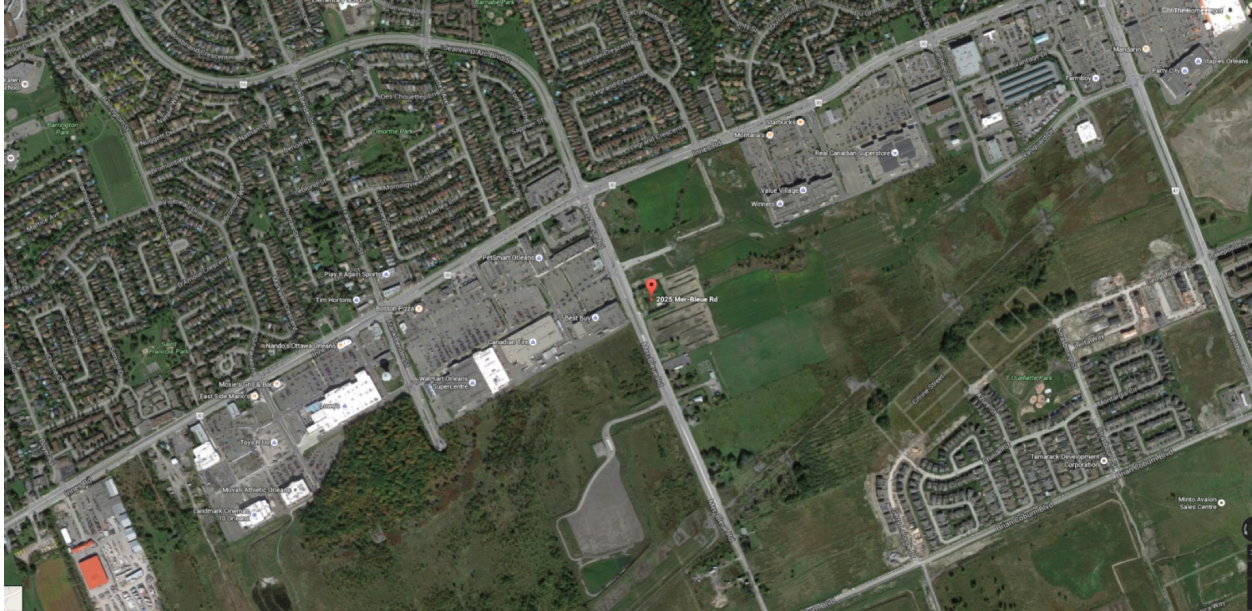




PLANNING RATIONALE

2025 Mer Bleue - Phase 2 > Site Plan Control Application



Prepared for:

SmartCentres

700 Applewood Cres., Suite 200

Vaughan, ON L4K 5X3

Prepared by:

Lloyd Phillips & Associates Ltd.

File: 1729

Date: November 3, 2017





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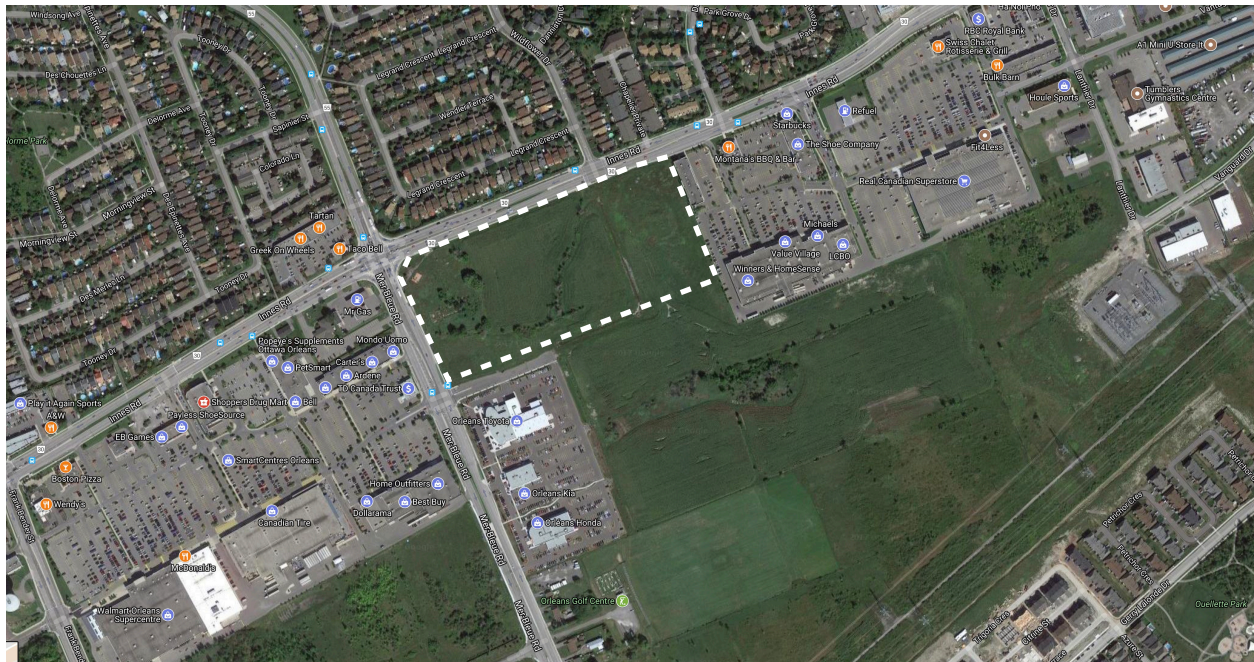
1. OVERVIEW

This is a Planning Rationale in support of a proposed Site Plan Control application for Phase 2 of a development at 2025 Mer Bleue Road.

The site has an area of 82,192 m² (8.2 ha) and has frontage of 405.77 m on Innes Road and 211.77 m of frontage on Mer Bleue Road. The site is mostly currently vacant (with the exception of current ongoing construction) and relatively flat. Other than a few deciduous trees, the vegetation is mainly grass. There are sidewalks bordering the property along the Mer Bleue frontage as well as the Innes Road frontage.

In 2016, an application for Site Plan Control for Phase 1 was submitted and since been approved. Now, Lloyd Phillips & Associates Ltd, on behalf of the applicant is submitting a Site Plan Control Application for Phase 2 of this development. The following image outlines the entire site.

Phase 1 accounted for the west third of the site and Phase 2 captures the middle north of the site and partially extends into the previously approved Phase 1.





2. SITE AND CONTEXT

2.1. Site

The site has an area of 82,192 m² (8.2 ha) and has frontage of 405.77 m on Innes Road and 211.77 m of frontage on Mer Bleue Road. The site is currently vacant and relatively flat. Other than a few deciduous trees, the vegetation is mainly grass. There are sidewalks bordering the property along the Mer Bleue frontage as well as the Innes Road frontage.

The area of Phase 1 accounts for 28,570.81 m² (2.86 ha.), and is identified below in green.

There are two easements on the property: Parts 3 to 10 and 13 are easements for sewer, water and rights of way (OC1410495). Parts 2 and 3 are an easement for Hydro (OC1643380).

The property is legally known as CUMBERLAND CON 11 PT LOT 1; RP 4R26139 PARTS 3 AND 4 &; RP 50R- 2763 PARTS 1 & 2. It is located in Ward 19 - Cumberland. The property is currently owned by Mer Bleue Orleans Co-Tenancy.

The following map illustrates the site broken down into the respective phases. Please note that as part of the Phase 2 Site Plan Revision Application, some minor changes to two of the buildings in Phase 1 are also being captured.





IMAGE LOOKING AT SITE FROM THE CORNER OF INNES ROAD AND MER BLEUE ROAD



IMAGE LOOKING SOUTH AT PROPERTY FROM MER BLEUE ROAD



IMAGE LOOKING EAST ON INNES ROAD, PHASE ONE ON THE RIGHT, PHASE TWO ON THE LEFT



IMAGE LOOKING EAST ON INNES ROAD, PHASE ONE ON THE RIGHT, PHASE TWO ON THE LEFT



BIRDS EYE VIEW OF SITE



2.2. Context

The site is located in Orleans, Ward 19 – Cumberland, on a vacant parcel of land between a long stretch of retail that characterizes the south stretch of Innes Road (a major four-lane arterial). The majority of this retail is broken down in three main formats: large-box retail, multi-tenant buildings, and stand-alone retail units commonly used for banks, restaurants, and fast-food restaurants, with the occasional gas station or other auto-related service establishment.

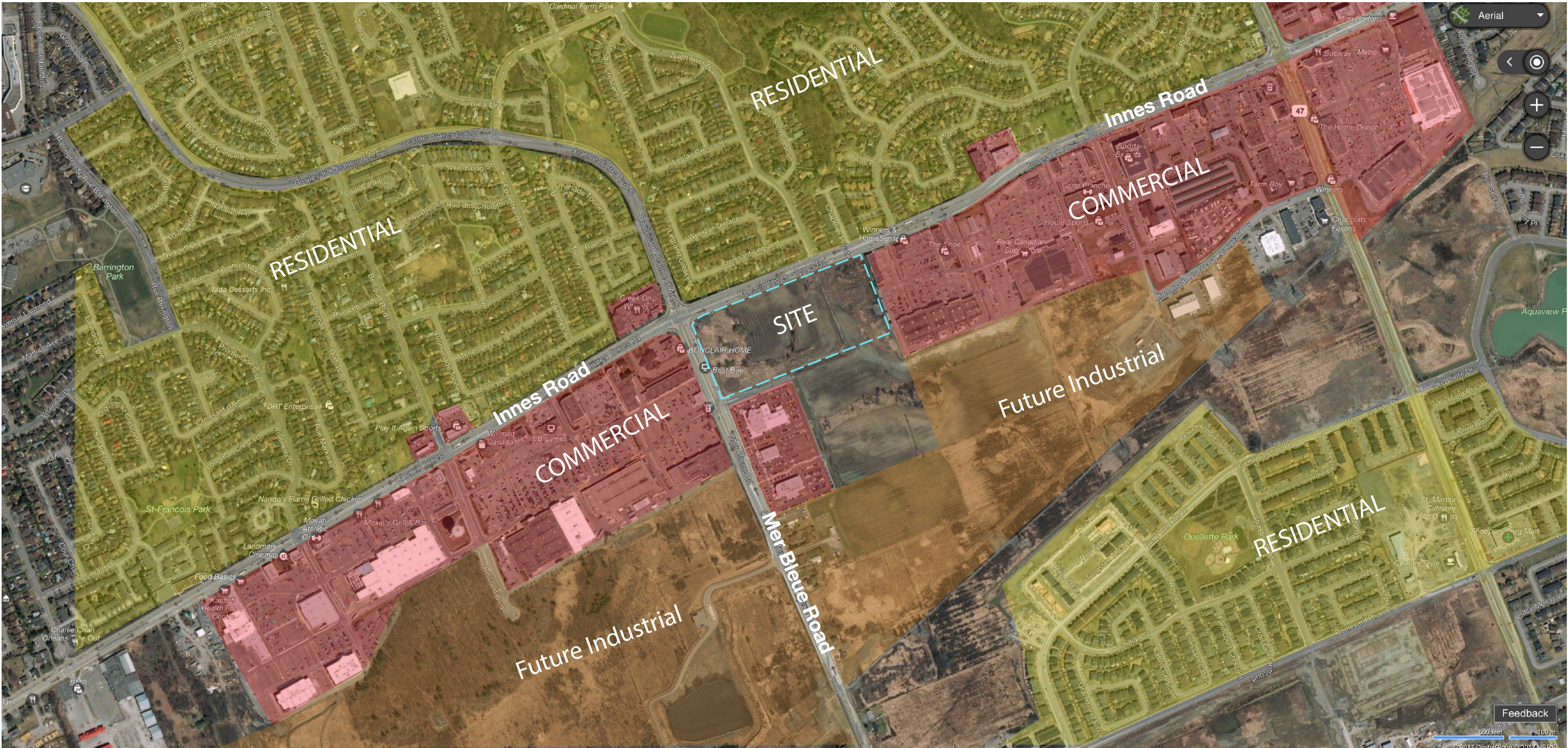
The area to the north is mostly residential except for a small corner of strip retail containing a KFC and Taco Bell fast-food establishment.

To the east and west are retail/commercial developments. The development to the west contains the following major retail stores: Home Outfitters, Future Shop (now Best Buy), Canadian Tire, and Wal-Mart, there are also some restaurants, smaller retail, a Shoppers Drug Mart, and a Mr. Gas fuelling station at the south west corner of Mer Bleue and Innes.

The development to the east contains Winners/HomeSense, Value Village, Michaels, and LCBO. Further east is the Canadian Superstore. There are additional small retail buildings on the north side of the site. Generally, this stretch of Innes Road from roughly Boyer Road to about Dorima Street is highly characterized by its large-format retail, and four-lane arterial road. The majority of large-format is on the south side of Innes Road, with the smaller, strip-mall style, multi-unit retail buildings on the north side of Innes Road.

The area to the south is mainly vacant fields that are zoned Industrial, Mixed-Use and some Development Reserve, and there is also some residential development towards the south east.

Generally, the majority of development to the north of Innes Road is Residential, as can be seen in the contextual map on the following page.





3. PROPOSED DEVELOPMENT

The proposed development is looking to construct two new buildings fronting onto Innes Road, and to revise two of the already approved buildings from Phase 1. The reason for the latter change results from a change of tenants and required design.

In the northwest area of the site, the development of Phase 1 includes a retail/commercial shopping centre with four buildings, which are one storey in height.

3.1.Phase 1 Revisions

Building K was previously identified as a restaurant with a total GFA of 604 m² and a patio facing Innes Road. The newly revised Building K has now been identified as a Lone Star Restaurant and will have a total GFA of 623.8 m² (consisting of 583.4 m² on the ground floor and a mezzanine of 40.4 m²). Further, the patio location has been relocated from facing Innes Road to the west side of the building which will further reduce any noise considerations, such as those raised during the rezoning applications for the overall site.

Building N was previously identified as a multi-tenant (3) mixed use building with a total GFA of 485.5 m². It is now proposed as a single use tenant building with a total GFA of 461.04 m² (36.62 m² of this is for a mezzanine.) A drive-through is still being proposed and is relatively unchanged.

3.2.Phase 2

Proposed Phase 2 consists of two new buildings. Both are multi-tenant facing Innes Road. Building M contains two tenants, one of which will offer a small patio on the east side of the building. The entire GFA of the Building M is 877.93 m². The largest tenant, at 696.77 m² is identified as a Ren's Pet store.

Building L, on the east side of the frontage contains four tenancies likely to be fast-food restaurants. The building proposes a total GFA of 624.68 m². The largest tenant, on the west, with a GFA of 250.84 m², will also have a drive-through, similar to that shown in the overall plan that was filed and approved during the Phase 1 Site Plan Control Application.

The third phase of this development is situated on the east side of the site and the details are not currently known.

Access

Vehicular access to Phase 2 is achieved through an east-west connection from Phase 1 via the internal road to the east of Phase 1, and also through the aisles from Phase 1.



Pedestrian access enters from Innes Road at a couple of locations and also from the proposed sidewalk down the east road connection across from Wildflower Drive that will be constructed as part of Phase 1.

Parking

A total of 88 parking spaces are to be provided. Based on the applicable parking rate of 3.6 spaces per every 100 m² of Gross Leasable Floor Area (GLFA), the total required parking is 53 spaces.

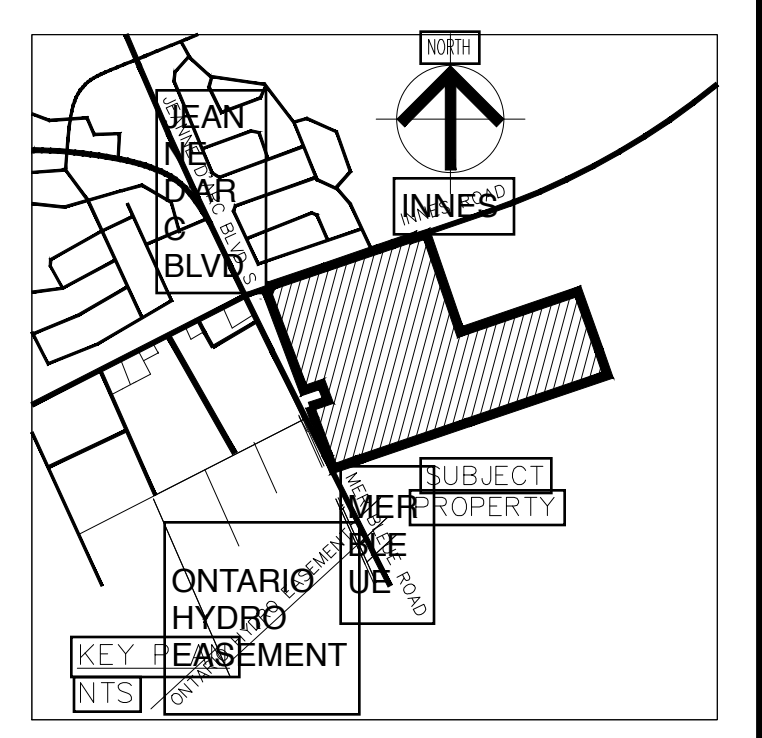
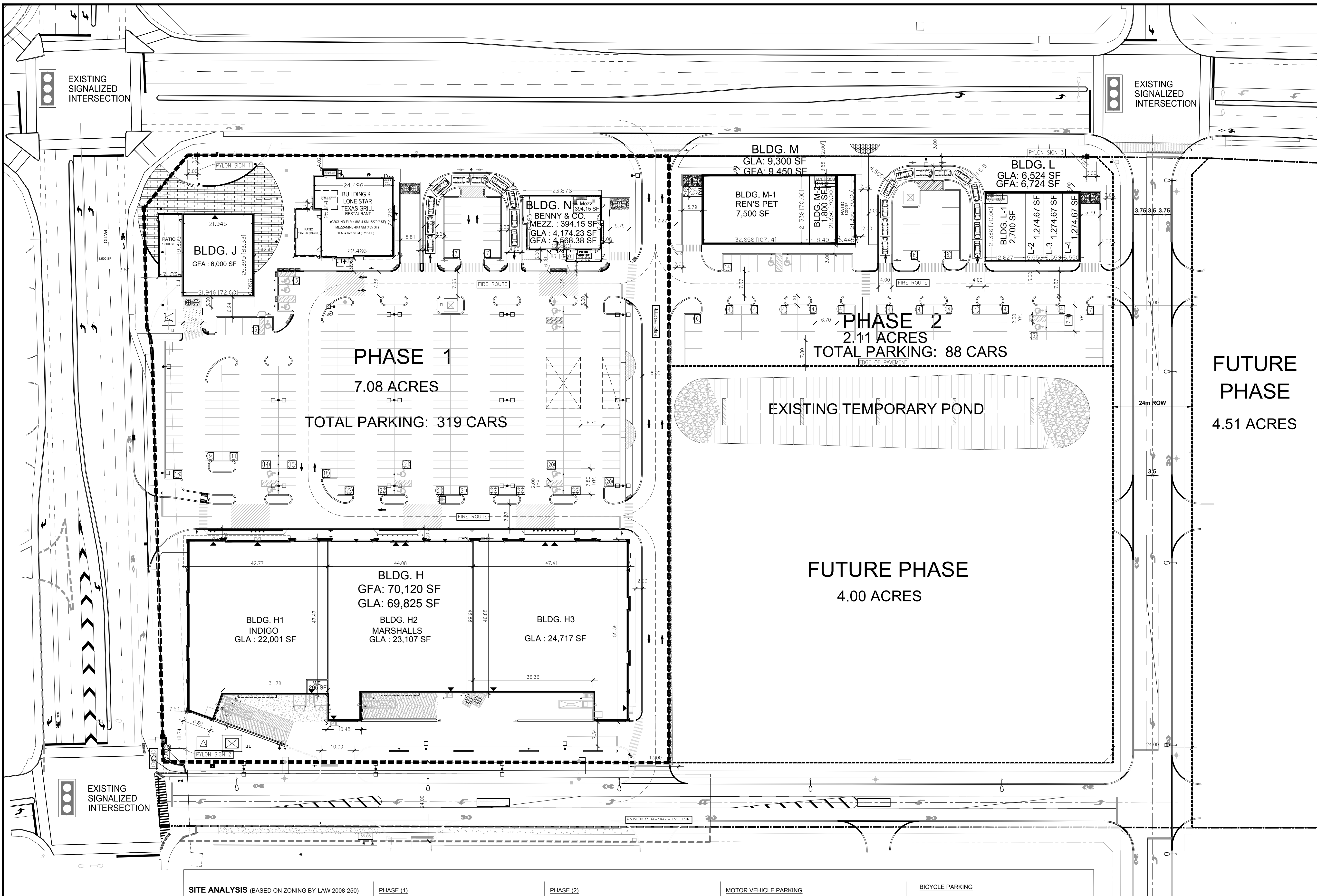
Six bicycle parking spaces are required for Phase 2 and 8 have been provided. For both phases 1 and 2, a total of 39 bicycle parking spaces are required and 52 have been provided.

Landscaping

The proposed landscape plan identifies multiple pockets of landscaping throughout Phase 2. Overall, the parking lot contains landscaped strips along the east and west sides of Phase 2, including a spine of vegetation somewhat down the middle of the parking area.

Smaller curbed islands punctuated the south limit of the drive aisle to the north. There are additional landscaped islands provided adjacent to the drive-through, both along the interior boundary and exterior boundary.

Lastly, there is significant landscaping along the frontage on Innes, which wraps around Buildings M and L.



THIS SITE PLAN DRAWING AND THE SITE BOUNDARIES BEARINGS ARE PREPARED BASED ON PLAN OF SURVEY OF PART OF LOT 1, CONCESSION 11, GEOGRAPHIC TOWNSHIP OF CUMBERLAND AND CITY OF OTTAWA, PIN (ALL OF: 14683-0600) PREPARED BY STANTEC GEOMATICS LTD., DATED JULY 17, 2015.

SA-002B

APPROVED ☐ REFUSED ☐
THIS ____ DAY OF _____, 20____
JEFF MCEWEN, P. ENG. MANAGER
DEVELOPMENT REVIEW EAST, PLANNING,
INFRASTRUCTURE AND ECONOMIC
DEVELOPMENT DEPARTMENT, CITY OF OTTAWA

NO.	REVISIONS	DATE	BY	CHK
1	ISSUED FOR COORDINATION	001-12-2017	PC	
2	ISSUED FOR COORDINATION	001-05-2017	PC	
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SITE PLAN

SCALE: 1:450

COMMERCIAL DEVELOPMENT
INNES ROAD & MER BLEUE
ORLEANS, ONTARIO
SmartREIT

PETROFF PARTNERSHIP ARCHITECTS
PETROFF

260 TOWN CENTRE BLVD., SUITE 300
MARKHAM ONTARIO CANADA L3R 8H8
TEL. 905.470.7000 FAX. 905.470.2500

DRAWN BY: PC
CHECKED BY: MW
DATE: SEPTEMBER 27, 2017
ISSUED: OCTOBER 12, 2017

PROJECT No: 06099
DWG. No: SA-002B

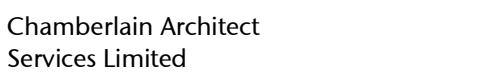
SITE ANALYSIS (BASED ON ZONING BY-LAW 2008-250)
MUNICIPAL ADDRESS: 2025 MER BLEUE ROAD, OTTAWA, ONTARIO
ZONING DESIGNATION: AM(1607)H(21)-h
AREA ON SCHEDULE 1: AREA "C"
SITE AREA
OVERALL LOT/SITE AREA: 82,192 M² (20.31 ACRES)
NEW ROADS AREA: 10,563 M² (2.61 ACRES)
PHASES NET AREA:
PHASE (1) NET AREA: 28,652 M² (7.08 ACRES)
PHASE (2) NET AREA: 8,547 M² (2.11 ACRES)
FUTURE PHASE AREA: 16,209 M² (4.00 ACRES)
PHASE (3) NET AREA: 18,256 M² (4.51 ACRES)
TOTAL SITE NET AREA: 72,330 M² (17.70 ACRES)

PHASE (1)
PHASE (1) AREA: 28,652 M² (7.08 ACRES)
BUILDING GROSS FLOOR AREA (GFA):
BLDG. 'J': 6,514 M² (70,120 SF)
BLDG. 'K': 557 M² (6,000 SF)
BLDG. 'L': 624 M² (6,715 SF)
BLDG. 'N': 424 M² (4,568 SF)
TOTAL GFA: 8,119 M² (87,403 SF)
BUILDING GROSS LEASEABLE AREA (GLA) (INCL. MEZZ.):
BLDG. 'J': 6,487 M² (69,825 SF)
BLDG. 'K': 557 M² (6,000 SF)
BLDG. 'L': 624 M² (6,715 SF)
BLDG. 'N': 424 M² (4,568 SF)
TOTAL GLA: 8,092 M² (87,108 SF)

PHASE (2)
PHASE (2) AREA: 28,613 M² (6.11 ACRES)
BUILDING GROSS FLOOR AREA (GFA):
BLDG. 'L': 625 M² (6,724 SF)
BLDG. 'M': 978 M² (10,450 SF)
TOTAL GFA: 1,503 M² (16,174 SF)
BUILDING GROSS LEASEABLE AREA (GLA):
BLDG. 'L': 606 M² (6,524 SF)
BLDG. 'M': 864 M² (9,300 SF)
TOTAL GLA: 1,470 M² (15,824 SF)
LOT COVERAGE (PHASE 1 & 2): 9,622/53,408 = 18.02%
MAX. PERMITTED BUILDING HEIGHT: 21.0M

MOTOR VEHICLE PARKING
RATE OF CALCULATION: 3.4 SPACES/100 M² GLA FOR SHOPPING CENTRE.
REQUIRED (MIN.): 3.4 X (8,092 M² + 1,470 M²/100) = 325 SPACES
PROVIDED: 407 SPACES
BARRIER FREE PARKING SPACE
REQUIRED (MIN.): 5 SPACES
PROVIDED: 16 SPACES
LOADING SPACE
REQUIRED (MIN.): 4 STANDARD SPACE + 1 OVERSIZED SPACE FOR SHOPPING CENTRE OVER 2,000 M² GFA.
PROVIDED: 7 STANDARD SPACES + 1 OVERSIZED SPACE

BICYCLE PARKING
BLDG. 'H' (RETAIL): @ 1 SPACE/250 M² GFA REQ'D (MIN.): 6,514 M²/250 = 26.06 SPACES
BLDG. 'J' (RESTAURANT): @ 1 SPACE/250 M² GFA REQ'D (MIN.): 557 M²/250 = 2.23 SPACES
BLDG. 'K' (RESTAURANT): @ 1 SPACE/250 M² GFA REQ'D (MIN.): 614 M²/250 = 2.46 SPACES
BLDG. 'N' (RESTAURANT): @ 1 SPACE/250 M² GFA REQ'D (MIN.): 424 M²/250 = 1.70 SPACE
BLDG. 'L' (RETAIL): @ 1 SPACE/250 M² GFA REQ'D (MIN.): 625 M²/250 = 2.50 SPACES
BLDG. 'M' (RETAIL): @ 1 SPACE/250 M² GFA REQ'D (MIN.): 878 M²/250 = 3.51 SPACES
TOTAL REQ'D (MIN.): 39 SPACES
TOTAL PROVIDED: PHASE 1 44 SPACES
TOTAL PROVIDED: PHASE 2 8 SPACES
TOTAL PROVIDED: 52 SPACES



Tel: 905.631.7777

www.chamberlainIPD.com

NO.	ISSUED	DATE
1	PERMIT	Sept. 28, 2017

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SEAL



2025 MER-BLEUE ROAD
ORLEANS, ON K4A 3T9

SHEET NAME

EXTERIOR
ELEVATION I

START DATE AUGUST 2017

DRAWN BY TP/PD

CHECKED BY _____ TP _____

SCALE $3/16" = 1'-0"$

PROJECT NO. 447894

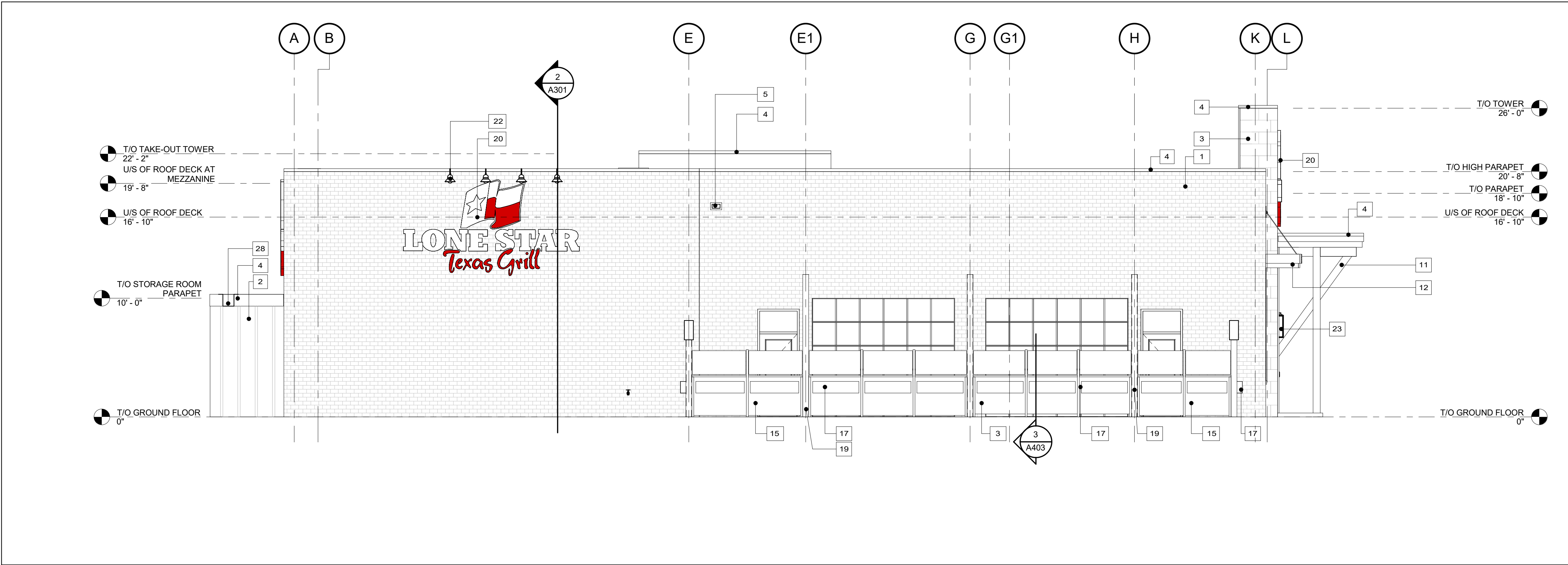
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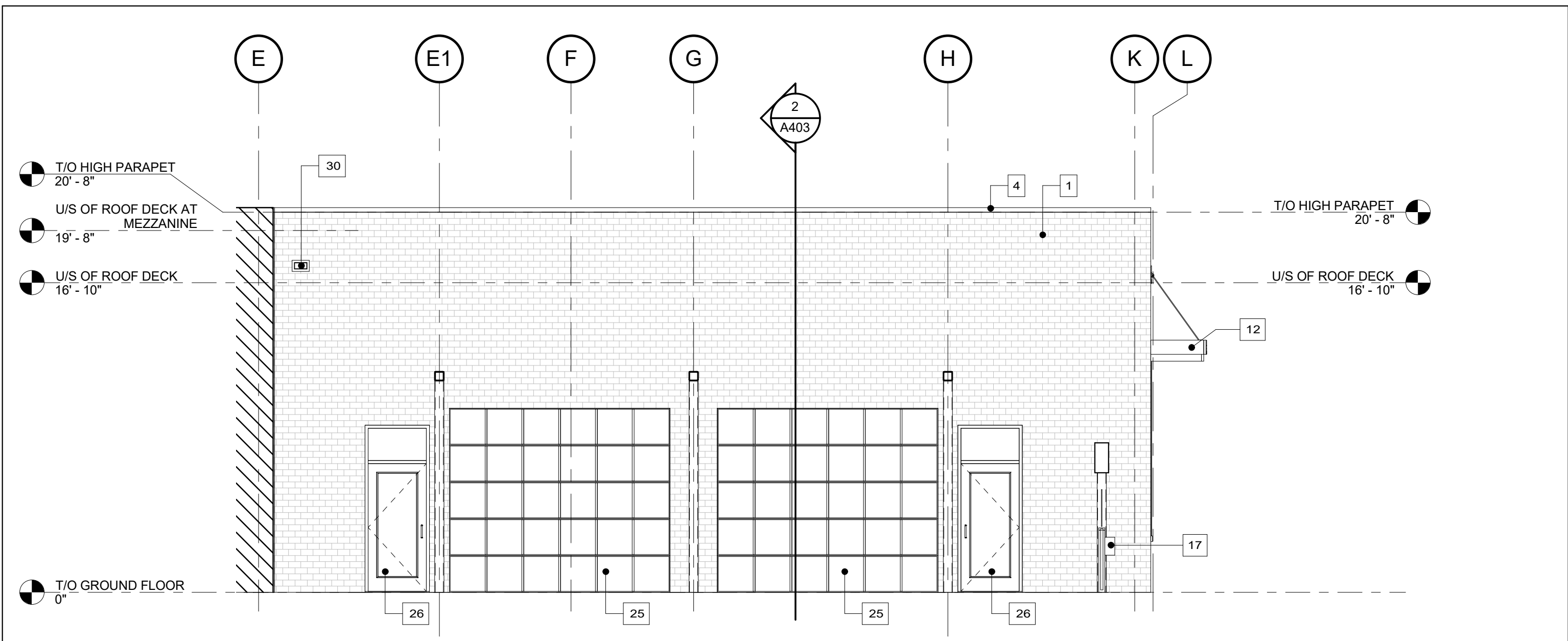


- | | |
|----|---|
| 1 | BRICK FACADE, WELLBAND BY FORTERRA BRICK. CSR SIZE. PAINT FINISH. COLOUR TO BE DETERMINED. |
| 2 | BOARD & BATTEN SIDING - HARDIE PLANK VERTICAL FIBER CEMENT SIDING WITH 2 1/2" BATTENS AT 16" O/C. PAINT FINISH TO BE DETERMINED |
| 3 | DECORATIVE BLOCK FACADE, FINESSE BY BRAMPTON BRICK. SIZE: 3 1/2"x11.38"x20 1/4". PAINT FINISH. COLOUR TO BE DETERMINED. |
| 4 | PREFINISHED METAL CAP FLASHING, COLOUR: BLACK |
| 5 | SCUPPER. REFER TO DETAIL 5/A501 |
| 6 | 1" x 6" METAL TRIM AT WINDOWS. PREFINISHED COLOUR: BLACK. REFER TO DETAIL 2/A502 |
| 7 | WINDOW W/ CLEAR GLAZING & MULLIONS. PREFINISHED COLOUR: BLACK |
| 8 | CORNER WINDOW CAP FLASHING BY WINDOW MANUFACTURER. PREFINISHED COLOUR: BLACK |
| 9 | ALUMINUM DOORS AND SIDELITES W/ CLEAR GLAZING AND MULLIONS. PREFINISHED COLOUR: BLACK. REFER TO DOOR SCHEDULE |
| 10 | 1'-0 1/4" DECORATIVE METAL DOOR TRIM. PREFINISHED COLOUR: BLACK |
| 11 | CEDAR WOOD MARQUEE STRUCTURE |
| 12 | CEDAR CANOPIES W/ BLACK METAL TIE BACKS. REFER TO DRAWING A503. |
| 13 | BARRIER FREE PUSH BUTTON. |
| 14 | PATIO: TIKI TORCH MOUNTED TO TOP OF 6" x6" HSS POSTS. PREFINISHED COLOUR: BLACK |
| 15 | PATIO RAILING: 2 1/2" x 4 5/8" HSS POSTS. PREFINISHED COLOUR: BLACK |
| 16 | PATIO EXIT DOOR: ALUMINUM DOOR FRAME WITH CEDAR WOOD PANEL INSERT SECURED WITH CLIPS. REFER TO DOOR SCHEDULE |
| 17 | PATIO: 11 3/4" DEEP WOOD PLANTER BOXES. SEE PLAN FOR SIZES. PREFINISHED COLOUR: BLACK. REFER TO INTERIOR DESIGN DRAWINGS. |
| 18 | MECHANICAL EXHAUST FAN GRILLS. SEE MECHANICAL DWGS. |
| 19 | 6" x 6" HSS POSTS. PREFINISHED COLOUR: BLACK |
| 20 | ILLUMINATED SIGNAGE BY SIGN COMPANY |
| 21 | SIGNAGE BY SIGN COMPANY |
| 22 | SIGN LIGHT: BARN LIGHT ELECTRIC MODEL #: 14RES12 WITH 15" GOSENECK ARM - RGN15 |
| 23 | FRONT ENTRY WALL SCONCE: MURRAY FELSS (GERRIE) MODEL #: OL11102DW |
| 24 | HOLLOW METAL DOOR PAINT FINISH TO MATCH ADJACENT SIDING |
| 25 | TEMPERED GLASS PANEL GARAGE DOOR. SEE DOOR SCHEDULE |
| 26 | EXTERIOR ALUMINUM DOOR WITH TEMPERED GLASS INSERT. PREFINISHED COLOUR: BLACK. SEE DOOR SCHEDULE |
| 27 | DECORATIVE METAL STAR TO MATCH THE BACKS |
| 28 | SCUPPER. REFER TO DETAIL 4/A402 |

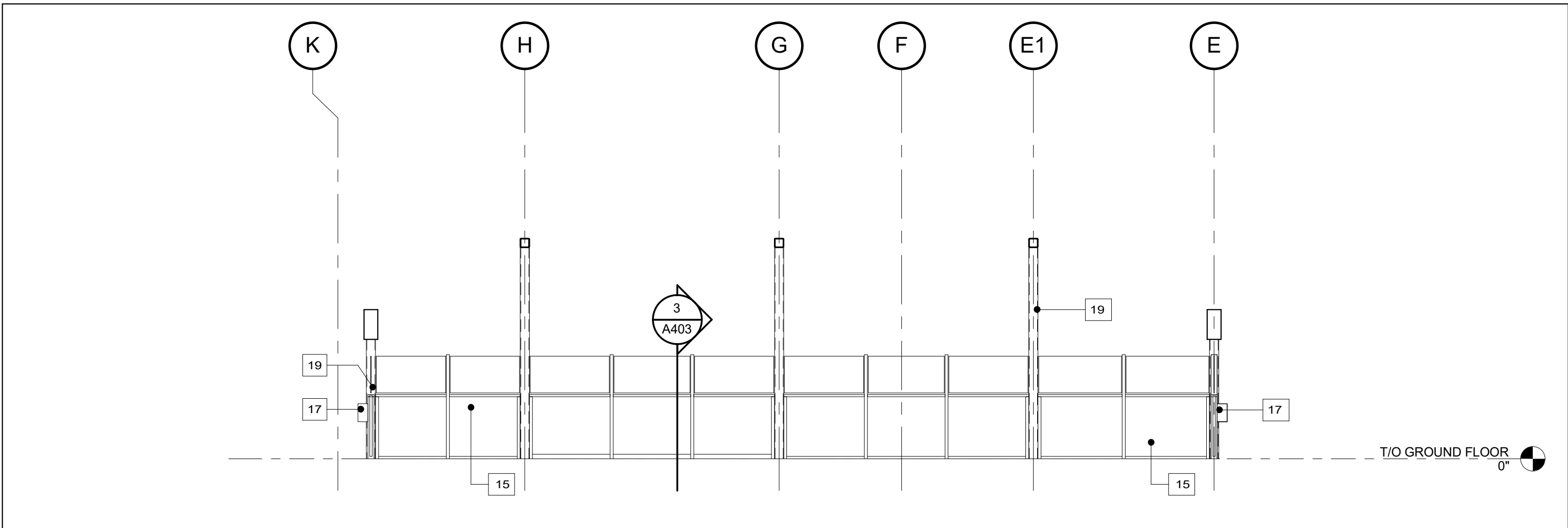
ALL SIGNAGE TO COMPLY WITH THE CITY'S SIGNAGE BYLAW. SIGN COMPANY TO OBTAIN PERMIT FOR ALL SIGNAGE & GRAPHICS.



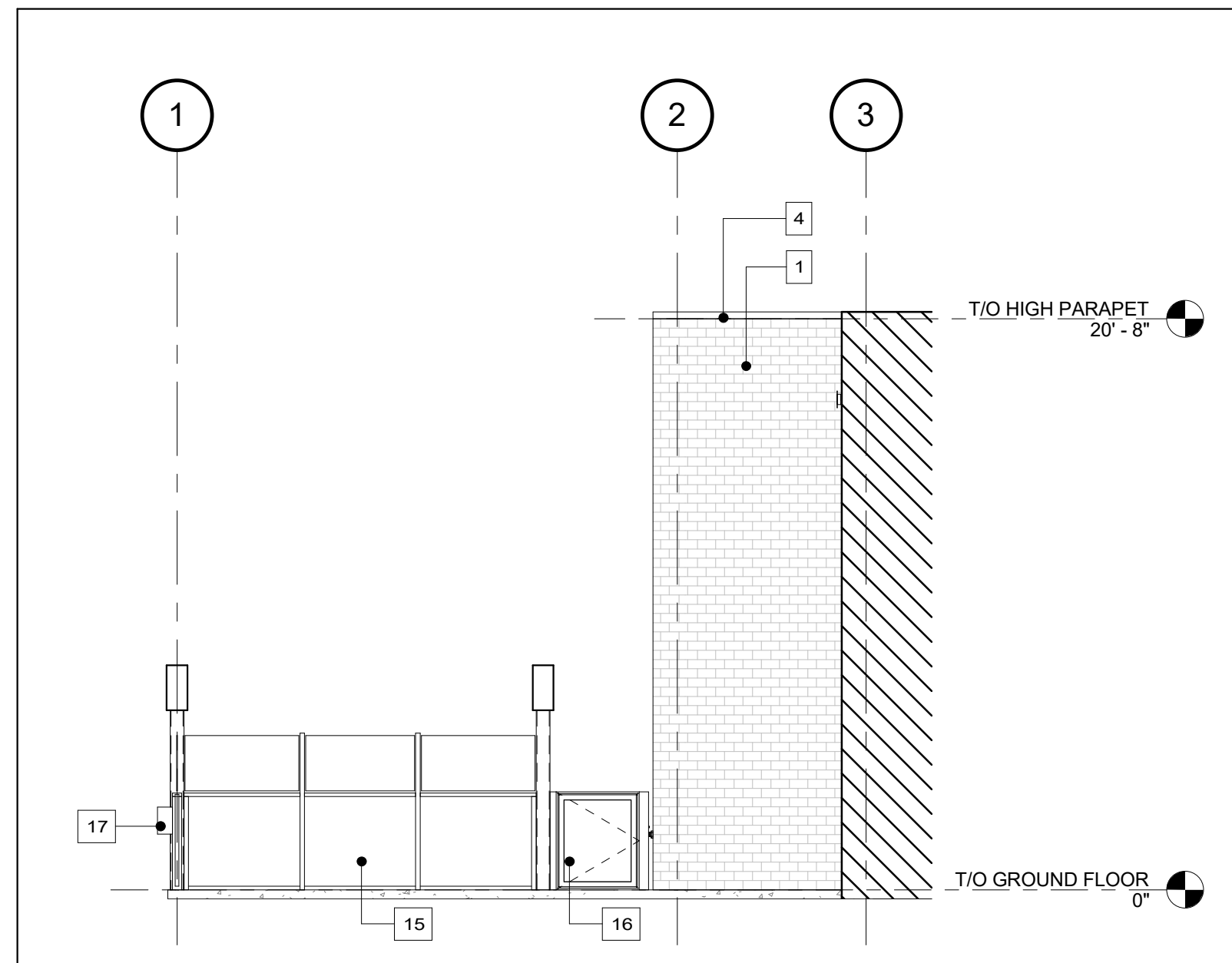
4 SOUTH (PATIO) ELEVATION
3/16" = 1'-0"



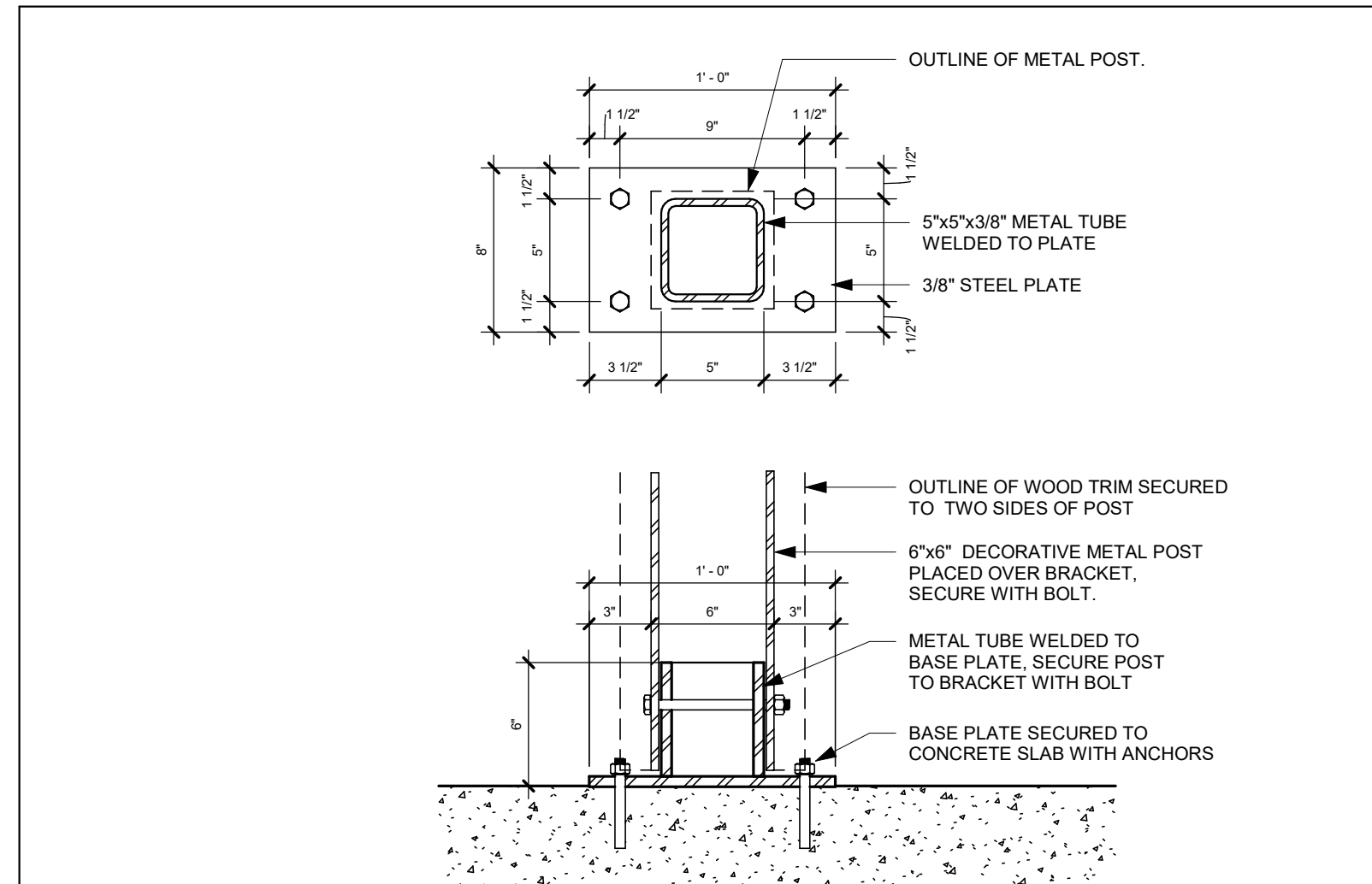
2 PATIO ELEVATION 1
3/16" = 1'-0"



1 PATIO ELEVATION 3
3/16" = 1'-0"



3 PATIO ELEVATION 2
3/16" = 1'-0"



6 DECORATIVE METAL POST BRACKET
1 1/2" = 1'-0"

EXTERIOR ELEVATION NOTES:

- BRICK FACADE, WELLAND BY FORTERRA BRICK. CSR SIZE. PAINT FINISH, COLOUR TO BE DETERMINED.
- BOARD & BATTEN SIDING - HARDIE PLANK VERTICAL FIBER CEMENT SIDING WITH 2 1/2" BATTONS AT 16" O.C. PAINT FINISH TO BE DETERMINED.
- DECORATIVE BLOCK FACADE, FINESSE BY BRAMPTON BRICK. SIZE: 3 1/2"x11 3/8"x23 1/4". PAINT FINISH, COLOUR TO BE DETERMINED.
- PREFINISHED METAL CAP FLASHING, COLOUR: BLACK.
- SCUPPER, REFER TO DETAIL 5/A501.
- 1" x 6" METAL TRIM AT WINDOWS, PREFINISHED COLOUR: BLACK. REFER TO DETAIL 2/A502.
- WINDOW W/ CLEAR GLAZING & MULLIONS, PREFINISHED COLOUR: BLACK.
- CORNER WINDOW CAP FLASHING BY WINDOW MANUFACTURER, PREFINISHED COLOUR: BLACK.
- ALUMINUM DOORS AND SIDELITES W/ CLEAR GLAZING AND MULLIONS, PRE-FINISHED COLOUR: BLACK. REFER TO DOOR SCHEDULE.
- 1'-0 1/4" DECORATIVE METAL DOOR TRIM, PREFINISHED COLOUR: BLACK.
- CEDAR WOOD MARQUEE STRUCTURE.
- CEDAR CANOPIES W/ BLACK METAL TIE BACKS, REFER TO DRAWING A503.
- BARRIER FREE PUSH BUTTON.
- PATIO: TIKI TORCH MOUNTED TO TOP OF 6" x 8" HSS POSTS, PREFINISHED COLOUR: BLACK.
- PATIO RAILING: 2 1/2" x 4 5/8" HSS POSTS, PREFINISHED COLOUR: BLACK.
- PATIO EXIT DOOR: ALUMINUM DOOR FRAME WITH CEDAR WOOD PANEL, INSERT SECURED WITH CLIPS, REFER TO DOOR SCHEDULE.
- PATIO: 11 3/4" DEEP WOOD PLANTER BOXES, SEE PLAN FOR SIZES, PREFINISHED COLOUR: BLACK. REFER TO INTERIOR DESIGN DRAWINGS.
- MECHANICAL EXHAUST FAN GRILLS, SEE MECHANICAL DWGS.
- 6" x 6" HSS POSTS, PREFINISHED COLOUR: BLACK.
- ILLUMINATED SIGNAGE BY SIGN COMPANY.
- SIGNAGE BY SIGN COMPANY.
- SIGN LIGHT: BARN LIGHT ELECTRIC MODEL #: 14RES12 WITH 15" GOOSENECK ARM - RGN15.
- FRONT ENTRY WALL SCONCE: MURRAY FELSS (GERRIE) MODEL #: OL11102DWZ.
- HOLLOW METAL DOOR PAINT FINISH TO MATCH ADJACENT SIDING.
- TEMPERED GLASS PANEL GARAGE DOOR, SEE DOOR SCHEDULE.
- EXTERIOR ALUMINUM DOOR WITH TEMPERED GLASS INSERT, PREFINISHED COLOUR: BLACK, SEE DOOR SCHEDULE.
- DECORATIVE METAL STAR TO MATCH TIE BACKS.
- SCUPPER, REFER TO DETAIL 4/A402.

ALL SIGNAGE TO COMPLY WITH THE CITY'S SIGNAGE BYLAW. SIGN COMPANY TO OBTAIN PERMIT FOR ALL SIGNAGE & GRAPHICS.

Chamberlain Architect
Services Limited

4671 Palladium Way
Unit 1
Burlington, Ontario L7M 0W9
CANADA

Tel: 905.631.7777

www.chamberlainIPD.com

NO.	ISSUED	DATE
1	PERMIT	Sept. 26, 2017

DO NOT SCALE DRAWINGS. USE ONLY DRAWINGS MARKED "ISSUED FOR CONSTRUCTION". VERIFY CONFIGURATIONS AND DIMENSIONS ON SITE BEFORE BEGINNING WORK. NOTIFY ARCHITECT IMMEDIATELY OF ANY ERRORS, OMISSIONS OR DISCREPANCIES.

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SEAL



**LONE STAR TEXAS
GRILL ORLEANS**

2025 MER-BLEUE ROAD
ORLEANS, ON K4A 3T9

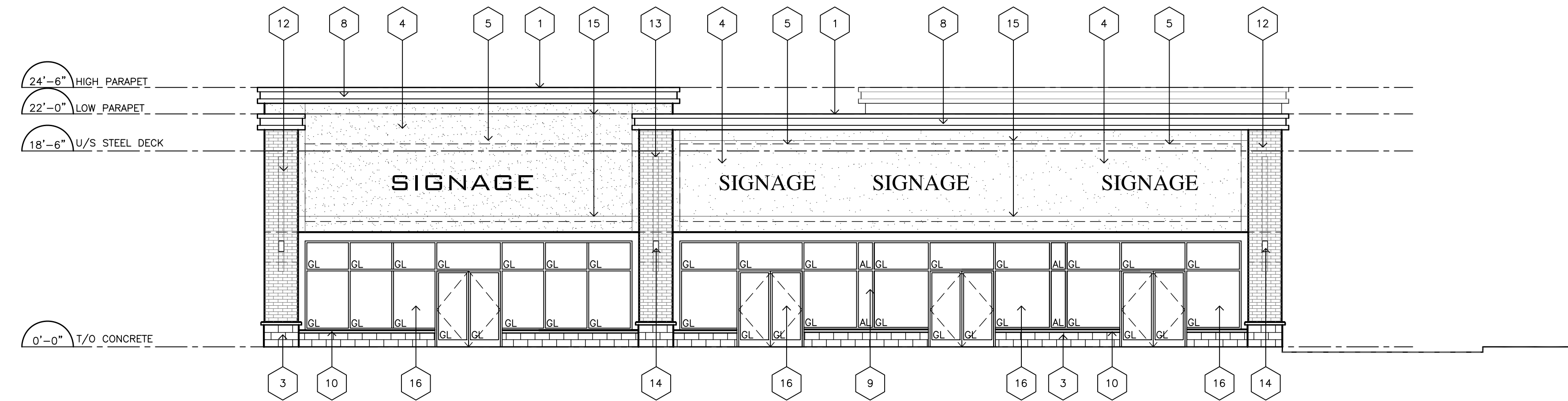
SHEET NAME

**EXTERIOR
ELEVATION II**

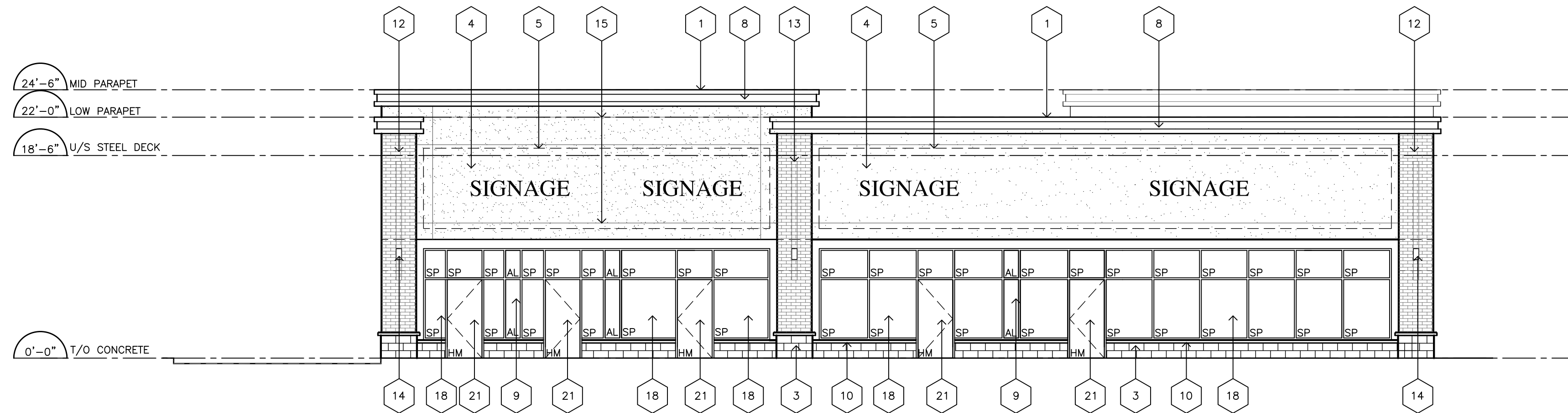
START DATE	AUGUST 2017
DRAWN BY	TP/PD
CHECKED BY	TP
SCALE	As indicated
PROJECT NO.	117081

DRAWING

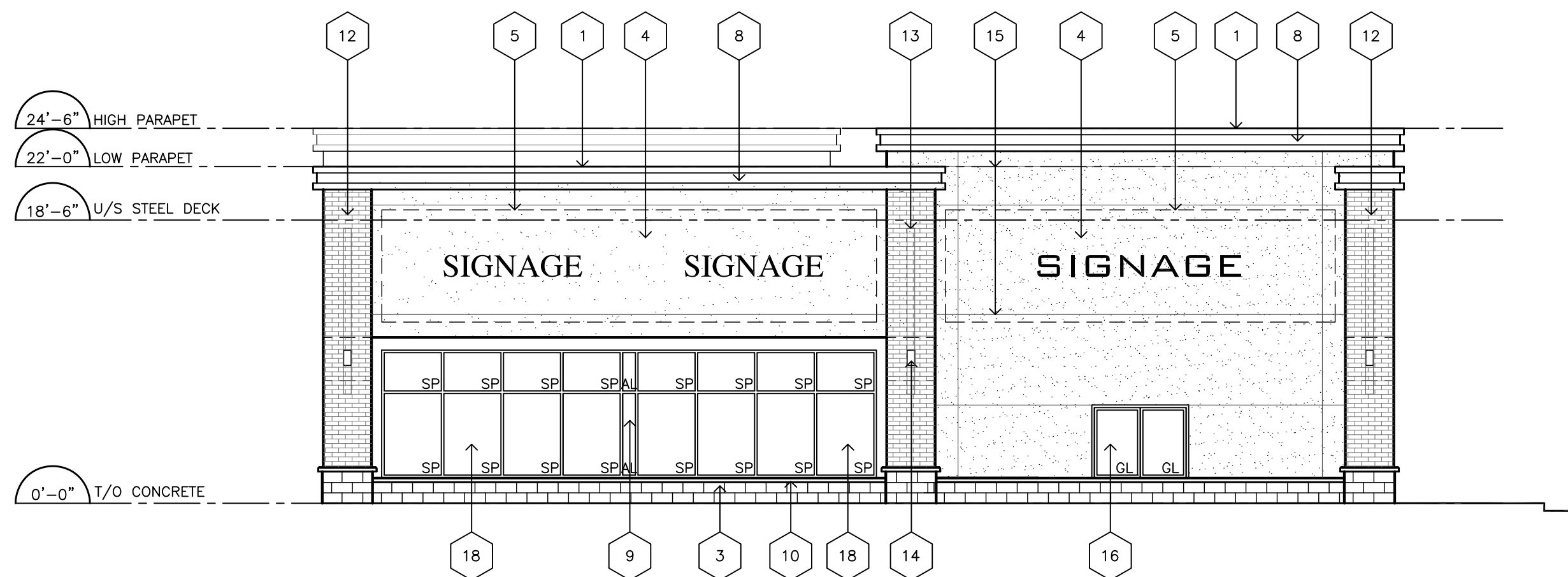
A202



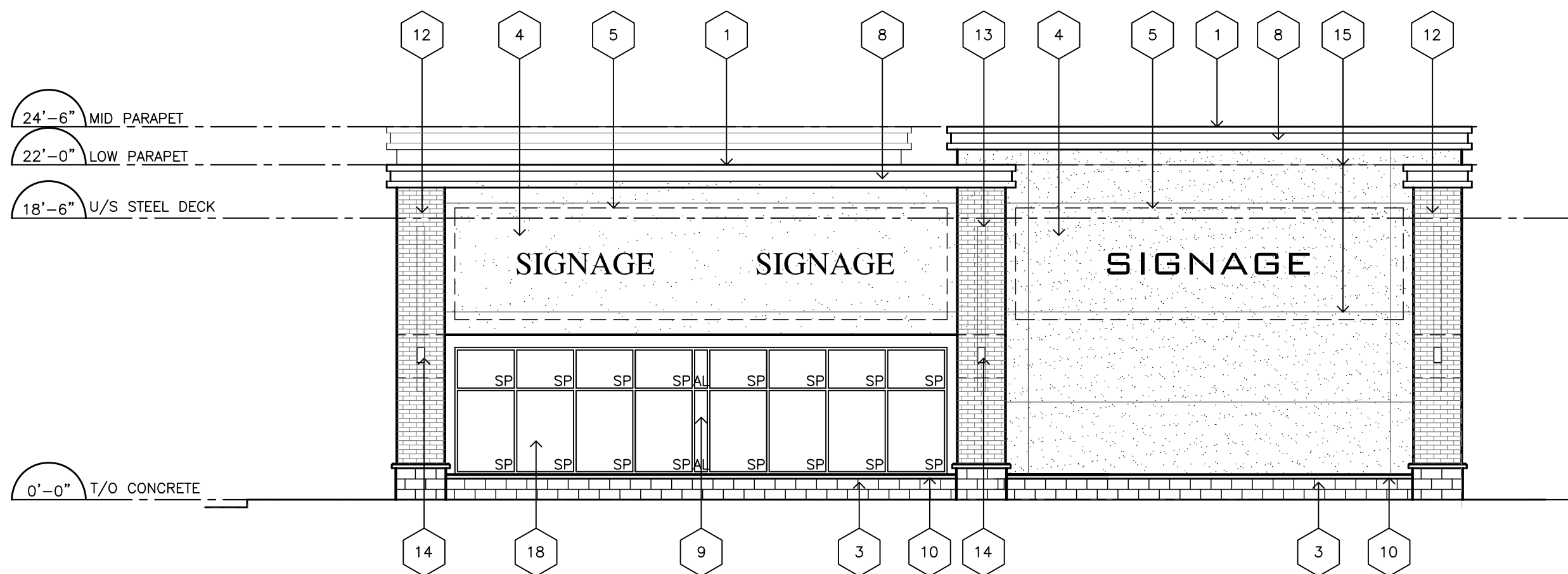
1 SOUTH ELEVATION
A1



2 NORTH ELEVATION
A1



3 WEST ELEVATION
A1



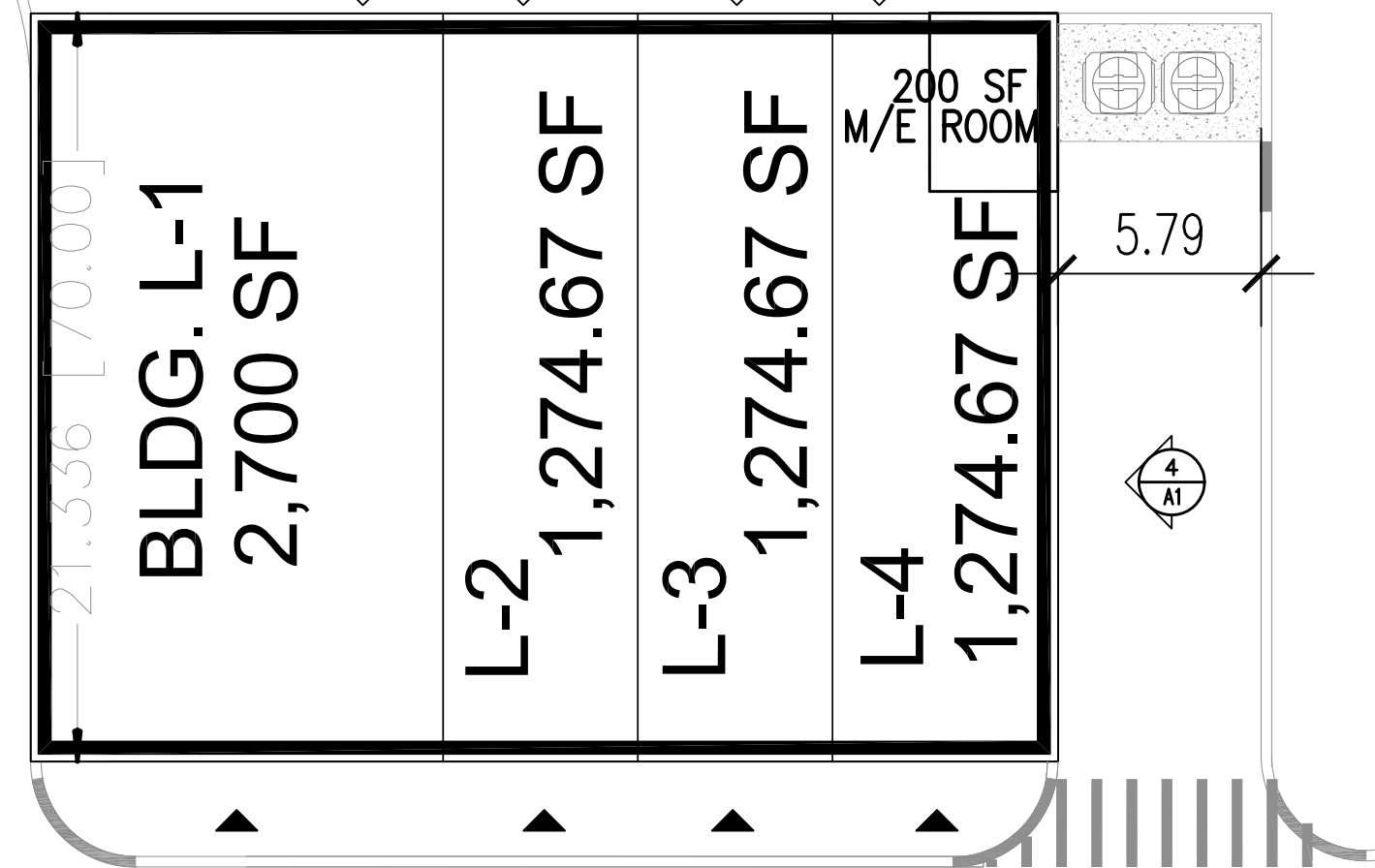
4 EAST ELEVATION
A1

LEGEND

- 1 PREFINISHED METAL FLASHING - VIOLET
COLOUR: VW 56072 CHARCOAL
- 2 E.I.F.S. - DRYVIT - SANDPEBBLE
COLOUR: BM WROUGHT IRON
- 3 CONCRETE BLOCK - PERMACON - SPLIT FACE
COLOUR: CHARCOAL
- 4 E.I.F.S. - DRYVIT - SANDPEBBLE
COLOUR: BM COLLINGWOOD
- 5 SIGNAGE
3/4" 3/4" EXTERIOR GRADE PLYWOOD BEHIND E.I.F.S. FOR SIGNAGE SUPPORT.
(FIRE RATED WHERE APPLICABLE.) FINAL CONNECTION BY TENANT ELECTRICAL
CONTRACTOR TYP.
- 6 RESERVED:
- 7 RESERVED:
- 8 PREFINISHED METAL PARAPET CORNICE -
COLOUR: VW 56068 BLACK
- 9 ALUMINIUM COVER PANEL -
COLOUR: VW 56068 BLACK
- 10 CONCRETE SILL - PERMACON - BUFFED
COLOUR: CARERA WHITE
- 11 GLAZING IN CLEAR ANODIZED ALUMINIUM FRAME
- 12 FACE BRICK - HANSON - MATT
COLOUR: RIVERDALE
- 13 BRICK GROOVE -
150MM (W) X 75MM (D)
- 14 DECORATIVE LIGHT FIXTURE
MOUNT TO 3/4" EXTERIOR GRADE PLYWOOD CONCEALED BEHIND WALL FINISH
- 15 STUCCO REVEAL
16MM U-SHAPED
- 16 GLAZED DOORS & WINDOWS: (GL)
CLEAR LOW E GLAZING IN BLACK ANODIZED THERMALLY BROKEN ALUMINIUM
FRAMES (UNLESS OTHERWISE NOTED)
- 17 RESERVED:
- 18 SPANDREL PANEL
GLASS SPANDREL WITH INSULATED METAL BACK PAN IN BLACK ANODIZED
THERMALLY BROKEN ALUMINIUM FRAMES - PPG, BLACK MAGIC PPG1001-7
- 19 RESERVED:
- 20 RESERVED:
- 21 PAINTED HOLLOW METAL INSULATED DOOR
COLOUR: CHARCOAL

5 N.T.S.

BLDG. L
GLA: 6,524 SF
GFA: 6,724 SF



6 KEY PLAN
A1 N.T.S.

CLIENT:

SmartREIT
(ORLEANS II) Inc.
And Mer Bleue
Shopping Centres
Limited

PROJECT:

Building L
2025 Mer Bleue Rd.
Orleans, ON

SHEET TITLE:

BUILDING
ELEVATIONS

PROJECT NO.
7034

SCALE:
1/8"=1'-0"

DRAWN:
KS

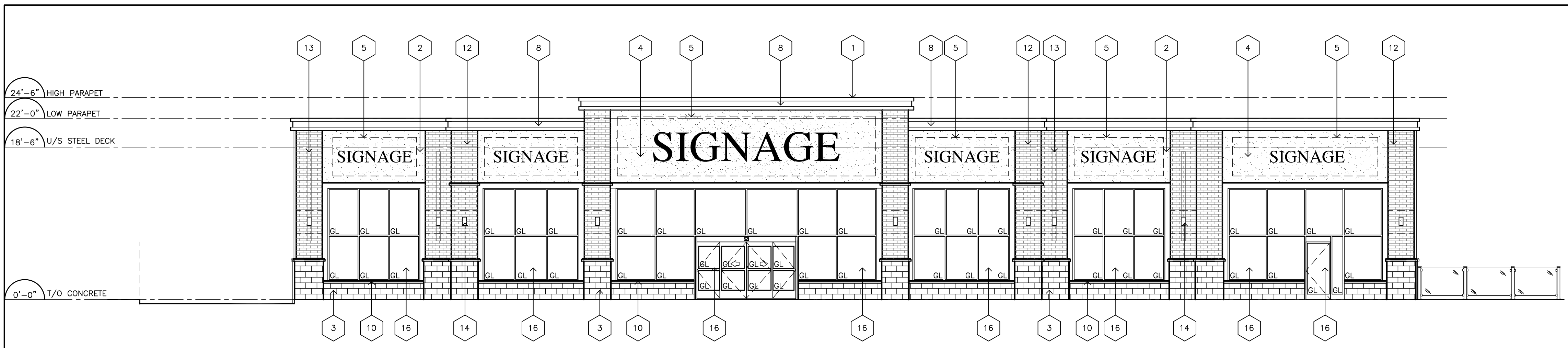
CHECKED:
GP

FILE NO.
7034

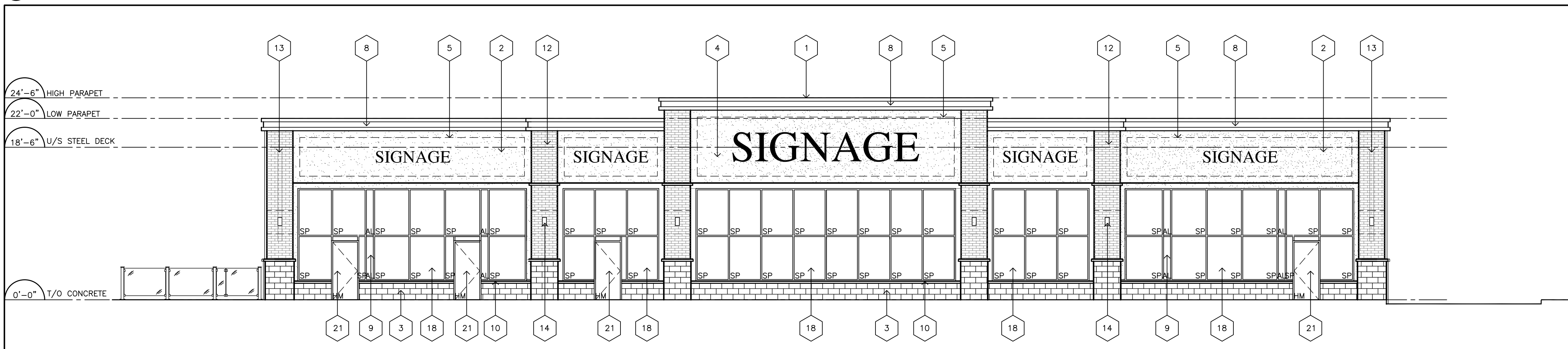
DATE:
SEE REV.

SHEET NO:

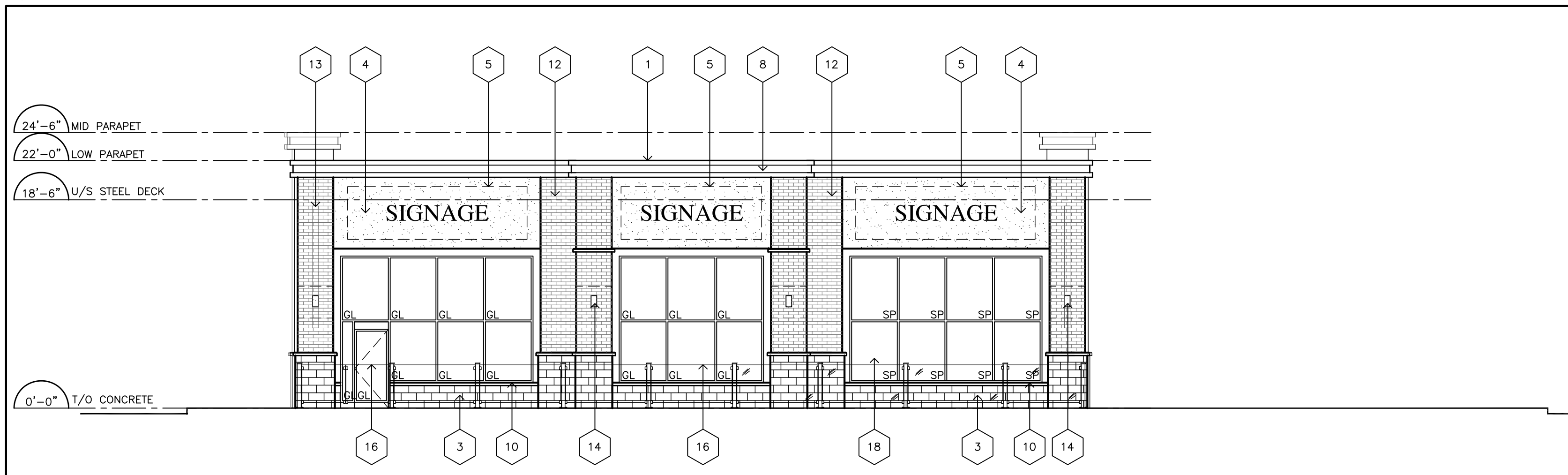
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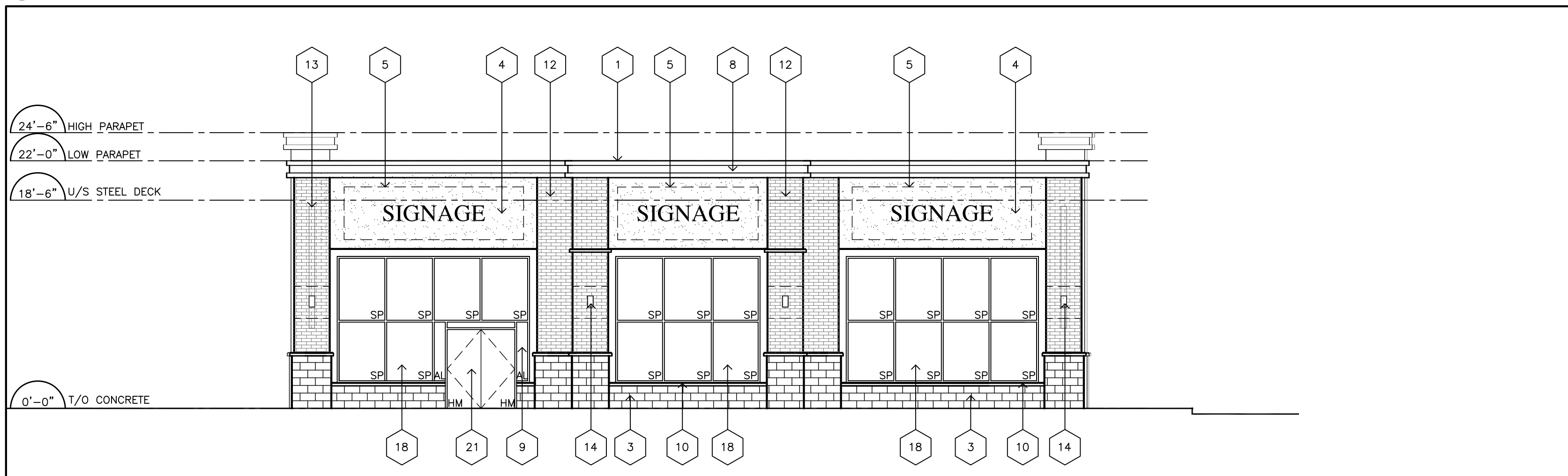
1 SOUTH ELEVATION
A2



2 NORTH ELEVATION
A2



3 EAST ELEVATION
A2

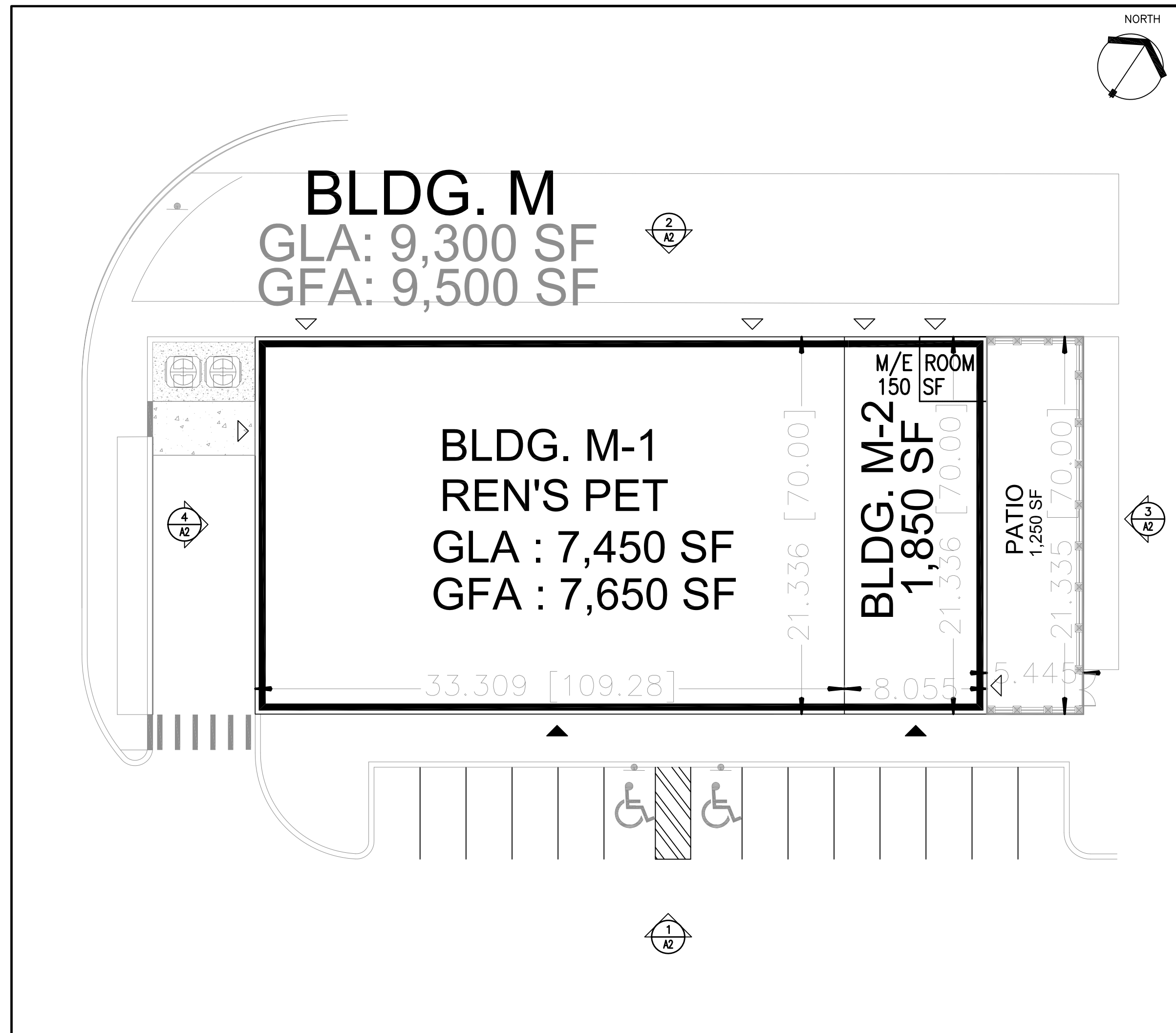


4 WEST ELEVATION
A2

LEGEND

- 1 PREFINISHED METAL FLASHING - VICWEST
COLOUR: VW 56072 CHARCOAL
- 2 E.I.F.S. - DRYVIT - SANDPEBBLE
COLOUR: BM WROUGHT IRON
- 3 CONCRETE BLOCK - PERMACON - SPLIT FACE
COLOUR: CHARCOAL
- 4 E.I.F.S. - DRYVIT - SANDPEBBLE
COLOUR: BM COLLINGWOOD
- 5 SIGNAGE
6/8" 3/4" EXTERIOR GRADE PLYWOOD BEHIND E.I.F.S. FOR SIGNAGE SUPPORT,
(FIRE RATED WHERE APPLICABLE.) FINAL CONNECTION BY TENANT ELECTRICAL
CONTRACTOR TYP.
- 6 RESERVED:
- 7 RESERVED:
- 8 PREFINISHED METAL PARAPET CORNICE -
COLOUR: VW 56068 BLACK
- 9 ALUMINIUM COVER PANEL -
COLOUR: VW 56068 BLACK
- 10 CONCRETE SILL - PERMACON - BUFFED
COLOUR: CARERA WHITE
- 11 GLAZING IN CLEAR ANODIZED ALUMINIUM FRAME
- 12 FACE BRICK - HANSON - MATT
COLOUR: RIVERDALE
- 13 BRICK GROOVE -
150MM (W) X 75MM (D)
- 14 DECORATIVE LIGHT FIXTURE
MOUNT TO 3/4" EXTERIOR GRADE PLYWOOD CONCEALED BEHIND WALL FINISH
- 15 STUCCO REVEAL
16MM U-SHAPED
- 16 GLAZED DOORS & WINDOWS: (GL)
CLEAR LOW E GLAZING IN BLACK ANODIZED THERMALLY BROKEN ALUMINIUM
FRAMES (UNLESS OTHERWISE NOTED)
- 17 RESERVED:
- 18 SPANDREL PANEL
GLASS SPANDREL WITH INSULATED METAL BACK P.S. IN BLACK ANODIZED
THERMALLY BROKEN ALUMINIUM FRAMES - PPG, BLACK MAGIC PPG1001-7
- 19 RESERVED:
- 20 RESERVED:
- 21 PAINTED HOLLOW METAL INSULATED DOOR
COLOUR: CHARCOAL

5 N.T.S.



6 KEY PLAN
A2 N.T.S.

CLIENT:

SmartREIT
(ORLEANS II) Inc.
And Mer Bleue
Shopping Centres
Limited

PROJECT:

Building M
2025 Mer Bleue Rd.
Orleans, ON

SHEET TITLE:

BUILDING
ELEVATIONS

PROJECT NO.

7034

SCALE:

1/8"=1'-0"

DRAWN:

KS

CHECKED:

GP

FILE NO.

7034

DATE:

SEE REV.

SHEET NO:

A2



2	ISSUED FOR SPA	NOV 01/17
1	ISSUED FOR REVIEW	SEP 20/17
No.	ISSUE	DATE

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CLIENT:

SmartREIT
(ORLEANS II) Inc.
And Mer Bleue
Shopping Centres
Limited

PROJECT:

Building N
2025 Mer Bleue Rd.
Orleans, ON

SHEET TITLE:

BUILDING ELEVATIONS

PROJECT NO.

7034

SCALE:
1/8"=1'-0"

DRAWN:
KS

CHECKED
GP

FILE NO
7034

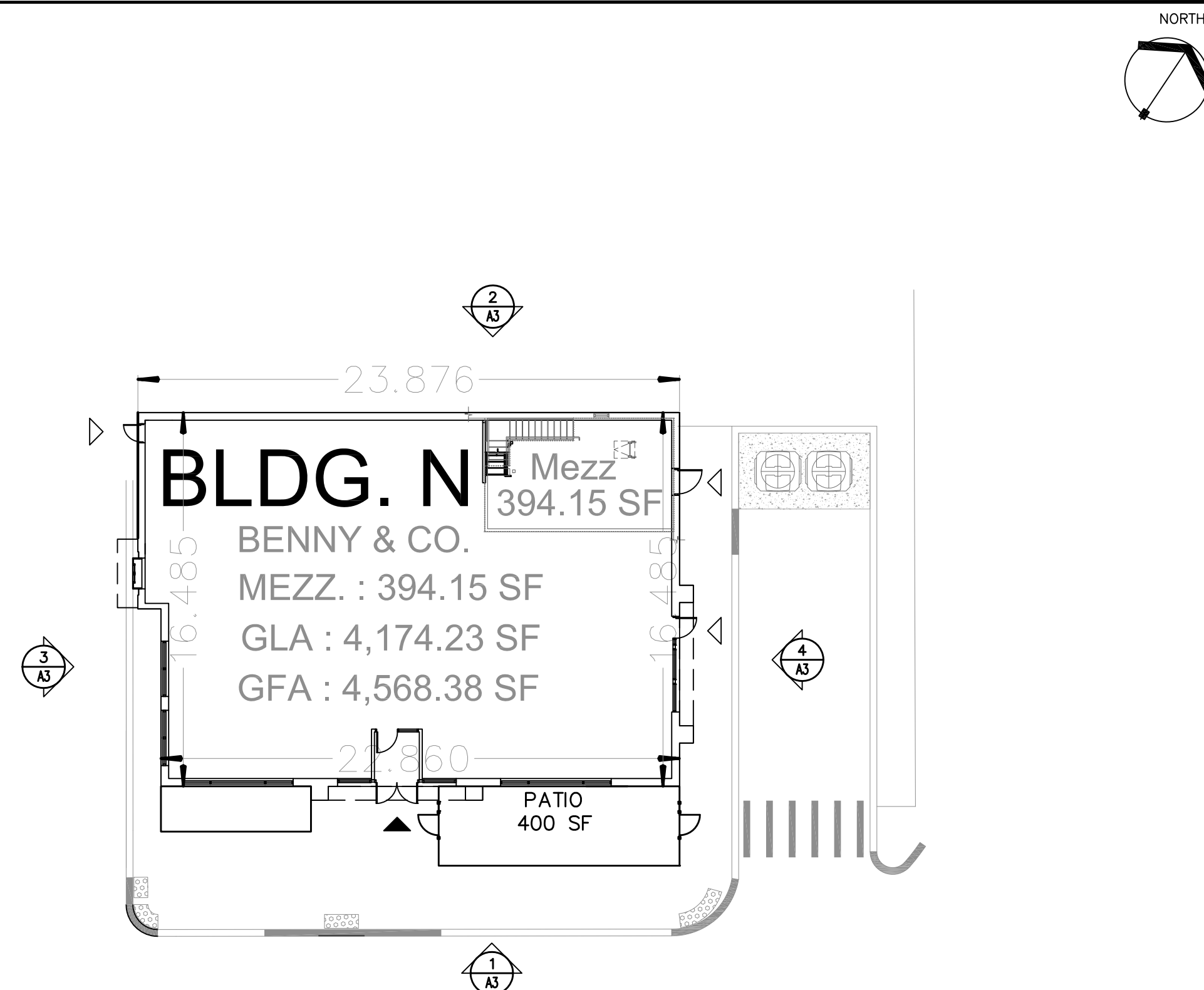
DATE:
SEE REV.

A3

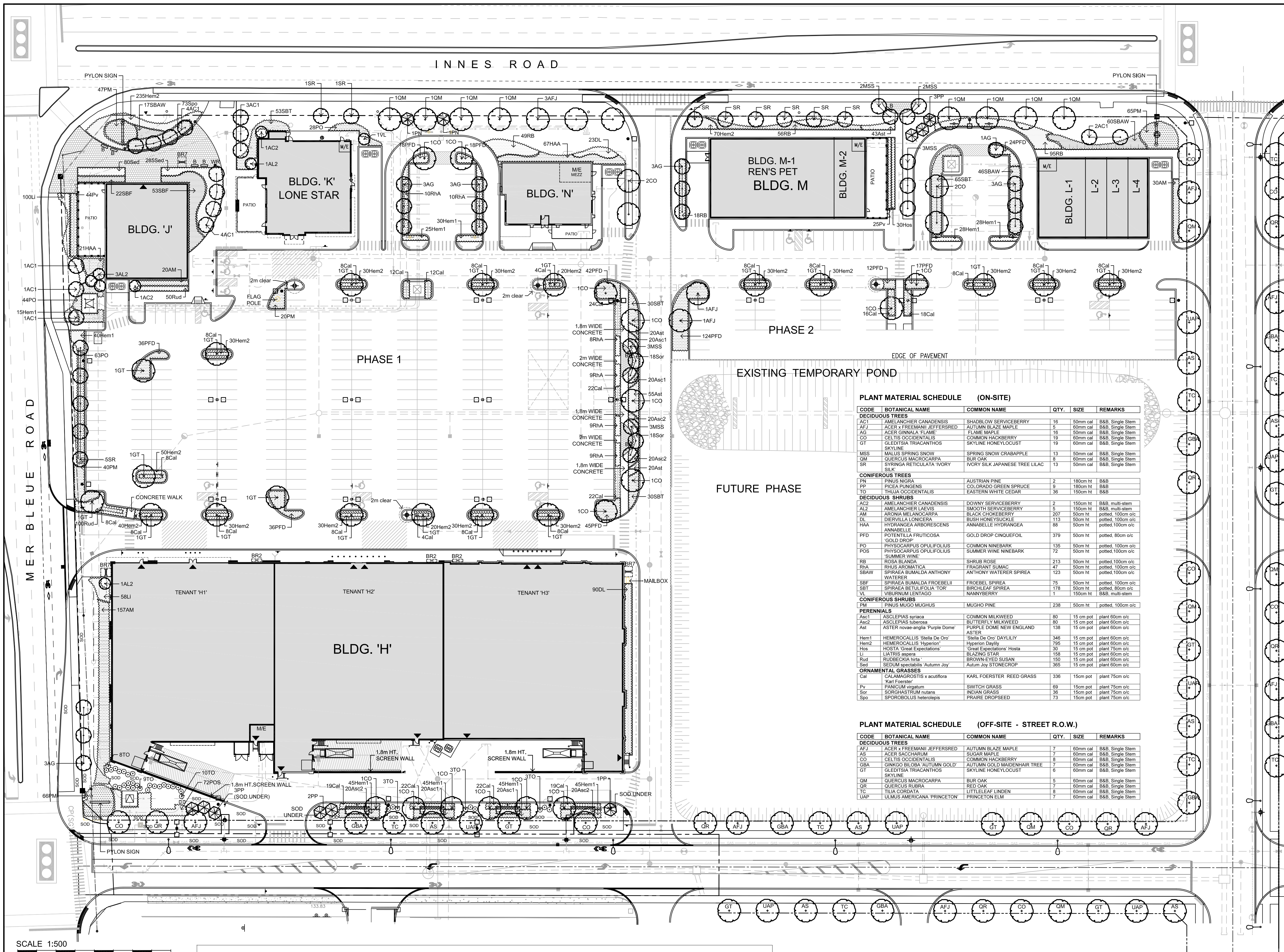


- | | |
|----|--|
| 1 | DECORATIVE LIGHT FIXTURE
MOUNT TO 3/4" EXTERIOR GRADE PLYWOOD CONCEALED BEHIND WALL |
| 2 | PREFINISHED METAL CANOPY - VICWEST
COLOUR VW GC-16080 |
| 3 | DRIVE-THRU GLAZED WINDOW
CLEAR LOW E GLAZING IN CLEAR ANODIZED THERMALLY BROKEN ALUMINIUM
FRAMES (UNLESS OTHERWISE NOTED) |
| 4 | OPAQUE FILMED GLAZED WINDOWS: (OP3)
OPAQUE FILMED GLAZING IN BLACK ANODIZED THERMALLY BROKEN ALUMINIUM
FRAMES (UNLESS OTHERWISE NOTED) |
| 5 | PREFINISHED METAL SCUPPER - VICWEST
COLOUR VW GC-16080 |
| 6 | CLEAR GLAZED WINDOWS: (GL)
CLEAR LOW E GLAZING IN BLACK ANODIZED THERMALLY BROKEN ALUMINIUM
FRAMES (UNLESS OTHERWISE NOTED) |
| 7 | PAINTED HOLLOW METAL INSULATED DOOR
COLOUR TO MATCH ADJACENT WALL |
| 8 | PREFINISHED METAL DISPLAY FLIP FRAME
C/W 3/4" EXTERIOR GRADE PLYWOOD BEHIND E.I.F.S. FOR FRAME
SUPPORT, (FIRE RATED WHERE APPLICABLE). FINAL CONNECTION BY TENANT
ELECTRICAL CONTRACTOR TYP. COLOUR - TBD |
| 9 | PREFINISHED METAL SIDING
COLOUR - CEDAR WOOD IMITATION |
| 10 | E.I.F.S. - LES ENDUITS STEF - SANDPEBBLE
COLOUR: MATCH-9368 |
| 11 | E.I.F.S. - LES ENDUITS STEF - SANDPEBBLE
COLOUR: 6206-31 |
| 12 | E.I.F.S. - LES ENDUITS STEF - SANDPEBBLE
COLOUR: 6207-73 |
| 13 | STONE SILL - CENTRE DE PIERRES MIRABEL
MODEL: ST-MARC LIMESTONE |
| 14 | CONCRETE BLOCK - PERMACON
MODEL: PIERRE LAFFIT, NEWPORT GREY, GREY MORTAR |
| 15 | SIGNAGE:
C/W 3/4" EXTERIOR GRADE PLYWOOD BEHIND E.I.F.S. FOR SIGNAGE SUPPORT
(FIRE RATED WHERE APPLICABLE). FINAL CONNECTION BY TENANT ELECTRICAL
CONTRACTOR TYP. |
| 16 | PREFINISHED METAL CAP FLASING - VICWEST
COLOUR VW GC-16080 |
| 17 | RESERVED: |
| 18 | SPANDREL PANEL
LESS SPANDREL WITH INSULATED METAL BACK PAN IN BLACK ANODIZED
THERMALLY BROKEN ALUMINIUM FRAMES - PPG, BLACK MAGG PPG1001-7 |

5 LEGEND
A3 N.T.S.



6 KEY PLAN
A3 N.T.S.



LEGEND / SYMBOL

PROPOSED DECIDUOUS TREE

PROPOSED CONIFEROUS TREE

PROPOSED DECIDUOUS/CONIFEROUS SHRUBS AND GROUNDCOVER

UNIT PAVING
type: MELVILLE 80 (Permacore)
color: RANGE MARGAUX BEIGE
pattern: MODULAR

0.9m HT. MODULAR DOUBLE-SIDED WALL
FEATURE WALL

BENCH
type: LLY (Victor Stanley)
color: BLACK

WASTE RECEPTACLE
type: STANDARD PHOENIX
(Classic Displays)
color: BLACK

5-RING (7 CAPACITY)
BICYCLE RACK
color: BLACK

1-RING (2 CAPACITY)
BICYCLE RACK
color: BLACK

LIGHT STANDARD

GENERAL NOTES

1

This drawing shall be read in conjunction with all relevant Architectural, Engineering and related Drawings and Documents.

2

Refer to Engineering Drawings for Grading and Servicing.

3

Refer to Architectural Drawings for Site layout.

4

Contractor shall provide the location(s) of all services/utilities by consulting Municipal Authorities and Utility companies and shall be responsible for adequate protection from damage during construction.

5

Plant material shall be No. 1 Grade and shall comply with the Metric Guide Specifications For Nursery Stock (latest edition), published by the Canadian Nursery Trades Association.

6

Plant Material locations are Schematic / Approximate only. Contractor shall stake out locations on site prior to work.

7

Contractor shall guarantee all plant material for a period of one (1) full year from the date of final acceptance.

8

Shrubs shall be planted in a continuous prepared bed of 450mm depth planting soil mix covered over with a woven polypropylene weed control fabric (LANDSCAPE FABRIC-Green-Line by Thrace-LINQ) and 75mm depth composted pine mulch to finish grade, as specified.

9

Perennials and Ornamental Grasses shall be planted in a continuous prepared bed of 450mm depth planting soil mix covered over with 75mm depth composted pine mulch to finish grade (NO WEED CONTROL FABRIC), as specified.

10

Sod areas to receive 125mm topsoil. Sod shall be No. 1 quality conforming to the Canadian Nursery Sod Growers Specification.

11

Reinstate all areas damaged or disturbed beyond the limit of Work.

12

Plant Material substitutions shall not be permitted without written approval from the Consultant.

13

Contractor shall advise Consultant a minimum of 48hrs. prior to proceeding landscape work and any required Field Reviews.

14

TREE PLANTING SETBACK RESTRICTIONS FOR BUILDING FOUNDATIONS CONFORM TO RECOMMENDATIONS AND STANDARDS SET OUT IN THE GEOTECHNICAL REPORT.

15

THIS PLAN HAS BEEN PREPARED FOR MUNICIPAL SITE PLAN APPROVAL ONLY AND MAY NOT BE USED FOR ANY OTHER PURPOSE.

KEY

PLAN

(N.T.S.)

NOT FOR CONSTRUCTION

1.

OCT.25/17

ISSUE FOR SITE PLAN APPROVAL

2.

OCT.18/17

ISSUE FOR REVIEW / COORDINATION

no

date

revision

consultants

LEVSTEK CONSULTANTS

LANDSCAPE ARCHITECTS

5871 Hugh Crescent - Ottawa - Ontario - K0A 2W0

613 - 824-8578

project

SMARTREIT ORLEANS II DEVELOPMENT

2025 MER BLEUE ROAD, OTTAWA, ON

drawn

MB

checked

RL

date

OCT 2017

project no.

1111.03

drawing title

LANDSCAPE PLAN

PHASE 1 AND 2

revision

drawing no.

L1.01

DECIDUOUS TREE PLANTING

DO NOT CHANGE OR CUT LEADER DAMAGED OR BRUSHY LEADERS WILL NOT BE ACCEPTED

ARBORETE LOCATE 1/3 ABOVE GROUND TO FIRST LIMB.

1. TREE WRAPPING

2. STEEL "Y" POST (40X40X32440) 2 PER TREE

3. CUT & REMOVE TOP 1/3 OF BURLAP AND BAGNET

4. 75mm MULCH

5. SAUCER

6. SCARIFY SIDES OF PIT

7. PLANTING SOIL MIX

8. SCARIFY SUBGRADE

N.T.S.

CONIFEROUS TREE PLANTING

DO NOT CHANGE OR CUT LEADER DAMAGED OR BRUSHY LEADERS WILL NOT BE ACCEPTED

ARBORETE LOCATE 1/3 ABOVE GROUND

1. STEEL "Y" POST (40X40X32440) 2 PER TREE

2. 75mm MULCH

3. CUT & REMOVE TOP 1/3 OF BURLAP AND BAGNET

4. 450mm DEPTH PLANTING SOIL MIX

5. SCARIFY SUBGRADE

N.T.S.

SHRUB PLANTING

C/C SPACING AS REQUESTED ON PLANT LIST

1. PRUNE BROKEN BRANCHES & REMOVE HAZARDOUS BRANCHES

2. 75mm MULCH

3. CUT & REMOVE TOP 1/3 OF BURLAP AND BAGNET

4. 450mm DEPTH PLANTING SOIL MIX

5. SCARIFY SUBGRADE

N.T.S.

TYPICAL WALL ELEVATION (planting at foreground) NTS



4. DESIGN BRIEF

The majority of our design review from Phase 1 is reflected again in this application. There is little difference between proposed Phase 2 shown in the overall plan that was previously submitted with Phase 1 compared to what is actually being proposed as per the submitted Site Plan.

Buildings M and L are located adjacent to Innes Road, one of which will contain a patio — a detail not included in the former application. The patio is located next to a pedestrian walkway and on the east side of Building M where it is visible from the street, contributing to a presence of activity. While it will have minimal noise impact based on its location, it will serve to add animation along the street.

Both buildings M and L have walkways along the front and a north-south walkway that connects to the main public sidewalk on Innes Road.

Building N, and Building N's patio connect to the north-south walkway on the east edge of Phase 1, where it then connects up to the City sidewalk or south to the other tenancies in Building H.

Phase 2 offers substantial landscaping throughout the site, in the form of trees and shrubs and grass. There is a large strip facing Innes Road, and surrounding the proposed drive-throughs. There is a large area of soft landscaping in front of Building N that will create an inviting natural space at one of the western most access into the site from Innes Road.

Further, there are landscaped islands throughout the proposed parking area, plus two bordering landscaped strips to the east and west. Additionally, a walkway and landscaped strip are provided in a spine down the rough centre of the parking area.

With regards to the elevations, the general colour theme is similar to that proposed for Phase 1, which includes black trim, brick vertical highlights, and light beige. Where full transparent glazing cannot be provided, glass spandrels have been added in abundance.

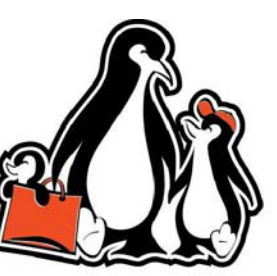
Only Building N's elevation scheme is different than the others with a red upper band of trim and beige along the lower portion, framed with brown brick in vertical columns extending towards the parapet design. Proposed Building N is planned to be a Benny&Co, a restaurant.

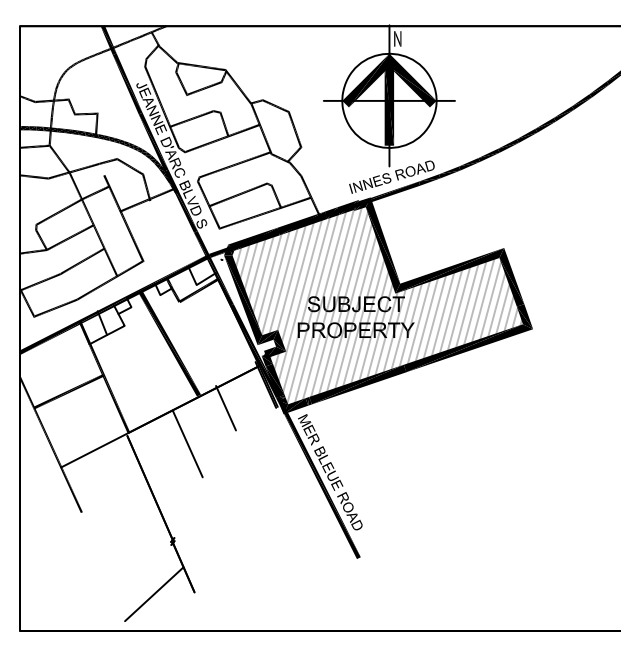
The proposed development is compatible and complements the existing development along Innes Road with large emphasis on richer textures, significant landscaping, walkways, and uses the offer patios which contributes to activity and vibrancy for the site.



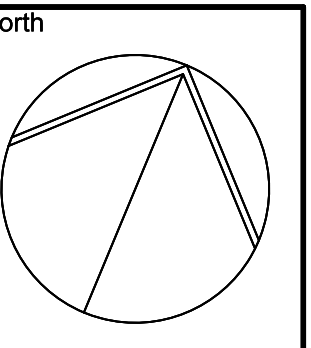
This application is subject to Urban Design Review and further recommendations and changes may result from this process. Please see the coloured elevations and coloured landscape plan provided below.





SMARTREIT


KEY PLAN (N.T.S.)

no	date	revision
1.	OCT. 18/17	ISSUE FOR REVIEW


north


professional seal

consultants
LEVSTEK CONSULTANT
LANDSCAPE ARCHITECTS
5871 Hugh Crescent - Ottawa - Ontario - K0A 2W0
613 - 826 - 9518

project
SMARTREIT ORLEANS II DEVELOPMENT
2025 MER BLEUE ROAD, OTTAWA, ON

drawn MB	checked RL
date OCT 2017	project no. 1111.03

drawing title
OVERALL LANDSCAPE CONCEPT

revision	drawing no. L1.00
----------	-----------------------------



2	ISSUED FOR SPA	NOV 01/17
1	ISSUED FOR REVIEW	SEP 20/17
No.	ISSUE	DATE

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CLIENT:

SmartREIT
(ORLEANS II) Inc.
And Mer Bleue
Shopping Centres
Limited

PROJECT:

Building L
2025 Mer Bleue Rd.
Orleans, ON

SHEET TITLE:

**BUILDING
ELEVATIONS**

PROJECT NO. 7034	
SCALE: 1/8"=1'-0"	DATE: SEE REV.
DRAWN: KS	SHEET NO: A1
CHECKED: GP	
FILE NO. 7034	



LEGEND

- 1 PREFINISHED METAL FLASHING - VICWEST
COLOUR: VW 56072 CHARCOAL
- 2 E.I.F.S. - DRYVIT - SANDPEBBLE
COLOUR: BM WROUGHT IRON
- 3 CONCRETE BLOCK - PERMACON - SPLIT FACE
COLOUR: CHARCOAL
- 4 E.I.F.S. - DRYVIT - SANDPEBBLE
COLOUR: BM COLLINGWOOD
- 5 SIGNAGE
6/8" 3/4" EXTERIOR GRADE PLYWOOD BEHIND E.I.F.S. FOR SIGNAGE SUPPORT,
(FIRE RATED WHERE APPLICABLE.) FINAL CONNECTION BY TENANT ELECTRICAL
CONTRACTOR TYP.
- 6 RESERVED:
- 7 RESERVED:
- 8 PREFINISHED METAL PARAPET CORNICE -
COLOUR: VW 56068 BLACK
- 9 ALUMINIUM COVER PANEL -
COLOUR: VW 56068 BLACK
- 10 CONCRETE SILL - PERMACON - BUFFED
COLOUR: CARERA WHITE
- 11 GLAZING IN CLEAR ANODIZED ALUMINIUM FRAME
- 12 FACE BRICK - HANSON - MATT
COLOUR: RIVERDALE
- 13 BRICK GROOVE -
150MM (W) X 75MM (D)
- 14 DECORATIVE LIGHT FIXTURE
MOUNT TO 3/4" EXTERIOR GRADE PLYWOOD CONCEALED BEHIND WALL FINISH
- 15 STUCCO REVEAL
16MM U-SHAPED
- 16 GLAZED DOORS & WINDOWS: (GL)
CLEAR LOW E GLAZING IN BLACK ANODIZED THERMALLY BROKEN ALUMINIUM
FRAMES (UNLESS OTHERWISE NOTED)
- 17 RESERVED:
- 18 SPANDREL PANEL
GLASS SPANDREL WITH INSULATED METAL BACK PAN IN BLACK ANODIZED
THERMALLY BROKEN ALUMINIUM FRAMES - PPG, BLACK MAGIC PPG1001-7
- 19 RESERVED:
- 20 RESERVED:
- 21 PAINTED HOLLOW METAL INSULATED DOOR
COLOUR: CHARCOAL

LEGEND

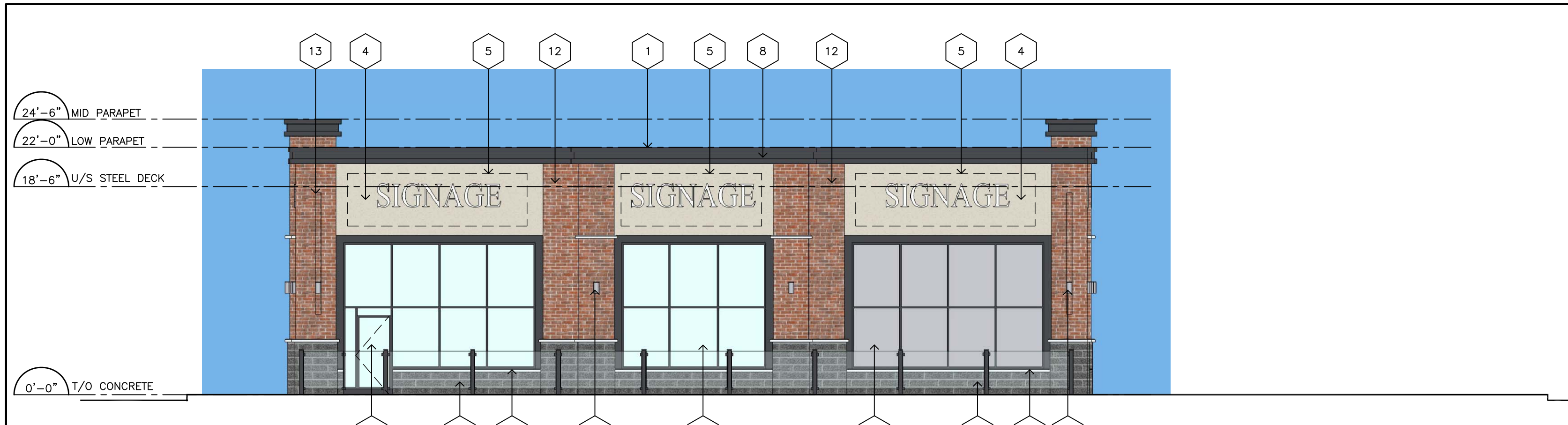
5
A2
N.T.S.



1
A2
SOUTH ELEVATION



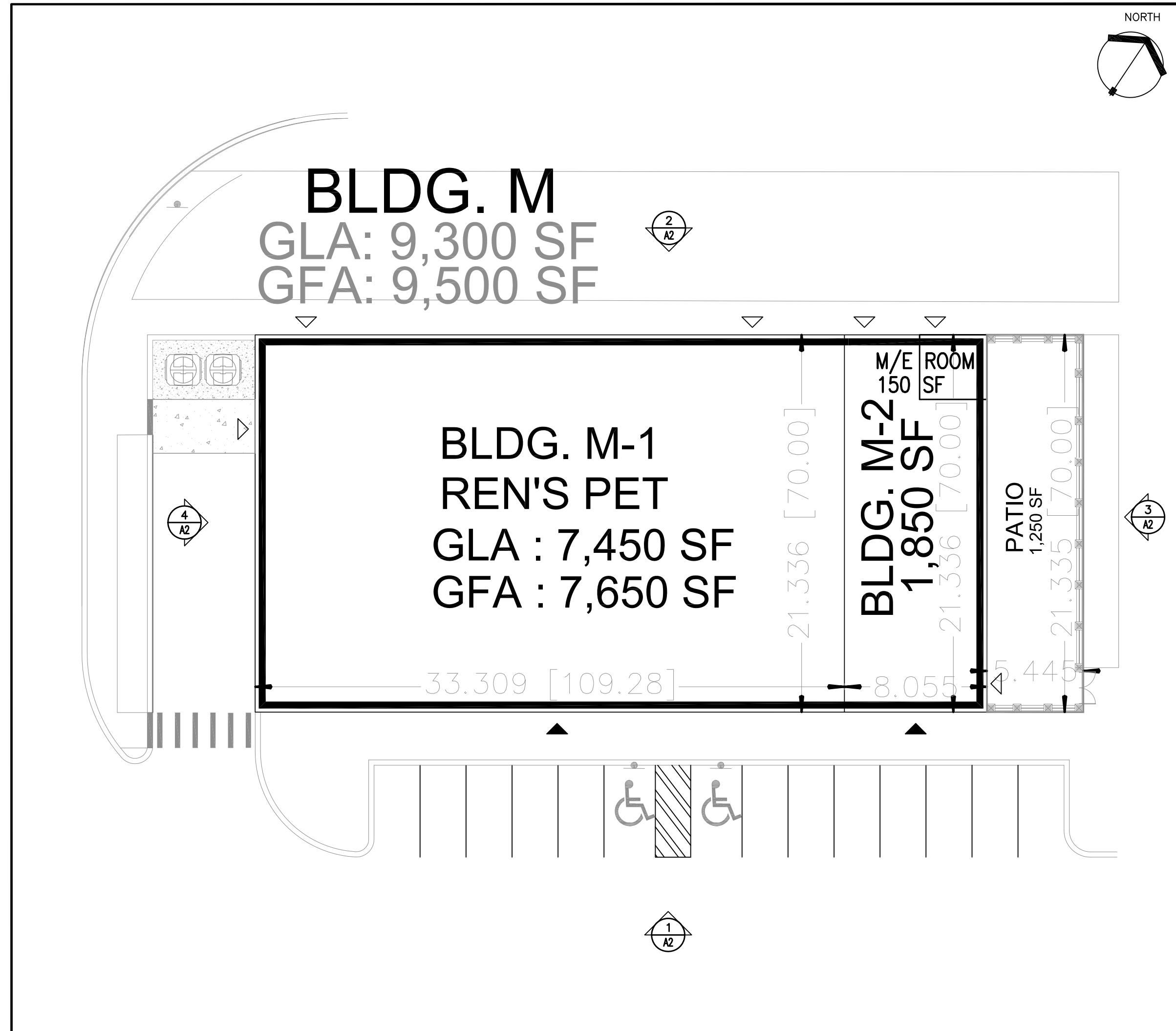
2
A2
NORTH ELEVATION



3
A2
EAST ELEVATION



4
A2
WEST ELEVATION



5
A2
KEY PLAN
N.T.S.

CLIENT:

SmartREIT
(ORLEANS II) Inc.
And Mer Bleue
Shopping Centres
Limited

PROJECT:

Building M
2025 Mer Bleue Rd.
Orleans, ON

SHEET TITLE:

BUILDING
ELEVATIONS

PROJECT NO.
7034

SCALE:
1/8"=1'-0"

DRAWN:
KS

CHECKED:
GP

FILE NO.
7034

DATE:
SEE REV.

SHEET NO:

A2

Tel.: (416) 440-0058
Fax: (416) 440-1404
mail@pointarchitects.ca
www.pointarchitects.ca



NOTE: This drawing is the property of the Architect and may not be reproduced or used without the expressed consent of the Architect. The contractor is to verify all dimensions and conditions on the project and to report any discrepancies to the Architect prior to commencing work. These drawings are not to be used for construction purposes unless indicated as "issued for construction".

SmartREIT
(ORLEANS II) Inc.
And Mer Bleue
Shopping Centres
Limited

Building N
2025 Mer Bleue Rd.
Orleans, ON

BUILDING ELEVATIONS

7034

SCALE:

SCALE:
1/8"=1'-0"

DRAWN:
KS

CHECKED:

GP
FILE NO

7034

A3



- | | |
|----|---|
| 1 | DECORATIVE LIGHT FIXTURE
MOUNT TO 3/4" EXTERIOR GRADE PLYWOOD CONCEALED BEHIND WALL FINISH |
| 2 | PREFINISHED METAL CANOPY - VICWEST
COLOUR VW QC-16080 |
| 3 | DRIVE-THRU GLAZED WINDOW
CLEAR LOW E GLAZING IN CLEAR ANODIZED THERMALLY BROKEN ALUMINIUM
FRAMES (UNLESS OTHERWISE NOTED) |
| 4 | OPAQUE FILMED GLAZED WINDOWS: (OP)
OPAQUE FILMED GLAZING IN BLACK ANODIZED THERMALLY BROKEN ALUMINIUM
FRAMES (UNLESS OTHERWISE NOTED) |
| 5 | PREFINISHED METAL SLOPPER - VICWEST
COLOUR VW QC-16080 |
| 6 | CLEAR GLAZED WINDOWS: (CL)
CLEAR LOW E GLAZING IN BLACK ANODIZED THERMALLY BROKEN ALUMINIUM
FRAMES (UNLESS OTHERWISE NOTED) |
| 7 | PAINTED HOLLOW METAL INSULATED DOOR
COLOUR TO MATCH ADJACENT WALL |
| 8 | PREFINISHED METAL DISPLAY FLIP FRAME
C/W 3/4" EXTERIOR GRADE PLYWOOD BEHIND E.I.F.S. FOR FRAME
SURFACE (FIRE RATED WHERE APPLICABLE). FINAL CONNECTION BY TENANT
ELECTRICAL CONTRACTOR TYP. COLOUR - TBD |
| 9 | PREFINISHED METAL SIDING
COLOUR - CEDAR WOOD IMITATION |
| 10 | E.I.F.S. - LES ENDUITS STEF - SANDPEBBLE
COLOUR: MATCH-9368 |
| 11 | E.I.F.S. - LES ENDUITS STEF - SANDPEBBLE
COLOUR: 6206-31 |
| 12 | E.I.F.S. - LES ENDUITS STEF - SANDPEBBLE
COLOUR: 6207-73 |
| 13 | STONE SILL - CENTRE DE PIERRES MIRABEL
MODEL: ST-MARC LIMESTONE |
| 14 | CONCRETE BLOCK - PERMACON
MODEL: PIERRE LAFFIT, NEWPORT GREY, GREY MORTAR |
| 15 | SIGNAGE:
C/W 3/4" EXTERIOR GRADE PLYWOOD BEHIND E.I.F.S. FOR SIGNAGE SUPPORT
(FIRE RATED WHERE APPLICABLE). FINAL CONNECTION BY TENANT ELECTRICAL
CONTRACTOR TYP. |
| 16 | PREFINISHED METAL CLAP FASING - VICWEST
COLOUR VW QC-16080 |
| 17 | RESERVED: |
| 18 | SPANDREL PANEL
GLASS SPANDREL WITH INSULATED METAL BACK PAN IN BLACK ANODIZED
THERMALLY BROKEN ALUMINIUM FRAMES - PPG, BLACK MAGIC PPG1001-7 |

5 LEGEND
A3 N.T.S.





5. PROVINCIAL POLICY STATEMENT, 2014

The Provincial Policy Statement, 2014 (PPS) provides policy direction on planning matters for the Province of Ontario. Decisions affecting all planning matters shall be consistent with the Provincial Planning Policies. The proposed retail/commercial development at 2025 Mer Bleue Road is consistent with the policies of the PPS, 2014, as demonstrated below.

Policies:

Section 1.1.1 of the PPS sets out the criteria whereby healthy, livable, and safe communities are sustained. In short, these are: to promote efficient development and land use patterns which sustain the financial well-being of the Province and municipalities over the long turn; avoiding development and land use patterns which may cause environmental or public health and safety concerns; promote cost-effective development patterns; improve accessibility for persons with disabilities and older persons by identifying, preventing, and removing land use barriers which restricts their full participation in society; ensuring there is necessary infrastructure; and, promoting development and land use patterns that conserve biodiversity.

In the above list of criteria, there is a large emphasis on efficient development that is safe, respects the natural environment, uses available infrastructure, and provides for the needs of the community both in the short term and long term.

Section 1.5.1 of the PPS states that healthy, active communities should be promoted by: planning public streets, spaces and facilities to be safe, meet the needs of pedestrians, foster social interaction and facilitate active transportation and community connectivity.

The proposal contains a number of pedestrian access points and pathways that promote the site's overall connection to nearby communities and off-site pedestrian connections. These types of pedestrian linkages connecting to and located on the site will help to promote active transportation.

Section 1.6.6 of the PPS states that planning for sewage and water services shall direct and accommodate expected growth or development in a manner that promotes the efficient use and optimization of existing: municipal sewage services and municipal water services.

Section 1.7.1 of the PPS states that long-term economic prosperity should be supported by: promoting opportunities for economic development and community investment-readiness; and, maintaining and, where possible, enhancing the vitality and viability of downtowns and mainstreets.



The proposed development will fill in one of the last undeveloped parcels fronting the south side of Innes Road. The site is located in Orleans within the City's urban area, and thus will be capable of connecting to available city services and infrastructure.

This location allows for an efficient use of land that is compatible with the existing land uses to the north, east, south, and west. The site will also be compatible with the future land uses to the east, which will be supported through a number of planned vehicle and pedestrian connections.

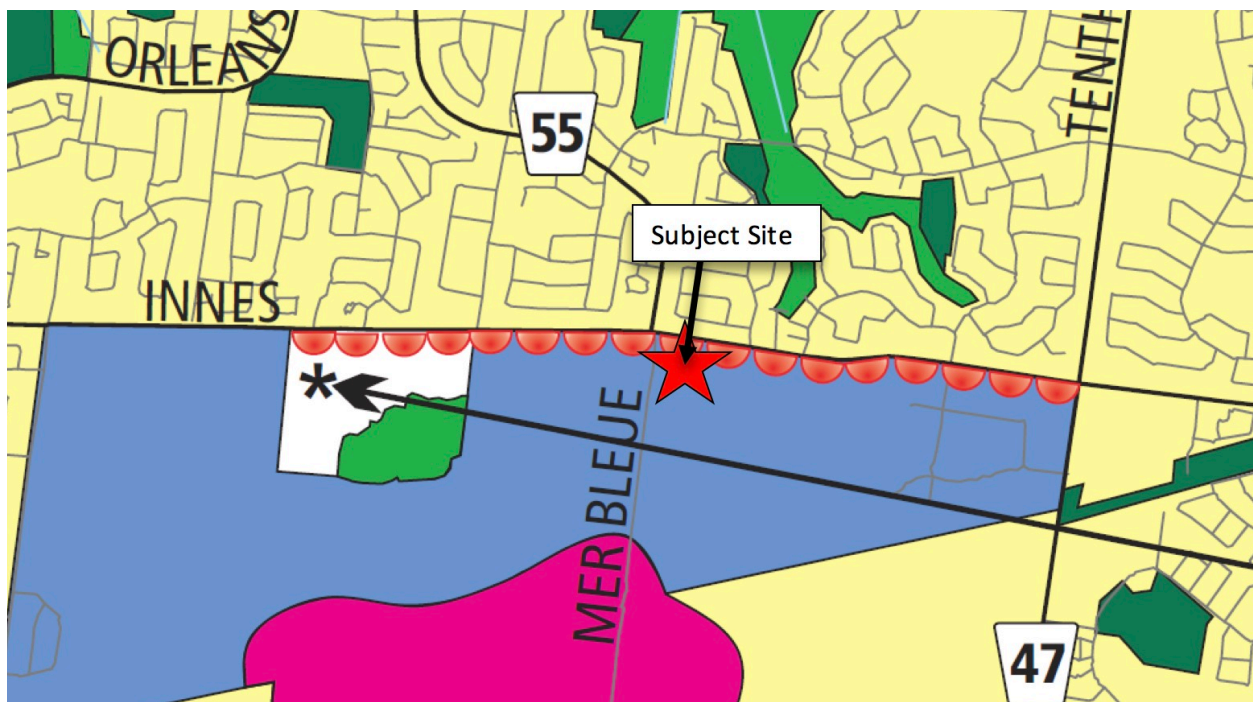
Overall, the proposal is consistent with the applicable policies of the PPS, 2014.



6. CITY OF OTTAWA OFFICIAL PLAN

The City of Ottawa Official Plan designates the site at 2025 Mer Bleue Road as Arterial Mainstreet. The general character of Arterial Mainstreets is described in Section 3.6.3 of the Official Plan, as such: being built after 1945; having larger lots and buildings; displaying varied setbacks; having lower densities; being primarily automobile-oriented; typically, being divided by 4 or more traffic lanes; and, having large parking areas separating predominantly large commercial and retail buildings from the street.

The Arterial Mainstreet designation permits a variety of uses, including the proposed retail and commercial uses at 2025 Mer Bleue Road.



Policy 2 of Section 3.6.3 states that:

[...] On lots where development has the potential to develop both adjacent to the street and to the rear of the property, the Mainstreet designation will apply to the entire lot and development situated on the rear portions will not be considered to be non-conforming by virtue of not being located adjacent to the street. Where the depth of lots fronting the road is sufficient to enable development to occur both adjacent to the street and to the rear of the property, and where development is initially unlikely to occupy the entire frontage immediately adjacent to the street, the site should be planned in a coordinated fashion that will facilitate:

- multi-modal (pedestrian, cycling, transit and vehicular) access between the site and the public street(s),



- attractive, safe and usable pedestrian and cycle connections between the site and adjacent communities,
- an enhanced interconnected pedestrian environment that links individual uses on the site, transit stops and continuous public sidewalks on the adjoining streets, and which is generally distinct from internal vehicle routes,
- measures of sufficient size and quality to relieve the visual impact of surface parking areas,
- the provision of adequate landscaped areas, particularly trees, along the perimeter of the site and street frontages,
- the provision of coordinated signage, and
- over time, a development that is oriented to the Mainstreet.

Policy 3 of Section 3.6.3 states that:

A broad range of uses is permitted on Traditional and Arterial Mainstreets, including retail and service commercial uses, offices, residential and institutional uses. Uses may be mixed in individual buildings or occur side by side in separate buildings. Where a Mainstreet abuts an Employment Area, the zoning by-law may prohibit noise-sensitive uses on the Mainstreet where appropriate.

The proposal is consistent in the context of the Arterial Mainstreet Designation and its related policies under section 3.6.3 of the Official Plan. The site is located within close proximity to major transportation routes and transit uses, and also provides direct access for pedestrians through active transportation linkages. The mainstreet designation follows along Innes Road, and in order for the proposal to support its function as a mainstreet, access to the site will be provided along both Innes Road and Mer Bleue Road. The proposed buildings are compatible with the envisioned mix and scale of uses that the Official Plan has identified for this area.

The City of Ottawa recently adopted Official Plan Amendment 150 to implement the results of the overall 5-year review of the Official Plan, and the proposal is evaluated based on these updates. OPA 150 introduce a new preamble to Section 3.6.3 (Mainstreets), as seen below, which provides language that further supports the proposed development.

"The Mainstreet designation identify streets that offer significant opportunities for intensification through medium-density and mixed-use development, along streets that are Transit Priority Corridors or are well-served by transit. Mainstreets are the corridors that traverse long areas of the city, connecting different communities and changing in character along their



length. They include nodes of activity at various scales, from high schools and small offices to hospitals and shopping centres. Some segments mark the boundaries of established residential areas, while other segments serve as shopping streets for adjacent communities or larger areas. Focusing intensification on Mainstreets allows for less disruption and more convenient services for adjacent communities and more efficient use of transit. The objective of the Mainstreet designation is to encourage more dense and mixed-use development that supports, and is supported by, increased walking, cycling and transit use. Intensification is most likely to occur over time through the redevelopment of sites such as vacant lots, aging strip malls, and former automobile sales lots, parking lots and gas stations, as well as through additions to existing buildings. Mainstreets are diverse in character and change and renewal that will takes into account the character of the street and adjacent areas.”

6.1.Design Priority Area

The site is identified in the Official Plan as being within a Design Priority Area and the proposal is therefore subject to the Urban Design Review Panel (UDRP).

The purpose of the UDRP and design guidelines is to review a number of development and design factors to help encourage the continuity of an area’s unique character and function.

6.2.Urban Design Guidelines for Arterial Mainstreets

The purpose of the Arterial Mainstreet guidelines is to provide urban design guidance in order to assess, promote and achieve appropriate development along Arterial Mainstreets.

Innes Road is identified on Schedule B as an Arterial Mainstreet; a four-lane high-traffic east-west route through the south part of Orleans. The following reviews the applicable guidelines as incorporated through this development plan.

In accordance with the design guidelines for arterial mainstreet development, the proposed buildings have been located at the public street edge. This proposed layout frames the public realm, provides efficient movement for vehicle traffic and maximum visibility from various access points, which contributes to wayfinding. Full pedestrian sidewalks will be maintained along Innes Road, with connecting pathways running south into the site and at the east edge of Phase 2. These pathways will be clearly identified and safe, not interfering with any obstructions.

A mix of hard and soft landscaping is provided along the front of this Phase, adjacent to Innes Road, with further landscaping and trees provided throughout the site, along the street edges, and within the parking area where space permits.



The proposed buildings along Innes Road are placed, in conjunction with the proposed landscaping, to create the sense of a continuous streetscape and welcoming pedestrian landscaped environment in an otherwise high-traffic area.

Furthermore, the proposed development has been designed to be compatible and functional with other retail and commercial developments nearby, which are designed in a similar fashion. The design of the proposal allows for greater densification over time, with the available space to incorporate additional internal roads as necessary and reduce parking at a time when it would be appropriate to do so.

The proposed development fills in a notable gap along the street for the south side of Innes Road, it proposes, at full build out, a mixed-use concept with a high level of integration into the surrounding area.

As discussed in the Design Brief, the proposed design of the buildings will use darker, rich colours and textures to create visual interest and differentiate the site along Innes Road.

While the guidelines for Arterial Mainstreets remain the same regardless of location, the context of Arterial Mainstreets across the City varies greatly. Innes Road is a unique location in that it does not enjoy the same pedestrian density and activity as other parts of the City. This is mainly due to its suburban context and the spacing of developments, but also due to the size, span, and speeds along Innes Road that do not contribute to an inviting pedestrian context. Therefore, the majority of consumers opt to utilize the more efficient means of vehicular travel in this part of Orleans compared to other Arterial Mainstreets.

The proposed development, as such, recognizes this unique context, and understands that the majority of its consumers arrive by vehicle.

Given this context, and the larger scale aspect of retail on Innes Road which generally necessitates the vehicle as a mode of transport, the main entrances for the units on Innes Road will be oriented to the corners or the south elevation (parking area) where the majority of customers will arrive from. Notwithstanding, a high level of design will face Innes Road, and will include landscaping and a variety of colours, textures, and spandrels.

Between Building M and Building L, while there is a drive through, there is also now a proposed patio which will further animate the street frontage and provide a human scale connection between the development and the adjacent pedestrian connections along the street.



The proposed parking layout is sized and designed appropriately to meet the demands of the initial portion of Phase 1. Given the size of this phase, the available walkways provided at the north and east borders will be sufficient to safely circulate pedestrians through the site.

Landscaping is continuous and ample along the northern frontage of the site, with species that will be tolerant of urban conditions, and in a strip wide enough to screen the drive-through.

Signage, illumination, and other forms of retailer identification will all be designed and located in suitable manners that do not create visual clutter or light spillover.

All service and utility areas will be screened from the Mainstreet, as well as from other sensitive uses.

The proposed design at 2025 Mer Bleue for Phase 2 has regard to and is consistent with the design guidelines as applicable to the site, context, and where feasible.

6.3. Urban Design Guidelines for Large Format Retail

While the overall development plan for this site does include the presence of large format retail, Phase 2 does not currently propose any large format retail at this stage and as such these guidelines will not be reviewed as part of this application.

However, it is important to note that a future section of Phase 2 is likely to see some form of larger format retail. When this future site plan application is made, a review of these guidelines will be incorporated.

6.4. Outdoor Patio Urban Design Guidelines

The proposed patio for Building K is located on the west side of the building, adjacent to a pedestrian area. The proposed patio for Building M is on the east side of the building and is also adjacent to a pedestrian area.

While the guidelines do not exactly identify patios in this particular situation, the closest comparison would be for patios which are located in a parking lot. No other circumstance applies.

- 1) Patios should be separated from parking by planted islands and/or medians in the parking lot
 - a. The proposal adheres to this guideline.
- 2) Pedestrian access should be from a plaza walkway along the face of the building or directly to the street.
 - a. The proposal adheres to this guideline.



- 3) A clear minimum width of 1.5 metres should be maintained for pedestrian movement from the parking lot street to a patio.
 - a. The proposal adheres to this guideline.
- 4) A patio should be set back a minimum of 6 metres from a suburban arterial roadway.
 - a. The proposal adheres to this guideline.
- 5) A patio should be setback a minimum of 3 metres from a parking lot.
 - a. The proposal adheres to this guideline.

6.5. Drive-Through Facilities Urban Design Guidelines

On the proposed site (Phase 2) there is a single proposed drive-through facility as part of Building L at the northeast corner of this phase. With regards to design, the proposed Building L will reflect the same architectural details in terms of colour and material use as the rest of the proposed site.

The building is located close to the street edge, maintaining desired street definition through the positioning of the drive-through on the west side of the building, where sufficient and dense landscaping will screen the drive-through use from the public sidewalk on Innes Road.

The less active façade facing Innes road will maintain the same level of design as the rest of the site with a mix of materials, spandrels, and building articulation, as well as landscaping to provide visual interest. The proposed customer doors are located nearest the parking and are not impeded by any structures.

The proposed access to the drive through queue line is as far away from accesses to the overall development as possible and designed to minimize impacts from queued vehicles blocking drive aisles. No escape lanes are provided due to the site design, however, the proposed stacking length is sufficient. A landscaped curb separates the adjacent parking from the drive-through lane.

The latter portion of Guidelines for Drive-Through facilities focus on the provision of landscaping, urban-environment tolerant plants, signage and other aspects that are present in the other applicable guidelines and have been previously discussed.

The proposed drive-through is consistent with the guidelines for drive-through facilities through it's design and adequate landscaping.



7. ZONING BY-LAW

The property is zoned AM[2413] H(21) – Arterial Mainstreet, exception 2413, with a maximum height limit of 21 metres. The details of the exception is presented below.

The proposed uses (retail, restaurant, patio, drive-through restaurant etc.) for Phase 2 are permitted under the current Zoning designation.

Zoning Compliance Chart

Zoning	AM[2413] H(21)	
	Required	Provided
Minimum Lot Area	No minimum	82,192 m ² (8.2 ha)
Minimum Lot Width	No minimum	405.77 m (Innes)
Minimum Front Setbacks	No minimum	Front: 4.02 m (Building K)
Minimum Interior Yard Setbacks	No minimum	n/a
Minimum Rear Yard Setback	Abutting a Street: 3m; Otherwise: No minimum	n/a
Maximum Building Height	21 m	8.84 m
Maximum FSI	2	Phase 1 & 2 = 1.8
Minimum Landscaped Width	Abutting a Street: 3m; Otherwise: No minimum.	3.0 m
Section 110 (1): 15% Landscaped Area within a Parking Lot	15% (includes landscape buffer around parking lot.	30% (Phase 2)

The following describes the provisions of Urban Exception 2413:

- < Land uses prohibited are: amusement park, automobile dealership, automobile service station, gas bar.
- < Additional provisions: a carwash is only permitted when located within an accessory parking garage associated with another principal use, an automobile rental establishment may not display or store vehicles outdoors.

The proposed Phase 2 Site Plan Application is consistent with the provisions of the applicable By-law, including the exception zone provisions.



8. SUPPORTING STUDIES

8.1. Site Servicing Study

Water Service

A Site Servicing Study was prepared by Stantec Consulting Inc., dated July 22, 2016 and further updated as per revision 4, dated October 20th, 2017. The report reviewed the serviceability of the site in regards to water, sanitary, and Storm Water Management.

The proposed development of Phase 2 will be serviced by a 200 mm watermain stub constructed as a part of Phase 1.

The full development will be serviced via an existing 300 mm watermain stub located approximately 150 m within the site originating from Mer Bleue Road, and via connection to the 600 mm watermain on Innes Road.

The report found that the proposed water servicing will provide sufficient capacity to sustain required domestic demands and emergency fire flow demands of the proposed site.

Sanitary

The site will be serviced via an existing 250 mm diameter stub sanitary sewer constructed as part of Phase 1, which is serviced via an existing 250 mm diameter sewer installed within an easement that runs parallel to Innes Road, and south of the existing commercial development to the east.

The sewer directs flows to an existing 525 mm diameter municipal main at the intersection of Lanthier Drive and Vanguard Drive, and ultimately to the Tenth Line Road pumping station.

The proposed sanitary sewer network is sufficiently sized to provide gravity drainage to the site.

Fire Flow Analysis

A fire flow analysis was carried out and indicated that flows in excess of 11,000L/min can be delivered while still maintaining a residual pressure of 140 kPa. Further details are provided in the Site Servicing Study.

Stormwater Management



Based on background information, the peak post-development discharge from the subject site to the minor system is to be limited to 50 L/s/ha of contributing area. The proposed stormwater management plan is in compliance with the goals specified through consultation with the City of Ottawa. An on-site catchbasin and connected ICD has been proposed to limit peak storm sewer inflows to downstream storm sewers to 50 L/s/ha. The downstream receiving sewer has sufficient capacity to receive runoff volumes from the site. Please see the report for further details.

8.2. Archaeological Assessment

An Archaeological Assessment was prepared by Stantec Consulting, report dated July 6, 2016. This report concluded that the area still retained its archaeological potential and as such required a Stage 2 Archaeological Assessment.

A Stage 2 Assessment was undertaken, including pedestrian and test pit excavation and no archaeological resources were identified.

8.3. Geotechnical Report

The Geotechnical Investigation Report was prepared by Paterson Group and dated March 2, 2017. It summarizes the existing soil conditions for the entirety of the development, including construction recommendations.

8.4. Noise Study

PatersonGroup prepared an Environmental Noise Control Study, dated November 1, 2017. This report indicated that the drive-through is the primary source of noise but that its influence on the nearby residential falls below the MOECC threshold values. Therefore, the proposed development is considered acceptable from a noise attenuation perspective.

8.5. Phase I Environmental Site Assessment

A Phase I Environmental Site Assessment (ESA) was prepared by Paterson Group, dated May 2, 2016, to review the existing or historical presence of contamination, or possibilities thereof.

The investigation revealed that the former automotive garage on site represents an area of potential environmental concern (APEC) to the subject site, but that no evidence of subsurface contamination was identified during the previous subsurface investigation conducted immediately surrounding the former garage footprint. For that reason, the report recommends that the completion of a Phase II ESA is not considered to be a practical solution to assess this APEC. Instead of completing a Phase II ESA, it is the reports recommendation that the area immediately beneath the former garage slab be assessed at the time that the slab and foundation are removed for site redevelopment. It further recommends that Paterson be present to monitor this work.



Based on the previous Phase 1 ESA, several potable water wells may still be present on the site. If these wells are encountered during site redevelopment and they have not been abandoned, they should be decommissioned by a licensed well driller. Please see the report for further details.



9. CONCLUSION

The proposed retail development is the second of three phases at the property located at 2025 Mer Bleue Road. The proposal consists of two new buildings on Innes Road, and to revise two of the already approved buildings from Phase 1. A total of six tenancies are provided for in Phase 2.

The proposal develops an underdeveloped site, while making use of existing servicing infrastructure and respecting the existing character of the area. Overall, it is an appropriate and desired fit at this location.

The proposed development is consistent with the Provincial Policy Statement, the Official Plan, and the uses are permitted under the applicable zoning designation. The proposed urban design is reflective of the guidelines as set out by the City of Ottawa for development along Arterial Mainstreets.

Following this review, the proposed development is considered good land use planning and is recommended for approval.

Lloyd Phillips & Associates Ltd.

Christine McCuaig, MCIP RPP | LEED GA

Jill Stewart | LEED GA



APPENDIX A

Servicing Plan Set

GENERAL NOTES AND SPECIFICATIONS

- ALL MATERIALS AND CONSTRUCTION METHODS TO BE IN ACCORDANCE WITH OPS AND CITY OF OTTAWA STANDARD SPECIFICATIONS AND DRAWINGS AND OPSD SUPPLEMENT. ONTARIO PROVINCIAL STANDARDS WILL APPLY WHERE NO CITY STANDARDS ARE AVAILABLE.
- THE CONTRACTOR IS RESPONSIBLE FOR OBTAINING ALL PERMITS REQUIRED AND BEAR COST OF SAME INCLUDING WATER PERMIT AND ASSOCIATED COSTS.
- SERVICE AND UTILITY LOCATIONS ARE APPROXIMATE. CONTRACTOR TO VERIFY LOCATION AND ELEVATION OF EXISTING SERVICES AND UTILITIES PRIOR TO CONSTRUCTION. CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING LOCATES FROM ALL UTILITY COMPANIES TO LOCATE EXISTING UTILITIES PRIOR TO EXCAVATION. THE CONTRACTOR IS RESPONSIBLE FOR PROTECTION AND REINSTATEMENT.
- ALL DISTURBED AREAS SHALL BE REINSTATED TO EQUAL OR BETTER CONDITION TO THE SATISFACTION OF THE ENGINEER & THE CITY. PAVEMENT REINSTATEMENT FOR SERVICE AND UTILITY CUTS SHALL BE IN ACCORDANCE WITH OPSD 509.010 AND OPS5 310.
- ALL WORK SHALL BE COMPLETED IN ACCORDANCE WITH THE "OCCUPATIONAL HEALTH AND SAFETY ACT AND REGULATION FOR CONSTRUCTION PROJECTS". THE GENERAL CONTRACTOR SHALL BE DEEMED TO BE THE CONSTRUCTOR AS DEFINED IN THE ACT.
- THE CONTRACTOR SHALL SUBMIT AN EROSION AND SEDIMENTATION CONTROL PLAN THAT WILL IMPLEMENT BEST MANAGEMENT PRACTICES TO PROVIDE PROTECTION FOR RECEIVING STORM SEWERS OR DRAINAGE DURING CONSTRUCTION ACTIVITIES. THIS PLAN SHALL INCLUDE BUT NOT BE LIMITED TO CATCH BASINS, INSERTS, STRAW BALE CHECK DAMS AND SEDIMENT CONTROLS AROUND ALL DISTURBED AREAS. DEWATERING SHALL BE PUMPED INTO SEDIMENT TRAPS.
- SITE PLAN PREPARED BY ALCAIDE WEBSTER ARCHITECTS INC. DATED JAN 10, 2017 DRAWING SP-2, PROJECT No. 16-09.
- TOPOGRAPHIC SURVEY SUPPLIED BY STANTEC GEOMATICS LTD. PROJECT No.161613512-111. PART OF LOT 1, CONCESSION 11, GEOGRAPHIC TOWNSHIP OF CUMBERLAND, CITY OF OTTAWA.
- REFER TO LANDSCAPE ARCHITECTURE PLAN FOR ALL LANDSCAPING FEATURES (ie. TREES, WALKWAYS, PARK DETAILS, NOISE BARRIERS, FENCES etc.)
- GEOTECHNICAL INVESTIGATION PROPOSED COMMERCIAL DEVELOPMENT - PHASE 1 INNES ROAD AT MER BLEUE ROAD, OTTAWA, ONTARIO., PREPARED BY PATERSON GROUP INC. DATED MARCH 2, 2017. REPORT NO P00811-2 REVISION 1. GEOTECHNICAL INFORMATION PRESENTED ON THESE DRAWINGS MAY BE INTERPOLATED FROM THE ORIGINAL REPORT. REFER TO ORIGINAL GEOTECHNICAL REPORT FOR ADDITIONAL DETAILS AND TO VERIFY ASSUMPTIONS MADE HEREIN.
- STREET LIGHTING TO CITY OF OTTAWA STANDARDS.
- ALL DIMENSIONS ARE IN METRES UNLESS OTHERWISE STATED. DIMENSIONS SHALL BE CHECKED AND VERIFIED IN THE FIELD BY THE CONTRACTOR PRIOR TO THE START OF CONSTRUCTION. ANY DISCREPANCIES TO BE REPORTED IMMEDIATELY TO ENGINEER.
- THERE WILL BE NO SUBSTITUTION OF MATERIALS UNLESS PRIOR WRITTEN APPROVAL BY THE CONTRACT ADMINISTRATOR AND DIRECTOR OF ENGINEERING HAS BEEN OBTAINED.
- HERITAGE OPERATIONS UNIT OF THE ONTARIO MINISTRY OF CULTURE TO BE NOTIFIED IF DEEPLY BURIED ARCHEOLOGICAL REMAINS ARE FOUND ON THE PROPERTY DURING CONSTRUCTION ACTIVITIES.

ROADWORKS

- ALL TOPSOIL AND ORGANIC MATERIAL TO BE STRIPPED FROM WITHIN THE FULL RIGHT OF WAY PRIOR TO CONSTRUCTION.
- SUB-EXCAVATE SOFT AREAS & FILL WITH GRANULAR 'B' COMPACTED IN 0.30m LAYERS.
- ALL GRANULAR FOR ROADS SHALL BE COMPACTED TO A MINIMUM OF 98% STANDARD PROCTOR MAXIMUM DRY DENSITY (SPMDD).
- ROAD SUBDRAINS SHALL BE CONSTRUCTED AS PER CITY OF OTTAWA STANDARD R1.
- ASPHALT WEAR COURSE SHALL NOT BE PLACED UNTIL THE VIDEO INSPECTION OF SEWERS & NECESSARY REPAIRS HAVE BEEN CARRIED OUT TO THE SATISFACTION OF THE CONSULTANT.
- CONTRACTOR TO OBTAIN A ROAD OCCUPANCY PERMIT 48 HOURS PRIOR TO COMMENCING ANY WORK WITHIN THE MUNICIPAL ROAD ALLOWANCE IF REQUIRED BY THE MUNICIPALITY. ALL WORK ON THE MUNICIPAL RIGHT OF WAY AND EASEMENTS TO BE INSPECTED BY THE MUNICIPALITY PRIOR TO BACKFILLING.
- PAVEMENT REINSTATEMENT FOR SERVICE AND UTILITY CUTS SHALL BE IN ACCORDANCE WITH CITY OF OTTAWA STANDARD R10, AND OPSD 509.010, AND OPS5 310.
- CONCRETE CURBS SHALL BE CONSTRUCTED AS PER CITY STANDARD SC1.1 AND SC1.3 (BARRIER OR MOUNTABLE CURB AS SHOWN ON DRAWINGS).
- CONCRETE SIDEWALKS SHALL BE CONSTRUCTED AS PER CITY STANDARDS SC3 AND SC1.4.
- PAVEMENT CONSTRUCTION AS PER GEOTECHNICAL INVESTIGATION PROPOSED COMMERCIAL DEVELOPMENT - PHASE 1 INNES ROAD AT MER BLEUE ROAD - OTTAWA, ONTARIO., PREPARED BY PATERSON GROUP INC. DATED MARCH 2, 2017. REPORT NO P00811-2. REVISION 1

HEAVY DUTY ASPHALT
40mm HL3 OR SUPERPAVE 12.5 ASPHALTIC CONCRETE
50mm HL3 OR SUPERPAVE 19.0 ASPHALTIC CONCRETE
150mm OPSS GRANULAR A BASE
450mm OPSS GRANULAR B TYPE II

LIGHT DUTY AREAS
50mm HL3 OR SUPERPAVE 12.5 ASPHALTIC CONCRETE
150 OPSS GRANULAR 'A' BASE
400 OPSS GRANULAR 'B' TYPE II

PAVEMENT CONSTRUCTION AS PER GEOTECHNICAL INVESTIGATION PROPOSED ROADWAY SERVICE ALIGNMENT 2025 MER BLEUE ROAD - OTTAWA, ONTARIO., PREPARED BY PATERSON GROUP INC. DATED MARCH 20, 2017. REPORT NO P00811-LET.01.

LOCAL ROADWAYS WITH BUS AND HEAVY TRUCK TRAFFIC
40mm SUPERPAVE 12.5 ASPHALTIC CONCRETE
50mm SUPERPAVE 19.0 ASPHALTIC CONCRETE
50mm SUPERPAVE 19.0 ASPHALTIC CONCRETE
150mm OPSS GRANULAR A BASE
500mm OPSS GRANULAR B TYPE II

WATER SUPPLY SERVICING

- THE CONTRACTOR SHALL CONSTRUCT WATERMAIN, WATER SERVICES, CONNECTIONS & APPURTENANCES AS PER CITY OF OTTAWA SPECIFICATIONS & SHALL CO-ORDINATE AND PAY ALL RELATED COSTS INCLUDING THE COST OF CONNECTION, INSPECTION & DISINFECTION BY CITY PERSONNEL.
- WATERMAIN PIPE MATERIAL SHALL BE PVC CL150 DR18. DEFLECTION OF WATERMAIN PIPE IS NOT TO EXCEED 1/2 OF THAT SPECIFIED BY THE MANUFACTURER. PVC WATERMAINS TO BE INSTALLED WITH TRACER WIRE IN ACCORDANCE WITH CITY OF OTTAWA STANDARD W36.
- WATER SERVICES ARE TO BE TYPE K SOFT COPPER AS PER CITY OF OTTAWA STANDARD W26 (UNLESS OTHERWISE NOTED). WATER SERVICE TO EXTEND 1.0m BEYOND PROPERTY LINE. STAND POST TO BE INSTALLED AT PROPERTY LINE.
- FIRE HYDRANTS TO BE INSTALLED AS PER QTY OF OTTAWA STANDARDS W18 AND W19.
- WATER VALVES TO BE INSTALLED AS PER CITY OF OTTAWA STANDARD W24.
- WATERMAIN TRENCH AND BEDDING SHALL BE IN ACCORDANCE WITH CITY OF OTTAWA STD. W17 UNLESS OTHERWISE SPECIFIED. BEDDING AND COVER MATERIAL TO BE SPECIFIED BY PROJECT GEOTECHNICAL CONSULTANT.
- SERVICE CONNECTIONS SHALL BE INSTALLED A MINIMUM OF 2400mm FROM ANY CATCHBASIN MANHOLE. OR OBJECT THAT MAY CONTRIBUTE TO FREEZING. THERMAL INSULATION SHALL BE INSTALLED ON ALL PROPOSED CBS ON THE W/M STREET SIDE WHERE 2400mm SEPARATION CANNOT BE ACHIEVED.(AS PER CITY OF OTTAWA W22 & W23)
- CATHODIC PROTECTION TO BE SUPPLIED ON METALLIC FITTINGS AS PER CITY OF OTTAWA W40 AND W42.
- THRUST BLOCKS TO BE INSTALLED AS PER CITY OF OTTAWA STANDARDS W25.3 AND W25.4.
- WATERMAIN TO HAVE MIN. 2.4m COVER. WHERE WATERMAIN COVER IS LESS THAN 2.4m, INSULATION TO BE SUPPLIED IN ACCORDANCE WITH CITY STANDARD W22.
- WATERMAIN CROSSINGS ABOVE AND BELOW SEWERS TO BE INSTALLED AS PER CITY OF OTTAWA STANDARD W25 AND W25.2.
- PRESSURE REDUCING VALVES (PRV'S) IF REQUIRED, TO BE INSTALLED AS PER ONTARIO PLUMBING CODE.

STORM AND SANITARY SEWERS

- SANITARY SEWERS 375mm DIA. OR SMALLER SHALL BE PVC DR35. SANITARY SEWERS LARGER THAN 375mm SHALL BE CONCRETE CSA A 257.2 CLASS 100D AS PER OPSD 807.010.
- STORM SEWERS 375mm DIA. OR SMALLER SHALL BE PVC DR35. STORM SEWERS LARGER THAN 375mm DIA. SHALL BE CONCRETE CSA A 257.2 CLASS 100-D AS PER OPSD 807.010
- ALL STORM AND SANITARY SEWER BEDDING SHALL BE INSTALLED AS PER CITY OF OTTAWA STANDARDS S6 AND S7, CLASS "B" BEDDING, UNLESS OTHERWISE NOTED. SUITABLE BEDDING AND COVER MATERIAL TO BE SPECIFIED BY GEOTECHNICAL CONSULTANT.
- STORM AND SANITARY MANHOLES SHALL BE 1200mm DIAMETER IN ACCORDANCE WITH OPSD-701.01 (UNLESS OTHERWISE NOTED) c/w FRAME AND COVER AS PER CITY OF OTTAWA S24, S24.1, AND S25 WHERE APPLICABLE. CATCH BASIN MANHOLE FRAME AND COVERS PER S19, S28, AND S28.1 WHERE APPLICABLE. ALL STORM MANHOLES WITH SEWERS 900mm DIA SEWERS AND OVER IN SIZE SHALL BE BENCHED. ALL OTHER STORM MANHOLES SHALL BE COMPLETED WITH 300mm SUMPS AS PER CITY STANDARDS. SANITARY MANHOLES SHALL NOT HAVE SUMPS.
- ALL SEWERS CONSTRUCTED WITH GRADES 0.50% OR LESS, TO BE INSTALLED WITH LASER AND CHECKED WITH LEVEL INSTRUMENT PRIOR TO BACKFILLING.
- FOR STORM SEWER INSTALLATION (EXCLUDING CB LEADS) THE MINIMUM DEPTH OF COVER OVER THE CROWN OF THE SEWER IS 2.0m. FOR SANITARY SEWERS THE MINIMUM DEPTH OF COVER IS 2.5m OVER PIPE OBVERT.
- ALL STORM AND SANITARY SERVICES TO BE EQUIPPED WITH APPROVED BACKWATER VALVES.
- STORM AND SANITARY SERVICE LATERALS TO BE SDR 28 INSTALLED AT MIN. 1.0% SLOPE. SINGLE STORM SERVICES TO BE 100mmØ, SINGLE SANITARY SERVICES TO BE 135mmØ. (SERVICES TO EXTEND 2.0m BEYOND PROPERTY LINE)
- CATCH BASINS SHALL BE INSTALLED IN ACCORDANCE WITH CITY STANDARDS S1, S2, S3 c/w FRAME AND GRATE AS PER S19.1. CURB INLET FRAME AND GRATE PER S22 AND S23. PROVIDE 150mm ADJUSTED SPACERS. ALL CATCH BASINS SHALL HAVE SUMPS (800mm DEEP). STREET CATCH BASIN LEADS SHALL BE 200mm DIA/(MIN) PVC DR 35 AT 1.0% GRADE WHERE NOT OTHERWISE SHOWN ON PLAN. CATCH BASINS WILL BE INSTALLED WITH INLET CONTROL DEVICES (ICD) AS PER ICD SCHEDULE ON STORM DRAINAGE PLAN.
- STREET CATCH BASINS TO BE INSTALLED c/w SUBDRAINS 3m LONG IN FOUR ORTHOGONAL DIRECTIONS OR LONGITUDINALLY WHEN PLACED ALONG A CURB, AND AT AN ELEVATION OF 300mm BELOW SUBGRADE LEVEL.
- REAR LOT PERFORATED PIPE TO BE INSTALLED AS PER CITY OF OTTAWA STANDARDS S29. REAR LOT STRUCTURES TO BE INSTALLED AS PER CITY OF OTTAWA STANDARD W30 AND W31.
- CLAY SEALS TO BE INSTALLED AS PER CITY STANDARD DRAWING S8. THE SEALS SHOULD BE AT LEAST 1.5m LONG (IN THE TRENCH DIRECTION) AND SHOULD EXTEND FROM TRENCH WALL TO TRENCH WALL. GENERALLY, THE SEALS SHOULD EXTEND FROM THE FROST LINE AND FULLY PENETRATE THE BEDDING, SUBBEDDING AND COVER MATERIAL. THE BARRIERS SHOULD CONSIST OF RELATIVELY DRY AND COMPACTABLE BROWN SILTY CLAY PLACED IN MAXIMUM 225mm THICK LOOSE LAYERS COMPACTED TO A MINIMUM OF 96% OF THE MATERIAL'S SPMDD. THE CLAY SEALS SHOULD BE PLACED AT THE SITE BOUNDARIES AND AT STRATEGIC LOCATIONS AT NO MORE THAN 60m INTERVALS IN THE SERVICE TRENCHES. FOR DETAILS REFER TO GEOTECHNICAL INVESTIGATION.
- GRANULAR "A" SHALL BE PLACED TO A MINIMUM THICKNESS OF 300 mm AROUND ALL STRUCTURES WITHIN PAVEMENT AREA AND COMPACTED TO A MINIMUM OF 98% STANDARD PROCTOR DENSITY.
- CONTRACTOR SHALL PERFORM LEAKAGE TESTING, IN THE PRESENCE OF THE CONSULTANT, FOR SANITARY SEWERS IN ACCORDANCE WITH OPS5 410 AND OPS5 407. CONTRACTOR SHALL PERFORM VIDEO INSPECTION OF ALL STORM AND SANITARY SEWERS. A COPY OF THE VIDEO AND INSPECTION REPORT SHALL BE SUBMITTED TO THE CONSULTANT FOR REVIEW.
- ANY SEWER ABANDONMENT TO BE CONDUCTED ACCORDING TO CITY OF OTTAWA STANDARD S11.4

GRADING

- ALL GRANULAR BASE & SUB BASE COURSE MATERIALS SHALL BE COMPACTED TO 98% STANDARD PROCTOR MAX. DRY DENSITY.
- SUB-EXCAVATE SOFT AREAS & FILL WITH GRANULAR 'B' COMPACTED IN 0.15m LAYERS.
- ALL DISTURBED GRASSED AREAS SHALL BE RESTORED TO ORIGINAL CONDITION OR BETTER, WITH SOO ON MIN. 100mm TOPSOIL. THE RELOCATION OF TREES AND SHRUBS SHALL BE SUBJECT TO APPROVAL BY THE PROJECT LANDSCAPE ARCHITECT OR ENGINEER.
- 100 YEAR PONDING DEPTH TO BE 0.30m (MAXIMUM).
- EMBANKMENTS TO BE SLOPED AT MIN. 3:1, UNLESS OTHERWISE SPECIFIED.
- ALL SWALES TO BE MIN. 0.15m DEEP WITH MIN. 3:1 SIDE SLOPES UNLESS OTHERWISE NOTED. THE MINIMUM LONGITUDINAL SLOPE TO BE 1.5% OR 1.0% WHEN PERFORATED SUBDRAIN IS INSTALLED.
- ALL ROOF DOWNSPOUTS TO DISCHARGE TO THE GROUND ONTO SPLASH PADS AND SHALL NOT BE DIRECTED TO THE STORM SEWER, OR THE BUILDING FOUNDATION DRAIN.
- TOP OF GRATE (T/O) ELEVATIONS FOR ALL STREET CATCHBASINS SHOWN ON PLANS REFER TO THE ELEVATION AT EDGE OF PAVEMENT, OR GUTTERLINE WHERE APPLICABLE.
- ALL RETAINING WALLS GREATER THAN 1.0m IN HEIGHT ARE TO BE DESIGNED, APPROVED, AND STAMPED BY STRUCTURAL ENGINEER.
- FENCES OR RAILINGS ARE REQUIRED FOR RETAINING WALLS GREATER THAN 0.60m IN HEIGHT.
- EXCESS EXCAVATED MATERIAL SHALL BE REMOVED FROM THE SITE.
- ALL NECESSARY CLEARING AND GRUBBING SHALL BE COMPLETED BY THE CONTRACTOR. REVIEW WITH CONTRACT ADMINISTRATOR AND THE CITY OF OTTAWA PRIOR TO TREE CUTTING.
- REFER TO DRAWING EC DT-1 FOR EROSION AND SEDIMENT CONTROL DETAILS.

Best Management Practices

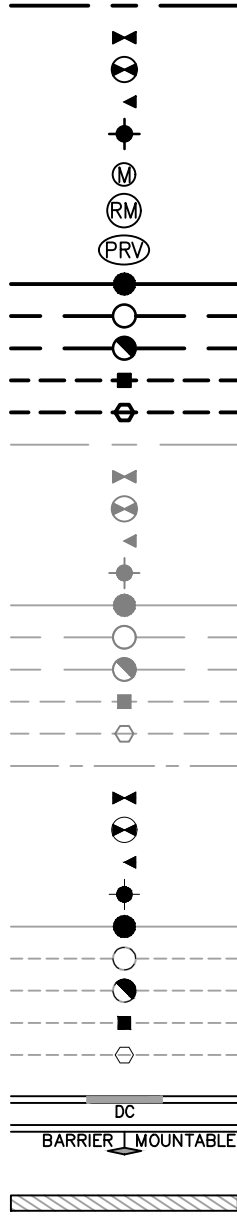
CONTRACTOR TO PROVIDE EROSION AND SEDIMENT CONTROLS (BEST MANAGEMENT PRACTICES) DURING CONSTRUCTION OF THIS PROJECT.

EROSION MUST BE MINIMIZED AND SEDIMENTS MUST BE REMOVED FROM CONSTRUCTION SITE RUN-OFF IN ORDER TO PROTECT DOWNSTREAM AREAS. DURING ALL CONSTRUCTION, EROSION AND SEDIMENTATION SHOULD BE CONTROLLED BY THE FOLLOWING TECHNIQUES:

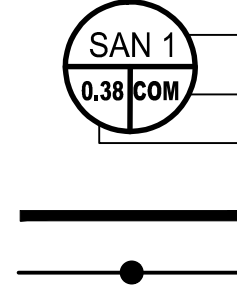
- LIMIT THE EXTENT OF EXPOSED SOILS AT ANY GIVEN TIME.
- REVEGETATE EXPOSED AREAS AND SLOPES AS SOON AS POSSIBLE.
- MINIMIZE AREA TO BE CLEARED AND GRUBBED.
- PROTECT EXPOSED SLOPES WITH PLASTIC OR SYNTHETIC MULCHES.
- INSTALL CATCH BASIN INSERTS OR EQUIVALENT IN ALL PROPOSED CATCH BASINS AND CATCH BASIN MANHOLES AND IN ALL EXISTING CATCH BASINS THAT WILL RECEIVE RUN-OFF FROM THE SITE.
- A SILT FENCE SHALL BE INSTALLED AROUND THE PERIMETER OF ALL AND ANY STOCKPILES OF MATERIAL TO BE USED OR REMOVED FROM SITE. (LOCATION TO BE DETERMINED)
- A VISUAL INSPECTION SHALL BE DONE DAILY ON SEDIMENT CONTROL MEASURES AND CLEANED OF ANY ACCUMULATED SILT AS REQUIRED. THE DEPOSITS WILL BE DISPOSED OFF SITE AS PER THE REQUIREMENTS OF THE CONTRACT.
- SEDIMENT CONTROL BARRIERS MAY ONLY BE REMOVED TEMPORARILY WITH APPROVAL OF CONTRACT ADMINISTRATOR TO ACCOMMODATE CONSTRUCTION OPERATIONS. ALL AFFECTED BARRIERS MUST BE REINSTATED AT NIGHT WHEN CONSTRUCTION IS COMPLETED. NO REMOVAL WILL OCCUR IF THERE IS A SIGNIFICANT RAINFALL EVENT ANTICIPATED (>10mm) UNLESS A NEW DEVICE HAS BEEN INSTALLED TO PROTECT EXISTING STORM AND SANITARY SEWER SYSTEMS, OR DOWNSTREAM WATERCOURSES.
- NO REFUELING OR CLEANING OF EQUIPMENT IS PERMITTED NEAR ANY EXISTING WATERWAY.
- CONTRACTOR SHALL REMOVE SEDIMENT CONTROL MEASURES WHEN, IN THE OPINION OF THE CONTRACT ADMINISTRATOR, THE MEASURE(S) IS NO LONGER REQUIRED. NO CONTROL MEASURES SHALL BE PERMANENTLY REMOVED WITHOUT PRIOR WRITTEN AUTHORIZATION FROM THE CONTRACT ADMINISTRATOR.
- THE CONTRACTOR SHALL PERIODICALLY, OR WHEN REQUESTED BY THE CONTRACT ADMINISTRATOR, CLEAN OUT ACCUMULATED SEDIMENTS AS REQUIRED.
- THE CONTRACTOR SHALL IMMEDIATELY REPORT TO THE ENGINEER ANY ACCIDENTAL DISCHARGES OF SEDIMENT MATERIAL INTO THE WATERCOURSE. APPROPRIATE RESPONSE MEASURES, INCLUDING ANY REPAIRS TO EXISTING CONTROL MEASURES OR THE IMPLEMENTATION OF ADDITIONAL CONTROL MEASURES, SHALL BE CARRIED OUT BY THE CONTRACTOR WITHOUT DELAY.
- CONTRACTOR SHALL INSTALL MUD MATS AT BOTH ENTRANCES TO THE SITE.
- STORMWATER SWALES TO BE COVERED WITH HYDRO-SEED AND MULCH.

LEGEND

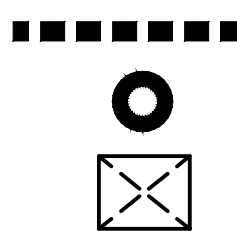
SERVICES



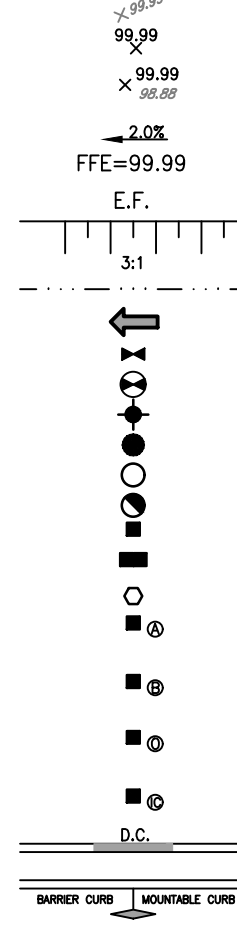
SANITARY DRAINAGE



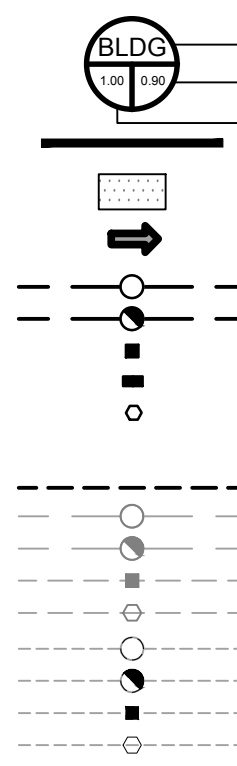
EROSION CONTROL



GRADING



STORM DRAINAGE



Stantec Consulting Ltd.
400 - 1331 Clyde Avenue
Ottawa ON
Tel. 613.722.4420
www.stantec.com

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Legend

Notes

10	ISSUED FOR SPA	MJS	DT	17.10.20
9	ISSUED FOR ADDENDUM # 3	MJS	GR	17.05.04
8	ISSUED FOR ADDENDUM # 1	MJS	GR	17.04.28
7	ISSUED FOR TENDER	MJS	GR	17.04.12
6	ISSUED FOR TENDER	MJS	GR	17.03.29
5	REVISED AS PER CITY COMMENTS	MJS	DT	17.03.28
4	REVISED AS PER CITY COMMENTS	MJS	DT	17.01.03
3	REVISED SERVICING BLDG H3	MJS	DT	17.01.03
2	REVISED AS PER CITY COMMENTS	MJS	DT	16.12.07
1	ISSUED FOR SPA	MJS	DT	16.07.27
Revision		By	Appd.	YY.MM.DD

File Name:	160401242 DB	MJS	DT	MJS	16.06.30
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Permit-Seal

Client/Project

SMARTREIT

ORLEANS II DEVELOPMENT
2025 MER BLEUE ROAD
OTTAWA, ON, CANADA

Title

NOTES AND LEGEND

Project No.	Scale	
160401242		
Drawing No.	Sheet	Revision

NL-1

1 of 6

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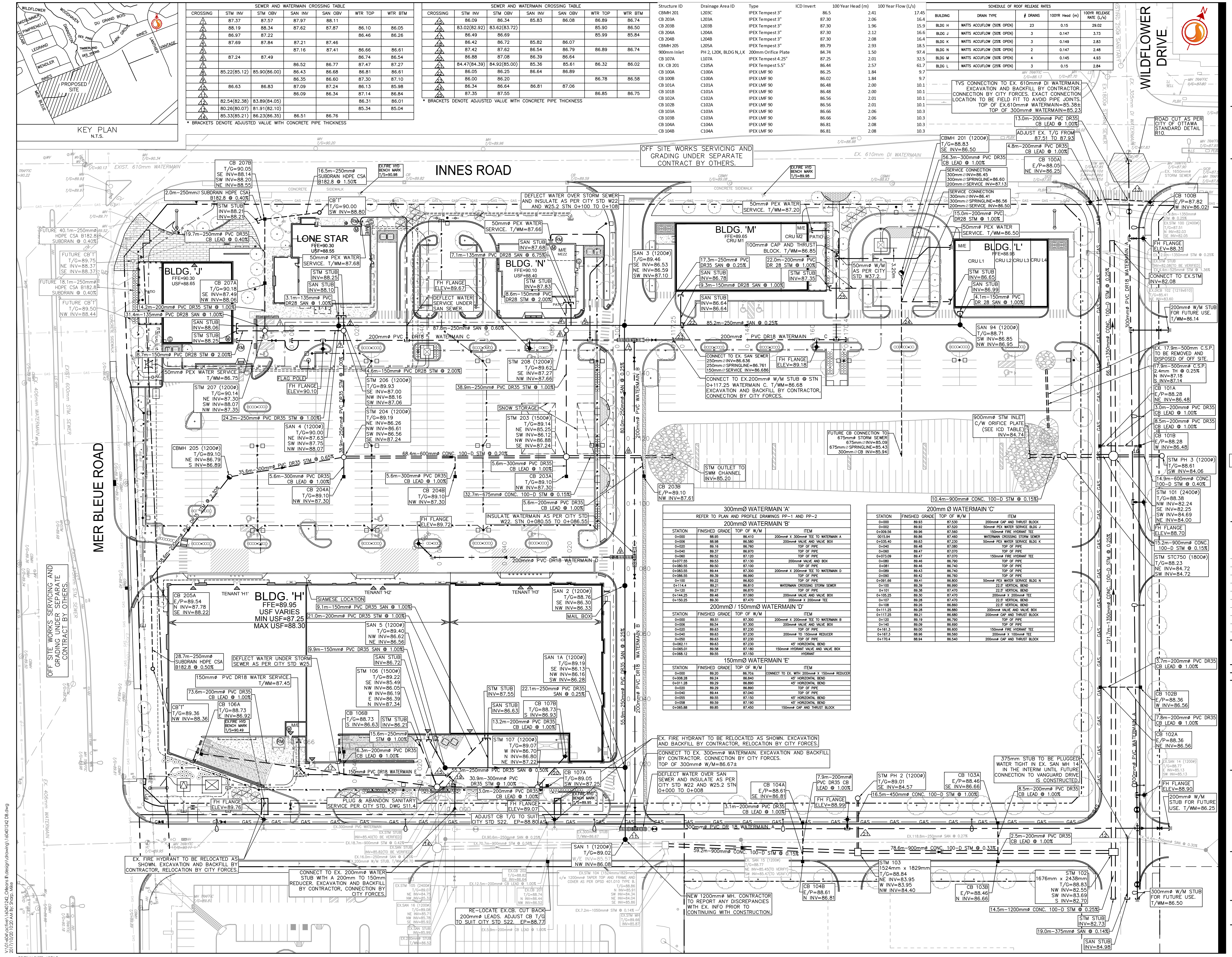
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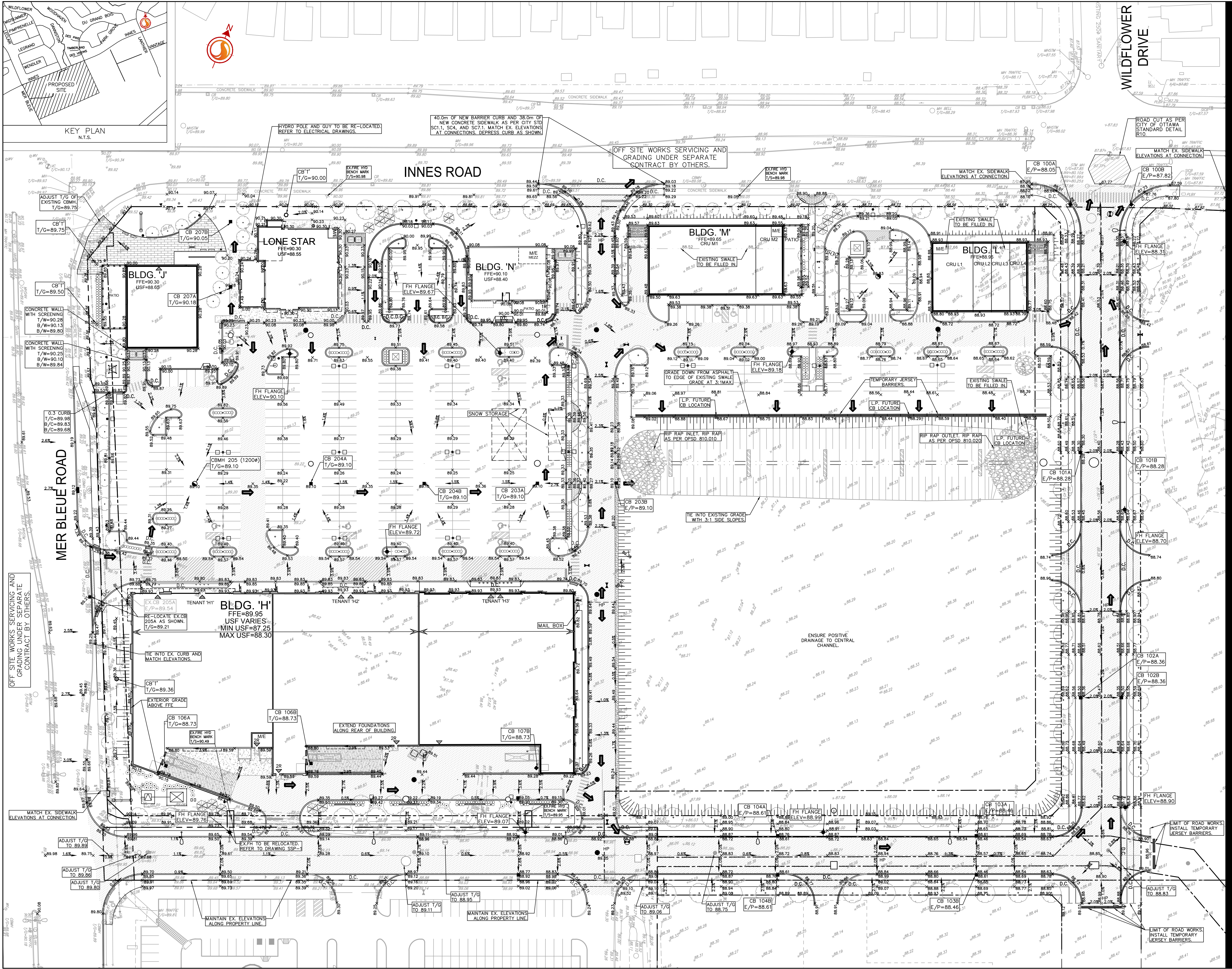
- PROPOSED WATERMAIN
- PROPOSED VALVE AND VALVE BOX
- PROPOSED REDUCER
- PROPOSED FIRE HYDRANT
- PROPOSED WATER METER
- PROPOSED REMOTE WATER METER
- PROPOSED PRESSURE REDUCING VALVE
- PROPOSED THURST BLOCK LOCATIONS
- PROPOSED SANITARY SEWER
- PROPOSED STORM SEWER
- PROPOSED CATCHBASIN MANHOLE
- PROPOSED CATCHBASIN
- EXISTING WATERMAIN
- EXISTING VALVE AND VALVE BOX
- EXISTING REDUCER
- EXISTING FIRE HYDRANT
- EXISTING SANITARY SEWER
- EXISTING STORM SEWER
- EXISTING CATCHBASIN (MANHOLE)
- EXISTING CATCHBASIN
- EXISTING SUBDRAIN CATCHBASIN
- FUTURE WATERMAIN
- FUTURE VALVE AND VALVE BOX
- FUTURE FIRE HYDRANT
- FUTURE FIRE HYDRANT
- FUTURE SANITARY SEWER
- FUTURE STORM SEWER
- FUTURE CATCHBASIN
- FUTURE SUBDRAIN CATCHBASIN
- PROPOSED DEPRESSION CURB LOCATIONS
- PROPOSED MOUNTABLE/BARRIER CURB LOCATION
- THERMAL INSULATION ON STORM SEWER WHERE COVER IS LESS THAN 1.5m. THERMAL INSULATION ON WATERMAIN WHERE COVER IS LESS THAN 2.4m AS PER W22. 300mm Thick 1.0m GRANULAR SHOULDER.
- PAD MOUNTED TRANSFORMERS AND SWITCH GEAR. REFER TO ELECTRICAL DRAWINGS FOR DETAILS.
- PROPOSED GAS MAIN
- PROPOSED SECONDARY GAS SERVICE
- PROPOSED HYDRO
- PROPOSED COMMUNICATIONS LINES
- PROPOSED GAS METER
- PROPOSED EARTH BINS.
- PROPOSED BOLLARDS (BY BUILDING CONTRACTOR)

- BUILDING FOUNDATION DRAINS TO BE CONNECTED TO STORM SERVICE LATERALS.
- FLAG POLE AND BASE INSTALLED BY OTHERS. BASE TO BE DESIGNED AND STAMPED BY LICENSED PROFESSIONAL ENGINEER IN THE PROVINCE OF ONTARIO.

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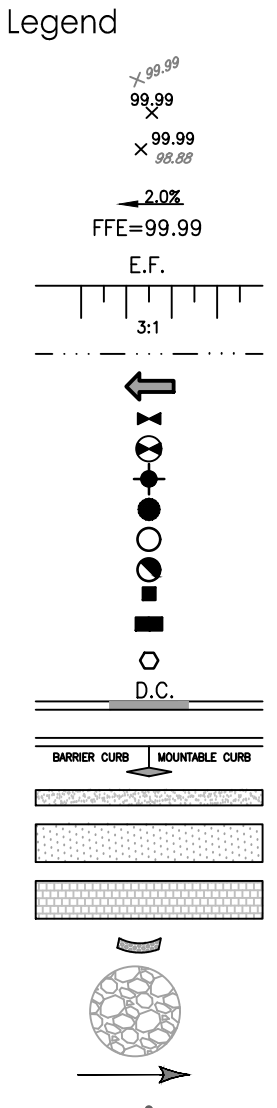


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Legend



ORIGINAL GROUND ELEVATION

PROPOSED LOT CORNER ELEVATION

EXISTING ELEVATION AT LOT CORNER

FLOW DIRECTION AND GRADE

FINISHED FIRST FLOOR ELEVATION

ENGINEERED FILL REQUIRED

TERRACING 3:1 SLOPE MAXIMUM (UNLESS OTHERWISE SHOWN)

PROPOSED SWALE

DIRECTION OF OVERLAND FLOW

PROPOSED VALVE BOX

PROPOSED VALVE CHAMBER

PROPOSED FIRE HYDRANT

PROPOSED SANITARY SEWER MANHOLE

PROPOSED STORM SEWER MANHOLE

PROPOSED CATCHBASIN MANHOLE

PROPOSED CATCHBASIN

PROPOSED DOUBLE CATCHBASIN

PROPOSED CATCHBASIN T

PROPOSED DEPRESSED CURB LOCATION

PROPOSED MOUNTABLE/BARRIER CURB LOCATIONS

300mm THICK, 1.0m wide GRANULAR A SHOULDER.

HEAVY DUTY ASPHALT.

INTERLOCK STONE AS PER LANDSCAPE PLAN.

TWSI AS PER CITY STANDARD SGT.3

RIP RAP AS PER OPSD 810.010

DIRECTION OF POSITIVE FLOW

PROPOSED BOLLARDS (BY BUILDING CONTRACTOR)

- Notes
- FLAG POLE AND BASE INSTALLED BY OTHERS. BASE TO BE DESIGNED AND STAMPED BY LICENSED PROFESSIONAL ENGINEER IN THE PROVINCE OF ONTARIO.
 - HEAVY DUTY ASPHALT
40mm HL3 OR SUPERPAVE 12.5 ASPHALTIC CONCRETE
50mm HL8 OR SUPERPAVE 19.0 ASPHALTIC CONCRETE
150mm OPSS GRANULAR A BASE
450mm OPSS GRANULAR B TYPE II
 - LIGHT DUTY AREAS
50mm HL3 OR SUPERPAVE 12.5 ASPHALTIC CONCRETE
150 OPSS GRANULAR A' BASE
400 OPSS GRANULAR B' TYPE II
 - ROADS WITH BUS TRAFFIC
40mm HL3 OR SUPERPAVE 12.5 ASPHALTIC CONCRETE
50mm HL8 OR SUPERPAVE 19.0 ASPHALTIC CONCRETE
50mm HL8 OR SUPERPAVE 19.0 ASPHALTIC CONCRETE
150mm OPSS GRANULAR A BASE
500mm OPSS GRANULAR B TYPE II

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File Name: 160401242.DB	MJS	DT	MJS 16.06.30
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Permit-Seal

Client/Project
SMARTREIT

ORLEANS II DEVELOPMENT
2025 MER BLEUE ROAD
OTTAWA, ON, CANADA

Title
GRADING PLAN

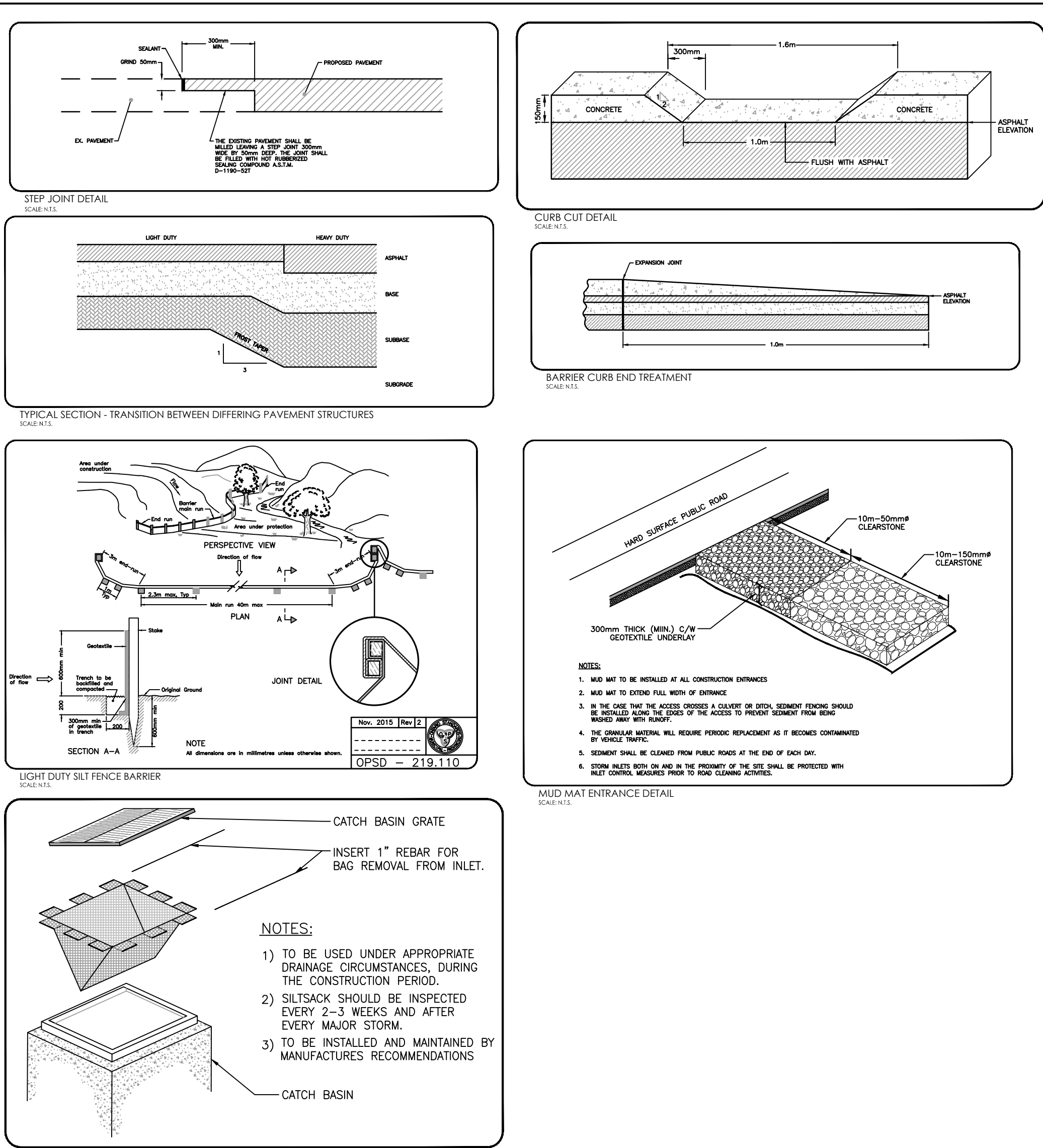
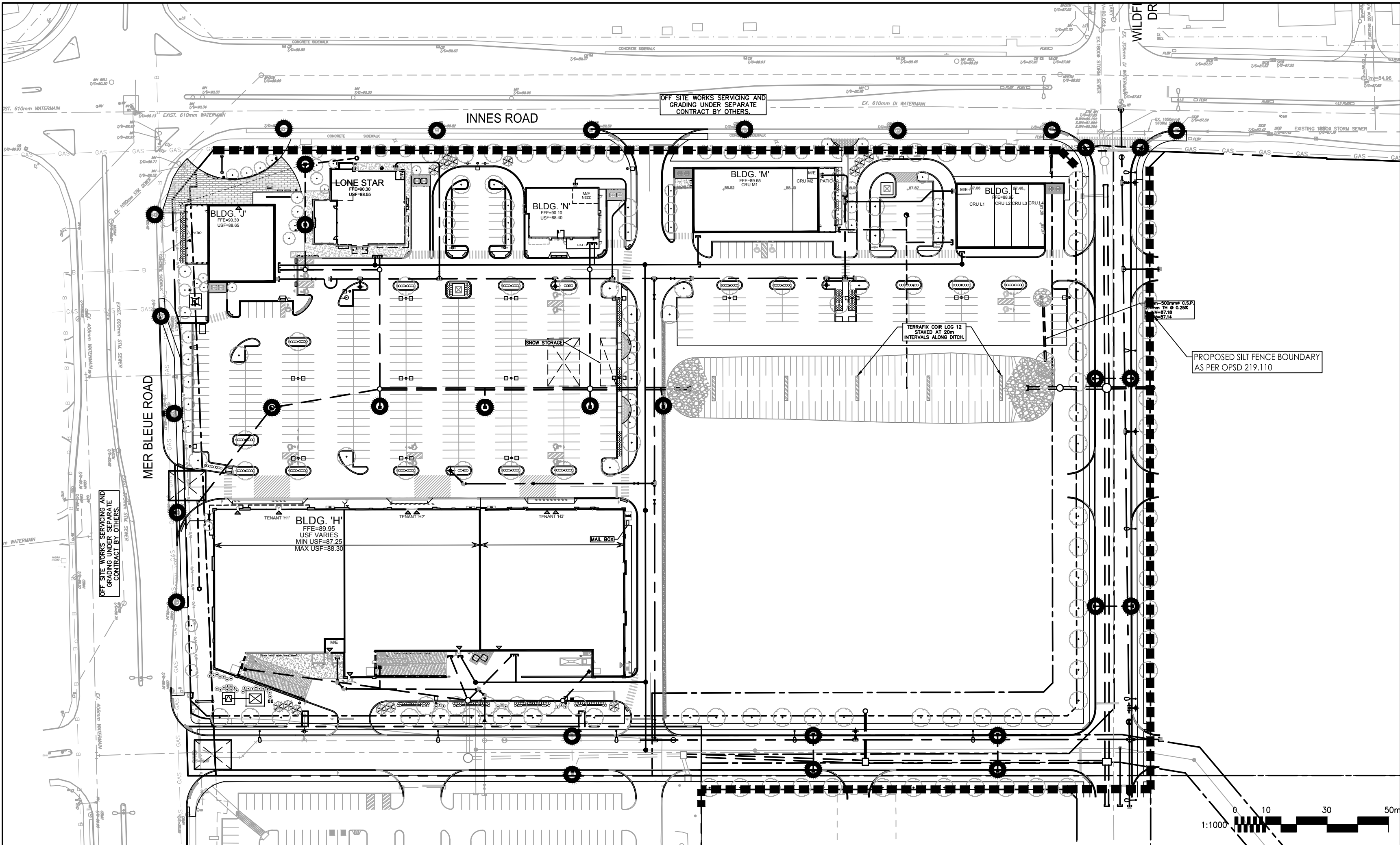
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Scale
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Drawing No.
GP-1

Sheet
3 of 6

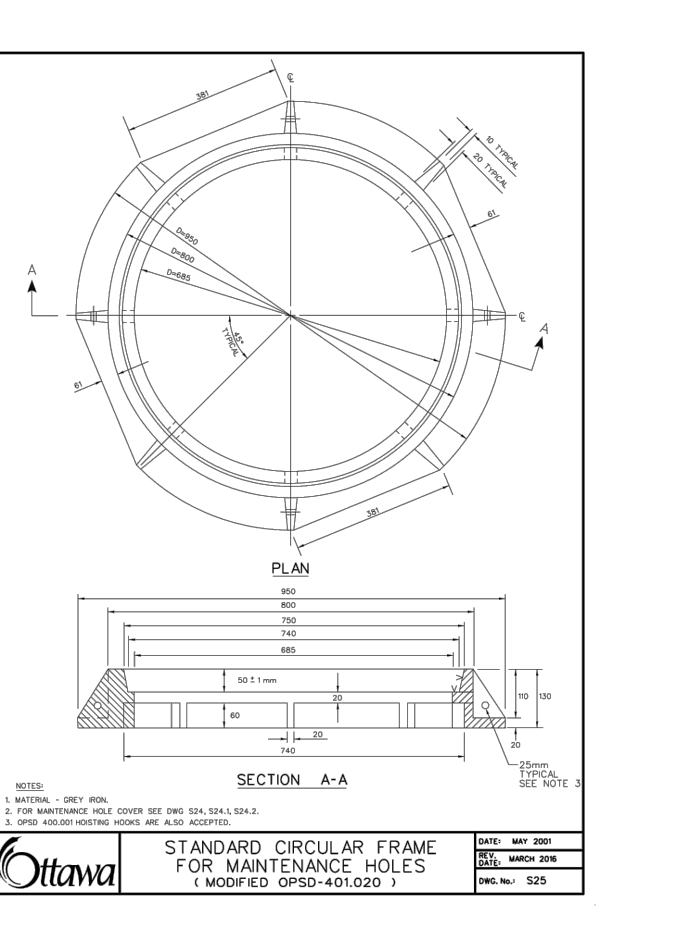
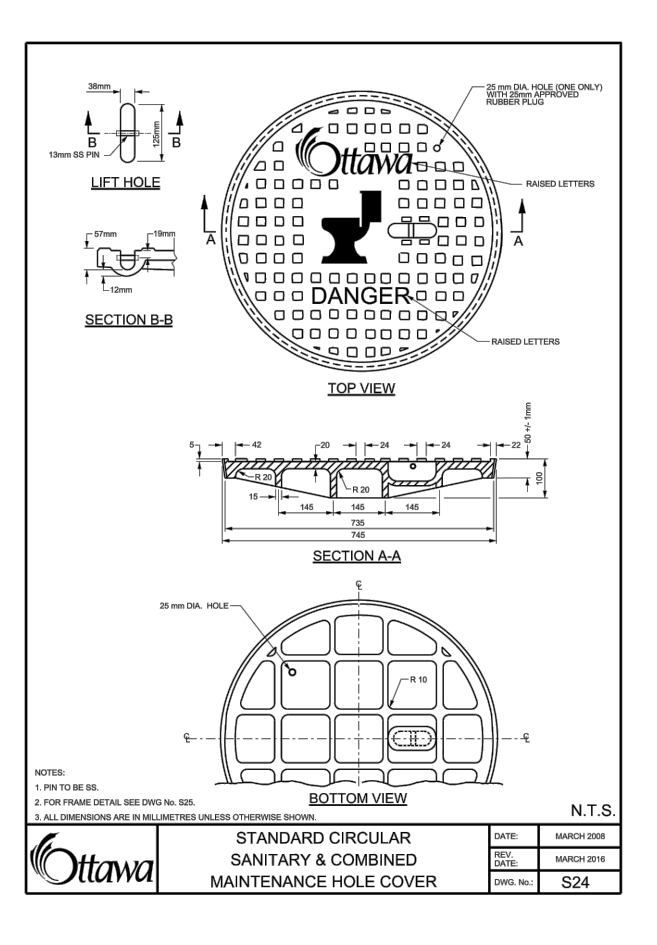
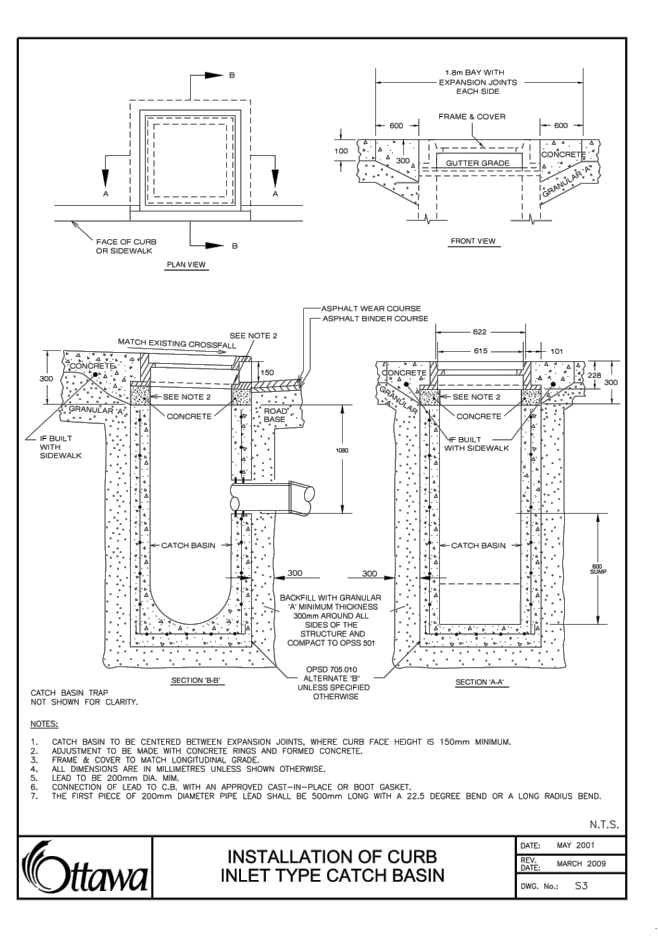
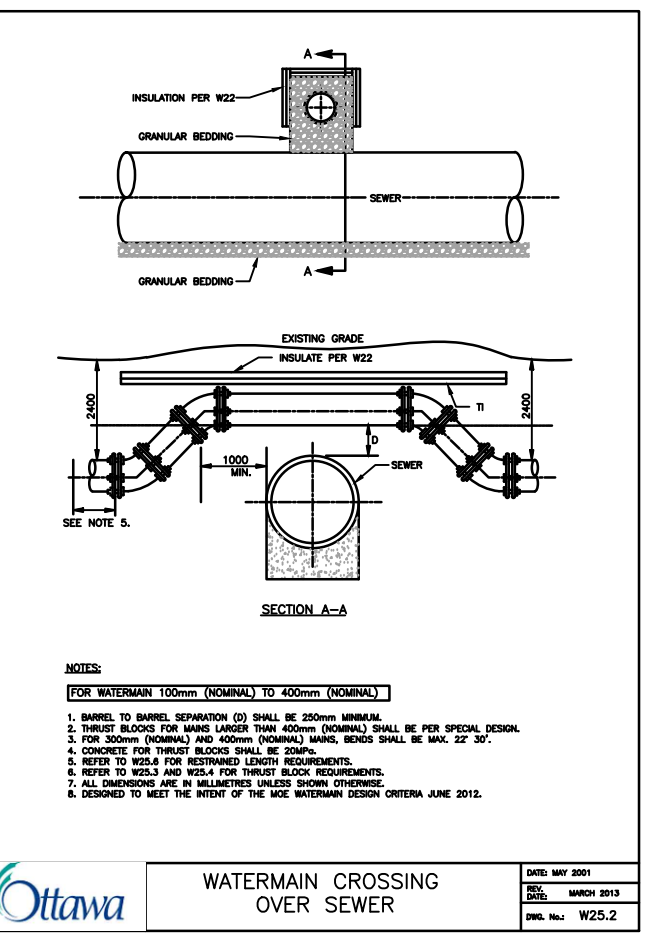
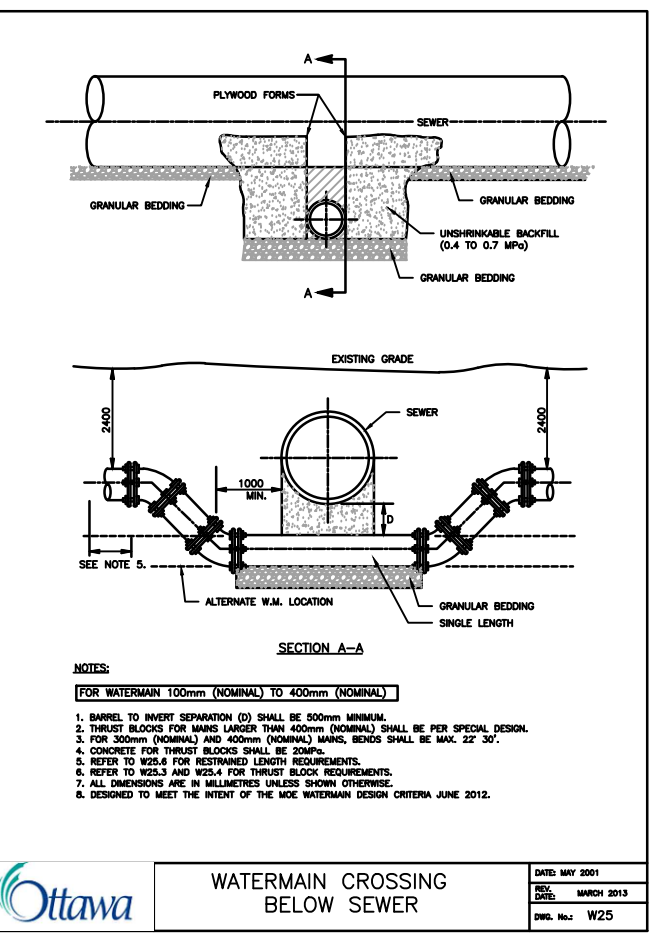
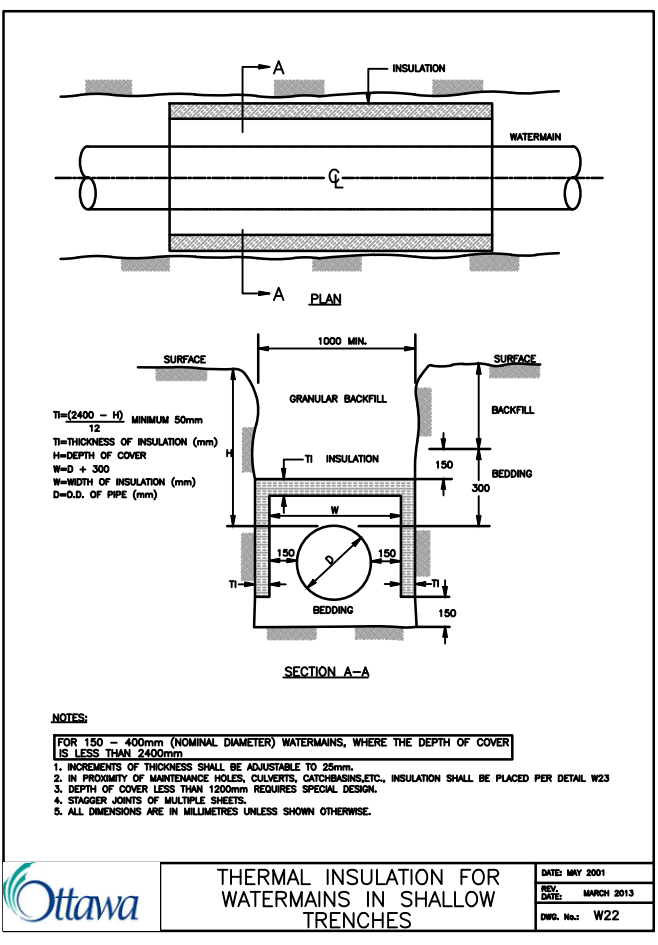
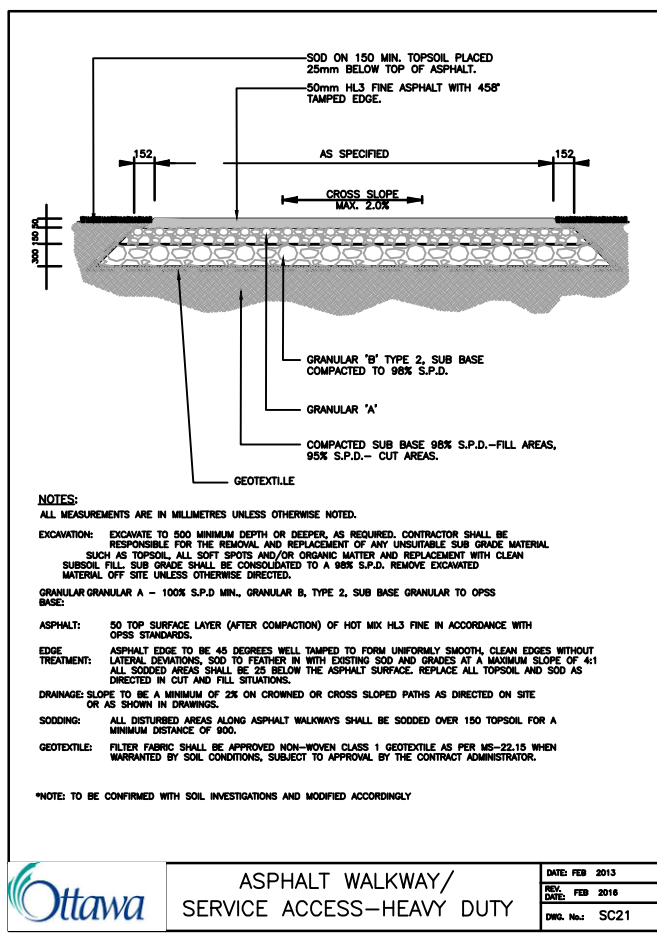
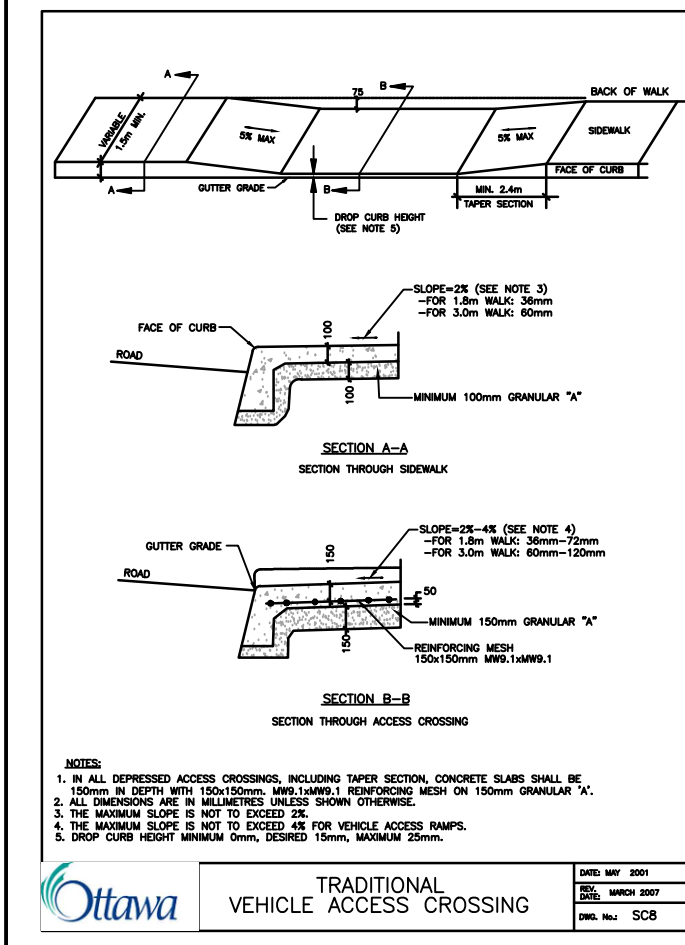
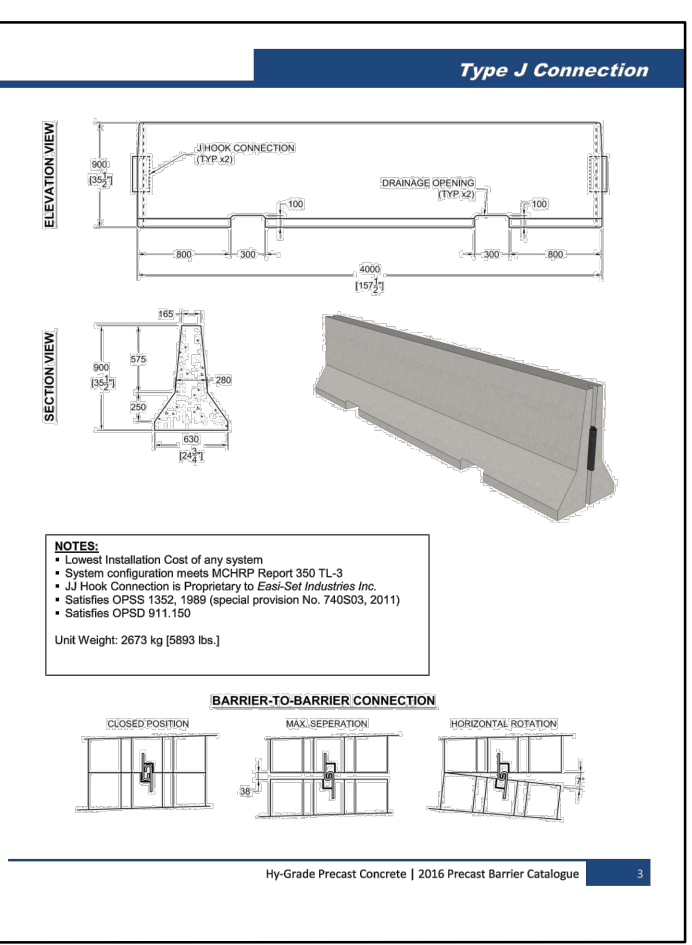
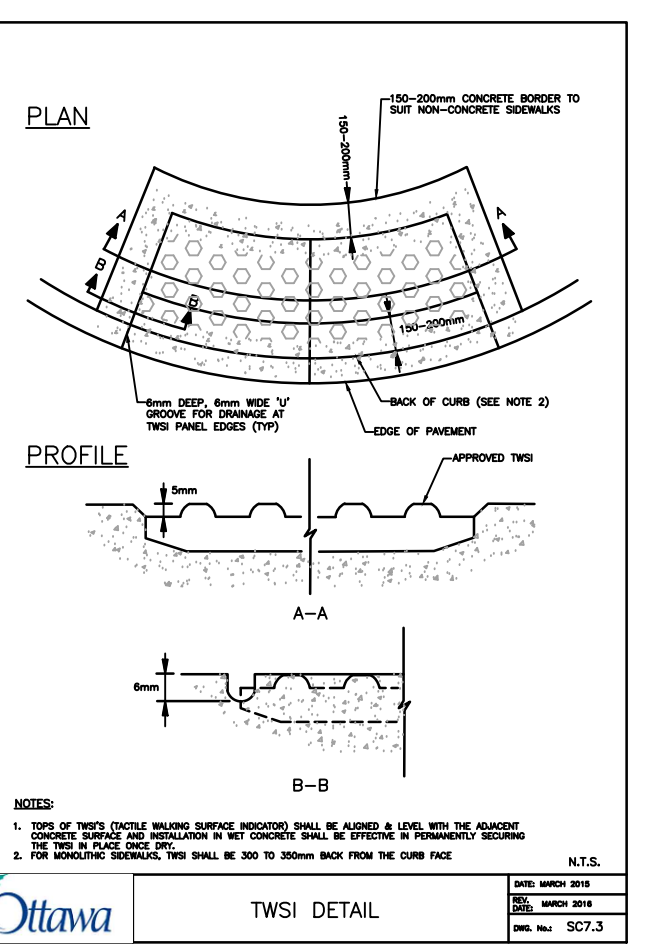
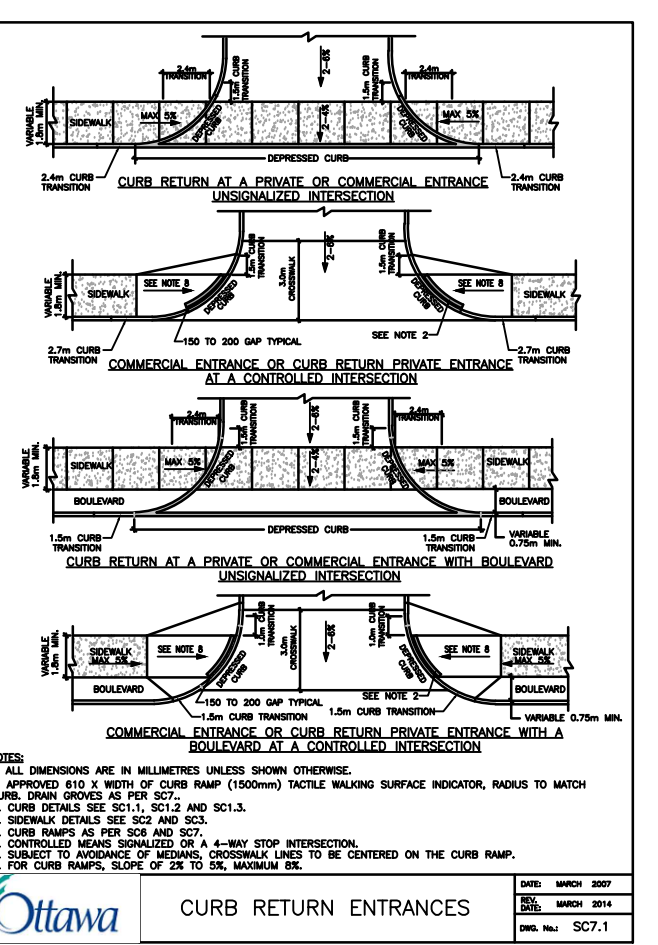
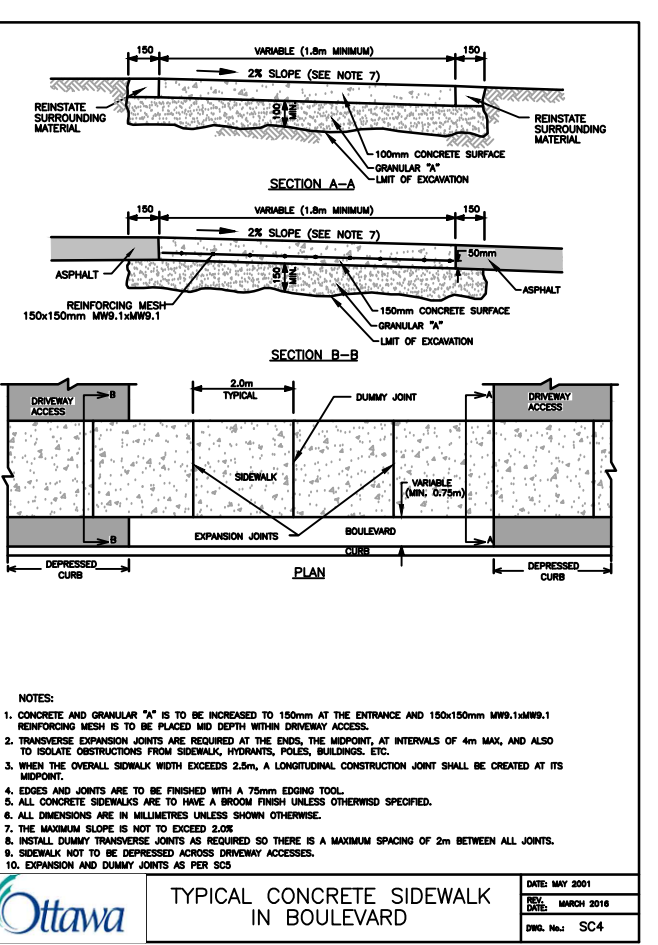
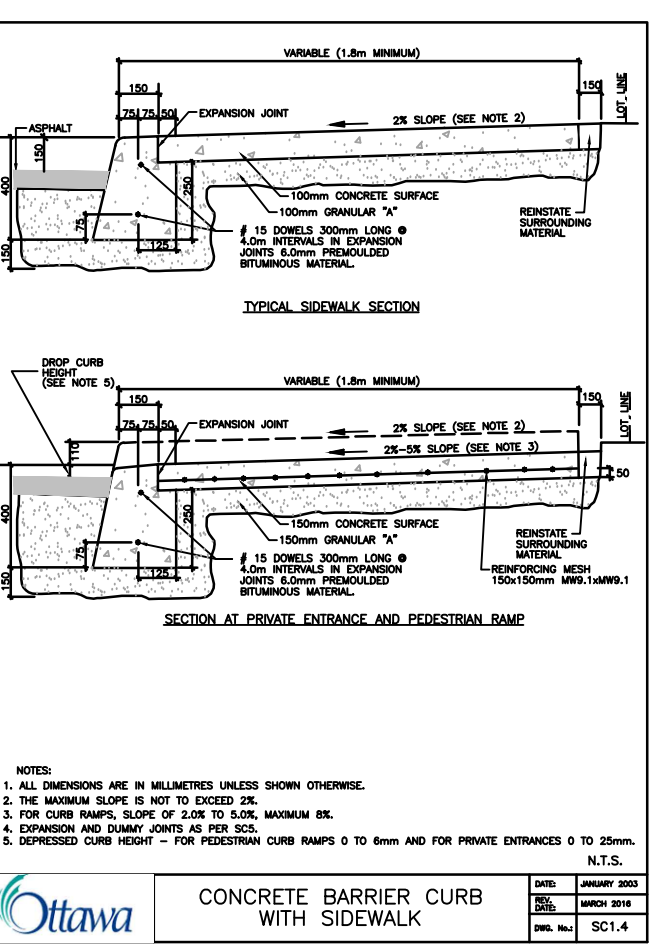
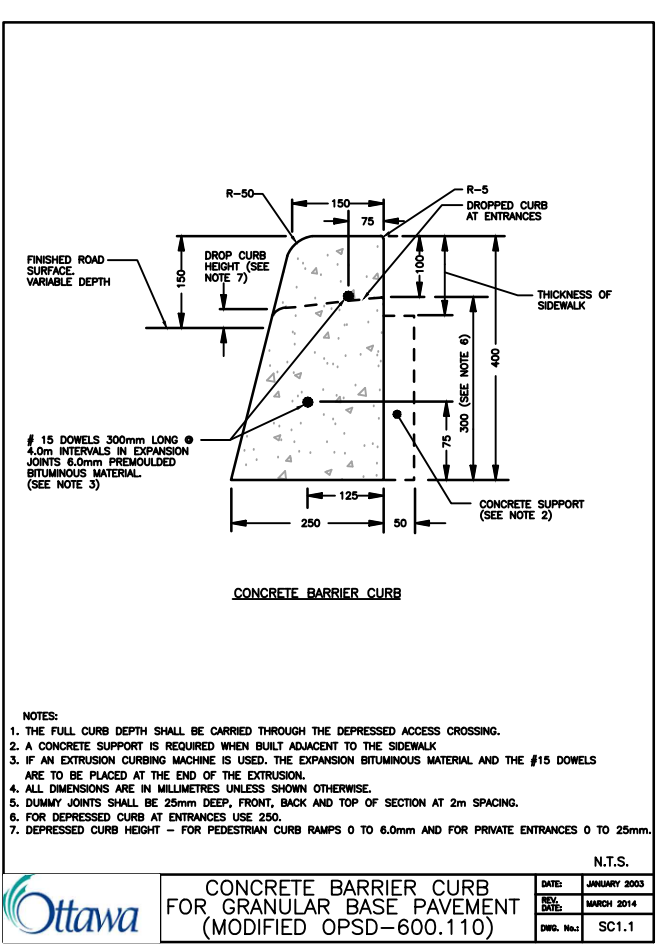
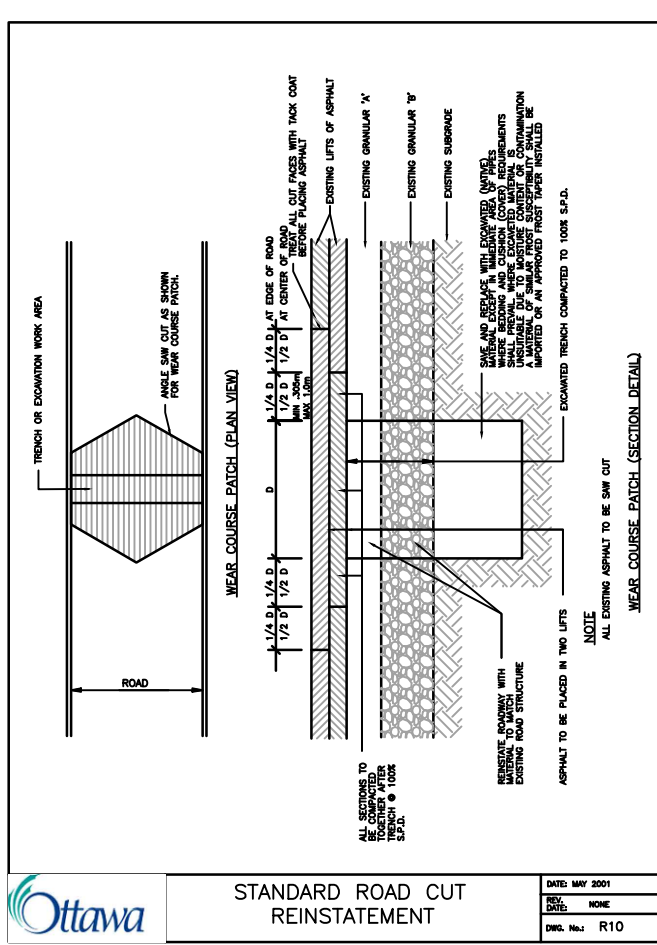
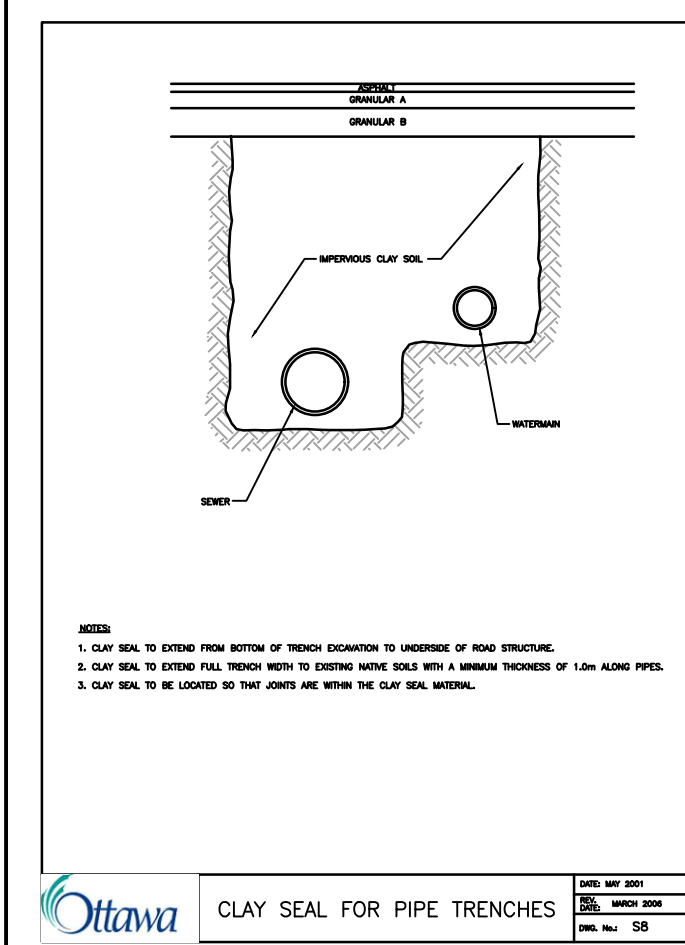
Revision
10



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- Legend**
- PROPOSED SILT FENCE BOUNDARY AS PER OPSD 219.110
 - PROPOSED MUD MAT LOCATION
 - PROPOSED CATCH BASIN PROTECTION AS PER DETAIL
 - PROPOSED TERRACE COIR LOG 12

- Notes**
- Best Management Practices**
 CONTRACTOR TO PROVIDE EROSION AND SEDIMENT CONTROLS (BEST MANAGEMENT PRACTICES) DURING CONSTRUCTION OF THIS PROJECT.
- EROSION MUST BE MINIMIZED AND SEDIMENTS MUST BE REMOVED FROM CONSTRUCTION SITE RUN-OFF IN ORDER TO PROTECT DOWNSTREAM AREAS. DURING ALL CONSTRUCTION, EROSION AND SEDIMENTATION SHOULD BE CONTROLLED BY THE FOLLOWING TECHNIQUES:
1. LIMIT THE EXTENT OF EXPOSED SOILS AT ANY GIVEN TIME.
 2. REVEGETATE EXPOSED AREAS AND SLOPES AS SOON AS POSSIBLE.
 3. MINIMIZE AREA TO BE CLEARED AND GRUBBED.
 4. PROTECT EXPOSED SLOPES WITH PLASTIC OR SYNTHETIC MULCHES.
 5. INSTALL CATCH BASIN INSERTS OR EQUIVALENT IN ALL PROPOSED CATCH BASINS AND CATCH BASIN MANHOLES AND IN ALL EXISTING CATCH BASINS THAT WILL RECEIVE RUN-OFF FROM THE SITE.
 6. A SILT FENCE SHALL BE INSTALLED AROUND THE PERIMETER OF ALL AND ANY STOCKPILES OF MATERIAL TO BE USED OR REMOVED FROM SITE. (LOCATION TO BE DETERMINED).
 7. A VISUAL INSPECTION SHALL BE DONE DAILY ON SEDIMENT CONTROL MEASURES AND CLEARED OF ANY ACCUMULATED SILT AS REQUIRED. THE DEPOSITS WILL BE DISPOSED OFF SITE AS PER THE REQUIREMENTS OF THE CONTRACT.
 8. SEDIMENT CONTROL BARRIERS MAY ONLY BE REMOVED TEMPORARILY WITH APPROVAL OF CONTRACT ADMINISTRATOR TO ACCOMMODATE CONSTRUCTION OPERATIONS. ALL AFFECTED BARRIERS MUST BE REINTEGRATED AT NIGHT WHEN CONSTRUCTION IS COMPLETED. NO REMOVAL WILL OCCUR IF THERE IS A SIGNIFICANT RAINFALL EVENT ANTICIPATED (>10mm) UNLESS A NEW DEVICE HAS BEEN INSTALLED TO PROTECT EXISTING STORM AND SANITARY SEWER SYSTEMS, OR DOWNSTREAM WATERCOURSES.
 9. NO REFUELING OR CLEANING OF EQUIPMENT IS PERMITTED NEAR ANY EXISTING WATERWAY.
 10. CONTRACTOR SHALL REMOVE SEDIMENT CONTROL MEASURES WHEN, IN THE OPINION OF THE CONTRACT ADMINISTRATOR, THE MEASURE(S) IS NO LONGER REQUIRED. NO CONTROL MEASURES SHALL BE PERMANENTLY REMOVED WITHOUT PRIOR WRITTEN AUTHORIZATION FROM THE CONTRACT ADMINISTRATOR.
 11. THE CONTRACTOR SHALL PERIODICALLY, OR WHEN REQUESTED BY THE CONTRACT ADMINISTRATOR, CLEAN OUT ACCUMULATED SEDIMENTS AS REQUIRED.
 12. THE CONTRACTOR SHALL IMMEDIATELY REPORT TO THE ENGINEER ANY ACCIDENTAL DISCHARGES OF SEDIMENT MATERIAL INTO THE WATERCOURSE. APPROPRIATE RESPONSE MEASURES, INCLUDING ANY REPAIRS TO EXISTING CONTROL MEASURES OR THE IMPLEMENTATION OF ADDITIONAL CONTROL MEASURES, SHALL BE CARRIED OUT BY THE CONTRACTOR WITHOUT DELAY.
 13. CONTRACTOR SHALL INSTALL MUD MATS AT BOTH ENTRANCES TO THE SITE.
 14. STORMWATER SWALES TO BE COVERED WITH HYDRO-SEED AND MULCH.



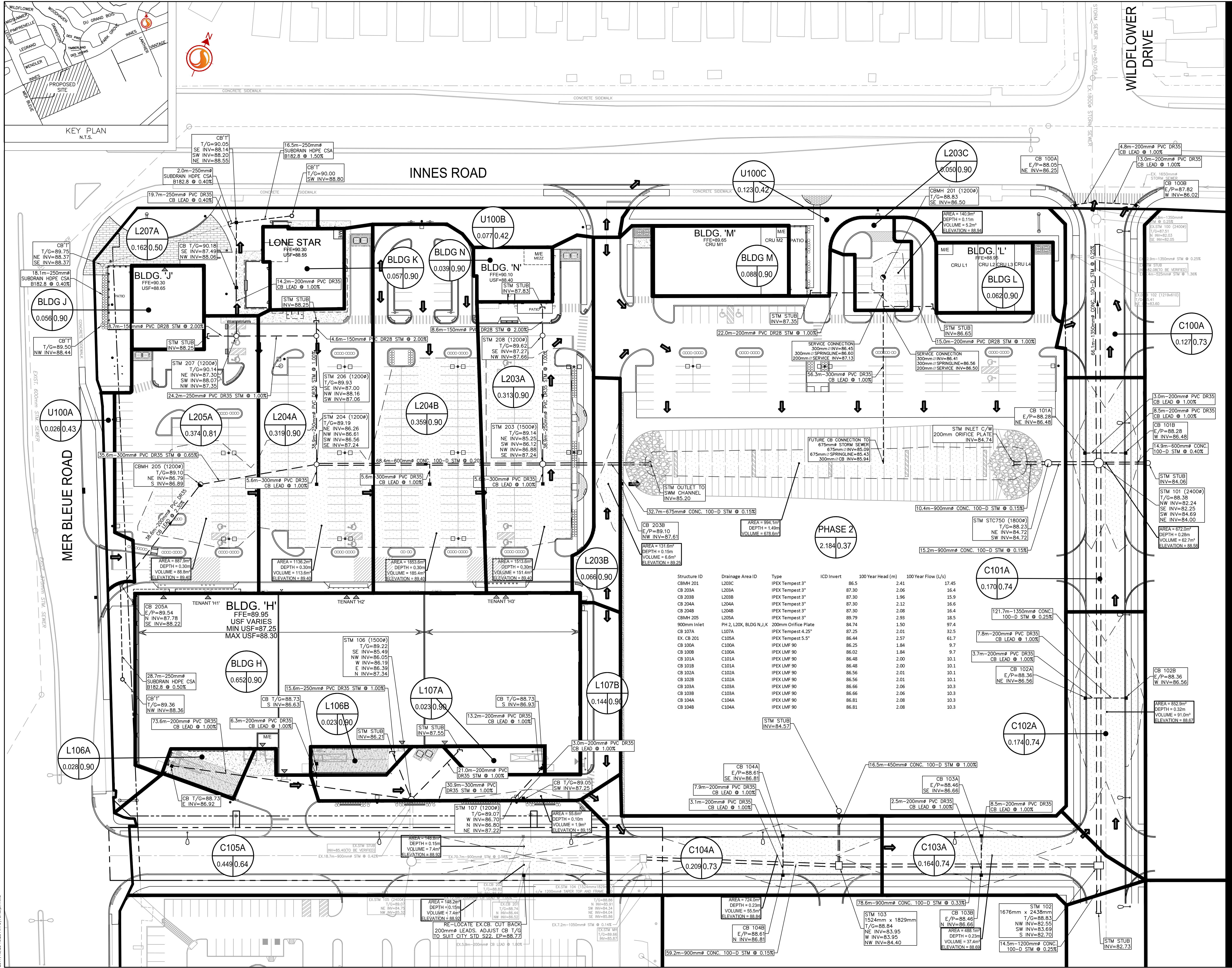
Client/Project
SMARTREIT

ORLEANS II DEVELOPMENT
 2025 MER BLEUE ROAD
 OTTAWA, ON, CANADA

Title
**EROSION CONTROL AND
 DETAIL SHEET**

Project No.	Scale	Drawing No.	Sheet	Revision
160401242				

EC/DS-1 4 of 6 10



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Legend

- BLDG AREA ID
- 1.00 0.90 RUNOFF COEFFICIENT
- STORM DRAINAGE AREA No.
- STORM DRAINAGE BOUNDARY
- MAXIMUM PONDING LIMITS
- DIRECTION OF OVERLAND FLOW
- PROPOSED STORM SEWER
- PROPOSED CATCHBASIN MANHOLE
- PROPOSED DOUBLE CATCH BASIN
- PROPOSED SUB DRAIN CATCH BASIN AS PER CITY OF OTTAWA STANDARD DETAIL DRAWINGS L10 AND L11
- PROPOSED PERFORMED SUBDRAIN
- EXISTING STORM SEWER
- EXISTING CATCHBASIN MANHOLE
- EXISTING CATCHBASIN
- EXISTING SUBDRAIN CATCHBASIN
- FUTURE STORM SEWER
- FUTURE CATCHBASIN MANHOLE
- FUTURE CATCHBASIN
- FUTURE SUBDRAIN CATCHBASIN

Notes

1. BUILDING FOUNDATION DRAINS TO BE CONNECTED TO STORM SERVICE LATERALS.

SCHEDULE OF ROOF RELEASE RATES				
BUILDING	DRAIN TYPE	# DRAINS	100YR Head (m)	100YR RELEASE RATE (L/s)
BLDG H	WATTS ACQUFLOW (50% OPEN)	23	0.15	29.02
BLDG J	WATTS ACQUFLOW (50% OPEN)	3	0.147	3.73
BLDG K	WATTS ACQUFLOW (25% OPEN)	3	0.149	2.83
BLDG N	WATTS ACQUFLOW (50% OPEN)	2	0.147	2.46
BLDG M	WATTS ACQUFLOW (50% OPEN)	4	0.145	4.93
BLDG L	WATTS ACQUFLOW (25% OPEN)	3	0.15	2.84

10	ISSUED FOR SPA	MJS	DT	17.10.20
9	ISSUED FOR ADDENDUM # 3	MJS	GR	17.05.04
8	ISSUED FOR ADDENDUM # 1	MJS	GR	17.04.28
7	ISSUED FOR TENDER	MJS	GR	17.04.12
6	ISSUED FOR TENDER	MJS	GR	17.03.29
5	REVISED AS PER CITY COMMENTS	MJS	DT	17.03.28
4	REVISED AS PER CITY COMMENTS	MJS	DT	17.03.03
3	REVISED SERVICING BLDG H3	MJS	DT	17.01.03
2	REVISED AS PER CITY COMMENTS	MJS	DT	16.12.07
1	ISSUED FOR SPA	MJS	DT	16.07.27
Revision		By	Appd.	YY.MM.DD

File Name:	160401242 SD	MJS	DT	MJS	16.06.20
		Dwn.	Chkd.	Dsgn.	YY.MM.DD

Permit-Seal

Client/Project
SMARTREIT

ORLEANS II DEVELOPMENT
2025 MER BLEUE ROAD
OTTAWA, ON, CANADA

Title
STORM DRAINAGE PLAN

Project No.
160401242

Scale
1:1000

Drawing No.
SD-1

Sheet
5 of 6

Revision



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C4K
0.38 COM
— SANITARY DRAINAGE AREA ID#
— COMMERCIAL OR INSTITUTIONAL FLOW RATES USED
— SANITARY DRAINAGE AREA no.

G3C
0.38 INF
— SANITARY DRAINAGE AREA ID#
— INFILTRATION RATES
— SANITARY DRAINAGE AREA no.

—
●
—
— SANITARY DRAINAGE AREA
— PROPOSED SANITARY SEWER

10	ISSUED FOR SPA	MUS	DT	17.10.20
9	ISSUED FOR ADDENDUM # 3	MUS	GR	17.05.04
8	ISSUED FOR ADDENDUM # 1	MUS	GR	17.04.28
7	ISSUED FOR TENDER	MUS	GR	17.04.12
6	ISSUED FOR TENDER	MUS	GR	17.03.29
5	REVISED AS PER CITY COMMENTS	MUS	DT	17.03.28
4	REVISED AS PER CITY COMMENTS	MUS	DT	17.01.03
3	REVISED SERVICING BLDG H3	MUS	DT	17.01.03
2	REVISED AS PER CITY COMMENTS	MUS	DT	16.12.07
1	ISSUED FOR SPA	MUS	DT	16.07.27

Revision

By	Appd.	Y17.MM.DD
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File Name: 160401242.D8	MJS	DT	MJS	16.06.30
	Dwn.	Chkd.	Dsgn.	YY.MM.DD

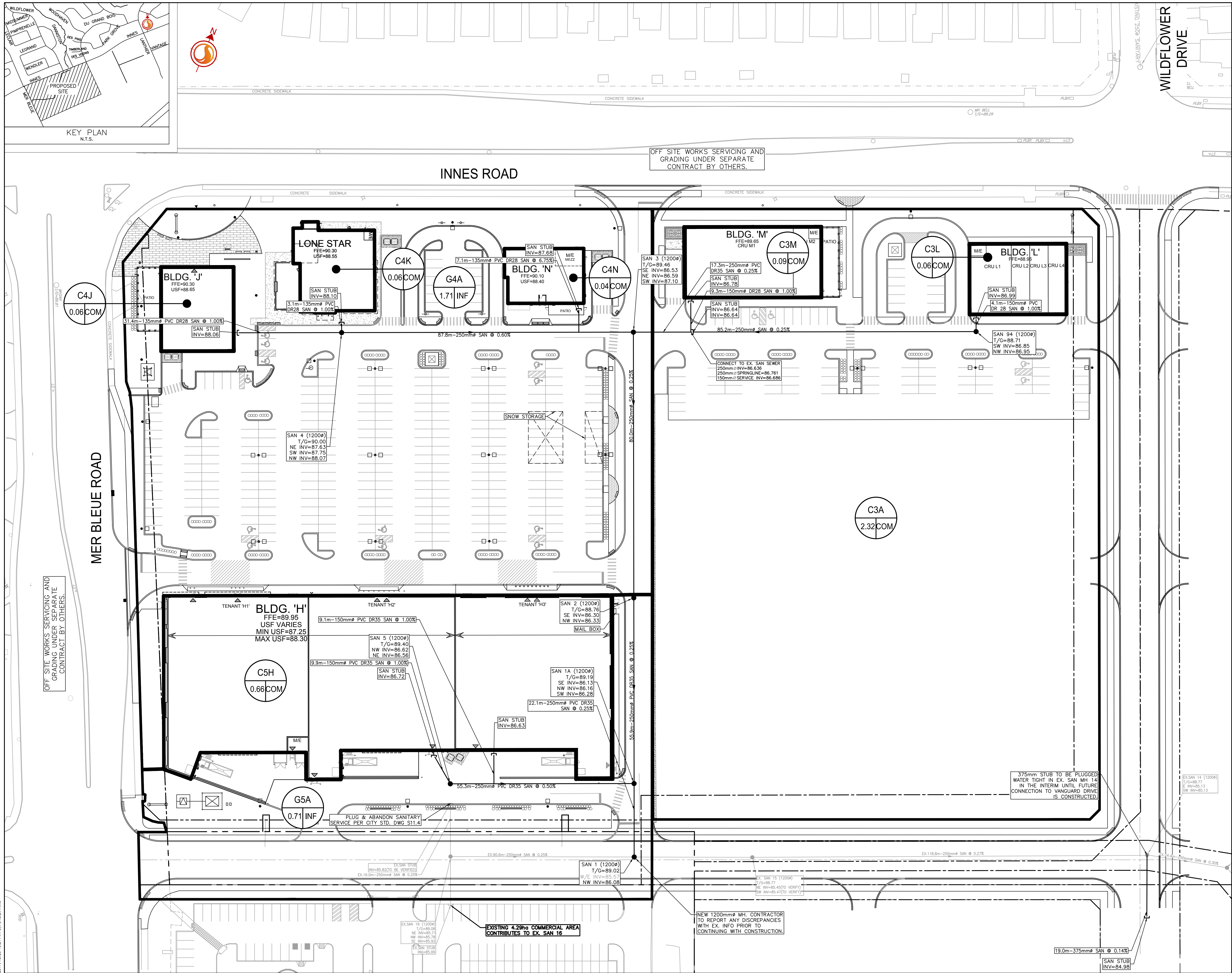
Client/Project
SMARTREIT

ORLEANS II DEVELOPMENT
2025 MER BLEUE ROAD
OTTAWA, ON, CANADA

Title
SANITARY DRAINAGE PLAN

Project No. 160401242

SA-1 6 of 6 10 17241



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